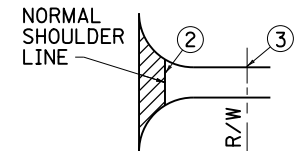


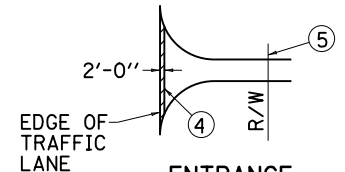
① PAVE TO R/W LINE OR END OF RADIUS, WHICHEVER IS FURTHER FROM THE EDGE OF TRAFFIC LANE.

ROAD AND STREET APPROACHES



② PAVED TO SHOULDER LINE
③ SURFACE TO R/W LINE OR TOUCHDOWN WITH TRAFFIC BOUND BASE.

ENTRANCE (RESIDENTIAL AND COMMERCIAL)



④ PAVE AS SHOWN WITH FLEXIBLE PAVEMENT.
⑤ SURFACE TO R/W LINE WITH TRAFFIC BOUND BASE.

ENTRANCE (FARM FIELD)

~ APPROACHES AND ENTRANCES ~

ROAD APPROACH ILLUSTRATION IS FOR MAINLINE ROAD, ADT 400 OR GREATER. PAVED SHOULDER PORTION SHOWN SHALL ONLY BE APPLICABLE WHERE THE MAINLINE SPECIFIES STABILIZED OR PAVED SHOULDERS. IF THE MAINLINE SHOULDER IS PAVED, THIS SHOULDER PORTION SHALL ALSO BE PAVED.

WHEN THE MAINLINE ADT IS UNDER 400, USE A 25' RADIUS WITH NO DECELERATION WIDTH PROVIDED.

THE PAVEMENT ON ENTRANCES AND APPROACHES THAT IS DISTURBED DURING NEW CONSTRUCTION OPERATIONS SHALL BE REPLACED WITH A PAVEMENT EQUIVALENT TO THE EXISTING PAVEMENT, REGARDLESS OF THE SURFACE MATERIAL USED ELSEWHERE. THE PAVEMENT DESIGN SHALL BE AS SHOWN ON THE PLANS OR AS APPROVED BY THE ENGINEER.

THE RADII ON COUNTY OR SECONDARY ROADS SHALL NOT BE LESS THAN 25' MEASURED TO THE INSIDE EDGE OF THE SURFACE. EACH ADDITIONAL FOOT OF SURFACE WIDTH WILL REQUIRE AN ADDITIONAL FOOT OF PIPE.

PIPE ILLUSTRATION IS BASED ON THE USE OF 15" PIPE. LARGER SIZES MAY BE INSTALLED WITH APPROPRIATE MODIFICATIONS. PIPES SMALLER THAN 15" DIAMETER ARE NOT TO BE USED EXCEPT IN SPECIAL CASES, WHEN SPECIFICALLY AUTHORIZED.

IN CUT SECTION, SIGHT DISTANCE SHALL BE PROVIDED ON ENTRANCES AND APPROACHES BY DAYLIGHTING THE CUT FROM THE POINTS WHERE THE RADII BEGINS, TO POINTS NOT LESS THAN 100' ON EACH OF THE INTERSECTING ROADWAY.

⑨ IF FEASIBLE, ALL APPROACHES AND ENTRANCES SHALL INTERSECT SHOULDER LINE AT RIGHT ANGLES. IF NOT AT RIGHT ANGLES, PIPE LENGTH SHALL BE INCREASED TO PROVIDE ACCURATE RADIUS.

MINIMUM PAVED AREAS FOR ENTRANCES AND APPROACHES. THESE PAVED AREAS MAY BE EXTENDED TO TOUCHDOWN OR TIE-DOWN POINT PROVIDED THE EXISTING IS PAVED.

~ MAIL BOX TURNOUT ~

THE 2'-0" WIDE FLEXIBLE PAVEMENT FOR THE LENGTH AS SHOWN, OR AS DETERMINED BY THE ENGINEER, SHALL BE APPLIED TO ALL MAIL BOX TURNOUTS. THE PAVEMENT DESIGN SHALL BE AS SHOWN ON THE PLANS OR AS APPROVED BY THE ENGINEER

FOR STABILIZED SHOULDERS, THIS AREA SHALL RECEIVE THE SAME TREATMENT AS THAT FOR ADJOINING STABILIZED SHOULDERS. FOR EARTH SHOULDERS THIS AREA SHALL RECEIVE 3" TO 5" OF COMPACTED DENSE GRADED AGGREGATE BASE, BANK GRAVEL, OR TRAFFIC BOUND BASE.

⑦ HIGH SPEED EQUALS 50 MILES PER HOUR OR GREATER. LOW SPEED EQUALS LESS THAN 50 MILES PER HOUR.

⑧ ADD 2'-0" FOR EACH ADDITIONAL MAIL BOX.

SECTION B-B

ENTRANCE GRADE TO BEGIN NO CLOSER TO EDGE OF PAVEMENT THAN WIDTH OF SHOULDER PLUS DITCH.

⑧ MAXIMUM GRADE FOR ENTRANCES 50' OR GREATER IN LENGTH:
MOUNTAINOUS TERRAIN - 20%
ROLLING TERRAIN - 16%
FLAT TERRAIN - 12%

KENTUCKY	
DEPARTMENT OF HIGHWAYS	
APPROACHES, ENTRANCES AND MAIL BOX TURNOUT	
STANDARD DRAWING NO. RPM-110-06	
SUBMITTED: <i>[Signature]</i>	12-2-11
DIRECTOR, DIVISION OF DESIGN	DATE
APPROVED: <i>[Signature]</i>	12-2-11
STATE HIGHWAY ENGINEER	DATE