



CATEGORICAL EXCLUSION ENVIRONMENTAL REVIEW

4. TABLE OF CONTENTS/ATTACHMENTS/APPENDICIES

Attachments include:

- A - Maps of project
- B - One page sheets for each bridge and the three concepts
- C - SHPO approvals
- D - Louisville MPO (KIPDA) TIP documentation
- E - NEF Forms
- F - MOT and Community Impact Assessment
- G - Special Note regarding relocation of unhoused populations residing beneath bridges
- H - Public Involvement Plan



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5. ENVIRONMENTAL MITIGATION/COMMITMENTS/COMMENTS

- A 10-day notice of intent (NOI) [DEP7036 Form](#) should be submitted to the KY Division of Air Quality prior to abatement, demolition, or renovation of any structure.
- No tree clearing June 1 to July 31.
- KPDES eNOI is to be submitted prior to construction activities if project will disturb more than 1 acre, a draft has been prepared.
- Cultural Historic Commitments to avoid certain historic sites must be followed. (See Attachment C)
- Sediment and erosion control measures are required.
- Relocation of any homeless in the right-of-way are to be removed by Louisville Metro. Contractor to call Metro Office of Resilience and Community Services, Homeless Services Division: 502-574-6967.
- Contractor to comply with Hazmat report and properly dispose of any contamination.
- Contractor to comply with MOT plan, including minimizing adverse impacts to east-west connectivity on surface streets.
- Public Involvement Plan must be implemented.


6. Project Termini

Project Length: 4.6 mile(s) **Project Termini:** MP 131.24 to 136.34

Termini Description:

The termini are the southern and northern limits of the pavement resurfacing. The 18 bridges are located within those limits, plus the three safety spot improvements. The entire 5.1-mile stretch will not be rebuilt. See Attachment B, project sheets, for specifics.

7. Roadway Conditions and Setting

Existing Functional Classification: Urban Interstate or other Expressway		Terrain: Level 
Current Year ADT: 133,000 (2022)	Design Year ADT: n/a	Existing Number of Lanes: 6
Existing Bike Accommodations: No	Existing Sidewalks: No	Proposed Number of Lanes: 6

Include any additional information to describe the roadway condition and setting:

Originally constructed between 1957 and 1963, four of the bridges in the Project area are currently in Poor Condition and 14 are in Fair Condition, at risk of falling into Poor Condition within the next three years. With Average Daily Traffic (ADT) of 119,270 daily vehicles in 2021, the four Poor Condition bridges within the Project limits are the most traveled structurally deficient bridges in Kentucky.

Regarding sidewalks, there is one pedestrian bridge over Hill Street/CSX railroad. The bridge has been rebuilt once, since the original construction (which replaced the original steps with a ramp), and will be rebuilt as part of this project to ADA standards.



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8. Purpose and Need

Purpose -- The purpose of the Project is to address the deterioration of structural elements of the 18 bridges, and improve safety through minor improvements at select locations. The goal of the bridge replacements is to extend the service life of the I-65 Interstate bridges by up to 70 years. The goal of the minor safety improvements is to reduce crashes and better manage traffic by maximizing the use of the existing infrastructure.

Needs -- If not implemented, an increase in closures and/or lane restrictions will be required to accomplish bridge inspections and repairs, thus causing additional inconvenience to the traveling public and additional cost later. The bridges could eventually deteriorate to the point of requiring more frequent bridge inspections, unplanned repairs, weight limit restrictions, or ultimately closure. This is neither prudent nor viable; the adverse ramifications of this scenario would extend far beyond the temporary impacts at the local, regional, and cross-state level.



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9. Preferred Alternative Description and Analysis

The No Build alternative would include no improvements to the structures other than routine maintenance. The structures would continue to deteriorate. This alternative does not effectively address the purpose and need of the project. However, it was considered for a baseline for comparing impacts among other alternatives.

The **preferred alternative** most effectively addresses the purpose and need of the project by constructing new bridges or rehabilitating existing bridges without load restrictions and a design life of 75 years. Interstate traffic would be routed onto other interstates, only, it would not be routed onto surface streets. Details of the MOT plan can be found in the attachment, MOT and Community Impact Assessment.

List of Bridges:

Item 5-10064.00	056B00179N	I-65 over Hill St./CSX/Burnett St.
Item 5-10065.00	056B00180N	I-65 over Eastern Parkway (US60A)
Item 5-10066.00	056B00183N	I-65 over E. Kentucky/S. Brook Streets
Item 5-10067.00	056B00184N	I-65 over St. Catherine Street
Item 5-10068.00	056B00191N	I-65 over Jacob/Broadway/Gray Streets
Item 5-10069.00	056B00192N	I-65 over Chestnut Street
Item 5-10070.00	056B00205N	I-65 over NS Railroad
Item 5-10071.00	056B00209N	I-65 over Phillips Lane
Item 5-10072.00	056B00210N	I-65 over Manning Road
Item 5-10073.00	056B00211N	I-65 over KFEC Gate 6 Drive
Item 5-10105.00	056B00181N	I-65 Over University Blvd. (Warnock St.)
Item 5-10104.00	056B00182N	I-65 Over Brandeis Ave.
Item 5-10103.00	056B00186N	I-65 Over E. Oak St.
Item 5-10099.00	056B00193N	I-65 Over Brook & Muhammad Ali
Item 5-10100.00	056B00194N	I-65 SB Ramp Bridge Over Muhammad Ali
Item 5-10101.00	056B00195R	I-65 Over Floyd Street
Item 5-10102.00	056B00197R	I-65 Over E. Liberty Street
Item 5-10106.00	056B00212N	I-65 Over Bradley Ave. (Fairgrounds Access)

Pavement rehabilitation:

Item 5-22070.00, MP131.24 to 136.34

Safety Spot Improvements:

Concept E: Restripe S. Preston St. at the NB entrance to I-65

Concept O: Extend the I-65 NB merge area from Crittenden Dr.

Concept Wa: Close Jacob St. and an alley that each cross the exit ramp to Broadway



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10. Comments and Coordination

A) Include the type of public, local government, and/or property owner outreach and summarize the type of comments received:

To date, meetings with the Kentucky Fair & Expo Center and Louisville Metro Government were held in January and February 2024. No controversies are identified, and both organizations look to work together to facilitate implementation and communications.

The project website is location here: <https://i65centralcorridor.com/>

A robust public involvement plan (PIP) has been developed (see Attachment H) and will be used to engage local stakeholder, neighborhoods, elected officials, and other organization of the project. The PIP will also be followed to inform the public of construction activities and their impact on traffic.

B) Was there controversy on the project?

Yes ☐ No ☒

C) If yes, describe the controversy and indicate if there is additional work needed to resolve all public, local government, and/or property owners' concerns.

N/A

11. Right-of-Way

A) Does the project require the acquisition of right-of-way?

Yes ☐ No ☒

B) Indicate right-of-way impacts in acreages for each type below.

Fee Simple: 0

Temporary Easement: 0

Permanent Easement: 0

C) Have any conservation easements been identified?

Yes ☐ No ☒

D) Are relocations required?

Yes ☐ No ☒

Number of Residential Relocations: 0

Number of Business Relocations: 0

Are suitable properties available for relocation?

Yes ☐ No ☐

E) Describe the right-of-way impacts.

None.



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12. Economic Impacts

A) Will the project have beneficial and/or negative economic impacts on the regional and/or local economy, such as effects on development, tax revenues and public expenditures, employment opportunities, accessibility, and retail sales?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
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B) Describe both positive and negative impacts anticipated as a result of the proposed project.

The No Build option would be expected to cause periodic emergency repairs and possible bridge closings, which would be expected to have negative economic impacts.

13. Business Impacts

A) Will the project affect established businesses or business districts?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
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B) Describe both positive and negative impacts anticipated as a result of the proposed project.

There could be temporary maintenance of traffic impacts to the traveling public that could cause minor changes to businesses, but they would be staged and minimized to the maximum extent possible. All interstate traffic would be rerouted onto other interstates to avoid congestion and adverse impacts to business and communities on the surface streets.

14. Farmland Impacts

A) Will the project affect prime farmlands?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
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B) Describe both positive and negative impacts anticipated as a result of the proposed project. If applicable, include a brief description of the FPPA ratings and information.

N/A.



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15. Social Impacts

A) Will the project affect neighborhoods or community cohesion for various social groups?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
B) Will the project affect travel patterns and accessibility (e.g. vehicular, commuter, bicycle, or pedestrian)?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
C) Will the project affect school districts, churches, businesses, police or fire departments, etc.?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
D) Will the project affect elderly, handicapped, non-drivers, or transit-dependent?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>

E) Describe both positive and negative impacts anticipated as a result of the proposed project.

Long term, the project will not have the above-listed effect and will provide a reliable transportation facility in the central corridor of Downtown Louisville. Short term, there would be minimal temporary traffic impacts during construction. KYTC to provide a robust public involvement plan to pro-actively communicate with local neighborhoods, stakeholders, agencies, elected officials and the traveling public. The MOT will include a number of scenarios to minimize impacts on I-65, and ensure east-west travel under I-65 is adequate.

Relocation of any homeless in the right-of-way are to be removed by Louisville Metro. Contractor to call Metro Office of Resilience and Community Services, Homeless Services Division: 502-574-6967.

16. Environmental Justice

A) Will the project have disproportionately high and adverse human health or environmental effects on minorities or low-income populations?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
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B) Describe both positive and negative impacts anticipated as a result of the proposed project.

Nearly all of the Census Tracts adjacent to project have high concentrations of EJ populations. The temporary traffic patterns could have some adverse effects, while minimized to the extent possible, but they would not be disproportionately high. Attachment F includes a Maintenance of Traffic (MOT) Community Impact Assessment that address environmental justice communities. The report concludes the project would not have a disproportionately high and adverse impact to environmental justice populations, mainly because the impacts would be temporary and minimized using accelerated bridge construction (ABC) methods with weekend closures only, and coordination to not close multiple east-west surface streets, and to include a robust public involvement campaign during the life of the project.

17. Local Land Use and Transportation Plan

A) Is there a local land use and/or local transportation plan?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>
B) Is the project consistent with the local land use plan?	NA <input type="checkbox"/>	Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
C) Is the project consistent with the local transportation plan?	NA <input type="checkbox"/>	Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>

D) Describe the consistencies and inconsistencies with the local land use and transportation plans.

Maintaining the existing infrastructure is consistent with the Long Range Plans for Louisville Metro (Move 2020), the Louisville MPO (KIPDA), and the KYTC. Each bridge and safety concept is included in the Louisville MPO (KIPDA) TIP. See Attachment D.



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18. Section 106: Architectural Historic Resources

A) Were any sites identified as eligible or potentially eligible for the National Register of Historic Places identified in the view shed of the project? Yes ☒ No ☐

B) What was the determination of effect from the proposed project?

No Effect ☐ No Adverse Effect ☒ Adverse Effect ☐

C) Discuss the analysis completed and any additional efforts required.

A cultural historic field survey was completed on 05/10/2023 by the KYTC DEA and KY SHPO staff. None of the existing bridges were determined to be eligible for the National Register of Historic Places (NRHP). Several adjacent sites within the 150 ft area of potential effect (APE) were determined to be eligible for listing on the NRHP. These properties are shown on the attachments and commitments have been made to place DND boundaries around each. The project was determined to have **No Adverse Effect** to historic properties. The Kentucky State Historic Preservation Officer (SHPO) agreed with this determination in a PA2 form on 06/07/2023.

Sites with the APE to be avoided are:

- One Historic Site in NW quadrant of Bridges 056B00195R and 056B00197R - on corner of S. Floyd St and E. Liberty St.
- Three Historic Sites along west side of Bridge 056B00184N, over E. St. Catherine St.

19. Section 106: Archaeological Resources

A) Were any sites identified as eligible or potentially eligible for the National Register of Historic Places identified within the project area? Yes ☐ No ☒

B) What was the determination of effect from the proposed project?

No Effect ☒ No Adverse Effect ☐ Adverse Effect ☐ Undetermined/Deferred ☐

C) Discuss the analysis completed and any additional efforts required.

There was a pedestrian survey of the area of potential effect (APE). No archaeological sites were identified during this investigation. The project was found to have "No Historic Properties Affected" by the KYTC and the The Kentucky State Historic Preservation Officer (SHPO) on 11/01/2023. The PA2 forms are included as Attachment C.

E) Is Native American Consultation required? Yes ☐ No ☒

F) Summarize any comments received during NAC, and if there was a tribal request for additional consultation.

Native American Consultation is not required for this project since there is no potential to affect Tribal lands, resources, or areas of historic significance. The pedestrian survey did not find any potential archaeology resources.



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20. Section 4(f)

A) Are Section 4(f) properties on/or adjacent to the project?

Yes ☒ No ☐

B) Is there a use of a Section 4(f) property?

NA ☐ Yes ☐ No ☒

C) Please indicate the type of 4(f) analysis required, if any.

De Minimis ☐ Programmatic ☐ Individual ☐

D) Discuss the analysis completed and any additional efforts required.

There are two adjacent areas with Section 4(f) properties (historic, only). SHPO agreed that the construction of a new bridges would have **No Adverse Effect** to the NRHP eligible properties. The new bridges would be of like mass, occupying the same footprint, and there will be no new vertical elements in the area. There is a Special Note to avoid the historic properties. The boundaries have been drawn on the construction plans with a note to Do Not Disturb.

- One Historic Site in NW quadrant of Bridges 056B00195R and 056B00197R - on corder of S. Floyd St and E. Liberty St.
- Three Historic Sites along west side of Bridge 056B00184N, over E. St. Catherine St.

21. Section 6(f)

A) Are Section 6(f) properties on/or adjacent to the project?

Yes ☐ No ☒

B) Is there a conversion of a Section 6(f) property?

NA ☒ Yes ☐ No ☐

C) Discuss the analysis completed and any additional efforts required.

22. Noise

A) Is this project considered a Type I project?

Yes ☐ No ☒

B) Discuss the analysis completed and any additional efforts required.

N/A. The bridge replacements are not Type I projects. No capacity is being added, and no horizontal or vertical changes would occur.



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23. Air Quality

A) Is the project located in an air quality non-attainment or maintenance area for ozone?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>
B) Is the project listed in an approved STIP and/or TIP?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>
STIP: KYTC STIP Admin Mod #2021.189 to incorporate: TIP: KIPDA 2020-2025 TIP, Admin Mod 46, May 15, 2023, all projects		
C) Is the project controversial or does the project HAVE or ADD a signalized intersection with a projected "open to traffic" year with an ADT>80,000 vehicles per day?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
D) Indicate the level of potential for Mobile Source Air Toxin Effects.		
No Potential (no analysis) <input checked="" type="checkbox"/> Low Potential (qualitative analysis) <input type="checkbox"/> Higher Potential (quantitative analysis) <input type="checkbox"/>		
E) Is the project in an area requiring PM 2.5 consideration?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
F) Discuss the analysis completed and any additional efforts required. Based on the data from current emissions models and the KYTC CO Screening Criteria, a project level CO analysis is not required for the project. All areas in Kentucky are in attainment for CO. This project is located in Louisville, which is an Ozone 8-hour Non-attainment area. This project is included in a transportation plan that demonstrates air quality conformity and therefore this project has been addressed for ozone. All areas in Kentucky are in attainment for PM _{2.5} and PM ₁₀ therefore, the conformity procedures of 40 CFR 93 do not apply to this project and no project level hot-spot analyses are necessary. All areas in Kentucky are in attainment for Nitrogen Dioxide (NO ₂) and project level conformity determinations are not necessary. The project is considered to have "No Potential For Meaningful MSAT Effects," as it is a CE document and as a bridge replacement project with no new rights-of-way or travel lanes [see KYTC Air Quality in NEPA Documents 2020 Update, Appendix A, Items 22, 26, & 28, Safety categories: "Bridge reconstruction (no additional travel lanes)" and "Projects that correct, improve, or eliminate a hazardous location or feature."] See Attachment I, for more discuss on the MSAT analysis.		

24. Hazardous Materials: Sites

A) Are known or potentially contaminated sites (service stations, landfills, automotive repair, junkyard, buildings with asbestos, etc.) located along the project corridor?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>
B) Is ROW required from, or extensive excavation required adjacent to, a potentially contaminated site?	NA <input type="checkbox"/>	Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
C) Discuss the analysis completed and any additional efforts required. No sites were identified. However, due to the historic urban land use "urban fill" should be expected with any digging for abutments or piers, tested and disposed of property. A Hazmat report will be prepared and provided to contractor, who must comply with all applicable laws and property dispose of any materials deemed contaminated above allowable limits, as documented in the Report. Within the project corridor, near the I-65 NB bridge over Liberty St. (5-10102, 056B00197R) there is one "covenant" site from the Louisville and Southern Indiana Ohio River Bridges (LSIORB) project that contains contaminated soils. The site has a 1-ft soil cap; thus, no impacts below 1 foot can occur. If unavoidable during construction the site should be covered with gravel, or some other surface, or fenced off, and marked on site with signage advising workers of the conditions. See Attachment B, pp 27.)		

25. Hazardous Materials: Bridges

A) Are there any bridges to be removed, refurbished, or repainted?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>
B) Will there be lead-based paint wastes?	NA <input type="checkbox"/>	Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
C) Discuss the analysis completed and any additional efforts required. Inspections for asbestos containing materials (ACM) and lead paint will be completed prior to any construction activities. A 10-day notice of intent for demolition, http://dep.ky.gov/formslibrary/Pages/default.aspx must be submitted to the Kentucky Division of Air Quality prior to abatement, demolition, or renovation of any building or structure in the Commonwealth. Any lead based paint will be disposed for properly per all applicable laws and policies.		



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26. Section 7: Threatened and Endangered Species

A) Is the project considered a No Effect by Definition?

Yes ☐ No ☒

B) List IPAC Species identified in project area, determination of effect, and additional analysis required.

Species	Determination of Effect	Additional Analysis Required
Gray Bat	No habitat, no effect.	None. Payment to IBCF will be completed by KYTC DEA.
Indiana Bat	May affect, likely to adversely affect - CMOA	
Northern Long-eared Bat	May affect, likely to adversely affect - DKey	No tree clearing June 1 -- July 30.
nine mussels	No habitat, no effect.	

C) Is the project located upstream of or within designated critical habitat?

Yes ☐ No ☒

D) Discuss the analysis completed and any additional efforts required.

In accordance with the April 17, 2020 Programmatic BO on the Effects of Transportation Projects in KY on the Indian Bat and Gray Bats, IB presence is assumed and a determination of **may affect, likely to adversely affect** applies. These proposed impacts will be mitigated for through the processes identified in the CMOA. No tree clearing June 1-July 31. The project is consistent with the Northern long-eared bat final 4(d) rule, and determination of **may affect, not likely to adversely effect**. No Effect Forms (NEFs) are included in Attachment E.

27. Water Resources

A) Does the project impact Waters of the U.S.?

Yes ☐ No ☒

B) Will the waters impacted include State Listed Special Use Waters or tributaries to Special Use Waters?

NA ☒ Yes ☐ No ☐

Indicate all types of Special Use Waters and/or tributaries impacted below.

<input type="checkbox"/>	Cold Water Aquatic Habitat	<input type="checkbox"/>	Federally Designated Scenic River
<input type="checkbox"/>	Reference Reach Stream	<input type="checkbox"/>	Exceptional Waters
<input type="checkbox"/>	Federally Designated Wild River	<input type="checkbox"/>	State Wild River
<input type="checkbox"/>	Outstanding National Resource Water		
<input type="checkbox"/>	Outstanding State Resource Water		

C) Is the project within the watershed of a significant water resource (private or public drinking water supply wellhead protection area, Special Use Water, etc.)?

Yes ☐ No ☒

D) Does the project involve impacts to a stream below the Ordinary High Water Mark or to a wetland?

NA ☒ Yes ☐ No ☐

Indicate all impacts below the OHWM.

<input type="checkbox"/>	Bridge/Pier/Abutment	<input type="checkbox"/>	Temporary Diversion
<input type="checkbox"/>	Culvert	<input type="checkbox"/>	Bank Stabilization
<input type="checkbox"/>	Low Water Crossing	<input type="checkbox"/>	Wetland Fill
<input type="checkbox"/>	Relocation/Channelization	<input type="checkbox"/>	Other:
<input type="checkbox"/>	Excess Excavation Site		



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E) Will the project impact a lake or pond requiring its draining or filling?	NA <input checked="" type="checkbox"/>	Yes <input type="checkbox"/>	No <input type="checkbox"/>
Does a stream enter the lake or pond?	NA <input type="checkbox"/>	Yes <input type="checkbox"/>	No <input type="checkbox"/>
Does a stream exit the lake or pond?	NA <input type="checkbox"/>	Yes <input type="checkbox"/>	No <input type="checkbox"/>

F) Discuss the analysis completed and any additional efforts required.

28. Permits

A) Is the project located partially or wholly within a designated MS4 community other than KYTC?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>
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Indicate any local ordinances, restriction, local permits, or other requirements that require consideration before, during, and after construction.

MSD is the Permittee for the Louisville MS4 community. KYTC has a statewide MS4 permit.

B) Will the project involve surface disturbance greater than one acre?	Yes <input checked="" type="checkbox"/>	No <input type="checkbox"/>
C) Are Section 401/404 permits likely to be required for this project?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>

Indicate permits expected to be required.

	USACE NW: BNR	USACE NW	USACE Letter of Permission	USACE Individual	KDOW General WQC	KDOW Individual WQC
Stream/Lake/Pond	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Wetland	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

D) Will this project affect navigable waters of the US as defined by the USACE and require a Section 10 permit?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
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E) Will this project affect a navigable body requiring a Coast Guard, Section 9 permit?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
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F) Does this project encroach upon the 100 year floodplain?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
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G) Is the project a candidate for application of the KYTC Karst policy?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
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H) Discuss the analysis completed and any additional efforts required.

All bridges are over surface streets. No Waters of the US will be involved.

A draft Kentucky Pollutant Discharge Elimination System KYR10 Best Management Practices (BMP) Plan for stormwater discharges associated with construction (KPDES KYR10) has been partially completed. If more than 1 acre of ground disturbance will occur, the KPDES KYR10 notice of intent (NOI) must be fully completed by the contractor and submitted to the Kentucky Division of Water (KDOW) 10 days prior to construction activities.



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29. Secondary and Cumulative Impacts

A) Will the project induce adverse or beneficial secondary and/or cumulative impacts? Yes ☐ No ☒

B) Describe any secondary and/or cumulative impacts anticipated as a result of the proposed project.

No secondary or cumulative impacts are anticipated.

30. Construction

A) Will excess excavation sites be required? Unknown ☐ Yes ☐ No ☒

B) Discuss potential impacts of construction activities pertaining to water quality, stream diversion, air quality, detours and delays of traffic, businesses, noise, etc.

Attachment F includes a Maintenance of Traffic (MOT) Community Impact Assessment.

Noise impacts originating from heavy equipment movement and other construction activities would be temporary and controlled in accordance with KYTC's Standard Specification, as directed by the KYTC project manager, and by using Best Management Practices.

Air quality impact would be temporary, and primarily in the form of diesel-powered construction equipment emissions and dust. Air pollution associated with airborne particle creations would be effectively controlled through the use of watering or the application of calcium chloride in accordance with KYTC's Standard Specifications, as directed by the KYTC project manager.

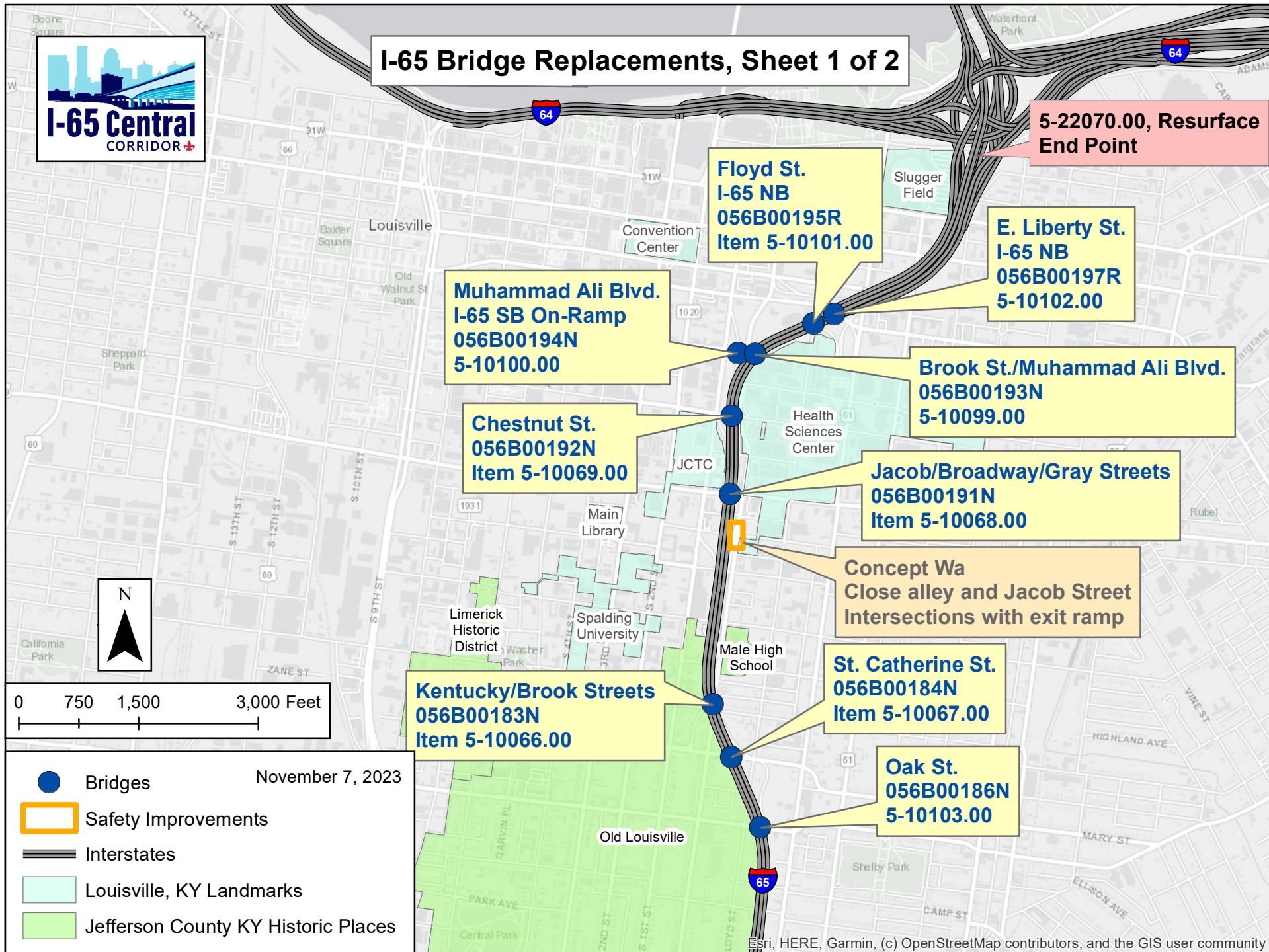
Construction activities, including **traffic maintenance**, would be planned and scheduled to minimize traffic delays. Signs would be used as appropriate to provide notice of pertinent information to the traveling public. Traffic will be maintained during construction.

Erosion and sediment control measures should be implemented as specified in the Kentucky Department of Highways Standard Specification (KHDSS) Section 212 and 213. KYTC or its contractors will control fugitive dust generation in accordance with KHDSS Section 107.01.04. Excess construction material will be managed in accordance with KHDSS Section 204. KYTC and their contractors will follow the Best Management Practices plan and groundwater protection plan provided with the project documents.

All waste material would be handled responsibly per policies--steel and rebar would be recycled, and concrete disposed of at a responsible or permitted site. No excess materials would be disposed of an unapproved locations.

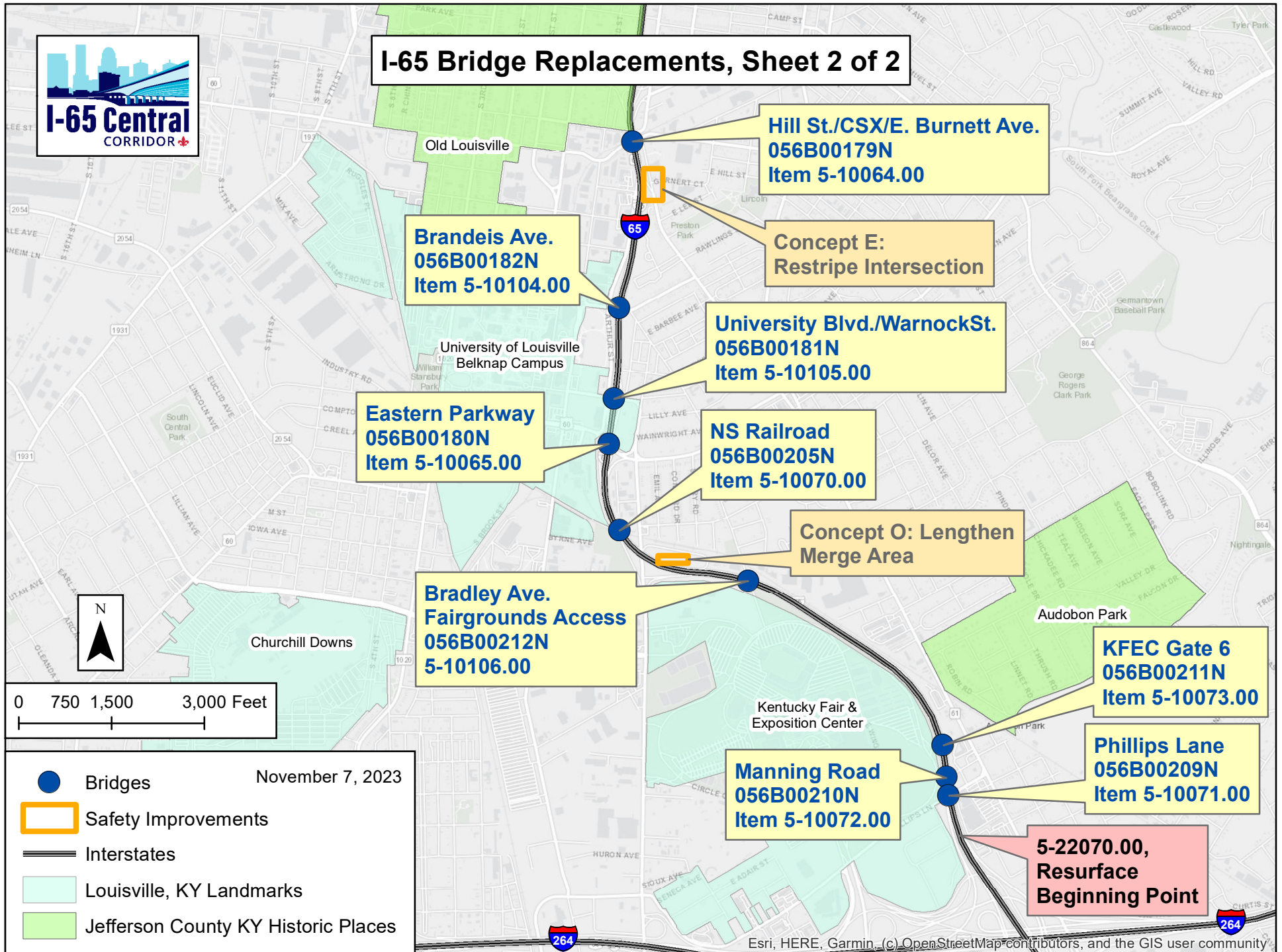


I-65 Bridge Replacements, Sheet 1 of 2





I-65 Bridge Replacements, Sheet 2 of 2



Bridge Number: 056B00179N I-65 over Hill/CSX/Burnett Item No.: 5-10064.00		Overall Bridge Rating: POOR	
Bridge Summary Sheet		Deck: 5 Super: 5 Sub: 4	
Project Description:		Work Type: Full Replacement	
BRIDGE PROJECT IN JEFFERSON COUNTY ON (056B00179N) I-65 AT HILL, CSX RR, & BURNETT (POTENTIAL CMGC DELIVERY PROJECT)			
MP: 133.878 Spans: 5 Deck Area: 39,600 SF Structure Length: 337.6 ft			
Bridge Type: Two simple spans and three continuous spans steel girders			
Identified Needs:		Proposed Benefits:	
<ul style="list-style-type: none"> Bridge Replacement required after multiple substructure repairs such as soil nail wall (November 2019). 		<ul style="list-style-type: none"> Improved condition ratings. Reduced maintenance costs. Reduced risk of major traffic disruption due to emergency repairs. Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed: 1957 / 1982		Design: \$ TBD	
Functional Class: Urban Interstate		Right-of-Way: \$ 75,000	
2021 ADT: 114308 VPD		Utilities: \$ 750,000	
Historic Resources: Old Louisville HD, inc. 4 sites: JFCO 1723, 1724, 1725, & 1726; individual site JFCO 1727 NR listed – SHPO approval of NAE 1/6/21		Construction: \$ 23,000,000	
In KIPDA TIP: Yes, Amendment 9		Total Cost: \$ 23,825,000	
Vertical Clearance: 22.417 ft			
Project Scope of Work:			
Bridge replacement. Temporary construction easements expected. Multiple existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Existing utilities from current facility mapping</u>			
- MSD: 108", 48", & 24" Sewer under Burnett Street - LG&E Gas: 6" Plastic under Burnett & Hill Streets - Louisville Water: 16" Under Hill Street			
<u>Potential utility conflicts from existing plans DN 18931</u>			
- Span #2 Burnett Ave: 4" & 10" gas; Buried telephone; 12" water main; 84" sewer - Span #4 Hill Street: 30" sewer; buried electric			
<u>Other potential utility issues</u>			
- Multiple utility poles with power lines above and under bridge deck. - Interstate lighting on bridge barrier and underneath within superstructure; conduit on east fascia beam - Interstate lighting conduit in outside bridge barrier walls			
<u>Pedestrian considerations</u>			
- Sidewalk on south side and bike path on north side of Hill Street - Pedestrian overpass over CSX on Hill Street			

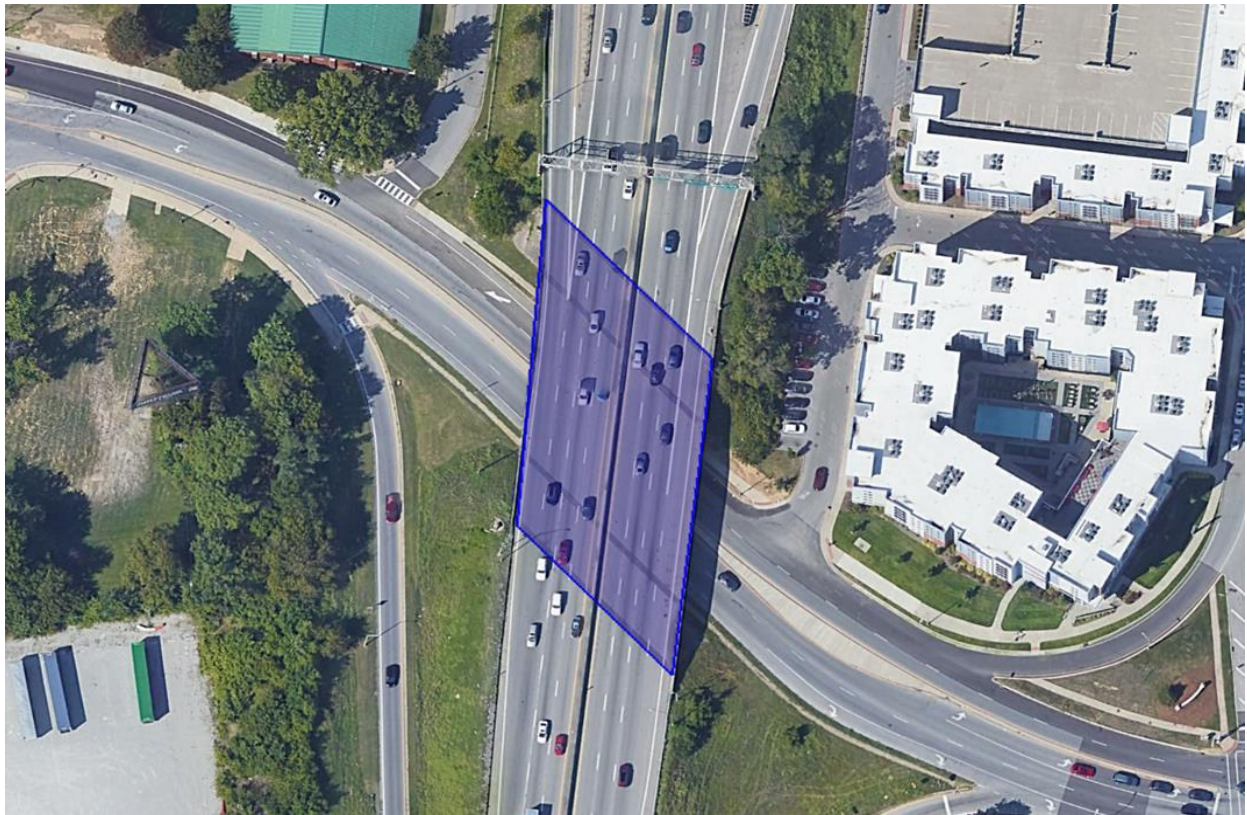


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING EAST

Bridge Number: 056B00180N I-65 over US-60A (Eastern PKWY) Item No.: 5-10065.00		Overall Bridge Rating: FAIR	
		Deck: 5 Super: 5 Sub: 5	
Project Description: Rehabilitation involving deck replacement of I-65 bridge over US-60A (Eastern Parkway), location is 2.1 miles north of I-264.		Work Type: Minor Rehabilitation	
MP: 132.914 Spans: 3 Deck Area: 25,853 SF Structure Length: 220.4 ft			
Bridge Type: Simple Span Steel Girder			
Identified Needs: <ul style="list-style-type: none"> • Clean and Paint – Laminar corrosion at beam ends below joints. • Painted UofL colors. • Reset and rehabilitate bearings. • Deck replacement. • Slope protection repairs. • Joint elimination. • Substructure patching, crack injection, spot replacement if required. 		Proposed Benefits: <ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1957 / 1982	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	114,308 VPD	Utilities:	\$ 75,000
Historic Resources:	Yes, Eastern Parkway, JFL 270 NR listed	Construction:	\$ 10,500,000
In KIPDA TIP:	Yes, ID 3112	Total Cost:	\$ 10,625,000
Vertical Clearance:	15.417 ft (substandard)		
Project Scope of Work: Bridge rehabilitation and deck replacement. Temporary construction easements expected. Multiple existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential Utility Issues</u> <ul style="list-style-type: none"> - TRIMARC cameras just off of Abutment #1 NB & SB - Underpass lighting between girders in Span #2 - Outside bridge barrier walls have electrical conduit for interstate lighting. - Conduits for underpass lighting attached to Pier #2 - 5" attached fiber optic line @ South Abutment 			
<u>Potential pedestrian issues</u> <ul style="list-style-type: none"> - Sidewalk on each side of Eastern Parkway 			

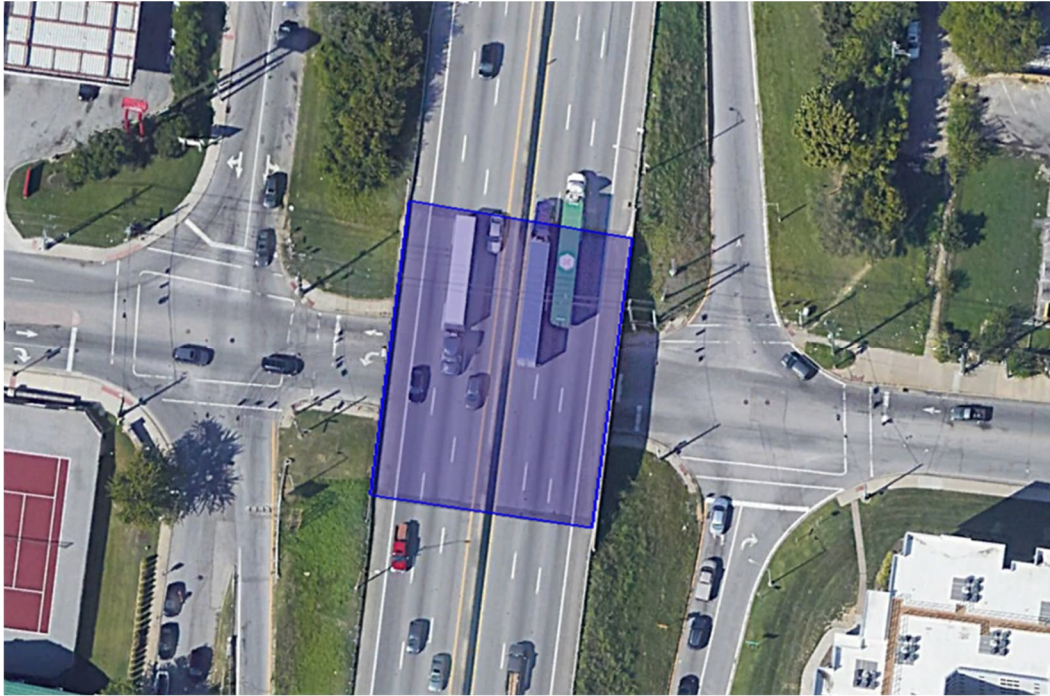


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST

Bridge Number: 056B00181N I-65 over Warnock/University Item No.: N/A		Overall Bridge Rating: FAIR	
Bridge Summary Sheet		Deck: 5	Super: 5
Sub: 5			
Project Description:		Work Type: Minor Rehabilitation	
Rehabilitation involving deck replacement of I-65 bridge over University Boulevard/Warnock Street, location is 0.15 miles north of US-60A (Eastern Parkway).			
MP: 133.062 Spans: 3 Deck Area: 13,159 SF Structure Length: 129.9 ft			
Bridge Type: Continuous span concrete tee beam			
Identified Needs:		Proposed Benefits:	
<ul style="list-style-type: none"> (Superstructure replacement may be a better solution due to difficulty replacing a deck on tee beam bridge). Painted UofL colors. Beam patching. Abutment backwalls need patched. Repair slope protection. 		<ul style="list-style-type: none"> Improved condition ratings. Reduced maintenance costs. Reduced risk of major traffic disruption due to emergency repairs. Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1957 / 1982	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	114,308 VPD	Utilities:	\$ 75,000
Historic Resources:	No	Construction:	\$ 5,300,000
In KIPDA TIP:	No		
Vertical Clearance:	13.668 ft (substandard)	Total Cost:	\$ 5,425,000
Project Scope of Work:			
Bridge rehabilitation. Temporary construction easements expected. Multiple existing utilities near the bridge to strive for avoidance or minimization of impacts.			
Potential utility issues			
<ul style="list-style-type: none"> - Overhead utility lines in vicinity of north end of Span #2. Utility pole very close to east side of Pier #2. - Utility line from same utility pole running under deck along front of Pier #2 - Conduit extending down from east barrier wall at Abutment #2 and attached to Abutment. - Underpass lighting attached to underside of deck - Electrical conduit for interstate lighting in outside barrier walls 			
Potential Pedestrian Issues			
<ul style="list-style-type: none"> - University of Louisville has sporting complexes in the NW & SW quadrants and student housing in the SE quadrant; all adjacent to bridge. 			

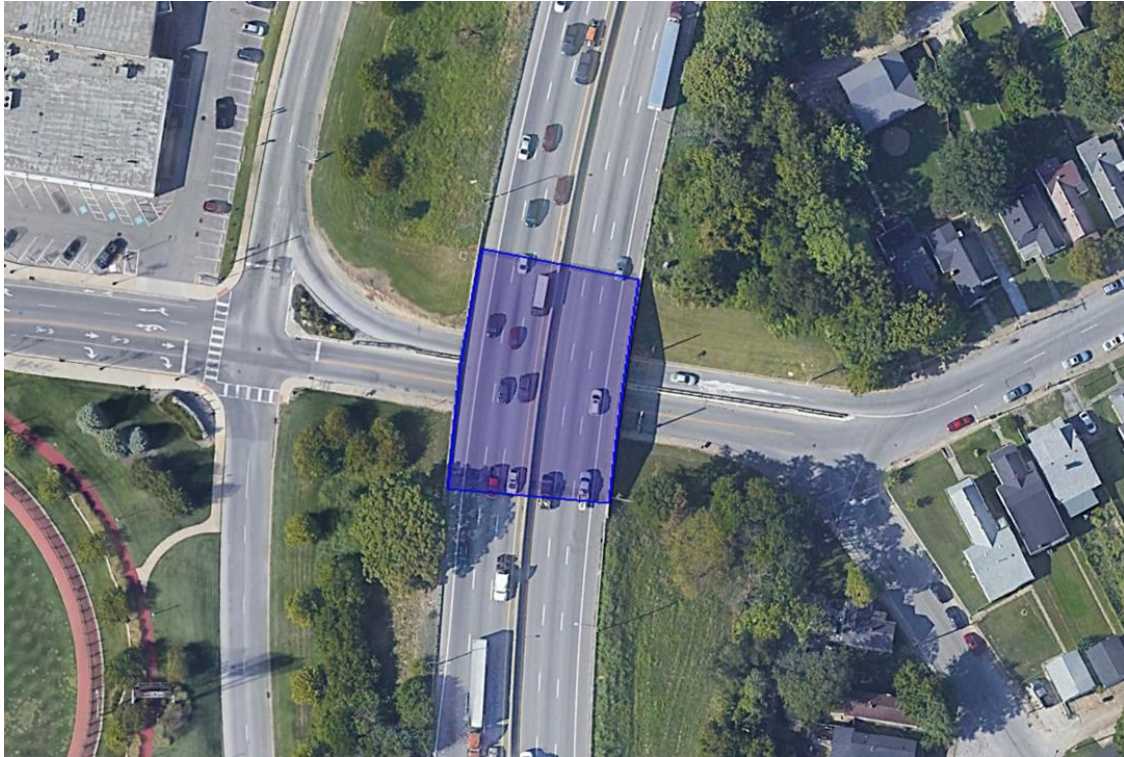


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST

Bridge Number: 056B00182N I-65 over Brandeis (KY-61) Item No.: N/A		Overall Bridge Rating: FAIR	
Bridge Summary Sheet		Deck: 5	Super: 5
Project Description: Rehabilitation involving deck replacement of I-65 bridge over KY-61/Brandeis Avenue, location is 0.4 miles north of US-60A (Eastern Parkway).		Work Type: Minor Rehabilitation	
MP: 133.337 Spans: 3 Deck Area: 13,969 SF Structure Length: 137.9 ft			
Bridge Type: Continuous concrete tee beam			
Identified Needs: <ul style="list-style-type: none"> Deck Replacement (Superstructure replacement may be a better solution due to difficulty replacing a deck on tee beam bridge). Painted UoFL colors. Beam patching Abutment backwalls need significant repair. Bearing replacement 		Proposed Benefits: <ul style="list-style-type: none"> Improved condition ratings. Reduced maintenance costs. Reduced risk of major traffic disruption due to emergency repairs. Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1957 / 1982	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	114,308 VPD	Utilities:	\$ 75,000TBD
Historic Resources:	No	Construction:	\$ 5,600,000
In KIPDA TIP:	No	Total Cost: \$ 5,725,000	
Vertical Clearance:	13.917 ft (substandard)		
Project Scope of Work: Bridge rehabilitation and superstructure replacement. Temporary construction easements expected. Minor existing utilities near the bridge to strive for avoidance or minimization of impacts. <u>Potential utility issues</u> <ul style="list-style-type: none"> - Overhead utility parallels bridge on east side - Underpass lighting in span #2 attached between girders - (4) electrical conduits attached to north abutment running under deck - Outside bridge barrier walls have electrical conduit for interstate lighting. <u>Potential pedestrian issues</u> <ul style="list-style-type: none"> - University of Louisville has facilities in the NW & SW quadrants with sidewalk on the south end under Span #2. - Heavy residential on east side of bridge. 			

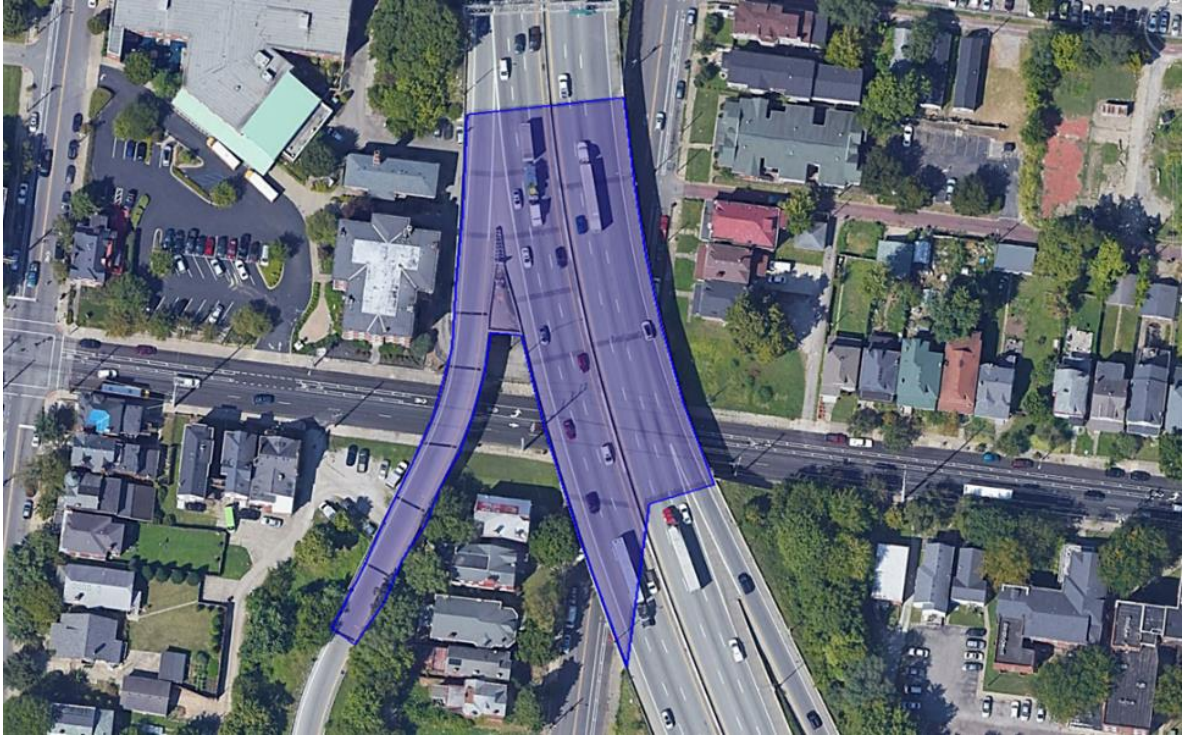


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING EAST

Bridge Number: 056B00183N I-65 over Kentucky/Brook Item No.: 5-10066.00		Overall Bridge Rating: POOR	
		Deck: 5	Super: 4
Bridge Summary Sheet			
Project Description:		Work Type: Full Replacement	
Replace the existing I-65 bridge over Brook / E Kentucky Streets, location is 0.5 miles south of US-150.			
MP: 134.74 Spans: 3 Deck Area: 53,393 SF Structure Length: 460.60 ft			
Bridge Type: Steel girders, complex framing, fracture critical steel cross girders, pin & hanger assemblies			
Identified Needs: <ul style="list-style-type: none"> Bridge replacement due to condition of piers and superstructure Multiple locations with active cracks in primary steel members Significant pier repair needed 		Proposed Benefits: <ul style="list-style-type: none"> Improved condition ratings. Reduced maintenance costs. Reduced risk of major traffic disruption due to emergency repairs. Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1959 / 1982 / Recent significant repairs	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 100,000
2021 ADT:	114,308 VPD	Utilities:	\$ 400,000
Historic Resources:	Yes, Old Louisville HD, 1 site: JFCN 10; Individual site JFCN 84 NR listed. – SHPO approval of NAE 1/6/21	Construction:	\$ 35,000,000
In KIPDA TIP:	Yes, Amendment 9		
Vertical Clearance:	15.083 ft (substandard)	Total Cost:	\$ 35,500,000
Project Scope of Work:			
Bridge replacement. Temporary construction easements expected. Multiple existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues from current facility mapping</u> <ul style="list-style-type: none"> - 60" & 66" MSD sewer under Kentucky Street; 15" & 27" MSD sewer under Brook Street - (2) 4" plastic gas line under Kentucky Street; 2" plastic gas line under Brook Street. - 16" water main under Kentucky Street; 10" water main under Brook Street. 			
<u>Potential utility issues from existing plans DN 19960</u> <ul style="list-style-type: none"> - 60" MSD Trunk Line running under centerline of Kentucky Street; 27" MSD Sewer running under centerline of Brook Street, west of Kentucky Street; 15" MSD Sewer running under centerline of Brook Street, west of Kentucky Street - 16" Water Main under Kentucky Street; 6" Water Main under Brook Street - 16" High Pressure Gas Main, 10" Gas Main, 8" Low Pressure Gas Main under Kentucky Street; 4" Low Pressure Gas Main under Brook Street 			
<u>Other potential utility issues</u> <ul style="list-style-type: none"> - Substantial utility poles in proximity with power lines over and under bridge - Interstate lighting on bridge barrier and under bridge in superstructure - Intersection signalization under bridge and adjacent - Outside bridge barrier walls have electrical conduit for interstate lighting. 			
<u>Potential pedestrian issues</u> <ul style="list-style-type: none"> - Heavy residential with sidewalks on Kentucky & Brook Streets. 			

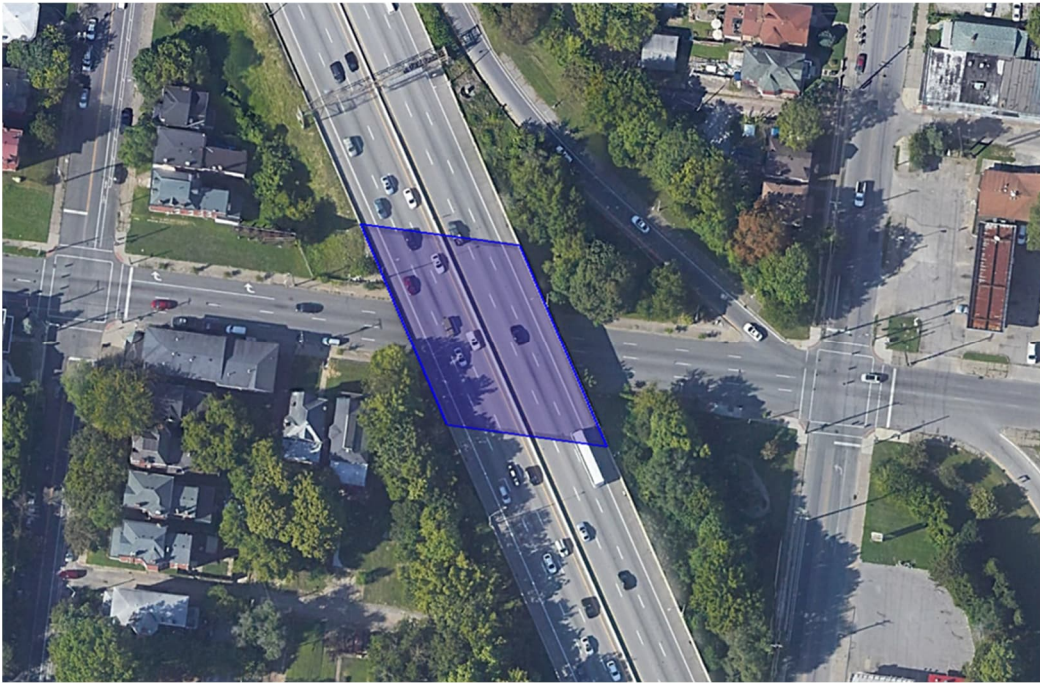


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING EAST

Bridge Number: 056B00184N I-65 over E St. Catherine Item No.: 5-10067.000		Overall Bridge Rating: FAIR	
Bridge Summary Sheet		Deck: 5	Super: 6
Project Description: Rehabilitation involving deck replacement of I-65 bridge over E St. Catherine Street, location is 0.4 miles south of East Breckinridge.		Work Type: Minor Rehabilitation	
MP: 134.621 Spans: 3 Deck Area: 17,638 SF Structure Length: 167.50 ft			
Bridge Type: Continuous span steel girder			
Identified Needs: <ul style="list-style-type: none"> • Deck replacement • Paint structural Steel • Reset and rehabilitate bearings. • Concrete substructure repairs. • Joint elimination. 		Proposed Benefits: <ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency. 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1960 / 1982	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	114,308 VPD	Utilities:	\$ 75,000
Historic Resources:	Yes, Old Louisville HD, inc. 2 sites: JFCO 1760 & 1759, NR listed	Construction:	\$ 7,000,000
In KIPDA TIP:	Yes, ID - 3114	Total Cost: \$ 7,125,000	
Vertical Clearance:	15.25 ft (substandard)		
Project Scope of Work: Bridge rehabilitation and deck replacement. Temporary construction easements expected. Multiple existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues</u> <ul style="list-style-type: none"> - Interstate lighting standards on outside barrier walls - TRIMARC overhead sign just north of north Abutment - Utility line running adjacent to north side of Pier #1 under bridge deck - Underpass lighting between beams in Span #2 - Utility box attached to pole NW quadrant - Outside bridge barrier walls have electrical conduit for interstate lighting. 			
<u>Potential pedestrian issues</u> <ul style="list-style-type: none"> -Heavy residential with sidewalks on both sides of St. Catherine Street. 			

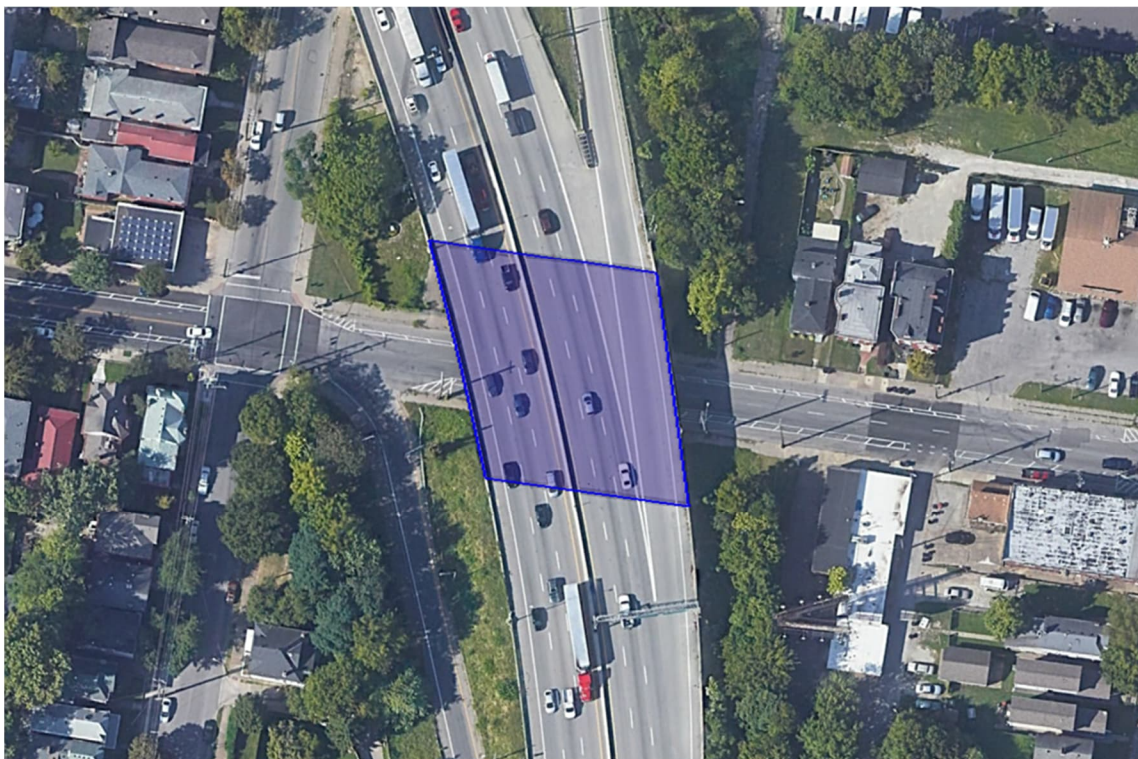


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST

Bridge Number: 056B00186N I-65 over E Oak St. Item No.: N/A		Overall Bridge Rating: FAIR	
Bridge Summary Sheet		Deck: 5	Super: 6
Project Description:		Work Type: Minor Rehabilitation	
Rehabilitation involving deck replacement of I-65 bridge over E Oak Street, location is 0.8 miles south of US-150.			
MP: 134.442	Spans: 3	Deck Area: 20,662 SF	Structure Length: 158.333 ft
Bridge Type: Continuous span steel girder			
Identified Needs:		Proposed Benefits:	
<ul style="list-style-type: none"> Deck replacement. Paint structural steel. Reset and rehabilitate bearings. Concrete substructure repairs. Joint elimination. 		<ul style="list-style-type: none"> Improved condition ratings. Reduced maintenance costs. Reduced risk of major traffic disruption due to emergency repairs. Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1960 / 1982	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2023 ADT:	114,308 VPD	Utilities:	\$ 75,000
Historic Resources:	Yes, Old Louisville HD, 1 site: JFCO 2061, NR listed	Construction:	\$ 8,000,000
In KIPDA TIP:	No	Total Cost: \$ 8,125,000	
Vertical Clearance:	14.667 ft (substandard)		
Project Scope of Work:			
Bridge rehabilitation and deck replacement. Temporary construction easements expected. Multiple existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues</u> <ul style="list-style-type: none"> - 2 light standards on outside barrier walls - Utility lines run overhead and underneath bridge deck south of Pier #2 - Underpass lighting in span #2 between girders. - Electrical conduit attached to base of Abutment #2 - Outside bridge barrier walls have electrical conduit for interstate lighting. - TRIMARC camera in NE quadrant. 			
<u>Potential pedestrian issues</u> <ul style="list-style-type: none"> - Heavy residential area with sidewalks on both sides of Oak Street. 			



AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING EAST

Bridge Number: 056B00191N I-65 over Jacob/Broadway/Gray Item No.: 5-10068.00		Overall Bridge Rating: POOR	
		Deck: 6	Super: 5
		Sub: 4	
Project Description:		Work Type: Major Rehabilitation	
Major rehabilitation of the existing I-65 bridge over E Jacob, E Broadway, and E Gray Street, location is 0.5 miles south of US-31E.			
MP: 135.273 Spans: 21 Deck Area: 127,202 SF Structure Length: 1208 ft			
Bridge Type: Varies: Continuous and simple span prestressed concrete I beams, continuous span steel girder over E Gray and E Jacob, continuous span concrete tee beam over E Broadway			
Identified Needs:		Proposed Benefits:	
<ul style="list-style-type: none"> Posted for SUV's: SUV5 – 37 tons, SUV6 – 38 tons, SUV7 – 39 tons Major substructure repairs. Partial superstructure replacement. Deck preservation. Clean and paint structural steel and bearings. Repair leaking joints. 		<ul style="list-style-type: none"> Improved condition ratings. Reduced maintenance costs. Reduced risk of major traffic disruption due to emergency repairs. Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1960 / 1980	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 75,000
2021 ADT:	114.308 VPD	Utilities:	\$ 75,000
Historic Resources:	Yes, 3 individual sites: JFCD 246, JFCH 701, & JFCH 704 NR listed. – SHPO approval of NAE 1/6/21	Construction:	\$ 63,000,000
In KIPDA TIP:	Yes, ID 3115	Total Cost:	\$ 63,150,000
Vertical Clearance:	14.667 ft (substandard)		
Project Scope of Work:			
Bridge rehabilitation. Temporary construction easements expected. Multiple existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues from current facility mapping</u>			
- 24" MSD sewer under Brook Street; 90" sewer under Broadway - 4" plastic gas under Grey Street; 2" plastic gas under Hartford Hall; 4" & 6" plastic under Broadway - 16" water main under Broadway			
<u>Other potential utility issues</u>			
- Interstate lighting standards on outside barrier walls - Outside bridge barrier walls have electrical conduit for interstate lighting. - Underpass lighting under most all spans - Overhead utility lines just south of Pier #122 (north side of Jacob Street.) - Telephone cable attached to SW corner of Pier 117 (alley between Jacob Street & Broadway.)			
<u>Other potential issues</u>			
- Overhead sign support with TRIMARC camera attached to outside bridge barrier wall in Spann #111 (just north of Broadway.) - Parking lots associated with University of Louisville & various hospitals under majority of structure - Parking meters on main roadways under bridge - Jefferson Community & Technical College and several hospitals in area. Heavy pedestrian traffic with sidewalks on all roads.			

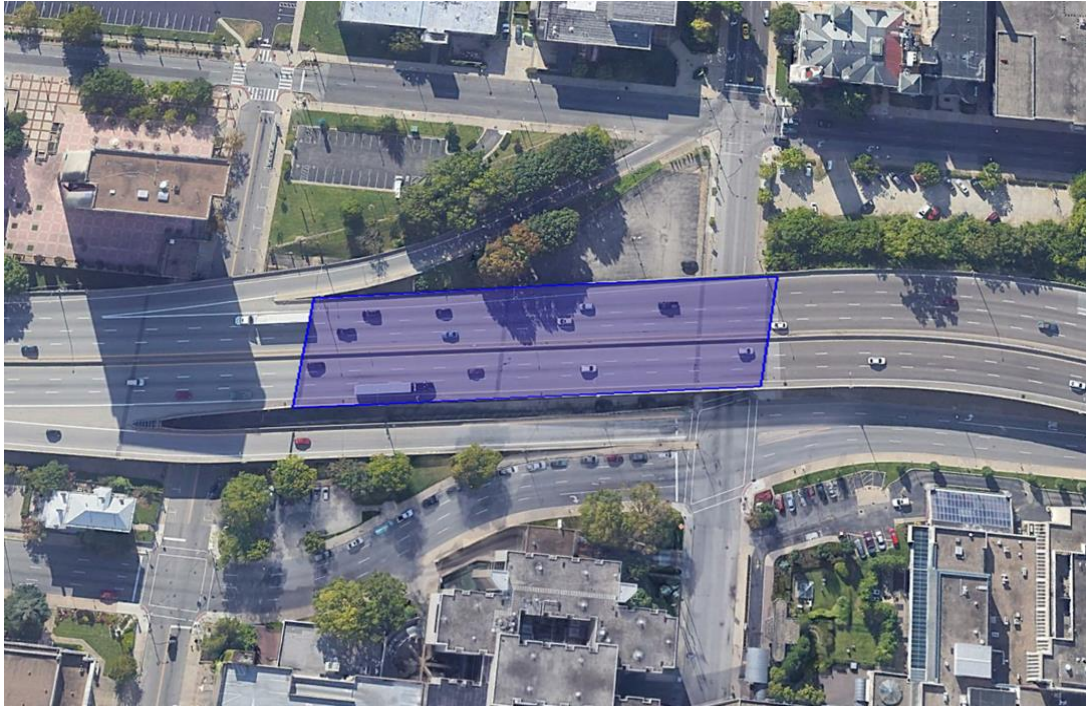


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING EAST ALONG BROADWAY

Bridge Number: 056B00192N I-65 over Chestnut St. Item No.: 5-10069.00		Overall Bridge Rating: FAIR	
		Deck: 6 Super: 5 Sub: 5	
Project Description: Major rehabilitation of the existing I-65 bridge over Chestnut Street, location is 0.18 miles north of US-150.		Work Type: Major Rehabilitation	
MP: 135.435 Spans: 9 Deck Area: 43,878 SF Structure Length: 435.3 ft			
Bridge Type: PCI Beam: Two 4-span continuous units and one simple span			
<ul style="list-style-type: none"> • PCI Beam end repairs. • Major substructure rehabilitation. • Deck preservation • Repair leaking joints. • Attached utilities and lighting. 		Proposed Benefits: <ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1963 / 1980	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	114,308 VPD	Utilities:	\$ 75,000
Historic Resources:	Yes, 1 individual site: JFCD 159 NR listed	Construction:	\$ 20,000,000
In KIPDA TIP:	No	Total Cost: \$ 20,125,000	
Vertical Clearance:	15.5 ft (substandard)		
Project Scope of Work: Bridge rehabilitation. Temporary construction easements expected. Multiple existing utilities near the bridge to strive for avoidance or minimization of impacts. <u>Potential utility issues from current facility mapping</u> - 12" MSD sewer; LGE 4" steel gas main; LWC 8" water main <u>Other potential utility issues</u> - Interstate lighting standards on outside bridge barrier walls - Outside bridge barrier walls have electrical conduit for interstate lighting. - Underpass lighting under most all spans - Fascia beam on east side has attached conduit along entire length - East side of north Abutment has utility meter attached <u>Other potential issues</u> - Parking lots under majority of structure - Parking meters on roadway under bridge - Several hospitals in area. Heavy pedestrian traffic with sidewalks on all roads.			

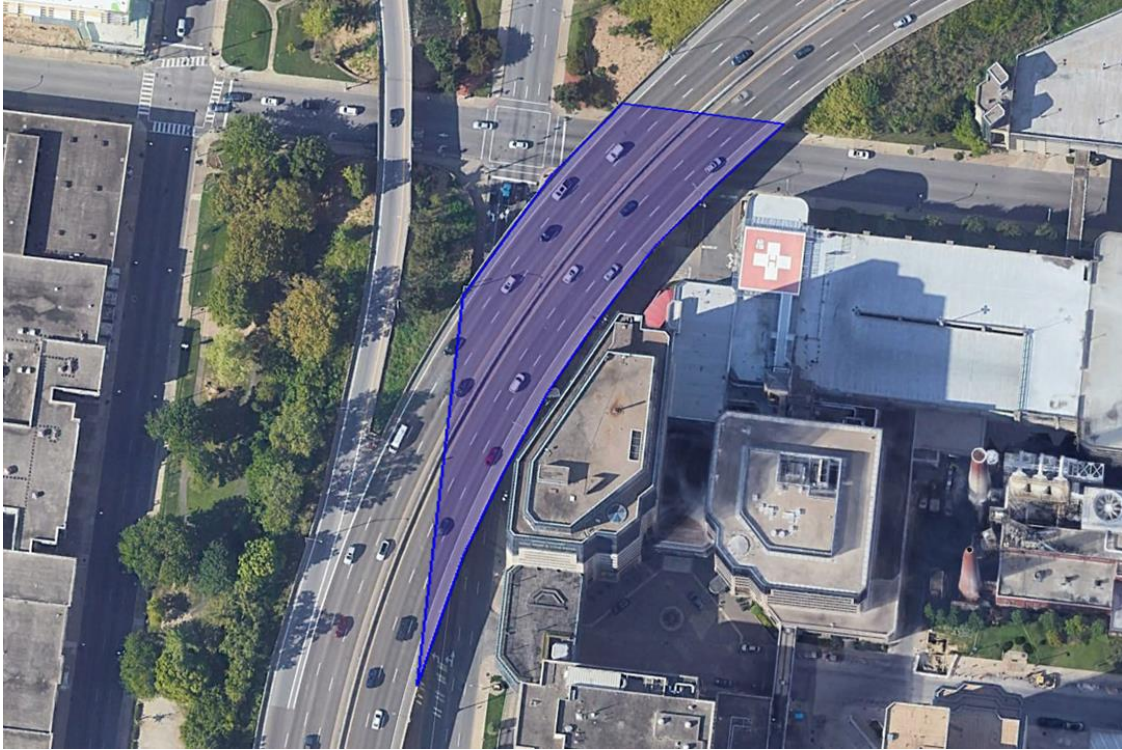


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING EAST

Bridge Number: 056B00193N I-65 over Brook/Muhammad Ali Item No.: N/A		Overall Bridge Rating: FAIR Deck: 5 Super: 5 Sub: 5	
Bridge Summary Sheet			
Project Description:		Work Type: Rehabilitation	
Rehabilitation including deck replacement and painting of the existing I-65 bridge over Brook Street and Muhammad Ali Street, location is 0.15 miles north of E Chestnut.			
MP: 135.584 Spans: 5 Deck Area: 34,776 SF Structure Length: 345.0 ft			
Bridge Type: Simple span steel girder			
Identified Needs:		Proposed Benefits:	
<ul style="list-style-type: none"> • Deck replacement. • Clean and paint structural steel and bearings. • Fatigue detail retrofits. • Concrete substructure repairs. • Joint replacement. 		<ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1963 / 1980	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	114,308 VPD	Utilities:	\$ 75,000
Historic Resources:	No	Construction:	\$ 12,500,000
In KIPDA TIP:	No	Total Cost: \$ 12,625,000	
Vertical Clearance:	15 ft (substandard)		
Project Scope of Work:			
Bridge rehabilitation, deck replacement, painting, and fatigue detail retrofits. Temporary construction easements expected. Minor existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues</u> <ul style="list-style-type: none"> - Interstate lighting standards on outside bridge barrier walls - Outside bridge barrier walls have electrical conduit for interstate lighting. - Underpass lighting under most all spans (attached to Pier Cap and Abutment wall with conduits attached. - Fascia beam on east side has attached conduit along entire length - East side of north Abutment has utility meter attached 			
<u>Other potential issues</u> <ul style="list-style-type: none"> - Parking lots under majority of structure - Parking meters on roadway under bridge - Several hospitals in area. Heavy pedestrian traffic with sidewalks on all roads. - Jewish Hospital on East side with helicopter landing pad very near interstate. 			

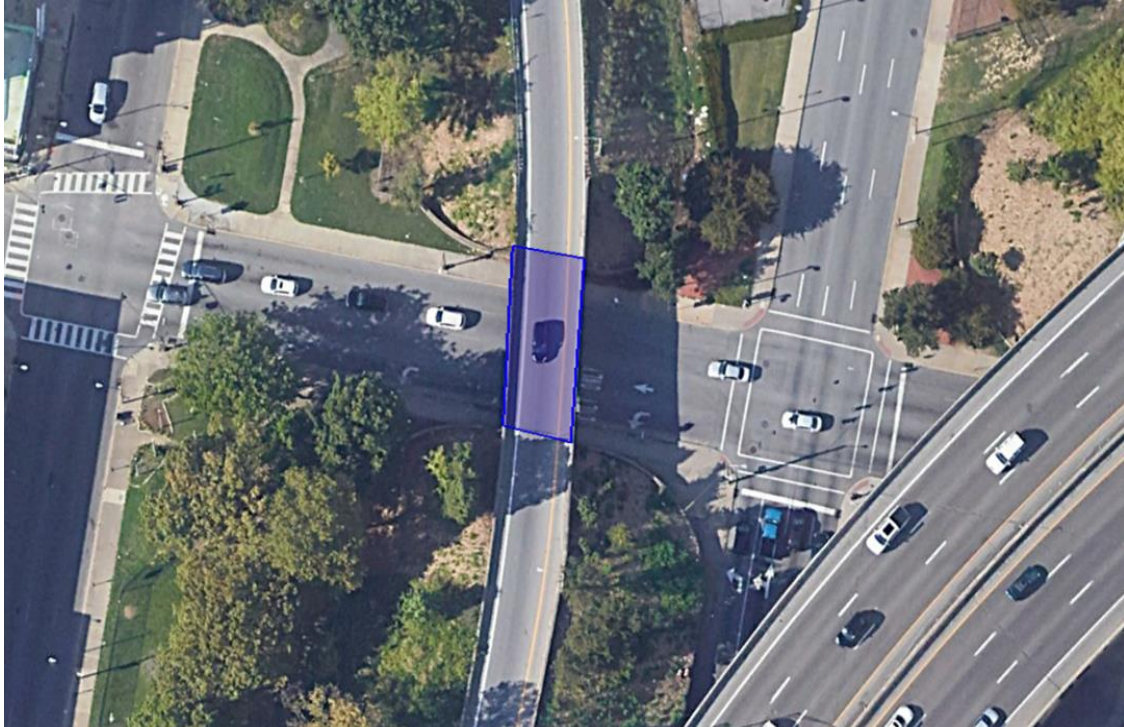


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST

Bridge Number: 056B00194N I-65 over Muhammad Ali Ramp Item No.: N/A		Overall Bridge Rating: FAIR	
Bridge Summary Sheet		Deck: 6	Super: 7
Project Description: Minor rehabilitation including overlay and substructure work of the existing I-65 bridge over Muhammad Ali Ramp, location is 0.1 miles south of 1 st Street.		Work Type: Minor Rehabilitation	
MP: 135.601 Spans: 1 Deck Area: 1,759 SF Structure Length 64.2 ft			
Bridge Type: Simple span steel girder			
Identified Needs: <ul style="list-style-type: none"> • Deck preservation – has asphalt overlay • Clean and paint structural steel. • Concrete substructure repairs. • Repair / eliminate leaking joints. 		Proposed Benefits: <ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1963 / 1980	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	114,308 VPD	Utilities:	\$ 75,000
In Historic District:	No	Construction:	\$ 800,000
In KIPDA TIP:	No		
Vertical Clearance:	15.333 ft (substandard)	Total Cost:	\$ 925,000
Project Scope of Work: Bridge rehabilitation, deck overlay and minor substructure repair. Temporary construction easements expected. Minor existing utilities near the bridge to strive for avoidance or minimization of impacts. <u>Potential utility issues</u> - Outside bridge barrier walls have electrical conduit for interstate lighting. - Underpass lighting under span <u>Potential pedestrian issues</u> -Business district with sidewalks on each side of roadway			



AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST

Bridge Number: 056B00195R I-65 over Floyd St. Item No.: N/A		Overall Bridge Rating: FAIR	
		Deck: 6	Super: 7
		Sub: 6	
Project Description: Minor rehabilitation, including an overlay, painting, and minor substructure work of the existing I-65 bridge over Floyd Street, location is 0.15 miles north of Muhammad Ali Boulevard.		Work Type: Minor Rehabilitation	
MP: 135.755 Spans: 1 Deck Area: 4,846 SF Structure Length: 80.1 ft			
Bridge Type: Simple span steel girder			
Identified Needs: <ul style="list-style-type: none"> • Deck preservation – has asphalt overlay. • Clean and paint structural steel. • Concrete substructure repairs. • Repair / eliminate leaking joints. 		Proposed Benefits: <ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1963 / 1980	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	114,308 VPD	Utilities:	\$ 75,000
Historic Resources:	No	Construction:	\$ 1,200,000
In KIPDA TIP:	No		
Vertical Clearance:	16.167 ft	Total Cost:	\$ 1,325,000
Project Scope of Work: Bridge rehabilitation, deck overlay, painting, and minor substructure repair. Temporary construction easements expected. Minor existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues</u> <ul style="list-style-type: none"> - Interstate lighting outside barrier wall - Outside bridge barrier wall and median barrier wall have electrical conduit for interstate lighting. - Underpass lighting under span - Conduit attached to upper portion of both abutments - Parking meters on roadway under bridge 			
<u>Potential pedestrian issues</u> <ul style="list-style-type: none"> - Business district with sidewalks on each side of roadway 			



AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST

Bridge Number: 056B00197R I-65 over E. Liberty St. Item No.: N/A		Overall Bridge Rating: FAIR	
Bridge Summary Sheet		Deck: 5	Super: 6
Sub: 5			
Project Description:		Work Type: Minor Rehabilitation	
Minor rehabilitation including a deck replacement, painting, and minor substructure work of the existing I-65 bridge over E. Liberty Street, location is 0.2 miles north of Muhammad Ali Boulevard.			
MP: 135.814 Spans: 1 Deck Area: 5,375 SF Structure Length: 97.9 ft			
Bridge Type: Simple span steel girder			
Identified Needs:		Proposed Benefits:	
<ul style="list-style-type: none"> • Deck replacement • Clean and paint structural steel. • Concrete substructure repairs. • Eliminate joints. • Reset / replace bearings. 		<ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1963 / 1980	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	114,308 VPD	Utilities:	\$ 75,000
In Historic District:	No	Construction:	\$ 2,500,000
In KIPDA TIP:	No		
Vertical Clearance:	17.583 ft	Total Cost:	\$ 2,625,000
Project Scope of Work:			
Bridge rehabilitation, deck replacement, painting, and minor substructure repair. Temporary construction easements expected. Minor existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues</u>			
- Underpass lighting supported between girders - Conduits attached to both abutment walls			
<u>Potential pedestrian issues</u>			
- Business district with sidewalks on each side of roadway			



AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST

FILE NAME: C:\Users\KVIN\Documents\Project Drawings\5-2080\08-KVC SITE MANAGEMENT PLAN 02&20 ACAD

USER: KVIN
DATE PLOTTED: JANUARY 28, 2020

R00100EC

E-SHEET NAME:

MICROSTATION V8.11.7.443

SOURCE: GOOGLE EARTH, IMAGERY DATE: OCTOBER 21, 2018

COUNTY OF	ITEM NO.	SHEET NO.
JEFFERSON	5-2080.00	8 OF 8

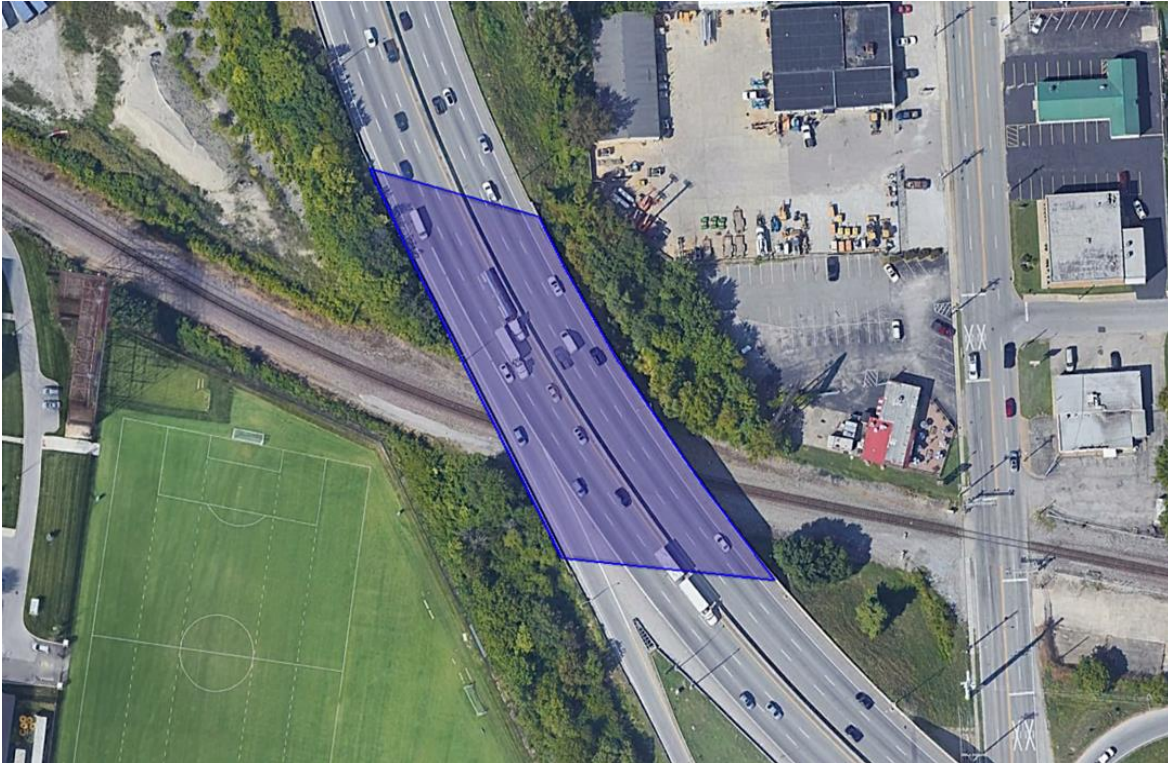
Hazmat Covenant Site
"Area 3" from LSIORB
Project. Site shall not be
disturbed below 2 feet.
Cover with gravel, or
other material, and place
signage to alert workers
to avoid.

AREA 3

DOWNTOWN CROSSING
WASTE MANAGEMENT AREA
FIGURE 8

SCALE: 1"=150'

Bridge Number: 056B00205N I-65 over Norfolk Southern RR Item No.: 5-10070.00		Overall Bridge Rating: FAIR	
		Deck: 5 Super: 5 Sub: 5	
Project Description:		Work Type: Deck Replacement	
Deck replacement and substructure rehabilitation of the existing I-65 bridge over Norfolk Southern Railroad, location is 500' north of Crittenden Drive.			
MP: 132.647 Spans: 4 Deck Area: 40,124 SF Structure Length: 367.10 ft			
Bridge Type: Continuous span steel girder			
Identified Needs: <ul style="list-style-type: none"> • Deck replacement. • Clean and paint structural steel. • Reset or replace bearings. • Concrete substructure repairs. • Joint elimination. • Embankment repairs. 		Proposed Benefits: <ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency. 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1957 / 1982	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	119,270 VPD	Utilities:	\$ 75,000
Historic Resources:	No	Construction:	\$ 16,000,000
In KIPDA TIP:	Yes, ID - 3035	Total Cost:	\$ 16,125,000
Vertical Clearance:	22.417 ft		
Project Scope of Work:			
Bridge rehabilitation and deck replacement. Temporary construction easements expected. Minor existing utilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues</u> <ul style="list-style-type: none"> - Interstate lighting on bridge barrier wall - Outside bridge barrier walls have electrical conduit for interstate lighting. - Norfolk Southern Railroad under bridge 			



AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST

Bridge Number: 056B00209N I-65 over Phillips Lane Item No.: 5-10071.00		Overall Bridge Rating: FAIR	
		Deck: 5 Super: 5 Sub: 5	
Project Description: Rehabilitation involving a deck replacement of existing I-65 bridge over Phillips Lane, location is 0.45 miles north of I-64.		Work Type: Minor Rehabilitation	
MP: 131.245 Spans: 3 Deck Area: 19,325 SF Structure Length: 145.3 ft			
Bridge Type: Continuous span steel girder			
Identified Needs: <ul style="list-style-type: none"> • Posted for EV's: EV Single – 16 tons, EV Tandem – 29 tons, EV Gross – 41 tons. • Deck replacement. • Clean and paint structural steel. • Repair impact damage to girders. • Concrete substructure repairs. • Reset / replace bearings. • Joint elimination. 		Proposed Benefits: <ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency. 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1957 / 1985	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	119,270 VPD	Utilities:	\$ 75,000
Historic Resources:	No	Construction:	\$ 7,800,000
In KIPDA TIP:	Yes, ID - 3119		
Vertical Clearance:	14.918 ft (substandard)	Total Cost:	\$ 7,925,000
Project Scope of Work: Bridge rehabilitation and deck replacement. Temporary construction easements expected. Minor existing utilities near the bridge to strive for avoidance or minimization of impacts. <u>Potential utility issues</u> - Outside bridge barrier walls have electrical conduit for interstate lighting. - Underpass lighting under span #2 between girders; conduit attached to pier. - Conduit on fascia of outside bridge barrier wall, each side. <u>Potential pedestrian issues</u> - Sidewalk on each side of Phillips Lane. Bridge adjacent to Kentucky Exposition Center.			



AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING EAST

Bridge Number: 056B00210N I-65 over Manning Item No.: 5-10072.00		Overall Bridge Rating: FAIR	
Bridge Summary Sheet		Deck: 5	Super: 5
Project Description: Rehabilitation involving deck replacement of the existing I-65 bridge over Manning Road, location is 0.55 miles north of I-264.		Work Type: Minor Rehabilitation	
MP: 131.299 Spans: 3 Deck Area: 18,922 SF Structure Length: 149.80 ft			
Bridge Type: Continuous span steel girder			
Identified Needs: <ul style="list-style-type: none"> Deck replacement. Clean and paint structural steel. Reset / replace bearings. Concrete substructure repairs. Repair embankment protection. Joint elimination. 		Proposed Benefits: <ul style="list-style-type: none"> Improved condition ratings. Reduced maintenance costs. Reduced risk of major traffic disruption due to emergency repairs. Improved transportation resiliency. 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1957 / 1985	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ TBD
2021 ADT:	119,270 VPD	Utilities:	\$ TBD
Historic Resources:	No	Construction:	\$ 7,500,000
In KIPDA TIP:	Yes, ID - 3120	Total Cost: \$ 7,500,000	
Vertical Clearance:	19.67 ft		
Project Scope of Work: Bridge rehabilitation and deck replacement. Temporary construction easements expected. Minor existing utilities near the bridge to strive for avoidance or minimization of impacts. <u>Potential utility issues</u> <ul style="list-style-type: none"> - Interstate light standard on onside bridge barrier NE end. - Outside bridge barrier walls have electrical conduit for interstate lighting. - Underpass lighting under span #2 between girders; conduit attached to pier. - Conduit on north face of Pier #1 & west outside barrier and overhang 			

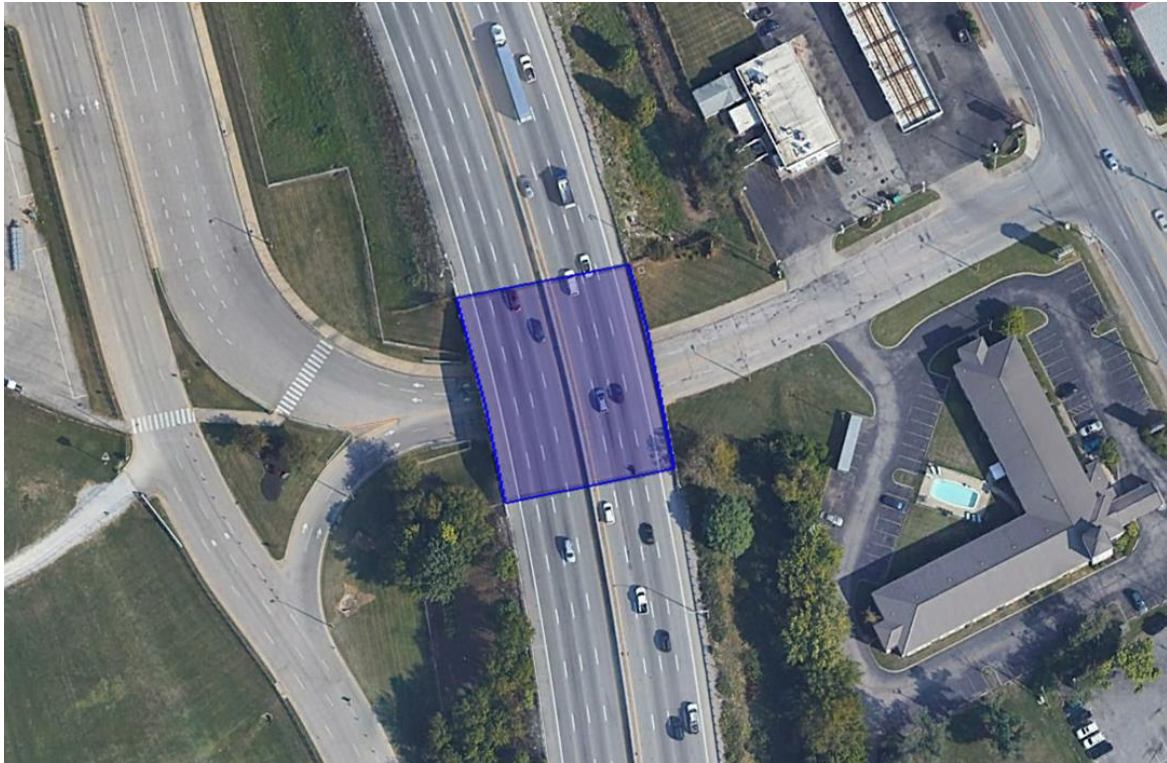


AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST

Bridge Number: 056B00211N I-65 over KFEC Gate 6 Item No.: 5-10073.00		Overall Bridge Rating: FAIR	
Bridge Summary Sheet			
Project Description:		Work Type: Minor Rehabilitation	
Rehabilitation involving a deck replacement of the existing I-65 bridge over KFEC Gate 6, location is 0.6 miles north of I-264.			
MP: 131.401 Spans: 3 Deck Area: 18,878 SF Structure Length: 150.90 ft			
Bridge Type: Simple span steel girder			
Identified Needs:		Proposed Benefits:	
<ul style="list-style-type: none"> • Deck replacement. • Clean and paint structural steel. • Reset / replace bearings. • Concrete substructure repair. • Joint elimination. 		<ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency. 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1957 / 1985	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	119,270 VPD	Utilities:	\$ 75,000
Historic Resources:	No	Construction:	\$ 7,500,000
In KIPDA TIP:	Yes, ID - 3123	Total Cost:	\$ 7,625,000
Vertical Clearance:	14.75 ft (substandard)		
Project Scope of Work:			
Bridge rehabilitation and deck replacement. Temporary construction easements expected. Minor existing utilities and TRIMARC facilities near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues</u> -Outside bridge barrier walls have electrical conduit for interstate lighting. -5" TRIMARC conduit fiber optic line at south abutment -2 TRIMARC cameras just north of north Abutment, 1 each side. -Underpass lighting and conduit in Span #2 attached to Piers #1 & #2 -Conduit on outside of east barrier Spans #1 & #2			
<u>Potential pedestrian issues</u> -Hotels and gas station on east side of bridge and Kentucky Exposition Center on west side. Sidewalk on north side of KFEC Gate 6 roadway leading into KEC.			



AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING WEST


Bridge Number: 056B00212N I-65 over Bradley Item No.: N/A		Overall Bridge Rating: POOR Deck: 5 Super: 4 Sub: 5	
Bridge Summary Sheet			
Project Description:		Work Type: Rehabilitation	
Rehabilitation involving a deck replacement of the existing I-65 bridge over Bradley Avenue, location is 0.7 miles south of US-60A (Eastern Parkway).			
MP: 132.209 Spans: 3 Deck Area: 16,139 SF Structure Length: 135.60 ft			
Bridge Type: Continuous span steel girder and continuous prestressed concrete box beams (widening)			
Identified Needs:		Proposed Benefits:	
<ul style="list-style-type: none"> • Deck replacement. • Clean and paint structural steel. • Reset / replace bearings. • Concrete substructure repairs. • Investigate potential foundation issues resulting in bearing/substructure distress. • Joint elimination. 		<ul style="list-style-type: none"> • Improved condition ratings. • Reduced maintenance costs. • Reduced risk of major traffic disruption due to emergency repairs. • Improved transportation resiliency. 	
Project Info:		Project Phase Estimates: (2023 Dollars)	
Built / Reconstructed:	1957 / 1988	Design:	\$ TBD
Functional Class:	Urban Interstate	Right-of-Way:	\$ 50,000
2021 ADT:	119,270 VPD	Utilities:	\$ 75,000
Historic Resources:	No	Construction:	\$ 6,500,000
In KIPDA TIP:	No		
Vertical Clearance:	15.167 ft (substandard)	Total Cost:	\$ 6,625,000
Project Scope of Work:			
Bridge rehabilitation and deck replacement. Temporary construction easements expected. Minor existing utilities and TRIMARC equipment near the bridge to strive for avoidance or minimization of impacts.			
<u>Potential utility issues</u> -5" TRIMARC conduit at SW end of bridge -Outside bridge barrier walls have electrical conduit for interstate lighting. -Pole with possible TRIMARC equipment attached just off SE end of bridge			



AERIAL VIEW OF EXISTING STRUCTURE HIGHLIGHTED IN BLUE



ELEVATION OF STRUCTURE LOOKING EAST

Concept E: Preston Striping		Short Term		Priority High
Project Description:		Work Type:		Striping
Re-stripe Preston Street at I-65 northbound on-ramp				
KY 61 MP 10.895 to MP 10.955		Project Length: 0.06 MI		
Identified Needs:		Proposed Benefits:		
<ul style="list-style-type: none">Poor delineation on local street leads to driver confusion		<ul style="list-style-type: none">Reduce driver confusion by visually defining movementsImproves pedestrian safety		
Project Info:		Project Phase Estimates: (2021 Dollars)		
KYTC/KIPDA ID:	N/A	Design:	\$	10,000
Functional Class:	Urban Local	Right-of-Way:	\$	0
2020 ADT:	8,000 vpd Preston 5,300 vpd ramp	Utilities:	\$	0
2045 No-Build ADT:	5,100 vpd ramp	Construction:	\$	25,000
2017-2019 Crashes:	3	Total Cost:	\$	35,000
Bike/Ped Facilities:	Sidewalks along both sides			
Project Concept:				
				

Concept O: Crittenden Loop Ramp		Short Term		Priority High
Project Description:		Work Type:	Reconstruct Ramp	
Lengthen/widen ramp from Crittenden to I-65 northbound to maximum extent possible without widening I-65 bridge over Crittenden Drive				
MP	132.3	to	MP 132.5	Project Length: 0.31 MI
Identified Needs:		Proposed Benefits:		
<ul style="list-style-type: none">Tight curve on ramp reduces available length to accelerate, compounded by short merge distance		<ul style="list-style-type: none">Improve safety and traffic flow with longer acceleration lane <p>Mid-term Spots P/Q addresses same need to meet current design standards</p>		
Project Info:		Project Phase Estimates: (2021 Dollars)		
KYTC/KIPDA ID:	CHAF IP20150178/Item 5-8102.3	Design:	\$	80,000
Functional Class:	Urban Interstate Ramp	Right-of-Way:	\$	0
2020 ADT:	2,200 vpd on ramp	Utilities:	\$	0
2045 No-Build ADT:	2,200 vpd on ramp	Construction:	\$	250,000
2017-2019 Crashes:	30 NB within 200 ft of ramp terminus	Total Cost: \$ 330,000		
Bike/Ped Facilities:	N/A			
Project Concept:				



◆ New Pavement

Concept W-a: Brook/Broadway Ramp		Long Term	Priority Low
Project Description:		Work Type:	Ramp Improvements
Rebuild I-65 off-ramp to Brook Street/Broadway to close both the alley and Jacob Street that currently intersect the ramp. Thru movements along Jacob Street and the Alley are both current eliminated.			
MP 135.060 to MP 135.200		Project Length: 0.14 MI	
Identified Needs:		Proposed Benefits:	
<ul style="list-style-type: none">Intersection 600 feet from ramp terminus, forcing vehicles to rapidly decelerate going downhill to complete left to JacobCross-ramp thru movement on Jacob discouraged with low median but not preventedPedestrian crossing concernsShort merge length approaching Broadway		<ul style="list-style-type: none">Reduces conflict points and driver confusionProvides turn lane to decelerate for lefts onto JacobRelocates pedestrian crossing	
Project Info:		Project Phase Estimates: (2021 Dollars)	
KYTC/KIPDA ID:	#264	Design:	\$ 100,000
Functional Class:	Urban Interstate Ramp	Right-of-Way:	\$ 0
2020 ADT:	9,300 vpd on ramp	Utilities:	\$ 260,000
2045 No-Build ADT:	11,700 vpd on ramp	Construction:	\$ 850,000
2017-2019 Crashes:	14 ramp crashes	Total Cost: \$ 1,210,000	
Bike/Ped Facilities:	N/A		
Project Concept:			

INCREASE CAPACITY ALONG EXIT 136A TO BROOK STREET TO BROADWAY

CLOSE ACCESS TO JACOB STREET FROM EXIT RAMP 136A

CLOSE ALLEY WAY WEST OF RAMP EXIT LANES

SOUTH BROOK ST.

EXISTING OFF RAMP WEAVING WITH BROOK STREET & JACOB CONFLICT POINT

CLOSE ACCESS TO JACOB STREET ON WEST SIDE OF EXIT 136A RAMP

E JACOB ST.

Legend: ◆ New Pavement ◆ Remove Pavement

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65

KYTC Archaeological Investigation Form

Project Description: Eighteen bridge projects along I-65 in Jefferson County. The project is evolving and details are not confirmed at this point. The current assumption is that all 18 bridges will require full replacement. All work will occur within existing ROW.

USGS Quad Name: Jeffersonville / Louisville West / Louisville East

USGS Date: 1993 / 1994 / 1994

Coordinates (Project center point) LAT: 38.2235 LONG: -85.7509 (the approximate center point)
Bridge coordinates are attached.

Project Type listed in Attachment 1 (in Section 106 Programmatic Agreement)?

☐ Yes (list project activity types) _____

☒ No (Continue)

Project Type listed in Attachment 2 (in Section 106 Handbook)?

☒ Yes (list project activity types) # 11 bridge rehab or replacement _____

Are all new or existing ROW areas previously disturbed?

☒ Yes (Describe disturbance or basis for conclusion. Attach photos or maps):

There will be no ground disturbing work. All areas previously disturbed by urban development, bridge construction, utility installation.

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65

No Historic Properties Affected

As Determined By:

Susan Neumeyer

October 26, 2023

KYTC Representative

Date

Stephanie Dooley

November 1, 2023

SHPO Representative

Date

(Concurrence is assumed if no response is received within 30 days)

Attachments

☐ Project Plans (show date on plans)

☐ Photos

☐ Mapping

☐ Other: _____

☒ Copy EPM

☒ Copy DEC

☐ Copy DEA Archaeologist

☐ Copy SHPO

If the project plans change then additional archaeological survey may be required. If human remains are discovered or a previously unidentified archaeological site is encountered, work must cease and the KYTC Division of Environmental Analysis be notified immediately.

The OSA GIS was consulted on October 25, 2023. Bridges number 1-12, and bridges 14 and 15 in the list below are in areas with no surveys and no sites documented in GIS. Number 13 is close to 15Jf716. Number 16 is adjacent to 15Jf717. Number 17 is within the boundaries of 15Jf717 and 15Jf718 and Number 18 is located over 15J718.

Sites 15Jf716, 15Jf717, and 15Jf718 consist of historic neighborhoods. OSA defined each site as encompassing entire blocks. The portions of sites 15Jf716, 15Jf717, and 15Jf718 that were impacted by the Louisville-Southern Indiana Ohio River Bridges project were subjected to Phase III mitigation in 2012-2013. Those portions were determined clear for archaeology due to exhaustion of their research potential through the Phase III process. No additional work is warranted for the bridge replacement projects in these areas.

In March 2005, the interstate highway exemption from Section 106 Review was adopted into law. Information about this exemption can be found at: <https://www.achp.gov/digital-library-section-106-landing/exemption-regarding-historic-preservation-review-process>.

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65

On January 6, 2021, Craig Potts concurred with a Conditional No Adverse Effect determination for archaeology and cultural historic resources for Item No. 5-22070 (previously 5-20061). This project involved replacing three I-65 bridges located at: Hill Street, Brooks Street, and Jacob/Broadway Streets. The project also involved paving I-65 between MP 131.24 and 135.672. These same bridges are in the current project list.

All construction related activities are currently anticipated to be fully confined to the I-65 corridor.

No further work is warranted for these projects.

<u>Item No.</u>	<u>Bridge Number</u>	<u>Location</u>
1. 5-10071	056B00209N	I-65 over Phillips Lane
2. 5-10072	056B00210N	I-65 over Manning Road
3. 5-10073	056B00211N	I-65 over KFEC Gate 6 Drive
4. 5-10106	056B00212N	I-65 over Bradley St. (Fairgrounds Access)
5. 5-10070	056B00205N	I-65 over NS Railroad
6. 5-10065	056B00180N	I-65 over Eastern Parkway
7. 5-10105	056B00181N	I-65 over University Blvd (Warnock St)
8. 5-10104	056B00182N	I-65 over Brandeis Ave.
9. 5-10064	056B00179N	I-65 over Hill St/CSX/Burnett St
10. 5-10103	056B00186N	I-65 over Oak St.
11. 5-10067	056B00184N	I-65 over St. Catherine St.
12. 5-10066	056B00183N	I-65 over Eastern KY/S. Brook Sts.
13. 5-10100	056B00194N	I-65 S-bound ramp bridge over M. Ali
14. 5-10068	056B00191N	I-65 over Jacob/Broadway/Gray Sts.
15. 5-10069	056B00192N	I-65 over Chestnut St.
16. 5-10099	056B00193N	I-65 over Brooks and Muhammad Ali
17. 5-10101	056B00195R	I-65 over Floyd St.
18. 5-10102	056B00197R	I-65 over E. Liberty St.

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65

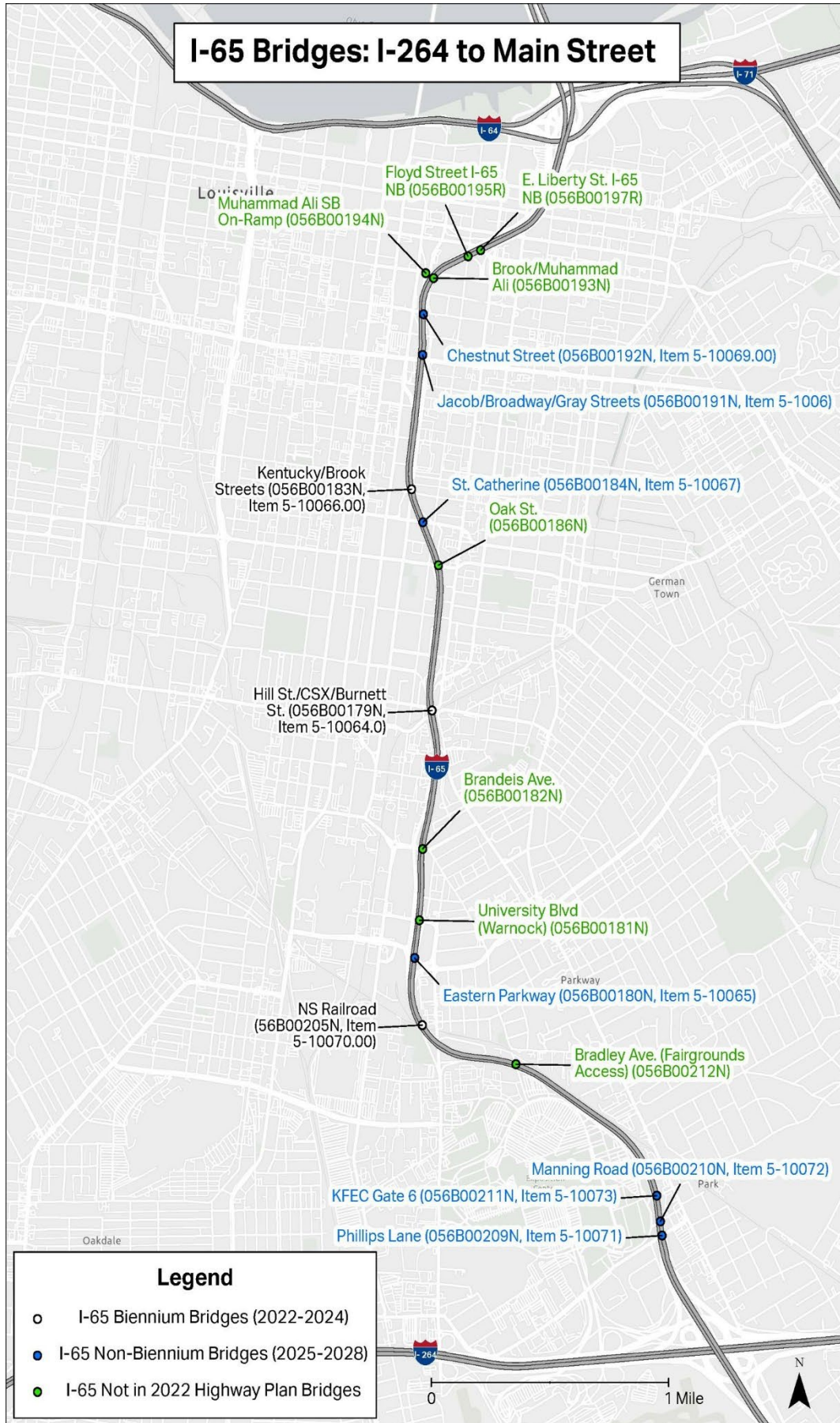
Latitude and Longitude Coordinates for the Bridge Projects

<u>2022 Highway Plan</u>	<u>Bridge No.</u>	<u>Description</u>		
Item 5-10071	<u>056B00209N</u>	I-65 over Phillips Lane	38°11'48.2"N 85°44'01.3"W	38.197547, -85.733753
Item 5-10072	<u>056B00210N</u>	I-65 over Manning Road	38°11'51.1"N 85°44'01.8"W	38.197505, -85.733817
Item 5-10073	<u>056B00211N</u>	I-65 over KFEC Gate 6 Drive	38°11'56.3"N 85°44'02.7"W	38.198967, -85.734084
Item 5-10106	<u>056B00212N</u>	I-65 Over Bradley Ave. (Fairgrounds Access)	38°12'22.6"N 85°44'41.9"W	38.206258, -85.744954
Item 5-10070	<u>056B00205N</u>	I-65 over NS Railroad	38°12'30.3"N 85°45'08.0"W	38.208416, -85.752216
Item 5-10065	<u>056B00180N</u>	I-65 over Eastern Parkway (US60A)	38°12'44.3"N 85°45'10.3"W	38.212293, -85.752843
Item 5-10105	<u>056B00181N</u>	I-65 Over University Blvd. (Warnock St.)	38°12'51.7"N 85°45'09.0"W	38.214324, -85.752484
Item 5-10104	<u>056B00182N</u>	I-65 Over Brandeis Ave.	38°13'06.0"N 85°45'08.0"W	38.218312, -85.752215
Item 5-10064	<u>056B00179N</u>	I-65 over Hill St./CSX/Burnett St.	38°13'33.2"N 85°45'05.4"W	38.225868, -85.751482
Item 5-10103	<u>056B00186N</u>	I-65 Over E. Oak St.	38°14'03.0"N 85°45'03.6"W	38.234158, -85.750995
Item 5-10067	<u>056B00184N</u>	I-65 over St. Catherine Street	38°14'11.8"N 85°45'08.1"W	38.236577, -85.752224
Item 5-10066	<u>056B00183N</u>	I-65 over E. Kentucky/S. Brook Streets	38°14'17.7"N 85°45'11.3"W	38.238263, -85.753049
Item 5-10100	<u>056B00194N</u>	I-65 SB Ramp Bridge Over Muhammad Ali	38°15'01.8"N 85°45'06.9"W	38.250561, -85.751915
Item 5-10068	<u>056B00191N</u>	I-65 over Jacob/Broadway/Gray Streets	38°14'41.7"N 85°45'08.6"W	38.244912, -85.752390
Item 5-10069	<u>056B00192N</u>	I-65 over Chestnut Street	38°14'51.9"N 85°45'07.7"W	38.247750, -85.752183
Item 5-10099	<u>056B00193N</u>	I-65 Over Brook & Muhammad Ali	38°15'01.2"N 85°45'04.7"W	38.250342, -85.751359
Item 5-10101	<u>056B00195R</u>	I-65 Over Floyd Street	38°15'05.1"N 85°44'55.2"W	38.251442, -85.748710
Item 5-10102	<u>056B00197R</u>	I-65 Over E. Liberty Street	38°15'06.4"N 85°44'51.4"W	38.251822, -85.747747

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65



Modified April 2013

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65

Photos of bridges to be replaced



5-10071 I-65 over Phillips Lane, looking west



KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65

5-10072, I-65 over Manning Road, looking east



5-10073, I-65 over KFEC Gate 6 Drive, looking west



KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65

5-10106, I-65 over Bradley Ave. (Fairground Access), looking east



5-10070, I-65 over NS Railroad, looking west



5-10065, I-65 over Eastern Parkway, looking west

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65



5-10105, I-65 over University Blvd. (Warnock St), looking west



5-10104, I-65 over Brandeis Ave., west profile

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65



5-10064, I-65 over Hill St/CSX/Burnett St., west profile



5-10103, I-65 over Oak St., west profile

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65



5-10067, I-65 over St. Catherine St., east profile



5-10066, I-65 over E. Kentucky St./S. Brook St., piers

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65



5-10100, S-bound ramp bridge over Muhammad Ali, west profile



5-10068, I-65 over Jacob /Broadway/Gray Streets, west profile

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65



5-10069, I-65 over Chestnut St., east profile



5-10099, I-65 over Brook St and Muhammad Ali, west profile

KYTC Item No: Multiple (See list)

County: Jefferson

Route: I-65



5-10101, I-65 over Floyd St., east profile



5-10102, I-65 over E. Liberty St., east profile

KYTC Item No: N/A

County: Jefferson

Route: Multiple

KYTC Archaeological Investigation Form

Project Description: Safety improvement projects along I-65 between MP 131.24 and 136.338 in Louisville. All work will occur within existing I-65 corridor.

USGS Quad Name: Louisville East, Louisville West

USGS Date: 1994, 1994

Coordinates (Project center point) See below

Project Type listed in Attachment 1 (in Section 106 Programmatic Agreement)?

☐ Yes (list project activity types) _____

☒ No (Continue)

Project Type listed in Attachment 2 (in Section 106 Handbook)?

☒ Yes (list project activity types) #10 highway safety _____

Are all new or existing ROW areas previously disturbed?

☒ Yes (Describe disturbance or basis for conclusion. Attach photos or maps):

All work will occur within existing I-65 right-of-way, disturbed by road and bridge construction, utilities

KYTC Item No: N/A

County: Jefferson

Route: Multiple

No Historic Properties Affected

As Determined By:

Susan Neumeyer

October 26, 2023

KYTC Representative

Date

Stephanie Dooley

November 1, 2023

SHPO Representative

Date

(Concurrence is assumed if no response is received within 30 days)

Attachments

☐ Project Plans (show date on plans)

☐ Photos

☐ Mapping

☐ Other: _____

☒ Copy EPM

☒ Copy DEC

☐ Copy DEA Archaeologist

☐ Copy SHPO

If the project plans change then additional archaeological survey may be required. If human remains are discovered or a previously unidentified archaeological site is encountered, work must cease and the KYTC Division of Environmental Analysis be notified immediately.

5-22070: This project will consist of pavement rehabilitation between I-65 MP 131.24 and MP 136.338 in Louisville. Most of this pavement project (MP 131.24 to MP 135.672) was reviewed and cleared with a Conditional No Adverse Effect determination for both archaeology and cultural history in a SHPO letter dated January 6, 2021, in accordance with the interstate exemption of 2005. No additional work is warranted for this pavement project.

Additional safety projects include:

- *Restriping an intersection (no ground disturbance will occur) at Preston Street and NB I-65; LAT: 38.2234 LONG: -85.7503*
- *Widen the NB merge lane to I-65 from Crittenden Drive: this project will take some of the existing shoulder to widen the lane. LAT: 38.2076 LONG: -85.7490*
- *Concept W-a: this is minor widening of the existing ramp to Broadway by paving the shoulder and closing Jacob Street alley (which dead-ends at the ramp). LAT: 38.2437 LONG: -85.7523*

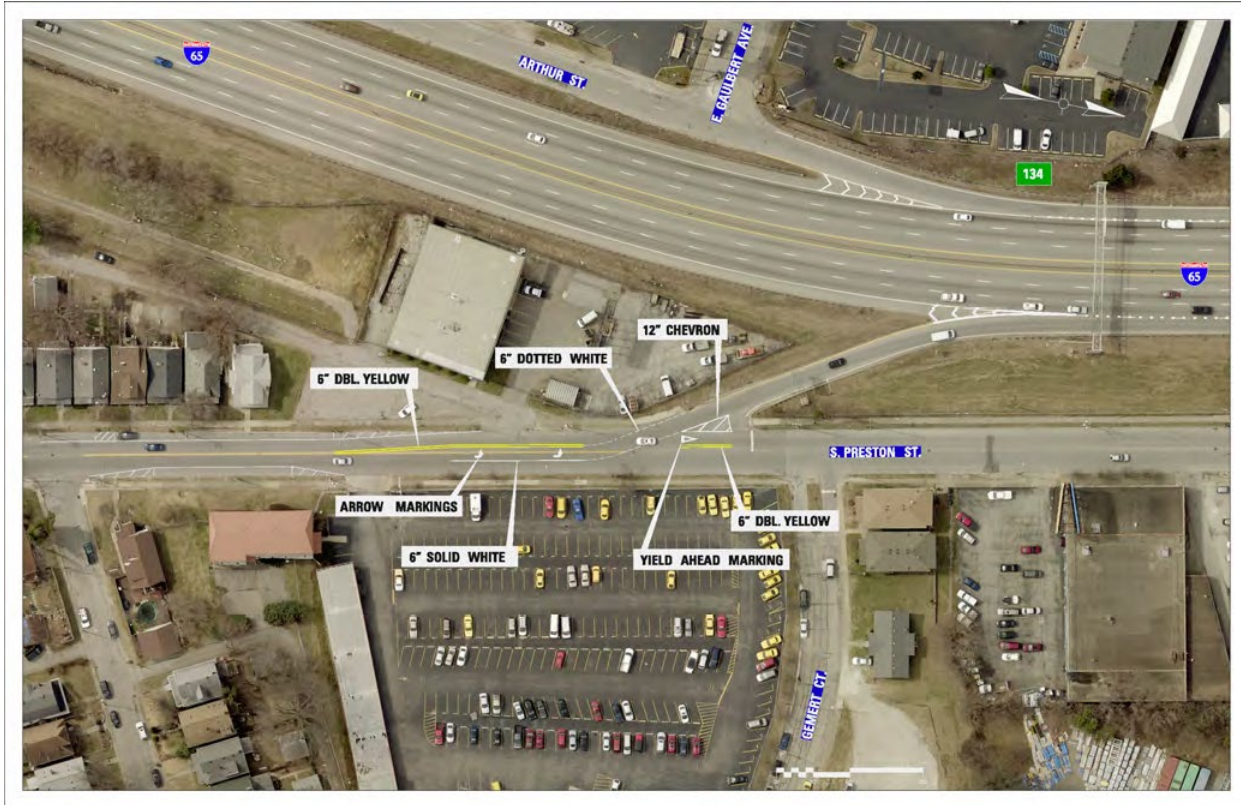
KYTC Item No: N/A

County: Jefferson

Route: Multiple

The OSA GIS was consulted on October 26, 2023. No sites or surveys are within or adjacent to these projects.

No additional work is warranted for these safety projects.



Restriping the intersection at Preston Street and NB I-65 on-ramp

KYTC Item No: N/A

County: Jefferson

Route: Multiple



Shoulder widening at I-65 and Crittenden Drive; north is to the right



Concept W-a at Broadway and Jacob Streets

Bridge Rehabilitations

KYTC Item No: Various

County: Jefferson

Route: I-65

KYTC Historic Architectural Investigation Form

Project Description: This project consists of **Bridge Rehabilitations**. This work will include deck overlay, painting, and minor substructure repair. Temporary construction easements (within the APE) are possible. These bridges are part of the Interstate Highway System and are exempt from Section 106 assessment, however the APE for each bridge was investigated and resources within were assessed.

Project Type listed in Attachment 1 (in Section 106 Programmatic Agreement)?

☐ Yes

☒ No (Continue)

Project Type listed in Attachment 2 (in Section 106 Handbook)?

☒ Yes (List project activity types) #11-Bridge rehabilitation, #21-Deck overlay/replacement

☐ No (This project is not considered a small scale project under the Section 106 Programmatic Agreement. This checklist cannot be used. Process with full baseline or joint memorandum)

☐ No (However, SHPO has agreed that this project may be documented using the Historic Architectural Investigation Form)

Project Area of Potential Effect is defined as:

☐ Within 150 feet of project centerline (Small Scale Project - within existing corridor)

☐ Within view shed of project (Discuss): _____

☒ Other (Discuss): APE is 200' from the centerline of the bridge

Are there Historical Resources within the project APE (per KHC database)?

☐ Yes

☒ No

☐ N/A (Explain):

KYTC Item No: Various

County: Jefferson

Route: I-65

Are there Historical Resources (50 years old or older) identified within the project APE based on field investigations?

☒ Yes

☐ No

Date of Field Investigation: 5/10/2023

Investigator Name(s): Jonna Wallace Mabelitini

Discuss Basis for finding

(Historic Mapping, PVA, Building Permit, Date of Construction, Deed/Title, etc.):

KHC database, site visit, mapping and site photos

NRHP listed or potentially eligible sites/districts (> 50 years old) are:

☒ Present within the APE (Continue)

☐ No Properties Eligible within APE

Sections below to be completed by KYTC Architectural Historian

Discuss eligibility determinations (criteria, integrity):

There are two resources eligible/listed on the NRHP.

JFCD-159 Louisville Medical College (Chestnut Street)

Jewish Hospital Doctors Office Building (Liberty Street) Designed by Jasper Ward. Excellent example of Modern Architecture. Eligible under B and C for the NRHP

Determination of Effect (when eligible sites have been identified):

☐ No Historic Properties Affected

☒ No Adverse Effect (May result in Section 4(f) *De minimis* finding – Document appropriately)

☐ Adverse Effect

KYTC Item No: Various

County: Jefferson

Route: I-65

Discuss No Effect/No Adverse Effect Determination:

This bridge rehabilitation project will have a No Adverse Effect to historic properties. There is one site at the Liberty Street Bridge and one site at the Chestnut Street Bridge. While they are within proximity of the project area (but not within the APE), they will not be adversely affected. There is potential for temporary easements, and the resource near the Liberty Street Bridge (Jewish Hospital Doctors Office Building) will have a Do Not Disturb note clearly noted on the construction plans. JFCD 159 is well outside the APE and will not be affected.

- ☐ No Historic Properties Affected
☒ No Adverse Effect to Historic Properties

As Determined By:

Jenna W. Mabeltini

5/19/2023

KYTC Historian

Date

L. Ralphe Jennings

6/7/2023

SHPO Representative

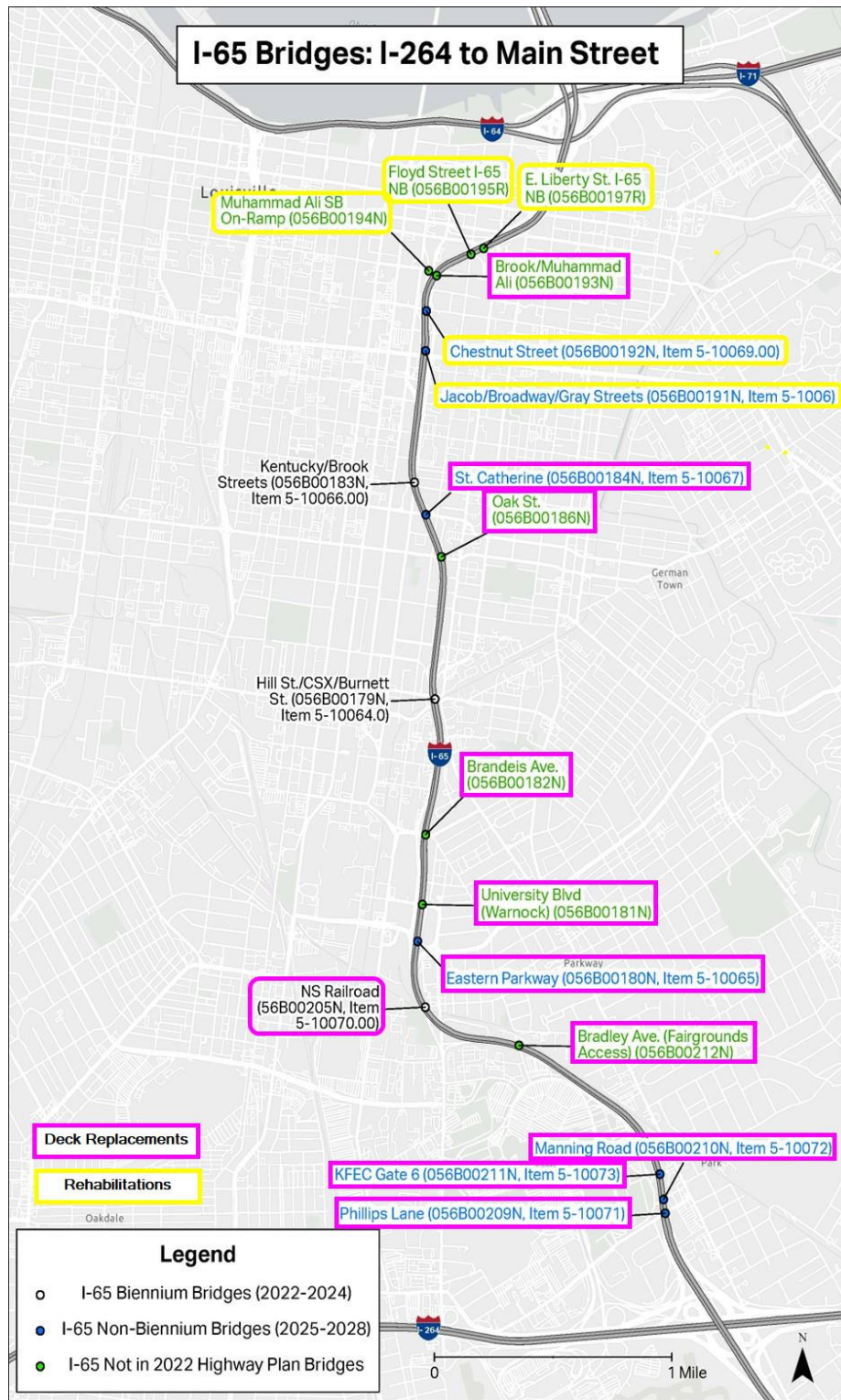
Date

Attachments:

- ☒ Map Showing APE and Identified Historic Resources
☒ Individual Site Maps
☒ Photographs
☐ Project Plans
☐ KHC Site Survey Forms
☐ Other (Describe): _____
☐ Copy EPM
☐ Copy DEC
☐ Copy DEA Project File
☐ Copy FHWA (w/De minimis Memo if appropriate)
☒ Copy SHPO

I-65 Bridges: Support Documentation

Louisville, Jefferson County, KY



Bridge Rehabilitation

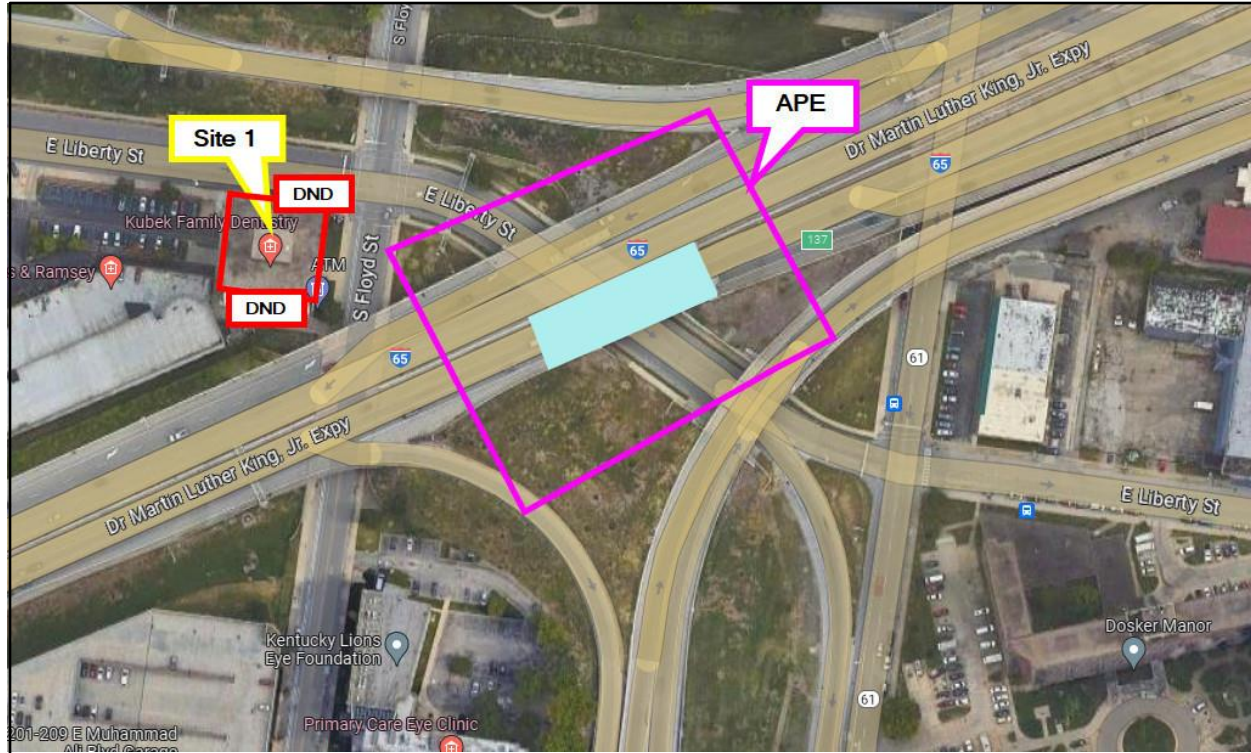
I-65 over E. Liberty St.

Bridge Number: 056B00197R

Item Number: N/A

Sites: 1-eligible (Not within the APE)

Effect: No Adverse Effect



APE

200' from centerline of the interstate



Do Not Disturb (DND) - Site 1

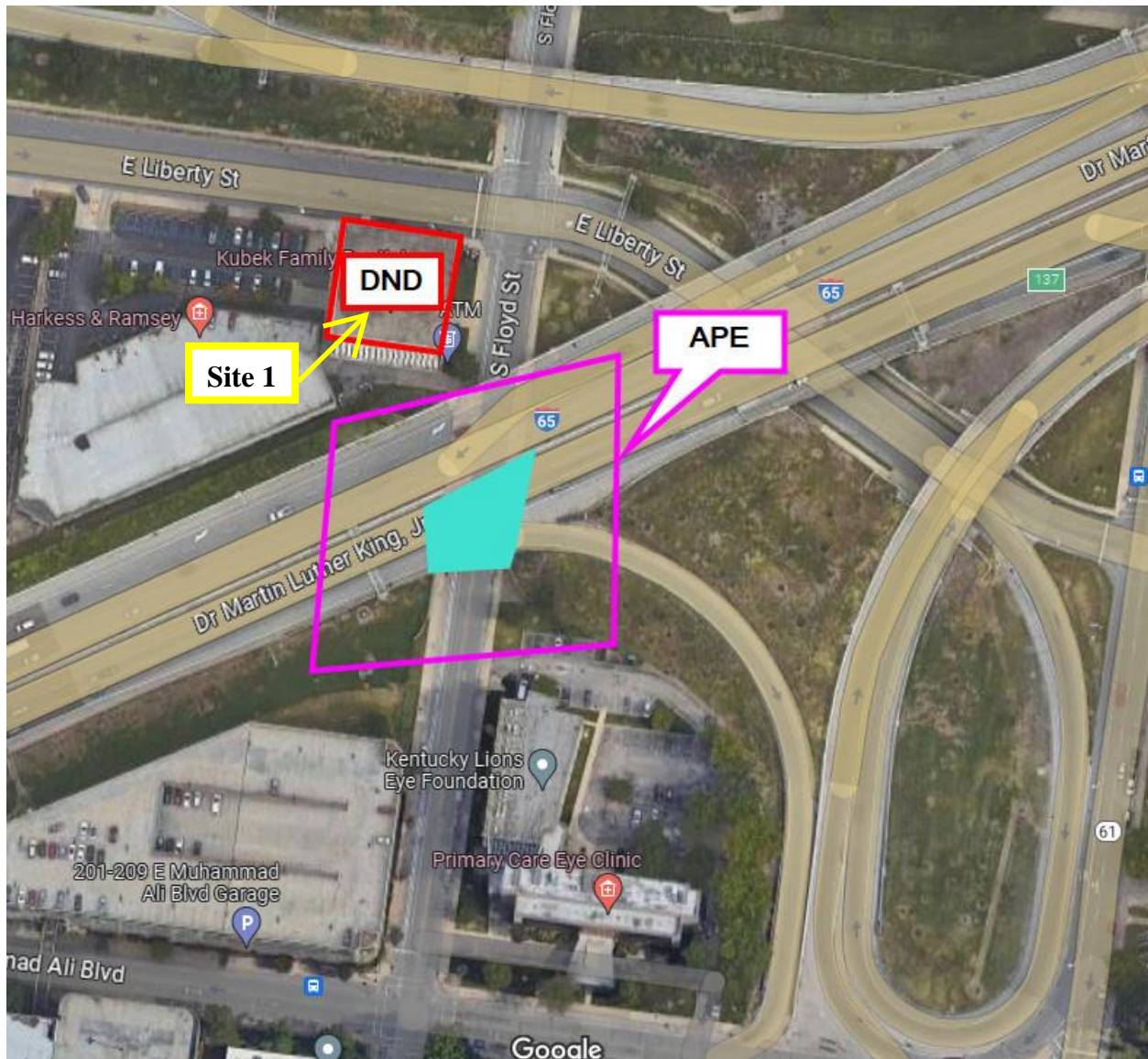
I-65 over Floyd St.

Bridge Number: 056B00195R

Item Number: N/A

Sites: No

Effect: No Effect



APE

200' from centerline of the interstate

I-65 over Muhammad Ali
Bridge Number: 056B00194N
Item Number: N/A
Sites: No
Effect: No Effect



APE

200' from centerline of the interstate

I-65 over Chestnut

Bridge Number: 056B00192N

Item Number: 5-10069

Sites: JFCD-159 (**Not** within APE)

Effect: No Effect



APE

200' from centerline of the interstate



JFCD 159
Not within the APE

Bridge Deck Replacements

KYTC Item No: Various

County: Jefferson

Route: I-65

KYTC Historic Architectural Investigation Form

Project Description: This portion of the I-65 bridges project consists of **bridge deck replacements**. This work includes deck replacement, painting, and minor fatigue detail retrofits of eleven bridges along I-65. Temporary construction easements (within the APE) are possible. These bridges are part of the Interstate Highway System and are exempt from Section 106 assessment, but historic resources within or adjacent to the APE were assessed.

Project Type listed in Attachment 1 (in Section 106 Programmatic Agreement)?

☐ Yes

☒ No (Continue)

Project Type listed in Attachment 2 (in Section 106 Handbook)?

☒ Yes (List project activity types) #21-Bridge deck overlays, deck replacements, painting etc.

☐ No (This project is not considered a small scale project under the Section 106 Programmatic Agreement. This checklist cannot be used. Process with full baseline or joint memorandum)

☐ No (However, SHPO has agreed that this project may be documented using the Historic Architectural Investigation Form)

Project Area of Potential Effect is defined as:

☒ Within 150 feet of project centerline (Small Scale Project - within existing corridor)

☐ Within view shed of project (Discuss): _____

☐ Other (Discuss): _____

Are there Historical Resources within the project APE (per KHC database)?

☒ Yes

☐ No

☐ N/A (Explain):

KYTC Item No: Various

County: Jefferson

Route: I-65

Are there Historical Resources (50 years old or older) identified within the project APE based on field investigations?

☒ Yes

☐ No

Date of Field Investigation: 5/10/2023

Investigator Name(s): Jonna Wallace Mabelitini

Discuss Basis for finding

(Historic Mapping, PVA, Building Permit, Date of Construction, Deed/Title, etc.):

Historic Mapping, KHC database, Google Maps, Site photos

NRHP listed or potentially eligible sites/districts (> 50 years old) are:

☒ Present within the APE (Continue)

☐ No Properties Eligible within APE

Sections below to be completed by KYTC Architectural Historian

Discuss eligibility determinations (criteria, integrity):

There are two contributing sites to the Old Louisville Residential Historic District, JFCO-1759 and JFCO-1760. They should remain contributing to the district, but are not individually eligible.

There is one listed resource, Eastern Parkway, JFCU 270. This parkway is part of the Olmstead Park System. It is listed on the NRHP and should remain.

Determination of Effect (when eligible sites have been identified):

☐ No Historic Properties Affected

☒ No Adverse Effect (May result in Section 4(f) *De minimis* finding – Document appropriately)

☐ Adverse Effect

KYTC Item No: Various

County: Jefferson

Route: I-65

Discuss No Effect/No Adverse Effect Determination:

For the eligible resources, there will be a No Adverse Effect (for the APE of two bridges). All work should take place on the bridge itself and may only need small temporary easements. There is a Do Not Disturb note on the contributing resources to the Old Louisville Residential Historic District (JFCU-1759 and JFCU-1760).

- ☐ No Historic Properties Affected
- ☒ No Adverse Effect to Historic Properties

As Determined By:

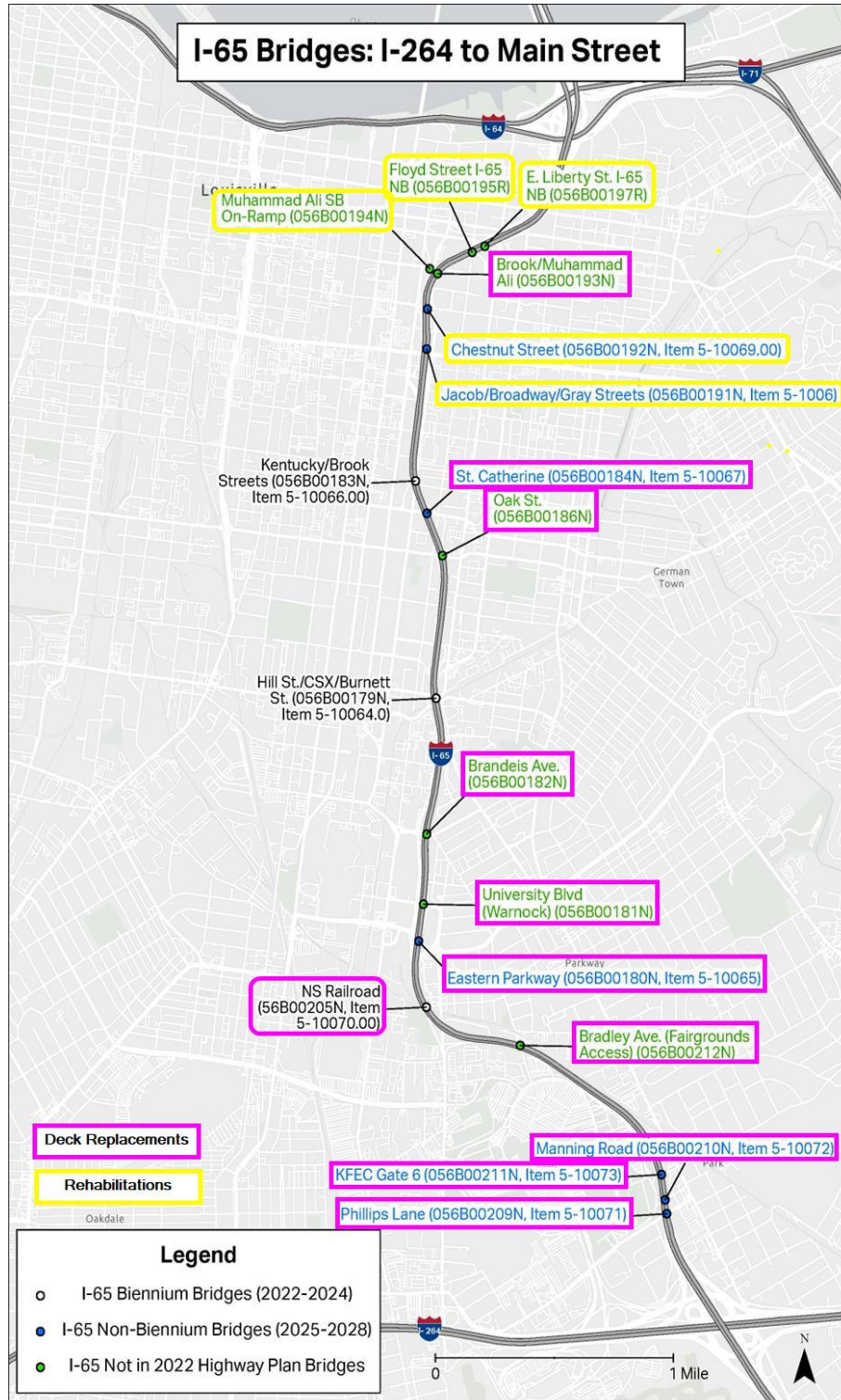
Jonna W. Mabelitini	6/5/2023	<i>L. Ralhee Jennings</i>	6/7/2023
KYTC Historian	Date	SHPO Representative	Date

Attachments:

- ☒ Map Showing APE and Identified Historic Resources
- ☒ Individual Site Maps
- ☒ Photographs
- ☐ Project Plans
- ☐ KHC Site Survey Forms
- ☐ Other (Describe): _____
- ☐ Copy EPM
- ☐ Copy DEC
- ☐ Copy DEA Project File
- ☐ Copy FHWA (w/De minimis Memo if appropriate)
- ☒ Copy SHPO

I-65 Bridges: Support Documentation

Louisville, Jefferson County, KY



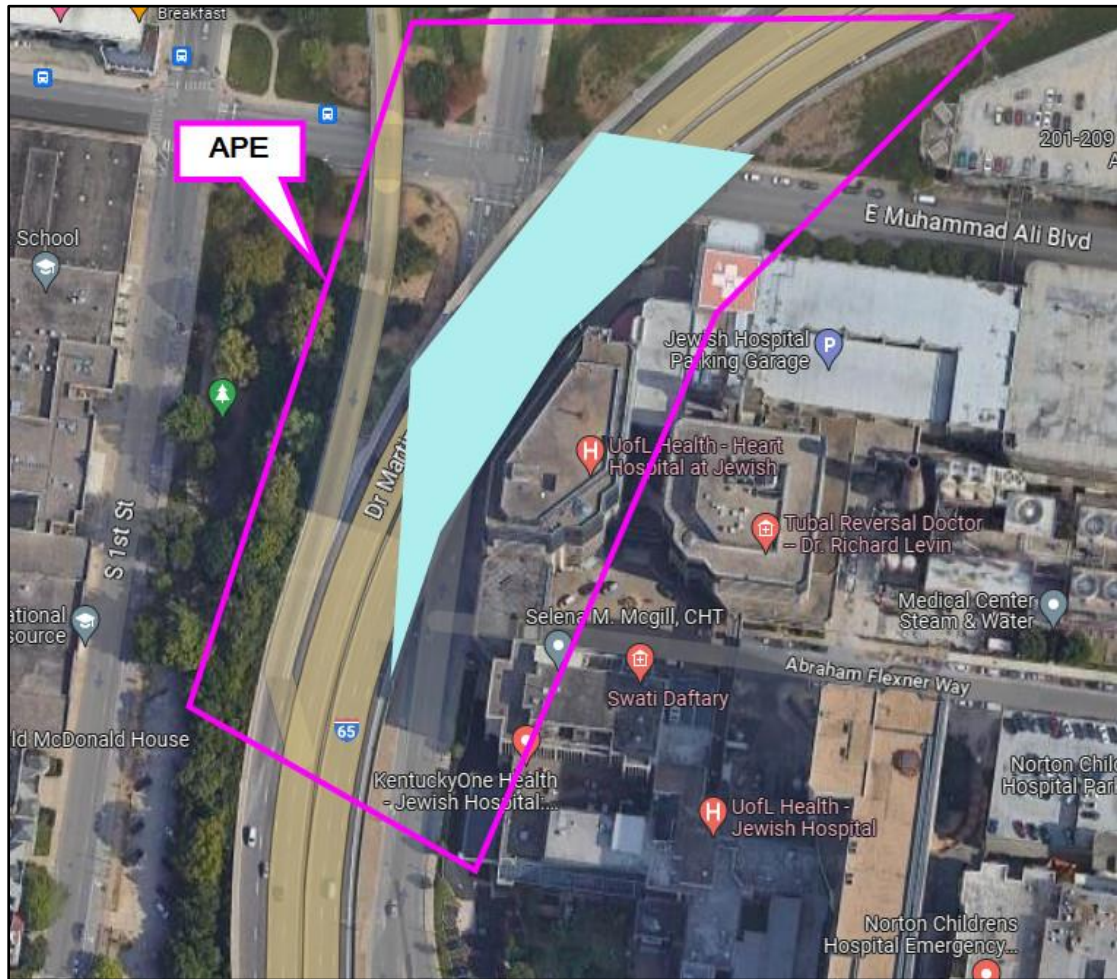
Bridge Deck Replacements

I-65 over Brook/Muhammad Ali

Bridge Number: 056B00193N

Item Number: N/A

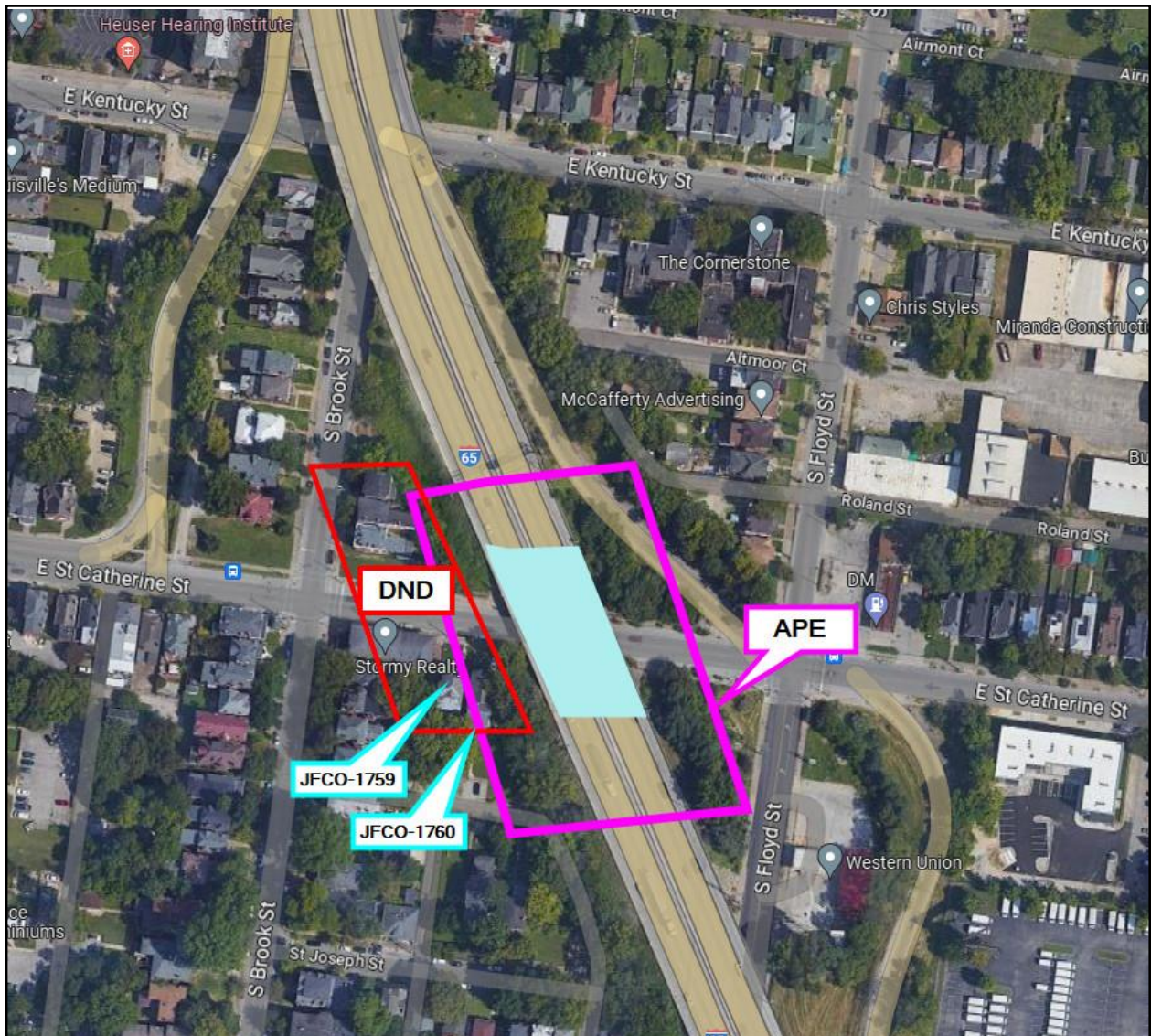
Effect: No Historic Properties Affected



APE

200' from centerline of interstate bridge

I-65 over St. Catherine Street
Bridge Number: 056B00184N
Item Number: 5-10067
Effect: No Adverse Effect

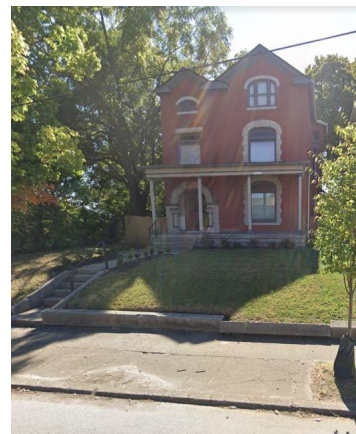


APE

150' from centerline of the bridge



JFCO 1759



JFCO 1760

I-65 over Oak Street

Bridge Number: 056B00186N

Item Number: N/A

Effect: No Historic Properties Affected



APE

150' from centerline of the bridge

I-65 over Brandeis Street

Bridge Number: 056B00182N

Item Number: N/A

Effect: No Historic Properties Affected



APE

150' from centerline of the bridge

I-65 over University Blvd.

Bridge Number: 056B00181N

Item Number: N/A

Effect: No Historic Properties Affected



APE

150' from centerline of the bridge

I-65 over US-60A (Eastern Parkway)

Bridge Number: 056B00180NItem

Number: 5-10065

Effect: No Adverse Effect



APE

150' from centerline of the bridge

Eastern Parkway is on the National Register of Historic Places (NRHP) as part of the Olmstead Park System. It retains its material integrity and should remain on the NRHP.

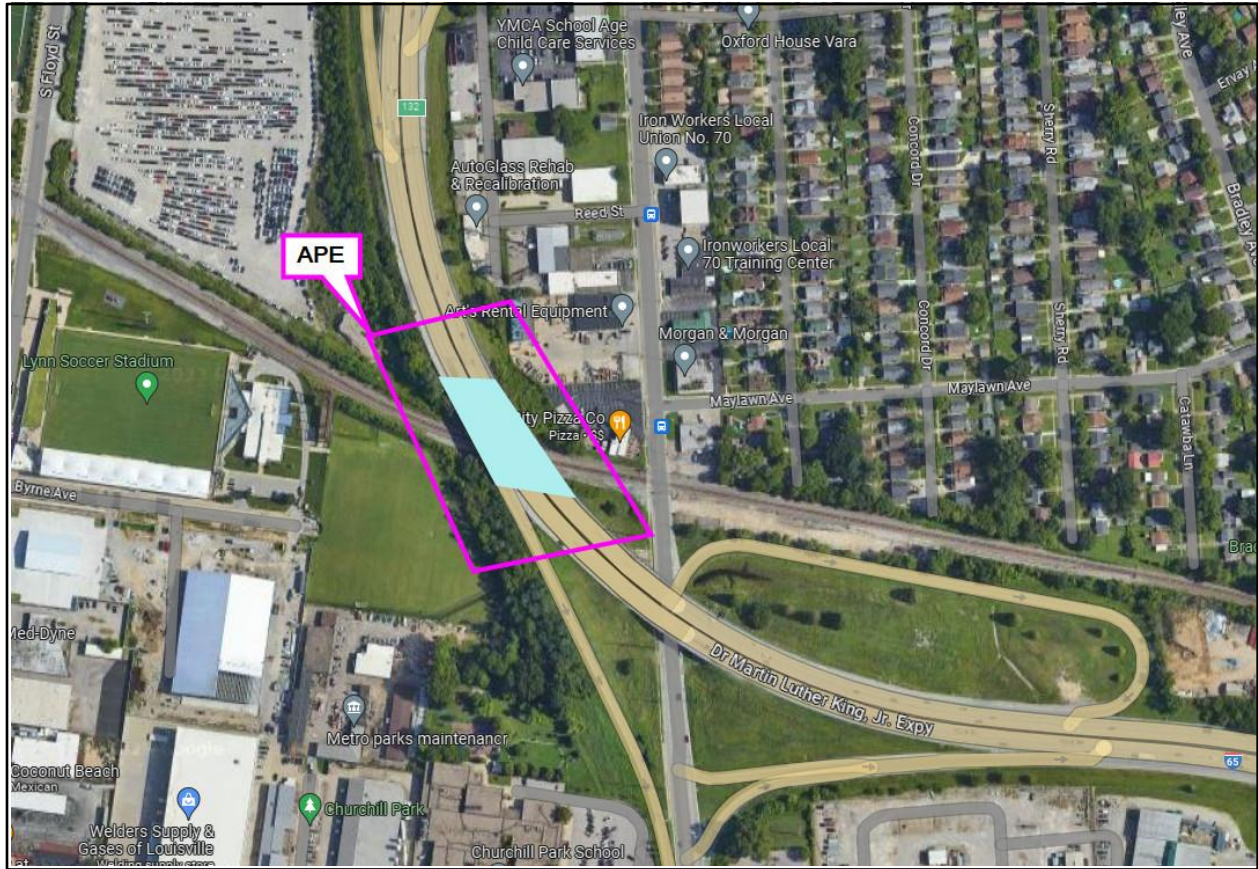
JFCU 270

I-65 over Norfolk Southern Railroad

Bridge Number: 056B00205N

Item Number: 5-10070

Effect: No Historic Properties Affected



APE

150' from centerline of the bridge

I-65 over Bradley Street

Bridge Number: 056B00212N

Item Number: N/A

Effect: No Historic Properties Affected



APE

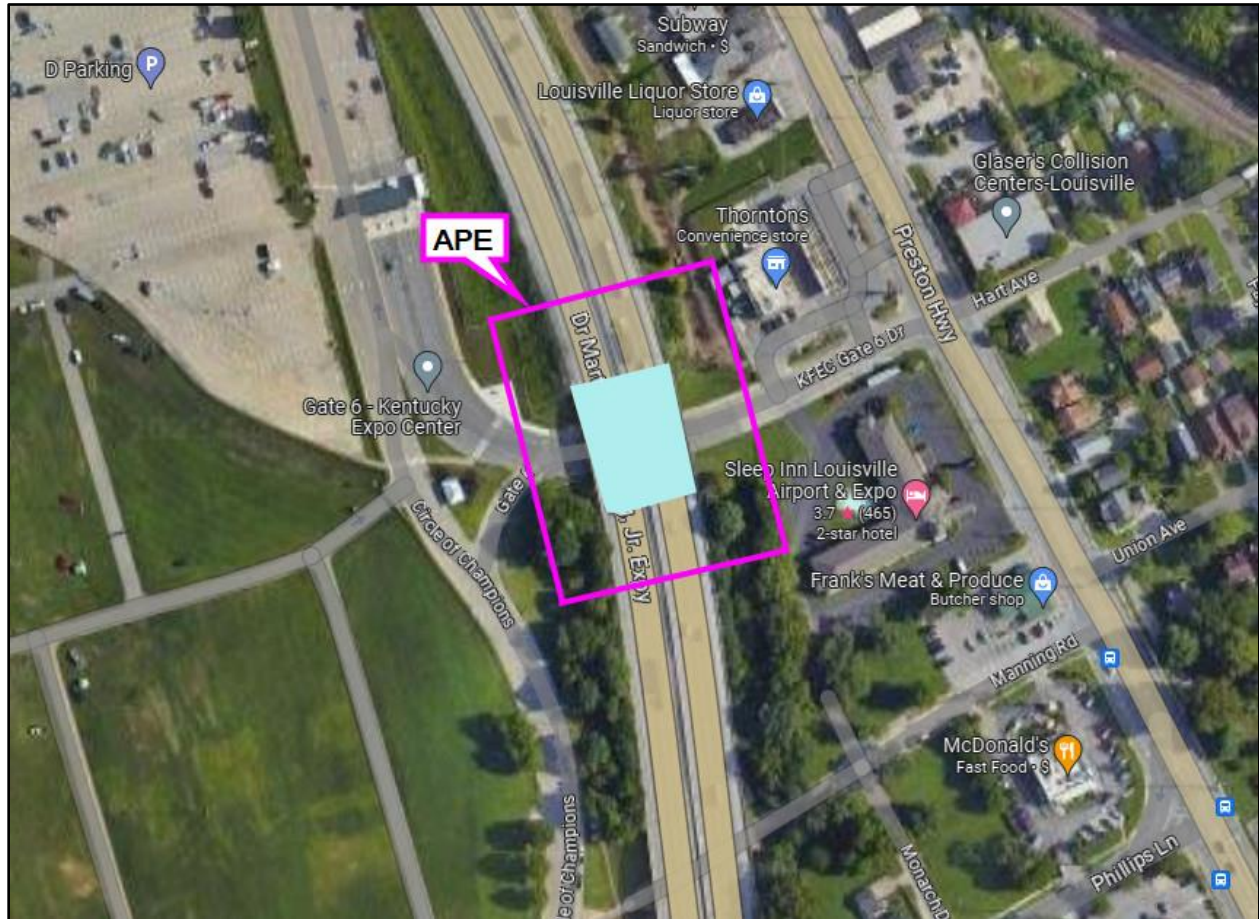
150' from centerline of the bridge

I-65 over KFEC Gate 6

Bridge Number: 056B00211N

Item Number: 5-10073.00

Effect: No Historic Properties Affected



APE

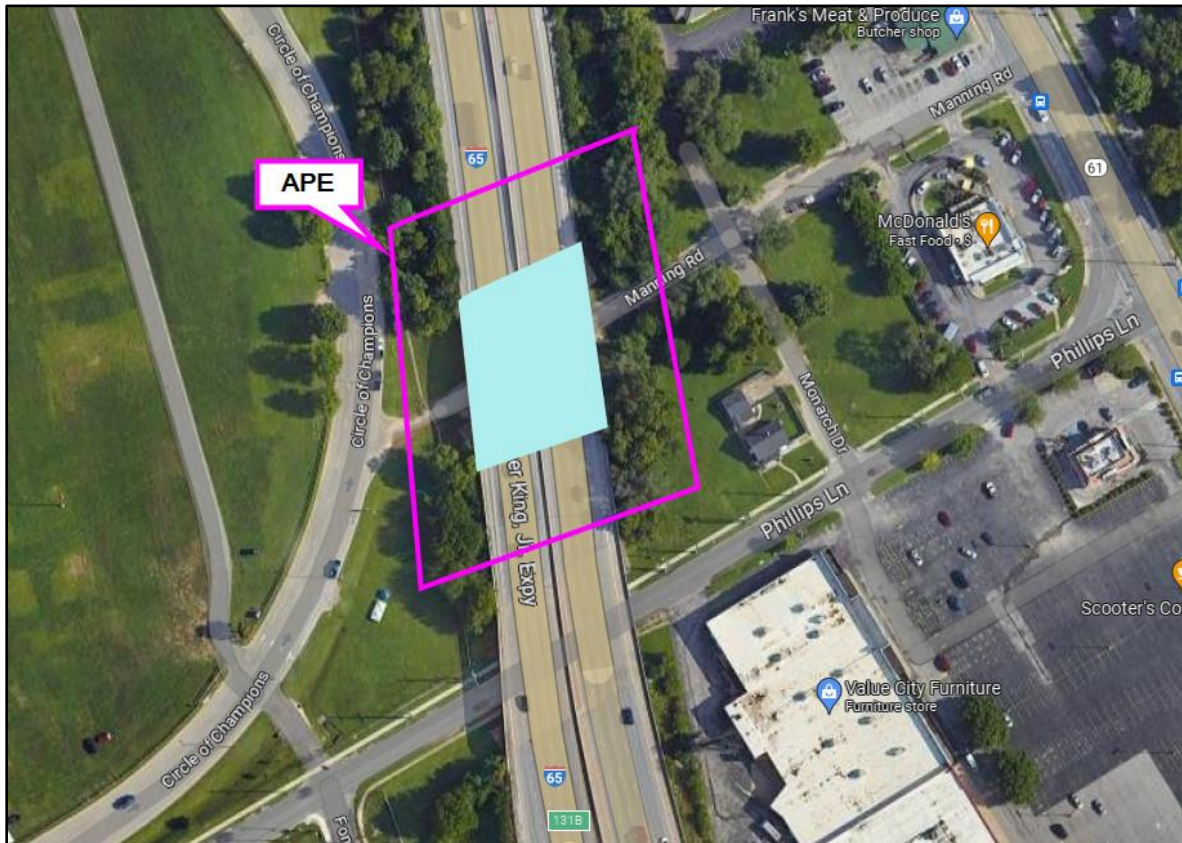
150' from centerline of the bridge

I-65 over Manning Rd.

Bridge Number: 056B00210N

Item Number: 5-10072.00

Effect: No Historic Properties Affected



APE

150' from centerline of the bridge

I-65 over Phillips Lane

Bridge Number: 056B00209N

Item Number: 5-10071.00

Effect: No Historic Properties Affected



APE

150' from centerline of the bridge

Bridge Replacement and Pavement Overlay



ANDY BESHEAR
GOVERNOR

TOURISM, ARTS AND HERITAGE CABINET
KENTUCKY HERITAGE COUNCIL
THE STATE HISTORIC PRESERVATION OFFICE

MICHAEL E. BERRY
SECRETARY

JACQUELINE COLEMAN
LT. GOVERNOR

410 HIGH STREET
FRANKFORT, KENTUCKY 40601
(502) 564-7005
www.heritage.ky.gov

CRAIG A. POTTS
EXECUTIVE DIRECTOR &
STATE HISTORIC PRESERVATION OFFICER

January 6, 2021

Daniel R. Peake, Director
Division of Environmental Analysis
Kentucky Transportation Cabinet
200 Mero Street
Frankfort, KY 40622

Re: Cultural Historic and Archaeological Conditional Approval Request for I-65 Pavement Overlay and Three Bridge Replacements in Louisville, Jefferson County, Kentucky (KYTC Item No. 5-569) CORRECTION KYTC Item No. 05-20061

Dear Mr. Peake:

2022 6YP Item No. 5-22070 -- MP 131.240--131.338

In accordance with Section 106 of the National Historic Preservation Act of 1966, as amended, the Kentucky Heritage Council, State Historic Preservation Office has received information concerning proposed pavement and bridge projects within the I-65 corridor between MP 131.24 and MP 135.672. More specifically, project activities will entail a thin pavement overlay to address deteriorating surface conditions and to replace three concrete interstate bridges spanning Hill Street (MP 133.873), Brook Street (MP 134.753) and Jacob Street and Broadway (MP 135.273). It is our understanding that all construction related activities will be confined to the right of way within the interstate corridor.

Considering that the interstate highway exemption from Section 106 Review was adopted in March of 2005, and considering that all construction related activities are currently anticipated to be fully confined to the I-65 corridor, we agree that the potential for adverse effects outside of the interstate right of way is very low. We are therefore providing a **Conditional No Adverse Effect** finding for the proposed undertaking as it relates to cultural historic and archaeological resources. Please note that consultation will need to be reopened if changes occur and if it is determined that project related activities could in fact directly or indirectly impact historic resources outside of the I-65 corridor between MP 131.24 and MP 135.672.

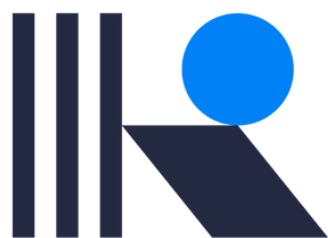
Thank you for consulting with our office on this undertaking. If you have any questions please don't hesitate to contact me at 502-892-3601 or at craig.potts@ky.gov.

Sincerely,

Craig A. Potts
Executive Director
Kentucky Heritage Council and
State Historic Preservation Officer



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K I P D A

Kentuckiana Regional Planning
& Development Agency

Administrative Modification 1

Fiscal Year 2023 - 2026 Transportation Improvement Program (TIP)

July 3, 2023



TIP Action:	Modify TIP funding, change funding source, update OTP, update phases				
Project Sponsor:	KYTC	KIPDA ID:	3115	State ID:	5-10068.00
County:	Jefferson	Parent ID:	N/A	Group ID:	N/A
Project Name:	I-65 Bridge at Jacob, Broadway, Gray St	Funding Source:	Federal Bridge Program- (FBP) Bridge Replacement - On System (BRO)	Open to Public Date:	2030 2026
Total Estimated Project Cost:	\$46,310,000 \$63,150,000		Total Cost Programmed in TIP to date:	\$46,310,000 \$63,150,000	
Description:	Bridge project in Jefferson County on (056B00191N) I-65 AT Jacob, Broadway, Gray St (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<div>FY 2024 Right of Way phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</div> <div>FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</div> <div>FY 2024 Construction phase with BRO funds: \$50,400,000 (Federal) + \$12,600,000 (Other) = \$63,000,000 (Total)</div> <div>FY 2025 Design phase with FBP funds: \$3,368,000 (Federal) + \$842,000 (Other) = \$4,210,000 (Total)</div> <div>*FY 2027 Construction phase with FBP funds: \$16,880,000 (Federal) + \$4,200,000 (Other) = \$21,000,000 (Total)</div> <div>*FY 2028 Construction phase with FBP funds: \$16,880,000 (Federal) + \$4,200,000 (Other) = \$21,000,000 (Total)</div>				

TIP Action:	Modify TIP funding, change funding source, update OTP, update phases				
Project Sponsor:	KYTC	KIPDA ID:	3117	State ID:	5-10069.00
County:	Jefferson	Parent ID:	N/A	Group ID:	N/A
Project Name:	I-65 Bridge at E Chesnut St	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2030 2026
Total Estimated Project Cost:	\$15,983,000 \$20,125,000		Total Cost Programmed in TIP to date:	\$15,983,000 \$20,125,000	
Description:	Bridge project in Jefferson County on (056B00192N) I-65 at E Chestnut St (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<div>FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total)</div> <div>FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</div> <div>FY 2024 Construction phase with BRO funds: \$16,000,000 (Federal) + \$4,000,000 (Other) = \$20,000,000 (Total)</div> <div>*FY 2027 Design phase with BRO funds: \$1,162,400 (Federal) + \$290,600 (Other) = \$1,453,000 (Total)</div> <div>*FY 2028 Construction phase with BRO funds: \$11,624,000 (Federal) + \$2,906,000 (Other) = \$14,530,000 (Total)</div>				

TIP Action:	Modify TIP funding, change funding source, update OTP, update phases				
Project Sponsor:	KYTC	KIPDA ID:	3114	State ID:	5-10067.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at St Catherine St	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2027 2026
Total Estimated Project Cost:	\$3,927,000 \$7,125,000		Total Cost Programmed in TIP to date:	\$3,927,000 \$7,125,000	
Description:	Bridge project in Jefferson County on (056B00184N) I-65 at St Catherine St (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<div>FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total)</div> <div>FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</div> <div>FY 2024 Construction phase with BRO funds: \$5,600,000 (Federal) + \$1,400,000 (Other) = \$7,000,000 (Total)</div> <div>FY 2025 Design phase with BRO funds: \$285,600 (Federal) + \$71,400 (Other) = \$357,000 (Total)</div> <div>FY 2025 Construction phase with BRO funds: \$2,856,000 (Federal) + \$714,000 (Other) = \$3,570,000 (Total)</div>				

TIP Action:	Modify TIP funding, change funding source, update OTP, update phases				
Project Sponsor:	KYTC	KIPDA ID:	3112	State ID:	5-10065.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at US 60A (Eastern Parkway)	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026 2028
Total Estimated Project Cost:	\$6,094,000 \$10,625,000		Total Cost Programmed in TIP to date:	\$6,094,000 \$10,625,000	
Description:	Bridge project in Jefferson County on (056B00180N) I-65 at US 60A (Eastern Parkway) (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<p>FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total)</p> <p>FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</p> <p>FY 2024 Construction phase with BRO funds: \$8,400,000 (Federal) + \$2,100,000 (Other) = \$10,500,000 (Total)</p> <p>FY 2026 Design phase with BRO funds: \$443,200 (Federal) + \$110,800 (Other) = \$554,000 (Total)</p> <p>FY 2026 Construction phase with BRO funds: \$4,432,000 (Federal) + \$1,108,000 (Other) = \$5,540,000 (Total)</p>				

TIP Action:	Modify TIP funding, change funding source, update OTP, update phases				
Project Sponsor:	KYTC	KIPDA ID:	3123	State ID:	5-10073.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at KFEC Gate 6	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2028 2026
Total Estimated Project Cost:	\$4,455,000 \$7,625,000		Total Cost Programmed in TIP to date:	\$4,455,000 \$7,625,000	
Description:	Bridge project in Jefferson County on (056B0021 IN) I-65 at KFEC Gate 6 (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<div>FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total)</div> <div>FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</div> <div>FY 2024 Construction phase with BRO funds: \$6,000,000 (Federal) + \$1,500,000 (Other) = \$7,500,000 (Total)</div> <div>FY 2026 Design phase with BRO funds: \$324,000 (Federal) + \$81,000 (Other) = \$405,000 (Total)</div> <div>FY 2026 Construction phase with BRO funds: \$3,240,000 (Federal) + \$810,000 (Other) = \$4,050,000 (Total)</div>				

TIP Action:	Modify TIP funding, change funding source, update OTP, update phases				
Project Sponsor:	KYTC	KIPDA ID:	3120	State ID:	5-10072.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at Manning Rd	Funding Source:	Federal Bridge Program (FBP) Bridge Replacement - On System (BRO)	Open to Public Date:	2028 2026
Total Estimated Project Cost:	\$6,204,000 \$7,625,000		Total Cost Programmed in TIP to date:	\$6,204,000 \$7,625,000	
Description:	Bridge project in Jefferson County on (056B00210N) I-65 at Manning Rd (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<div>FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total)</div> <div>FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</div> <div>FY 2024 Construction phase with BRO funds: \$6,000,000 (Federal) + \$1,500,000 (Other) = \$7,500,000 (Total)</div> <div>FY 2026 Design phase with FBP funds: \$451,200 (Federal) + \$112,800 (Other) = \$564,000 (Total)</div> <div>FY 2026 Construction phase with FBP funds: \$4,512,000 (Federal) + \$1,128,000 (Other) = \$5,640,000 (Total)</div>				

TIP Action:	Modify TIP funding, change funding source, update OTP, update phases				
Project Sponsor:	KYTC	KIPDA ID:	3119	State ID:	5-10071.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at Phillips Ln	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026 2028
Total Estimated Project Cost:	\$4,554,000 \$7,925,000		Total Cost Programmed in TIP to date:	\$4,554,000 \$7,925,000	
Description:	Bridge project in Jefferson County on (056B00209N) I-65 at Phillips Ln (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<p>FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total)</p> <p>FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</p> <p>FY 2024 Construction phase with BRO funds: \$6,240,000 (Federal) + \$1,560,000 (Other) = \$7,800,000 (Total)</p> <p>FY 2026 Design phase with FBP funds: \$331,200 (Federal) + \$82,800 (Other) = \$414,000 (Total)</p> <p>FY 2026 Construction phase with FBP funds: \$3,312,000 (Federal) + \$82,800 (Other) = \$414,000 (Total)</p>				

TIP Action:	Add to TIP by Group Projects category: Roadway and Bridge Preservation and Rehabilitation				
Project Sponsor:	KYTC	KIPDA ID:	TBD	State ID:	5-10099.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at Brook/Muhammad Ali	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026
Total Estimated Project Cost:	\$12,625,000		Total Cost Programmed in TIP to date:	\$12,625,000	
Description:	Bridge project in Jefferson County on 056B00193N I-65 at Brook/Muhammad Ali (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total) FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total) FY 2024 Construction phase with BRO funds: \$10,000,000 (Federal) + \$2,500,000 (Other) = \$12,500,000 (Total)				

TIP Action:	Add to TIP by Group Projects category: Roadway and Bridge Preservation and Rehabilitation				
Project Sponsor:	KYTC	KIPDA ID:	TBD	State ID:	5-10100.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at Muhammad Ali SB On-Ramp	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026
Total Estimated Project Cost:	\$925,000		Total Cost Programmed in TIP to date:	\$925,000	
Description:	Bridge project in Jefferson County on (056B00194N) I-65 At Muhammad Ali SB on-ramp (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total) FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total) FY 2024 Construction phase with BRO funds: \$640,000 (Federal) + \$160,000 (Other) = \$800,000 (Total)				
TIP Action:	Add to TIP by Group Projects category: Roadway and Bridge Preservation and Rehabilitation				
Project Sponsor:	KYTC	KIPDA ID:	TBD	State ID:	5-10101.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 NB Bridge at Floyd St	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026
Total Estimated Project Cost:	\$1,325,000		Total Cost Programmed in TIP to date:	\$1,325,000	
Description:	Bridge project in Jefferson County on (056B00195N) I-65 NB at Floyd St (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total) FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total) FY 2024 Construction phase with BRO funds: \$960,000 (Federal) + \$240,000 (Other) = \$1,200,000 (Total)				

TIP Action:	Add to TIP by Group Projects category: Roadway and Bridge Preservation and Rehabilitation				
Project Sponsor:	KYTC	KIPDA ID:	TBD	State ID:	5-10102.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 NB Bridge at E Liberty Street	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026
Total Estimated Project Cost:	\$2,625,000		Total Cost Programmed in TIP to date:	\$2,625,000	
Description:	Bridge project in Jefferson County on (056B00197R) I-65 NB at E Liberty St (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total)				
	FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)				
	FY 2024 Construction phase with BRO funds: \$2,000,000 (Federal) + \$500,000 (Other) = \$2,500,000 (Total)				
TIP Action:	Add to TIP by Group Projects category: Roadway and Bridge Preservation and Rehabilitation				
Project Sponsor:	KYTC	KIPDA ID:	TBD	State ID:	5-10103.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at Oak Street	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026
Total Estimated Project Cost:	\$8,125,000		Total Cost Programmed in TIP to date:	\$8,125,000	
Description:	Bridge project in Jefferson County on (056B00186N) I-65 at Oak St (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total)				
	FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)				
	FY 2024 Construction phase with BRO funds: \$6,400,000 (Federal) + \$1,600,000 (Other) = \$8,000,000 (Total)				

TIP Action:	Add to TIP by Group Projects category: Roadway and Bridge Preservation and Rehabilitation				
Project Sponsor:	KYTC	KIPDA ID:	TBD	State ID:	5-10104.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at Brandeis Ave	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026
Total Estimated Project Cost:	\$5,725,000		Total Cost Programmed in TIP to date:	\$5,725,000	
Description:	Bridge project in Jefferson County on (056B00182N) I-65 at Brandeis Ave (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total) FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total) FY 2024 Construction phase with BRO funds: \$4,480,000 (Federal) + \$1,120,000 (Other) = \$5,600,000 (Total)				
TIP Action:	Add to TIP by Group Projects category: Roadway and Bridge Preservation and Rehabilitation				
Project Sponsor:	KYTC	KIPDA ID:	TBD	State ID:	5-10105.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at University Blvd	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026
Total Estimated Project Cost:	\$5,425,000		Total Cost Programmed in TIP to date:	\$5,425,000	
Description:	Bridge project in Jefferson County in (056B00181N) I-65 at University Blvd (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total) FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total) FY 2024 Construction phase with BRO funds: \$4,240,000 (Federal) + \$1,060,000 (Other) = \$5,300,000 (Total)				

TIP Action:	Add to TIP by Group Projects category: Roadway and Bridge Preservation and Rehabilitation				
Project Sponsor:	KYTC	KIPDA ID:	TBD	State ID:	5-10106.00
County	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at Bradley Ave	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2026
Total Estimated Project Cost:	\$6,625,000		Total Cost Programmed in TIP to date:	\$6,625,000	
Description:	Bridge project in Jefferson County on (056B00212N) I-65 at Bradley Ave (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total) FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total) FY 2024 Construction phase with BRO funds: \$5,200,000 (Federal) + \$1,300,000 (Other) = \$6,500,000 (Total)				
TIP Action:	Add to TIP by Group Projects category: Roadway and Bridge Preservation and Rehabilitation				
Project Sponsor:	KYTC	KIPDA ID:	TBD	State ID:	5-946.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-71 Ramp 332	Funding Source:	HSIP-State	Open to Public Date:	2024
Total Estimated Project Cost:	\$105,000		Total Cost Programmed in TIP to date:	\$105,000	
Description:	Installation of High Friction Surface Treatment on Ramp 332, connecting the Gene Snyder Freeway (I-265 WB) to I-71 SB BMP 0.0 to EMP 0.267				
Justification:	To increase level of safety to vehicles in this ramp				
FY 23-26 TIP Funding:	FY 2023 Construction phase with HSIP-State funds: \$94,500 (Federal) + \$10,500 (Other) = \$105,000 (Total)				

TIP Action:	Modify TIP funding, change open to public date, shift construction phase, add new right-of-way and utilities phase, and cancel design phase				
Project Sponsor:	KYTC	KIPDA ID:	3195	State ID:	5-10064.00
County:	Jefferson	Parent ID:	N/A	Group ID:	N/A
Project Name:	I-65 Bridge at Hill, CSX RR & Burnett	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2025 2026
Total Estimated Project Cost:	\$25,168,000 \$23,825,000		Total Cost Programmed in TIP to date:	\$25,168,000 \$23,825,000	
Description:	Bridge project in Jefferson County on (056B00179N) I-65 at Hill, CSX RR & Burnett. (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<div>FY 2023 Design phase with BRO funds: \$1,830,400 (Federal) + \$457,600 (Other) = \$2,288,000 (Total)</div> <div>FY 2024 Right of Way phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</div> <div>FY 2024 Utilities phase with BRO funds: \$600,000 (Federal) + \$150,000 (Other) = \$750,000 (Total)</div> <div>FY 2023 Construction phase with BRO funds: \$18,304,000 (Federal) + \$4,576,000 (Other) = \$22,880,000 (Total)</div> <div>FY 2024 Construction phase with BRO funds: \$18,400,000 (Federal) + \$4,600,000 (Other) = \$23,000,000 (Total)</div>				

TIP Action:	Modify TIP funding, change open to public date, shift construction phase, add new right-of-way and utilities phase, and cancel design phase				
Project Sponsor:	KYTC	KIPDA ID:	3196	State ID:	5-10066.00
County:	Jefferson	Parent ID:	N/A	Group ID:	N/A
Project Name:	I-65 Bridge at E Kentucky & S Brook St	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2025 2026
Total Estimated Project Cost:	\$31,467,000 \$35,500,000		Total Cost Programmed in TIP to date:	\$31,467,000 \$35,500,000	
Description:	Bridge project in Jefferson County on (056B00183N) I-65 at East Kentucky & South Brook Street. (Potential CMGC Delivery Project)				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<div>FY 2023 Design phase with BRO funds: \$2,301,600 (Federal) + \$575,400 (Other) = \$2,877,000 (Total)</div> <div>FY 2024 Right of Way phase with BRO funds: \$80,000 (Federal) + \$20,000 (Other) = \$100,000 (Total)</div> <div>FY 2024 Utilities phase with BRO funds: \$320,000 (Federal) + \$80,000 (Other) = \$400,000 (Total)</div> <div>FY 2023 Construction phase with BRO funds: \$23,016,000 (Federal) + \$5,574,000 (Other) = \$28,590,000 (Total)</div> <div>FY 2024 Construction phase with BRO funds: \$28,000,000 (Federal) + \$7,000,000 (Other) = \$35,000,000 (Total)</div>				

TIP Action:	Modify TIP funding, change open to public date, shift construction phase, add new right-of-way and utilities phase, and cancel design phase				
Project Sponsor:	KYTC	KIPDA ID:	3035	State ID:	5-10070.00
County:	Jefferson	Parent ID:	N/A	Group ID:	2675
Project Name:	I-65 Bridge at NS Railroad	Funding Source:	Bridge Replacement - On System (BRO)	Open to Public Date:	2025 2026
Total Estimated Project Cost:	\$13,244,000 \$16,125,000		Total Cost Programmed in TIP to date:	\$13,244,000 \$16,125,000	
Description:	Bridge project in Jefferson County on (056B00205N) I-65 at Norfolk Southern Railroad (Potential CMGC Delivery Project).				
Justification:	Increase safety for all users. Maintain the existing transportation network in a state of good repair.				
FY 23-26 TIP Funding:	<div><div>FY 2023 Design phase with BRO funds: \$963,200 (Federal) + \$240,800 (Other) = \$1,204,000 (Total)</div><div>FY 2024 Right of Way phase with BRO funds: \$40,000 (Federal) + \$10,000 (Other) = \$50,000 (Total)</div><div>FY 2024 Utilities phase with BRO funds: \$60,000 (Federal) + \$15,000 (Other) = \$75,000 (Total)</div><div>FY 2023 Construction phase with BRO funds: \$9,632,000 (Federal) + \$2,408,000 (Other) = \$12,040,000 (Total)</div><div>FY 2024 Construction phase with BRO funds: \$12,800,000 (Federal) + \$3,200,000 (Other) = \$16,000,000 (Total)</div></div>				

Kentucky Maintenance Projects

Sponsor Agency: KYTC

Project Name: I-65

KIPDA ID:	State ID/DES #:	Open to Public:	County/Counties:	Project Cost:	AQ Analysis Status:
3093	5-22070.00	2026	Jefferson	\$6,050,000	Exempt

Project Description:

Address condition of I-65 from milepoint 131.24 to milepoint 136.338.

Justification:

Maintenance of the existing transportation network in a state of good repair.

Phase:	Year:	Funding Category:	Federal:	State/Local:	Total:
D	2025	NHPM	\$440,000	\$110,000	\$550,000
C	2025	NHPM	\$4,400,000	\$1,100,000	\$5,500,000
		Total	\$4,840,000	\$1,210,000	\$6,050,000

Sponsor Agency: KYTC

Project Name: I-65 Bridge at East Kentucky Street & South Brook Street

KIPDA ID:	State ID/DES #:	Open to Public:	County/Counties:	Project Cost:	AQ Analysis Status:
3196	5-10066.00	2025	Jefferson	\$31,467,000	Exempt

Project Description:

Bridge project in Jefferson County on (056B00183N) I-65 at East Kentucky & South Brook Street (Potential CMGC delivery project).

Justification:

Increase safety for all users. Maintain the existing transportation network in a state of good repair.

Phase:	Year:	Funding Category:	Federal:	State/Local:	Total:
D	2023	BRO	\$2,301,600	\$575,400	\$2,877,000
C	2023	BRO	\$23,016,000	\$5,574,000	\$28,590,000
		Total	\$25,317,600	\$6,149,400	\$31,467,000

Sponsor Agency: KYTC

Project Name: I-65 Bridge at Hill, CSX RR & Burnett

KIPDA ID:	State ID/DES #:	Open to Public:	County/Counties:	Project Cost:	AQ Analysis Status:
3195	5-10064.00	2025	Jefferson	\$25,168,000	Exempt



Project Description:

Bridge project in Jefferson County on (056B00179N) I-65 at Hill, CSX RR & Burnett.

Justification:

Increase safety for all users. Maintain the existing transportation network in a state of good repair.

Phase:	Year:	Funding Category:	Federal:	State/Local:	Total:
D	2023	BRO	\$1,830,400	\$457,600	\$2,288,000
C	2023	BRO	\$18,304,000	\$4,576,000	\$22,880,000
		Total	\$20,134,400	\$5,033,600	\$25,168,000

		Kentucky Transportation Cabinet Federal Highway Administration NO EFFECT FINDING																								
KYTC Item No:	05-8102.03	Route(s):	Crittenden Dr																							
County(ies):	Jefferson																									
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.) Lengthen/widen ramp from Crittenden to I-65 northbound to maximum extent possible without widening I-65 bridge over Crittenden Drive.																										
USFWS IPaC LISTED SPP FOR PROJECT SITE: <table border="0"> <tr><td>Gray Bat</td><td><i>Myotis grisescens</i></td></tr> <tr><td>Indiana Bat</td><td><i>Myotis sodalis</i></td></tr> <tr><td>Northern Long-eared Bat</td><td><i>Myotis septentrionalis</i></td></tr> <tr><td>Clubshell</td><td><i>Pleurobema clava</i></td></tr> <tr><td>Fanshell</td><td><i>Cyprogenia stegaria</i></td></tr> <tr><td>Northern Riffleshell</td><td><i>Epioblasma rangiana</i></td></tr> <tr><td>Orangefoot Pimpleback</td><td><i>Plethobasus cooperianus</i></td></tr> <tr><td>Pink Mucket</td><td><i>Lampsilis abrupta</i></td></tr> <tr><td>Rabbitsfoot</td><td><i>Theliderma cylindrica</i></td></tr> <tr><td>Ring Pink</td><td><i>Obovaria retusa</i></td></tr> <tr><td>Rough Pigtoe</td><td><i>Pleurobema plenum</i></td></tr> </table>					Gray Bat	<i>Myotis grisescens</i>	Indiana Bat	<i>Myotis sodalis</i>	Northern Long-eared Bat	<i>Myotis septentrionalis</i>	Clubshell	<i>Pleurobema clava</i>	Fanshell	<i>Cyprogenia stegaria</i>	Northern Riffleshell	<i>Epioblasma rangiana</i>	Orangefoot Pimpleback	<i>Plethobasus cooperianus</i>	Pink Mucket	<i>Lampsilis abrupta</i>	Rabbitsfoot	<i>Theliderma cylindrica</i>	Ring Pink	<i>Obovaria retusa</i>	Rough Pigtoe	<i>Pleurobema plenum</i>
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Methodologies: (Methods of assessment, who, what, when, resources, etc.) Biologist reviewed literature on listed species and used GIS mapping to investigate the conditions of the project area.																										
Results: (Compare habitat used by listed species with available habitat) <p>Bats: This project is located along existing ROW of I-65. Gray bats typically utilize bridges, caves, and other karst features for roosting habitat during warmer temperatures. Gray bats use low-cluttered riparian habitat for foraging. Given that this project is widening an existing on-ramp to I-65 and there are no bridges nor water sources present within the project corridor, therefore no riparian habitat nor roosting habitat exists within the project limits. Thus, a 'No habitat, no effect' determination for Gray bats.</p> <p>Mussels: Mussels typically require permanent perennial streams of varying sizes depending upon species. The clubshell mussel inhabits medium-sized to large rivers in sand and gravel substrates. Fanshell mussels are most often associated with stable substrates of sand, gravel, and cobble. They are usually found at depths less than three feet in strongly flowing water in large streams to large rivers. The northern riffleshell mussel inhabits small to large rivers in sand and gravel substrates. Orangefoot pimpleback mussels inhabit medium to large rivers in sand and gravel with flowing water. The pink mucket is known from medium to large rivers in sandy mud and gravel substrates. Rabbitsfoot mussels are primarily found in small to medium-sized streams and some larger rivers with substrates of gravel and sand. The ring pink mussel is known from medium-sized to large rivers in sand and gravel. The rough pigtoe mussel is known to medium-sized to large rivers in sand and gravel. There are no streams or water sources within the project boundaries. Thus, 'no habitat, no effect determination' for all mussel species.</p>																										

Determinations: no habitat, no effect for:

Gray Bat	<i>Myotis grisescens</i>
Clubshell	<i>Pleurobema clava</i>
Fanshell	<i>Cyprogenia stegaria</i>
Northern Riffleshell	<i>Epioblasma rangiana</i>
Orangefoot Pimpleback	<i>Plethobasus cooperianus</i>
Pink Mucket	<i>Lampsilis abrupta</i>
Rabbitsfoot	<i>Theliderma cylindrica</i>
Ring Pink	<i>Obovaria retusa</i>
Rough Pigtoe	<i>Pleurobema plenum</i>

The project has been assessed in accordance with the provisions of Section 7 of the Endangered Species Act. As a designated representative of the FHWA, the KYTC has determined that the project will have No Effect on any listed species or their critical habitat, and further Section 7(a)(2) consultation with the Service is not required.

Makayla Beckner
KYTC Signature

11/13/2023
Date



Makayla Beckner
Print Name

E.A.T.S. Milestones updated

Name

Date

ATTACHED: Agency Species List(s)

	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																								
KYTC Item No:	05-10064	Route(s):	I-65 bridge																						
County(ies):	Jefferson																								
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																									
ADDRESS DEFICIENCIES OF BRIDGE ON I-65 OVER E HILL STREET AND BURNETT AVE 056B00179N.																									
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Makayla Beckner
KYTC Signature

11/08/2023
Date

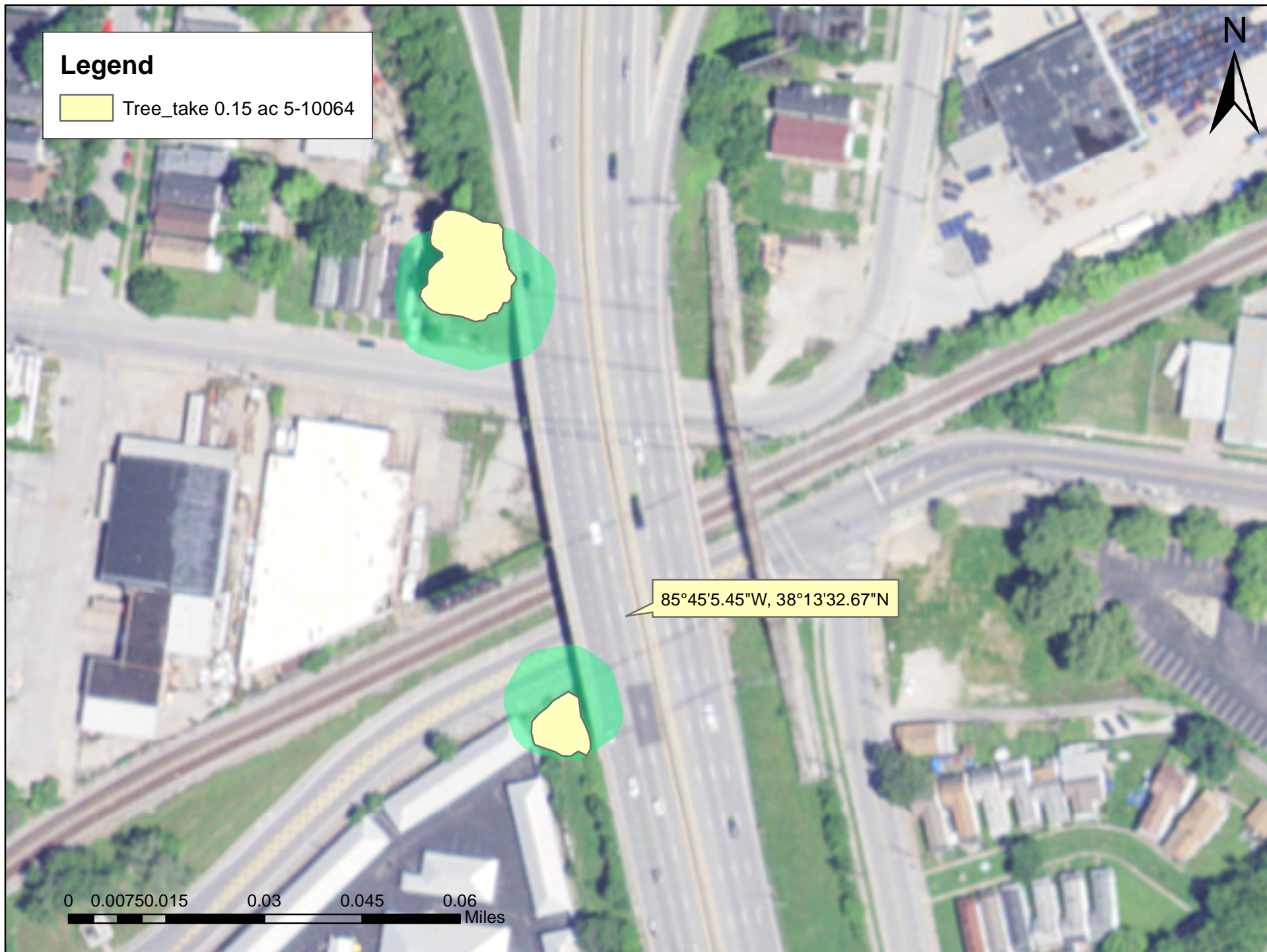
Makayla Beckner
Print Name



E.A.T.S. Milestones updated

Name

Date

ATTACHED: Agency Species List(s)



	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																								
KYTC Item No:	05-10065	Route(s):	I-65 bridge																						
County(ies):	Jefferson																								
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Makayla Beckner
KYTC Signature

11/08/2023
Date

Makayla Beckner
Print Name

E.A.T.S. Milestones updated

Name

Date

ATTACHED: Agency Species List(s)





Legend

Tree_take 0.18 ac 5-10065

85°45'10.25"W, 38°12'44.43"N

0 0.005 0.01 0.02 0.03 0.04 Miles

	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																						
KYTC Item No:	05-10066	Route(s):	I-65 bridge																				
County(ies):	Jefferson																						
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																							
ADDRESS DEFICIENCIES OF BRIDGE ON I-65 OVER KENTUCKY AND BROOKS STREET 056B00183N.																							
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Makayla Beckner
KYTC Signature

11/14/2023
Date

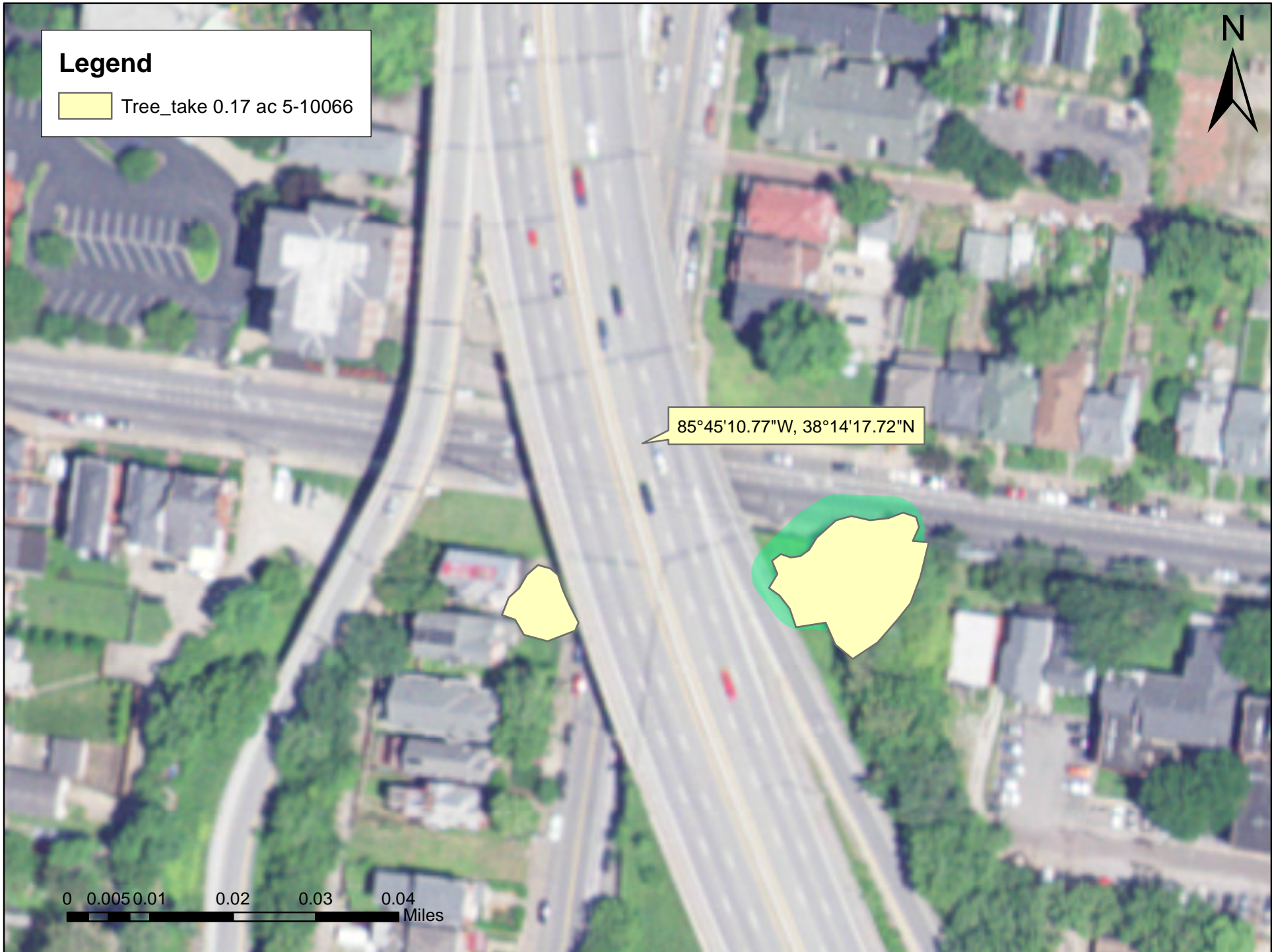
Makayla Beckner
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E.A.T.S. Milestones updated

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	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																						
KYTC Item No:	05-10067	Route(s):	I-65 bridge																				
County(ies):	Jefferson																						
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																							
ADDRESS DEFICIENCIES OF BRIDGE ON I-65 OVER ST. CATHERINE STREET 056B00184N.																							
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Makayla Beckner
KYTC Signature

11/14/2023

Date

Makayla Beckner
Print Name



E.A.T.S. Milestones updated

Name

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ATTACHED: Agency Species List(s)



	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration NO EFFECT FINDING</p>																						
KYTC Item No:	05-10068	Route(s):	I-65 bridge																				
County(ies):	Jefferson																						
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																							
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Makayla Beckner
KYTC Signature

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Date

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Print Name



E.A.T.S. Milestones updated

Name

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ATTACHED: Agency Species List(s)



	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																						
KYTC Item No:	05-10069	Route(s):	I-65 bridge																				
County(ies):	Jefferson																						
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Makayla Beckner
KYTC Signature

11/14/2023
Date

Makayla Beckner
Print Name



E.A.T.S. Milestones updated

Name

Date

ATTACHED: Agency Species List(s)



	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration NO EFFECT FINDING</p>																								
KYTC Item No:	05-10070	Route(s):	I-65 bridge																						
County(ies):	Jefferson																								
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																									
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Makayla Beckner
KYTC Signature

11/08/2023
Date

Makayla Beckner
Print Name

E.A.T.S. Milestones updated

Name

Date



ATTACHED: Agency Species List(s)



Legend

Tree_take 0.66 ac 05-10070

0 0.004 0.008 0.016 0.024 0.032 Miles

	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																								
KYTC Item No:	05-10071	Route(s):	I-65 bridge																						
County(ies):	Jefferson																								
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																									
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Makayla Beckner
KYTC Signature

11/08/2023
Date

Makayla Beckner
Print Name



E.A.T.S. Milestones updated

Name

Date

ATTACHED: Agency Species List(s)



	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																								
KYTC Item No:	05-10072	Route(s):	I-65 bridge																						
County(ies):	Jefferson																								
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Makayla Beckner
KYTC Signature

11/08/2023
Date

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Print Name



E.A.T.S. Milestones updated

Name

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KYTC Item No:	05-10073	Route(s):	I-65 bridge																						
County(ies):	Jefferson																								
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Makayla Beckner
KYTC Signature

11/08/2023
Date

Makayla Beckner
Print Name



E.A.T.S. Milestones updated

Name

Date

ATTACHED: Agency Species List(s)



	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																						
KYTC Item No:	05-10099	Route(s):	I-65 bridge																				
County(ies):	Jefferson																						
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																							
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Makayla Beckner
KYTC Signature

11/14/2023
Date

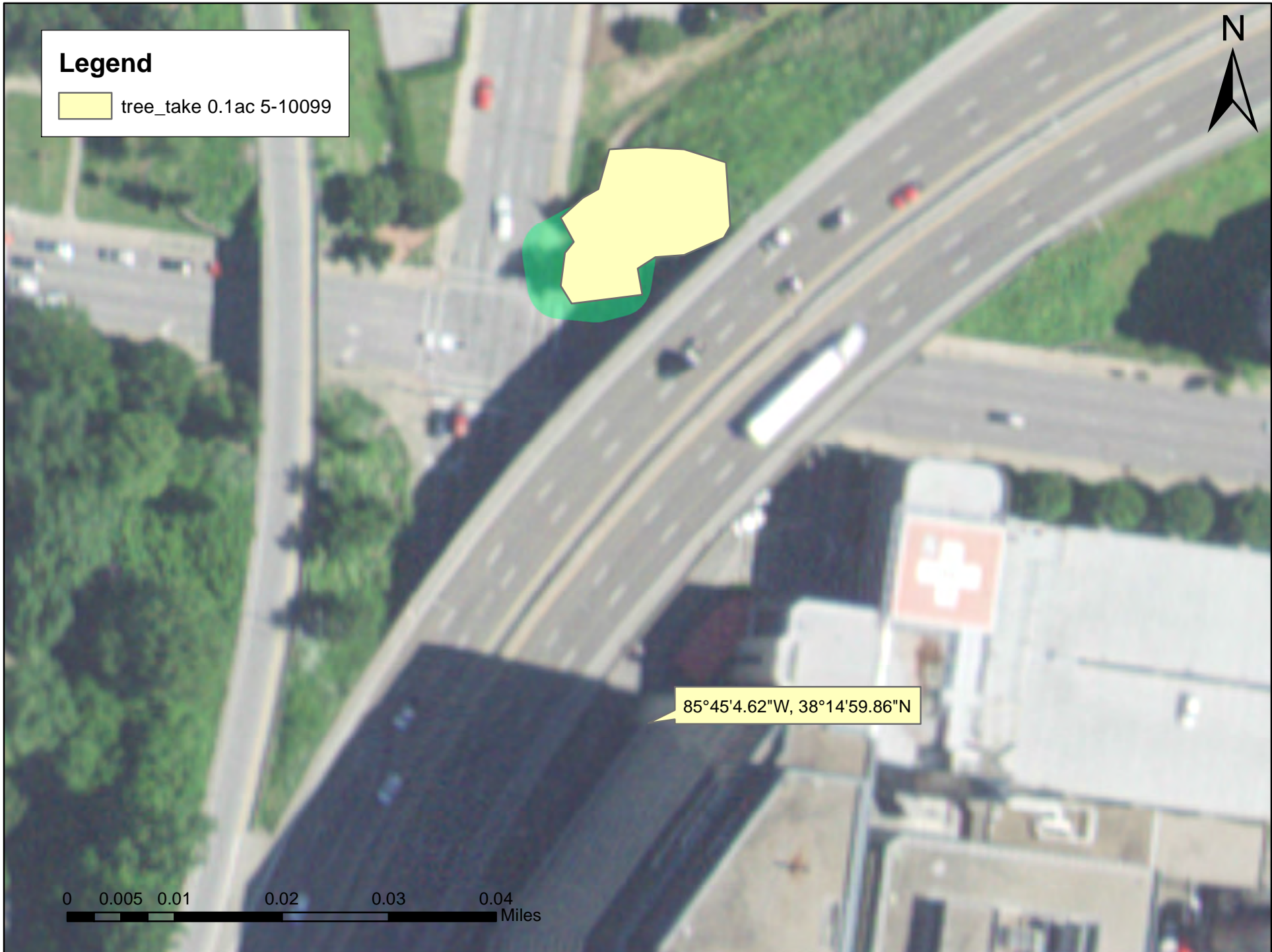
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E.A.T.S. Milestones updated

Name

Date

ATTACHED: Agency Species List(s)



	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																						
KYTC Item No:	05-10100	Route(s):	I-65 bridge																				
County(ies):	Jefferson																						
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																							
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Makayla Beckner
KYTC Signature

11/14/2023
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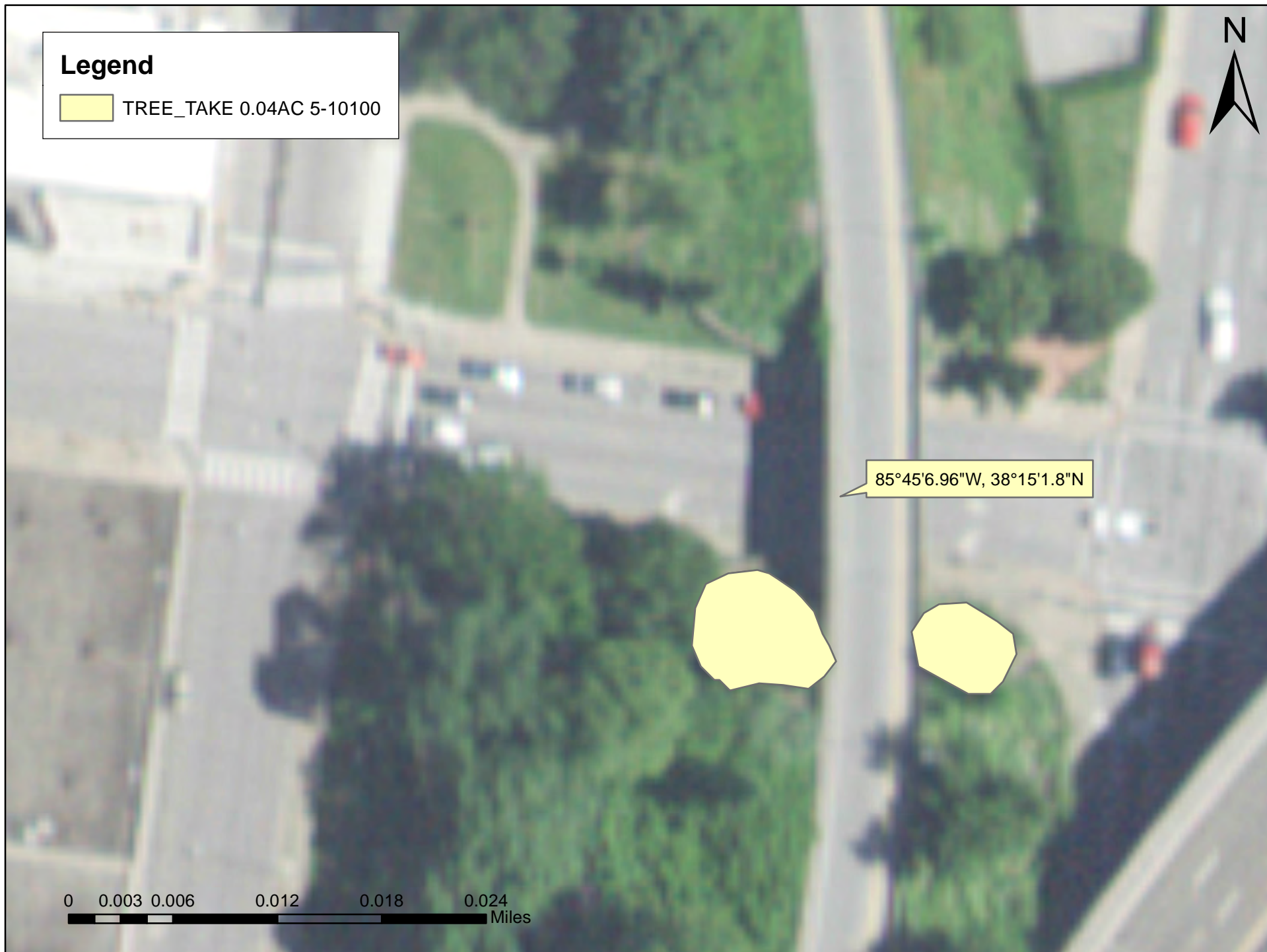
Makayla Beckner
Print Name

E.A.T.S. Milestones updated

Name

Date

ATTACHED: Agency Species List(s)





Legend

TREE_TAKE 0.04AC 5-10100

85°45'6.96"W, 38°15'1.8"N

0 0.003 0.006 0.012 0.018 0.024 Miles

	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration No Effect Finding</p>																						
KYTC Item No:	05-10101	Route(s):	I-65 bridge																				
County(ies):	Jefferson																						
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																							
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Makayla Beckner
KYTC Signature

11/14/2023

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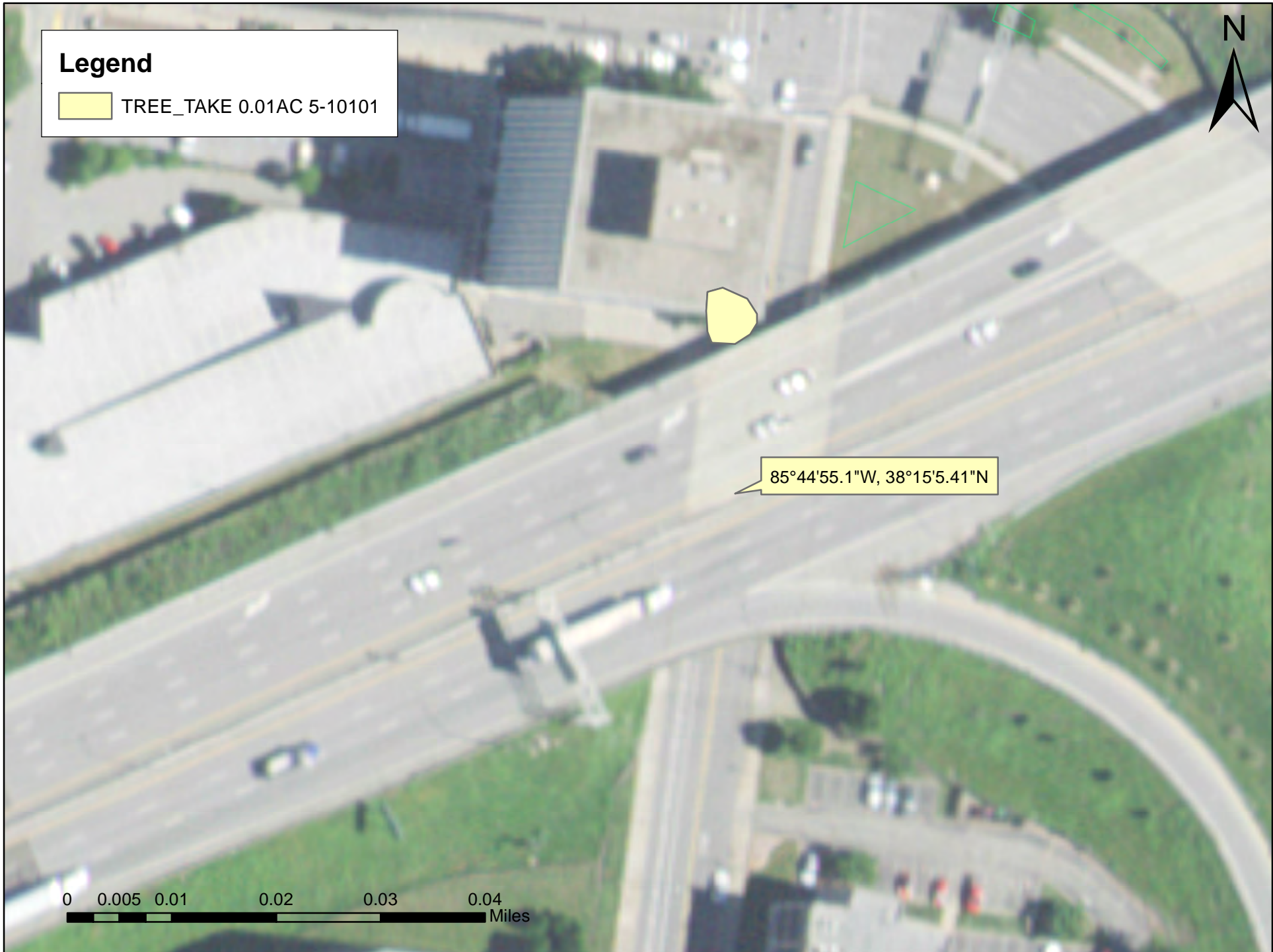
Makayla Beckner
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E.A.T.S. Milestones updated

Name

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ATTACHED: Agency Species List(s)



	<p align="center">Kentucky Transportation Cabinet Federal Highway Administration NO EFFECT FINDING</p>																						
KYTC Item No:	05-10102	Route(s):	I-65 bridge																				
County(ies):	Jefferson																						
Project Description: (Type of improvement, areas to be impacted, crossroad improvements, easements, etc.)																							
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E.A.T.S. Milestones updated

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human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:



<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do..>

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. Please include the Consultation Code in the header of

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KYTC Item No:	05-10103	Route(s):	I-65 bridge																				
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11/14/2023
Date

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E.A.T.S. Milestones updated

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KYTC Signature

11/08/2023
Date

Makayla Beckner
Print Name

E.A.T.S. Milestones updated

Name

Date

ATTACHED: Agency Species List(s)





Legend

tree_take 0.09ac 05-10104

85°45'7.86"W, 38°13'5.87"N

0 0.005 0.01 0.02 0.03 0.04 Miles

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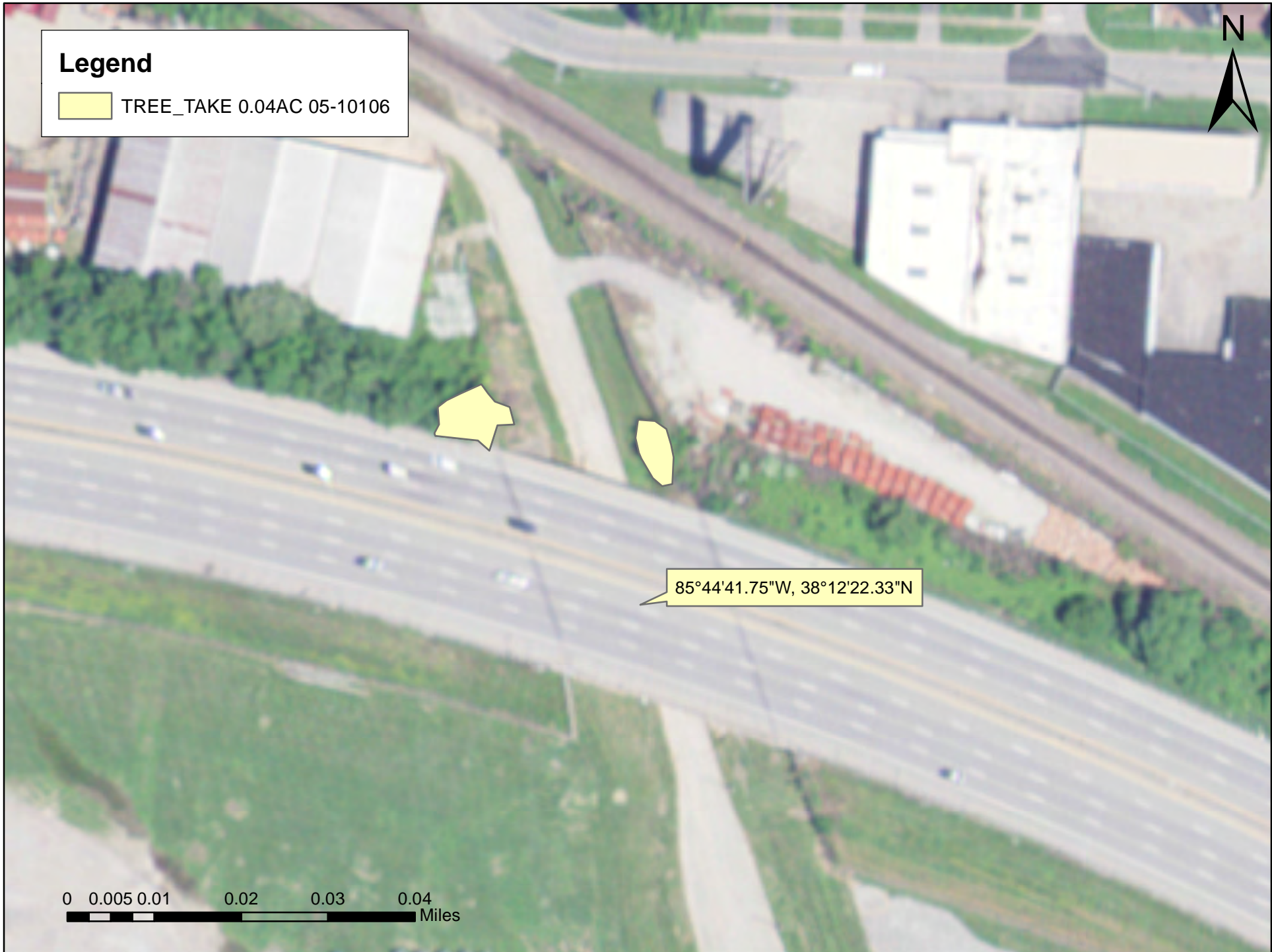
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Name

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Legend

TREE_TAKE 0.04AC 05-10106

85°44'41.75"W, 38°12'22.33"N

0 0.005 0.01 0.02 0.03 0.04 Miles





Maintenance of Traffic Community Impact Assessment

I-65 Bridge Bundling Project—Louisville, Kentucky

January 8, 2024

Executive Summary

The project is to replace or rehabilitate 18 bridges along a 4.6-mile section of elevated I-65 in downtown Louisville, Kentucky. The bridges are located from north of I-264 (MP 131.2) to south of I-64 (MP 135.8). The project is named “Central Corridor” because it is the backbone of the interstate highway system in Louisville and is one of the most heavily traveled sections of interstate in Kentucky, serving as a “Central Corridor” in the Commonwealth’s largest metropolitan area.

The corridor is bound by numerous urban developments and neighborhoods, including hospitals, universities, schools, destinations, and businesses. The purpose of this report is to document and disclose potential community impacts, and provide minimization, mitigation, and long-term enhancement efforts.

Key elements of the project are as follows:

- No new right-of-way, residential relocations, or commercial displacements are proposed.
- All adverse impacts would be related to Maintenance of Traffic (MOT) during construction, and therefore temporary impacts.
- Recently KYTC has had to close the interstate to perform emergency repairs, on various occasions this project would eliminate this ongoing maintenance issue and recurring disruption to the traveling public.
- All interstate traffic would be temporarily routed onto other interstates, not local roads.
- A variety of site-specific MOT actions will be employed along the corridor, such as, single lane closures, lane shifts, keeping 2 lanes open in each direction (out of 3), using a 2+1 lane option, overnight closures, weekend closures, and in rare instances longer term closures.
- East-West cross-interstate closures of local roads and pedestrian routes would be staged and coordinated by area (e.g., Medical District, College District, and Fairgrounds District) to minimize community impacts.
- A robust and multifaceted public involvement plan, with a focus on outreach to disadvantaged neighborhoods, stakeholders, and government agencies, will be implemented and maintained throughout the life of the project.

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Introduction

The project is to replace or rehabilitate 18 bridges along a 4.6-mile section of elevated I-65 in downtown Louisville, Kentucky. The bridges are located from north of I-264 (MP 131.2) to south of I-64 (MP 135.8). The project is named “Central Corridor” because it is one of the most heavily travelled sections of interstate in Kentucky, serving as a “Central Corridor” in the Commonwealth’s largest metropolitan area.

Originally constructed between 1957 and 1963, four of the bridges in the Project area are currently in Poor Condition and 14 are in Fair Condition, at risk of falling into Poor Condition within the next three years. The National Bridge Inventory (NBI) condition rating is determined by the lowest rating for the deck, superstructure, substructure. Ratings are categorized as Good (≥ 7), Fair (5-6), or Poor (≤ 4). A bridge is considered Structurally Deficient if any component is in Poor condition.

With average daily traffic (ADT) of 118,227 vehicles in 2022, the four Poor Condition bridges within the Project limits are the most traveled structurally deficient bridges in Kentucky. The corridor is bound by numerous urban developments and neighborhoods, including hospitals, universities, schools, destinations, and businesses. See **Figure 1**. The purpose of this report is to document and disclose potential community impacts, minimization, mitigation, and long-term enhancement efforts.

Purpose and Need

Purpose — The purpose of the Project is to address the deterioration of structural elements of the 18 bridges and improve safety at areas of concern. The goal of the bridge improvements is to extend the service life of the I-65 Interstate bridges by up to 75 years for full replacements and 30 years for rehabilitation. The goal of the minor safety improvements is to reduce crashes.

Needs — If not implemented, an increase in closures and/or lane restrictions will be required to accomplish reoccurring bridge inspections and repairs, thus causing additional inconvenience to the traveling public and additional cost. The bridges could eventually deteriorate to the point of requiring more frequent bridge inspections, unplanned repairs, weight limit restrictions, or ultimately closure. This is neither prudent nor viable; the adverse ramifications of this scenario would extend far beyond the temporary impacts at the local, regional, cross-state, and national level.

Four of the bridges in the Project area are currently in Poor Condition and 14 are in Fair Condition, at risk of falling into Poor Condition within the next three years. The number of Poor bridges has doubled since 2019. Most bridges in this corridor have active corrosion within the concrete at the abutments, piers, and concrete girder ends. See **Figure 2**. Many expansion joints have failed and are currently leaking. Brine runoff from deicing salts during winter months significantly accelerates the deterioration of concrete and steel reinforcement. Major deficiencies include decks moving independently of beams, extensive corrosion, loss of seal adhesion and failed joints, and exposed reinforcement. Bridge 056B00183N recently (November 2023) received a second emergency repair in three years for the failing substructures and additionally suffers from steel cracks expanding in the superstructure of this fracture critical bridge.

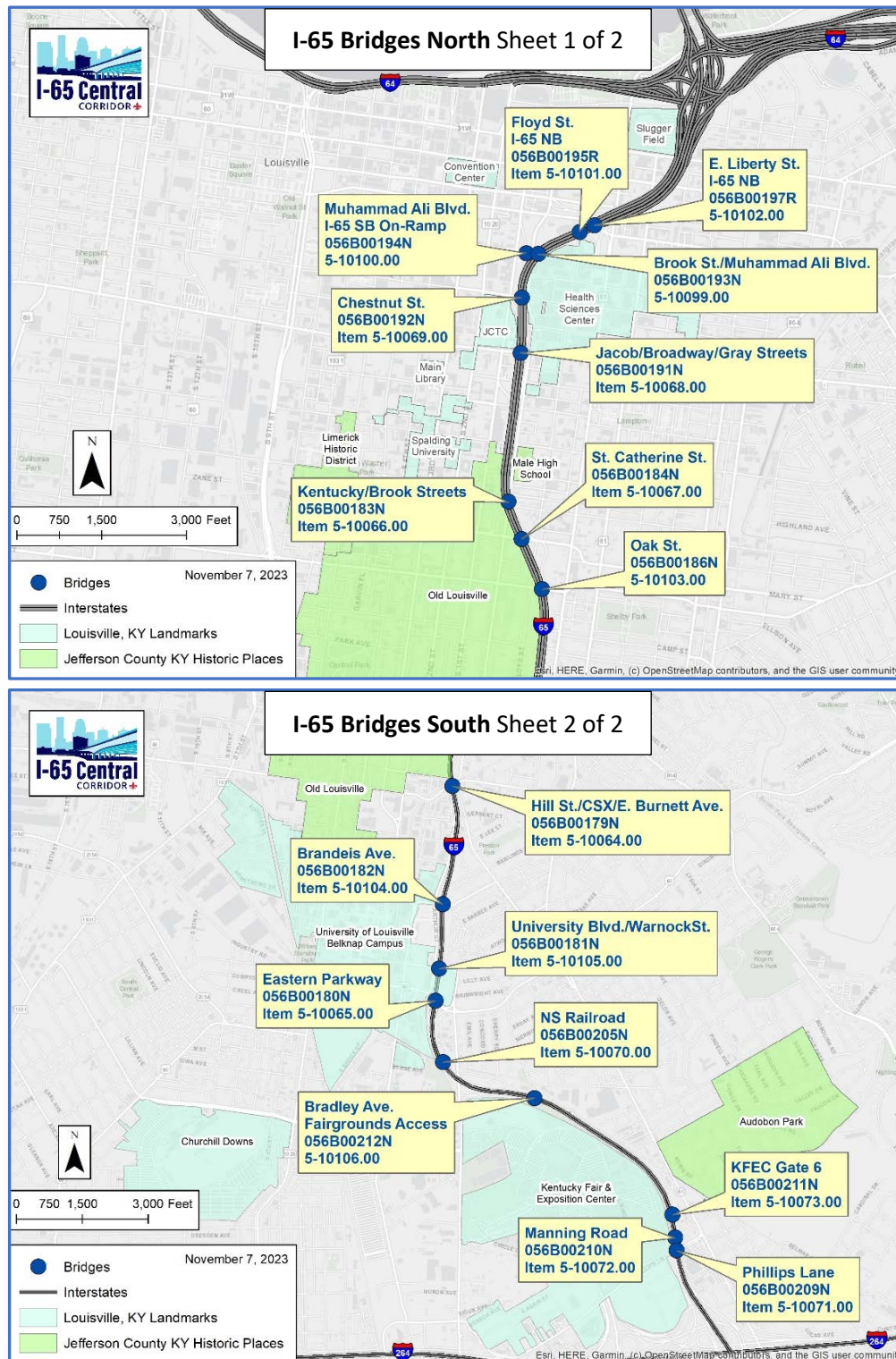


Figure 1. Bridge Locations

Table 1. Bridge Structural Condition

Bridge No.	Feature Intersected	Condition	Inspection Comments
056B00183N	E. Kentucky & S. Brook Street	Poor	At risk of being posted in near future
056B00179N	Hill, CSX RR & Burnett	Poor	At risk of being posted in near future
056B00212N	Bradley Avenue	Poor	At risk of being posted in near future
056B00191N	Jacob, Broadway, Gray Street	Poor	Already posted: SUV5 – 37 tons, SUV6 – 38 tons, SUV7 – 39 tons
056B00184N	St. Catherine Street	Fair	Structure will be in Poor Condition in less than 3 years due to lowest rating is a 5
056B00194N	E. Muhammad Ali Boulevard	Fair	Structure will be in Poor Condition within 3 years due to lowest rating is a 6
056B00195R	S. Floyd Street	Fair	Structure will be in Poor Condition within 3 years due to lowest rating is a 6
056B00205N	NS Railroad	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5
056B00180N	US 60A (Eastern Parkway)	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5
056B00181N	University Boulevard	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5
056B00192N	E Chestnut Street	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5
056B00193N	Brook Street & Muhammad Ali Boulevard	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5
056B00209N	Phillips Lane	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5. Already at risk. Posted Structure
056B00210N	Manning Road	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5
056B00211N	KFEC Gate 6 Drive	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5
056B00186N	E. Oak Street	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5
056B00182N	KY 61 (E. Brandeis Avenue)	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5

Bridge No.	Feature Intersected	Condition	Inspection Comments
056B00197R	E. Liberty Street	Fair	Structure will be in Poor Condition less than 3 years due to lowest rating is a 5



Bridge 056B00183N, E. Kentucky & S. Brook Street, temporary support for crumbling piers



Bridge 056B00183N, E. Kentucky & S. Brook Street, Current Conditions



Bridge 056B00183N: Exposed rebar and concrete spalling



Bridge 056B00191N, Jacob, Broadway, Gray Street, Existing Conditions



Bridge 056B00209N, Phillips Lane, deteriorating concrete abutment



Bridge 056B00192N, E. Chestnut Street, Current Conditions

Figure 2. Representative Structural Deficiencies

Proposed Scope of Action

The Project will deliver modern bridges to address critical reliability, structural, geometric, mobility, and safety issues. Local surface streets under the Project bridges will be restored in terms of surface street pavement, drainage, sidewalks, bicycle infrastructure, and lighting. In addition to rehabbing or replacing the 18 bridges, the Project includes three ramp improvements identified in a recent Planning Study that support safety and better traffic flow as well as pavement resurfacing for the entire 4.6-mile interstate corridor.

Of the 18 bridges, 15 are scoped as full replacements, two (056B00194N and 056B00195R) as deck replacements, and one (056B00209N) as a superstructure replacement as depicted on **Figure 3**.

- **Bundle 1:** This bundle will fully replace three priority bridges (056B00183N, 056B00179N, 056B00205N) which are most at risk and critical to the reliability of the I-65 corridor.
- **Bundle 2 (Medical District):** This bundle will replace two bridge decks for structures 056B00194N and 056B00195R, and will fully replace structures 056B00191N, 056B00192N, 056B00193N, and 056B00197R.
- **Bundle 3 (College District and Fairgrounds/Airport District):** This bundle will replace the superstructure on bridge 056B00209N and will fully replace structures 056B00184N, 056B00186N, 056B00182N, 056B00181N, 056B00180N,

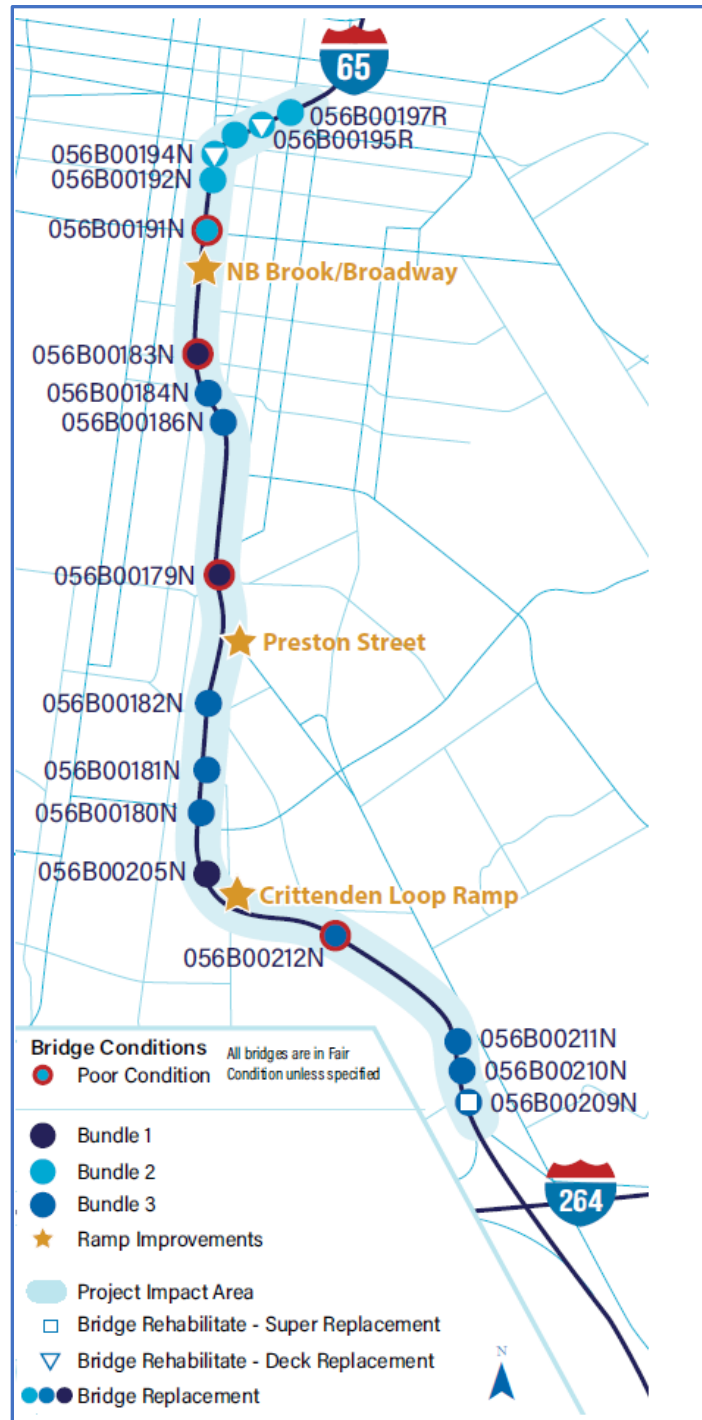


Figure 3. Bridge Bundles

056B00212N, 056B00211N, and 056B00210N.

Design and construction will be phased over the course of approximately three years, beginning in 2024. The total (Design and Construction) estimated cost is \$526,000,000. No residential relocations or commercial displacements would occur.

The three ramp improvements are minor efforts, as follows:

- **Preston Striping** at I-65 northbound (NB) on- ramp will address poor delineation of lanes leading to driver confusion. See **Figure 4**.
- The **NB Brook/Broadway** off ramp will close the through movement of Jacob Street traffic crossing the I-65 NB off-ramp. An existing pedestrian crossing will be relocated to increase safety by reducing conflict points and driver confusion while improving visibility. See **Figure 5**.
- **Crittenden Ramp** improvements will include lengthening the I-65 NB ramp at Crittenden Drive to improve traffic operation by increasing the acceleration lane and lengthening the inadequate merge distance. See **Figure 6**.



Figure 4. Preston Striping Safety Project

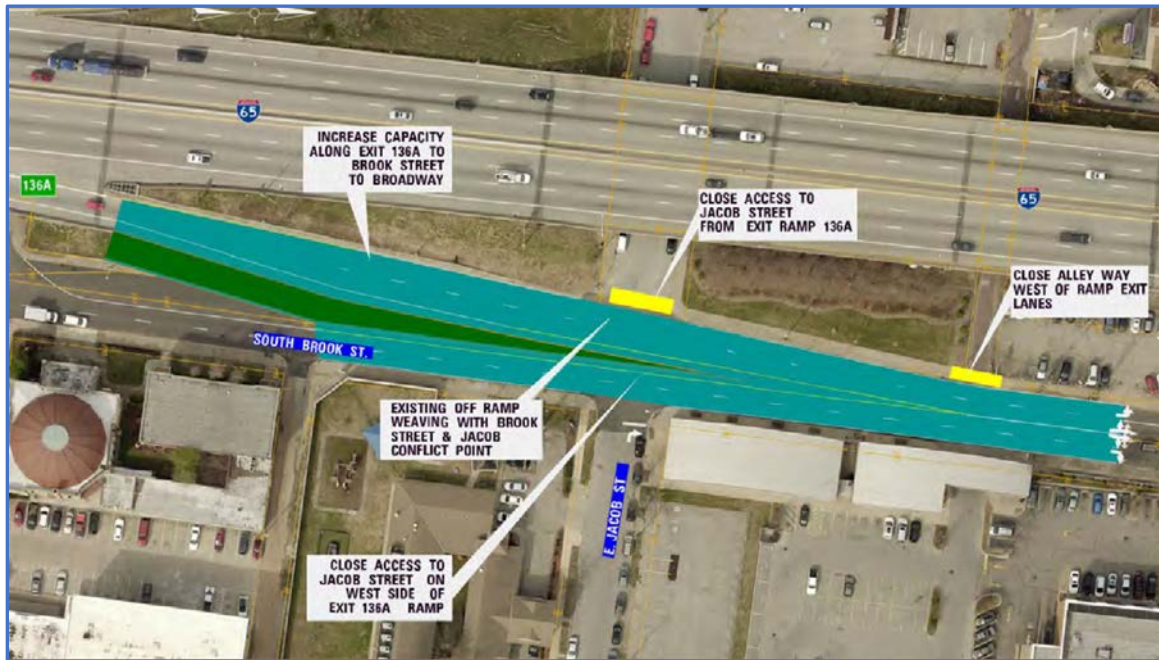


Figure 5. NB Brook/Broadway Safety Project



Figure 6. Crittenden Ramp Safety Project

Community Profile

The Project is located along a densely developed urban corridor that cuts through the heart of downtown Louisville and connects a diverse traffic mix to Louisville Muhammed Ali International Airport just south of the project area. The corridor provides access to the Louisville Central Business District (DBC), the highest concentration of businesses in the Commonwealth. It provides residents of adjacent neighborhoods access to significant local resources, including the seven hospitals near “Hospital Curve” (identified on **Figure 1** as “Health Sciences Center”), more than 20 schools and universities, and more than 40 places of worship. The City of Louisville has two fire stations and four police facilities within a half-mile of the Project area.

The demographics illustrated on **Figure 7** show the area has high concentrations of environmental justice populations—low-income and minority—as compared to Jefferson County. Within 1 mile of the project: 34% of residents are minorities, 20% are disabled, 17% are low-income, as compared to 28%, 14%, and 14% county-wide, respectively according to Census data.

These demographics are supported by observational data such as the presence of public housing, other subsidized housing, senior-living communities, municipal redevelopment efforts, and community resources aimed at supporting EJ communities. Many are illustrated on **Figure 8**. While these communities and the supporting facilities and services will experience temporary impacts, significant or long-term disruptions and loss of service is not anticipated. As described in the Maintenance of Traffic Section below, the closures and disruptions will be staged and short-term to reduce impacts.

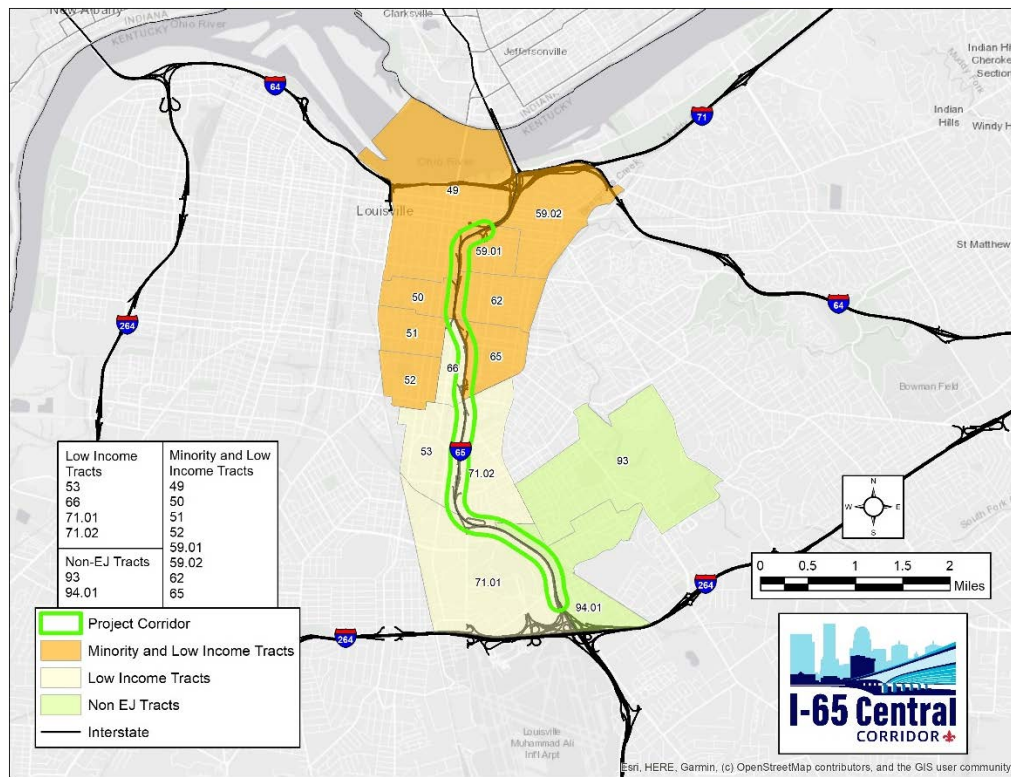


Figure 7. Environmental Justice Census Tracts Adjacent to Project

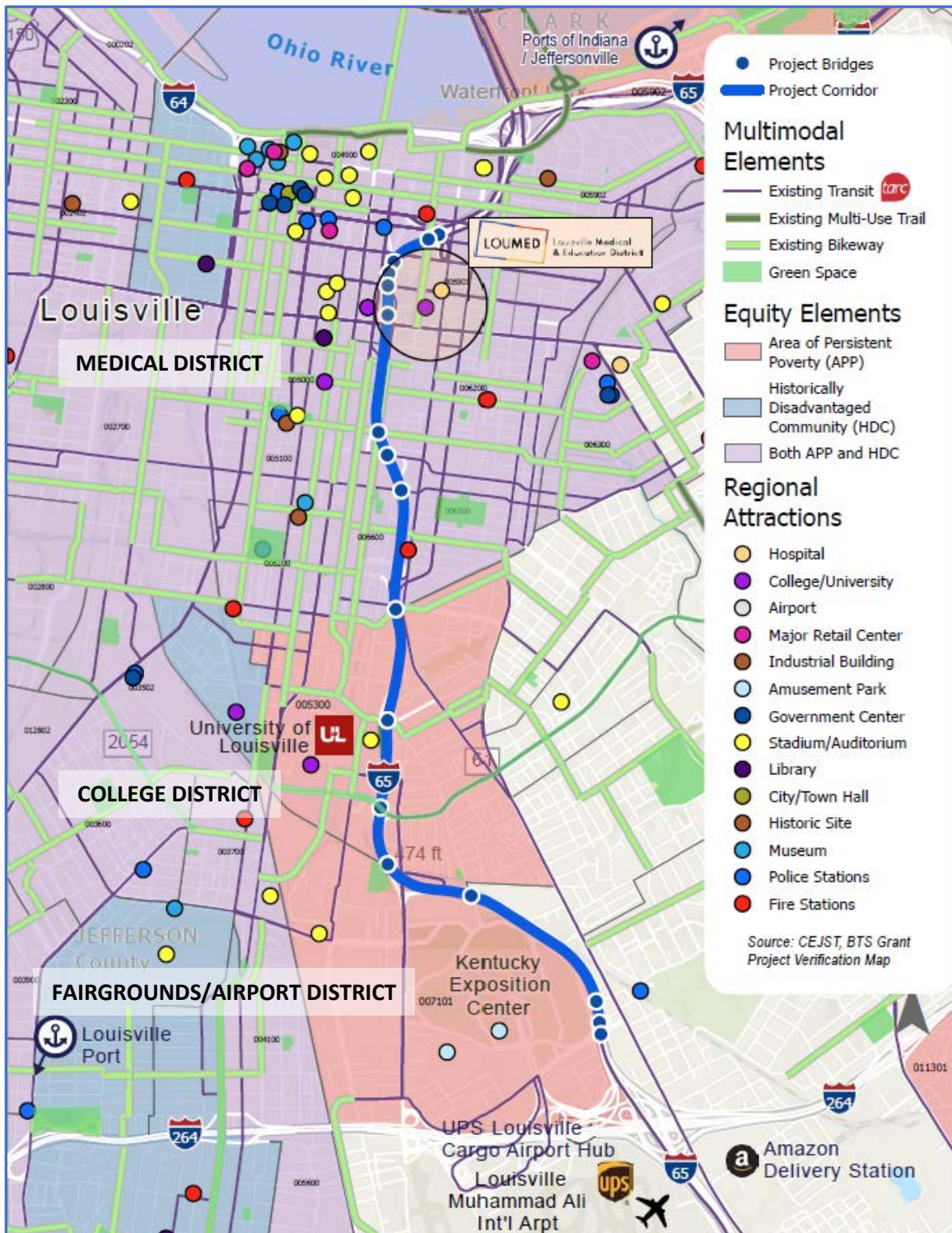


Figure 8. Community Resources in Project Area

Maintenance of Traffic (MOT) Options

One of the primary stated goals for the I-65 bridge project is to “minimize impacts to downtown businesses, major employers, and local destinations by shortening overall construction schedule and limiting lane/roadway closures”. The I-65 Project Team is committed to evaluating all maintenance of traffic alternatives that satisfy the project purpose and need while balancing construction and road user costs.

For full closures, all interstate traffic will be rerouted only onto other interstates. It will not be rerouted onto local streets. However, full closures will be minimal, as two travel lanes will be open to traffic for the majority of the project. Prior to describing the MOT philosophy, it is important to note that this is not the first time KYTC has closed I-65 temporarily for a major rehabilitation effort.

Past Closures of I-65

In 2000 and in 2012, KYTC performed a similar project in terms of MOT. The interstate was shut down in total for a variety of times to rehabilitate bridges and the roadway. The MOT plan, shown in **Figure 9**, routed all interstate traffic onto I-264 on the western side of the city to I-64, then I-265 in Indiana. This part of the City is lower-income and heavily minority, and continues to have a predominately high EJ population.

What is Different in 2023?

A lot has changed in Louisville since 2000. See **Figure 10**. Namely, a new East End River crossing is open, a new downtown interstate bridge is open, and the Kennedy Interchange Complex (KIC), where I-64, I-65, and I-71 merge has been rebuilt. These investments have significantly improved the connectivity of the interstate network, cross river mobility, and the overall interstate vehicle capacity within the city of Louisville. More recently KYTC has invested in the I-Move Kentucky (<https://i-moveky.com/faq/>) project to widen I-265 from KY 155 north to I-71, and I-71 Northeast seven miles, from four lanes to six, and reconstruct the I-265 system-to-system interchanges with I-64 and I-71. This project will be complete in Fall 2024. Concurrently INDOT is rehabilitating the I-64 Sherman Minton Bridge Ohio River Crossing on the west side of Louisville. This project is scheduled to be complete in 2024. (<https://shermanmintonrenewal.com/project-key-points/>). Both of these major investments will be complete prior to the traffic disruption from the I-65 Central Corridor bridges project.

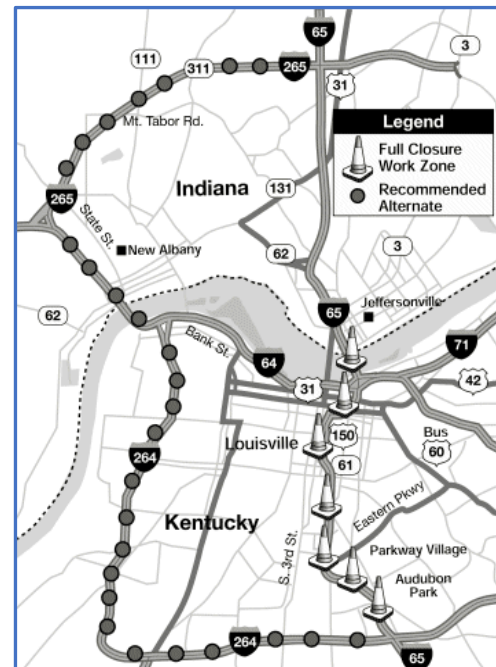


Figure 9. 2000 I-65 Closure MOT Plan

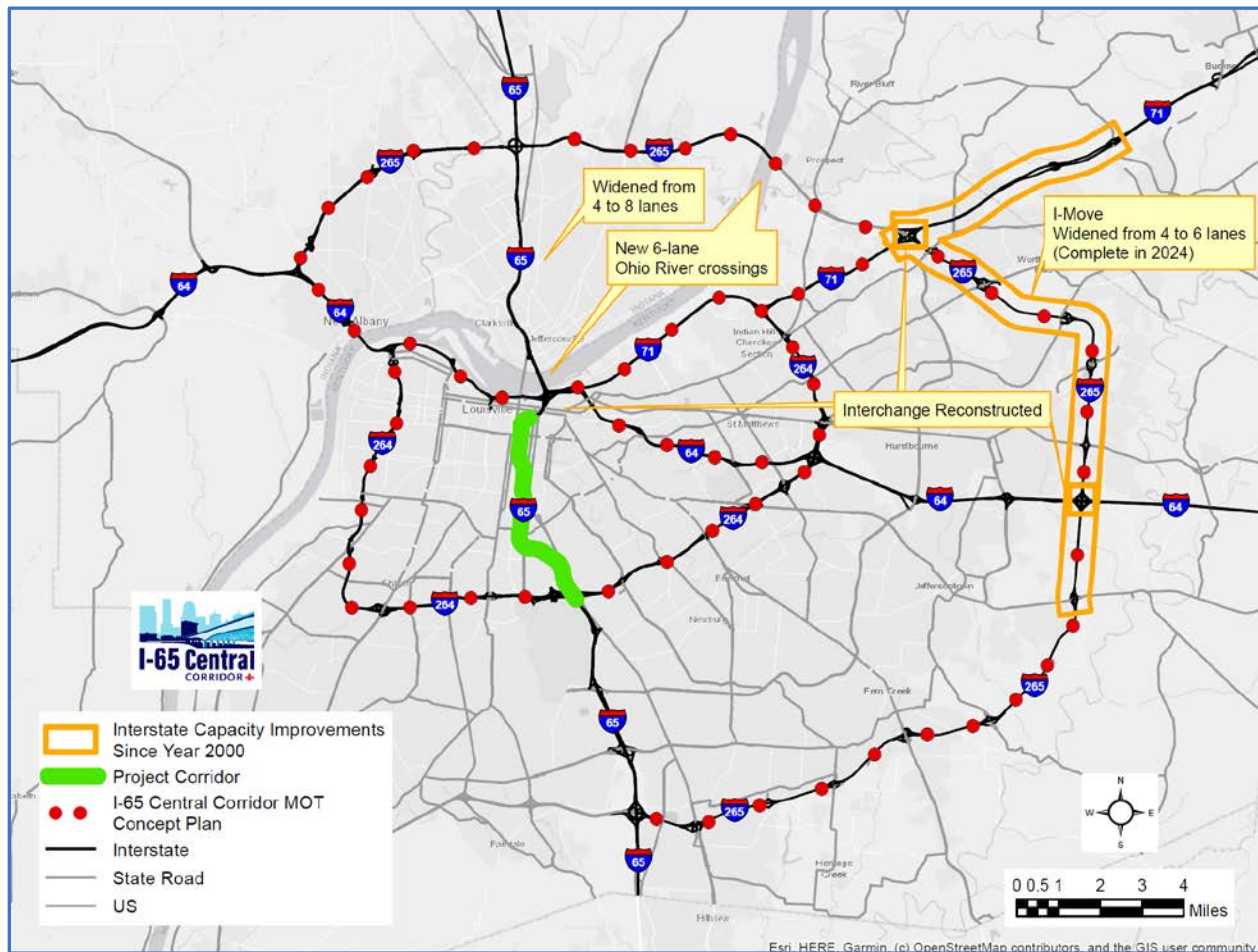


Figure 10. 2023 I-65 Closure MOT Plan

MOT Philosophy for I-65 Central Corridor

As a major traffic corridor through one of the most densely populated sections of Louisville, the project will have temporary MOT impacts that may be of concern to residents, businesses, exposition centers, educational institutions, and KYTC. KYTC's general philosophy will be to coordinate with these affected stakeholders as a key aspect of its overall stakeholder outreach plan to educate and inform stakeholders of project goals, benefits, costs, and impacts, including traffic patterns that can be integrated into the delivery schedule as well as incorporated into the final MOT delivery plan.

KYTC's approach to addressing the MOT requirements for the I-65 Central Corridor will be based on the final detailed scope of each specific bridge and stakeholder coordination. The current condition of the bridge structures will determine the timing means and methods of construction and ultimately the impacts on traffic operations of the corridor. A bridge-specific strategic public awareness plan will be developed based on the options selected and implemented to alert the community prior to any closures.

KYTC will:

- Accelerate the schedule through the use of Accelerated Bridge Construction (ABC) methods to minimize closures and delays to the extent feasible,
- Segment construction to minimize work zone lengths,
- Utilize short-term closures in place of long-term traffic capacity reductions for an overall minimized impact to the traveling public, and
- Phase construction in the various Districts (described below) to allow for cross-interstate (east-west) traffic adjacent to construction zones.

It is anticipated that at least two (2) lanes of traffic in each direction will be maintained at most times during the project. The Project Team will explore the costs and benefits of allowing short-term lane and/or directional road closures along with ABC construction techniques that shorten overall impacts to road users. Given the tight urban location of a couple bridges, plus the current condition of the bridges, it may not be possible for typical phased construction techniques. In those locations, a complete interstate closure, both north and south bound, would be explored with the use of ABC techniques to meet the stated goal of minimizing impacts. As stated above, interstate traffic will only be detoured on other interstates. Specifically, the proposed detour route will use I-64, I-264, and I-265, as illustrated on **Figure 10**.

The MOT options are temporary, ranging from nights, to weeks, and months to complete the various bridge projects. It is assumed that the entire Project will not be conducted under one MOT scenario; rather, as part of KYTC's Construction Manager/General Contractor (CM/GC) procurement, the CM/GC Contractor may choose to further optimize and refine the construction sequence by considering combinations of the options to expedite completion of portions of the construction work.

As part of the needed bridge replacement projects, the MOT will require temporary construction access and closures (ramps, lanes, bridge decks, MOT transitions, and short-term/night) for the work. Overall, while the closure will pose a temporary inconvenience to traveling motorists, and all inconveniences will cease upon project completion.

MOT By Bridge Bundles & Districts

As shown in **Figure 8**, there are 3 distinct districts within the I-65 Central Corridor, the Medical District (Bundle 2), the College District, and the Fairgrounds/Airport District.

Medical District MOT Approach

Due to the proximity of the University of Louisville Health / Jewish Hospital, Norton Healthcare Hospital, and the associated medical-service facilities, access to this section of the corridor will be a consideration to ensure emergency services are maintained and impacts to medical care are minimized. This district also has the most bridge construction with nine of the proposed 18 bridges.

In addition to the healthcare providers, this district is the entryway to the downtown business center, and the Kentucky International Convention Center, and has important cross-interstate connections for

the residents of Old Louisville and residents in the neighborhoods east of I-65. KYTC will engage these groups through a planned series of public outreach events, one-on-one meetings, and small group meetings to gain specific traffic concerns and considerations. These concerns will be considered in the development of the final MOT arrangement for the construction of the bridges in this district. MOT options for this section of the project include a 3-phase approach to shifting traffic in 2 lanes in each direction, a 2-phase scenario with traffic in two lanes in one direction and one lane in the opposite direction, or a full closure of I-65 for a multiple week duration to construct one phase of construction which would accommodate the open flow of 2 lanes in each direction. All options would require full or partial closure of the cross streets which will be determined in advance of construction in collaboration with the stakeholders. KYTC, through coordination with local stakeholders will develop strategies on which cross streets to close and when, so to provide adequate options for cross-interstate mobility within the District.

College District MOT Approach

The College District includes five of the 18 bridge locations. The connectivity between the educational facilities and student housing on either side of the interstate will be a primary consideration from a MOT standpoint. It is expected that a main consideration will be cross interstate connectivity during construction including vehicle and pedestrian detour routes.

KYTC will consider sequential phasing or other means to address this known concern. Public outreach will include the University of Louisville, Spalding University, Jefferson Community & Technical College, neighborhood associations, and businesses in the area. MOT options for this section include phased construction to include crossing all traffic to one side of I-65 to maintain two lanes in each direction or applying an ABC method approach of short full closures of I-65 (weekends). Both options will require full and partial closures of the cross-street locations which will be determined in advance of construction in collaboration with stakeholders.

Fairgrounds/Airport District MOT Approach

The Fairgrounds/Airport District includes four of the 18 bridges. All of these structures are adjacent to the Fairgrounds at the Kentucky Exposition Center. The connectivity to the Fairgrounds during major events and generally restricted access to the airport will be the primary concern from an MOT perspective. KYTC will engage the adjacent stakeholders and develop a calendar of major events that will be used to develop the final MOT approach.

MOT options for this section include phased construction to include crossing all traffic to one side of I-65 to maintain two lanes in each direction or applying an ABC method approach of short full closures of I-65 (weekends) to construct the bridges. In both options full and partial closures of the cross streets will be required, which will be determined in advance of construction in collaboration with stakeholders.

Public Involvement Effort

Public engagement is a core component of KYTC's project development process. For the I-65 Bridge Bundling Project, KYTC has engaged and will continue to engage key community stakeholders during the design phase and through construction.

KYTC has already begun outreach with Louisville Metro Government and will continue to work closely with the city, and organizations like TRIMARC and TARC, throughout the duration of the project. KYTC has also been reaching out to various stakeholders and elected officials to inform them about the project goals.

Collaboration with stakeholders and the public will continue as the project progresses. Small meetings will be held with major stakeholders, such as local elected officials, hospitals, and universities. Key outreach goals will include informing stakeholders about the project, its expected timeline, and any potential impacts. The project team will also collect feedback to consider during final design.

Project information, including project updates, will be available on the project website to help keep the general public informed. Lane closures and maintenance of traffic information will also be posted online during construction. The project website will also include a corridor map with detailed information about each of the 18 bridges.

In addition to the project website, project social media channels will be launched to share important project news, construction updates, and any traffic impacts. Updates will also be shared electronically, through text and email alerts, to directly communicate with stakeholders. Translations of all project materials will be available for non-English speakers.

An enhanced Public Involvement Plan has been developed that is specific to the project and directs engagement activities through construction. Special emphasis will be placed on reaching traditionally underserved communities through outreach events including pop-up events at groceries, bus route visits and walking tours with community organizations, driving engagement with members of the public who may not attend traditional public meetings or may not be reached through channels such as social media.

Public involvement, while key to the overall success of any transportation project, is an essential element of the process. Public involvement provides an opportunity for the full spectrum of public participation, from individuals, organizations, businesses, neighborhoods, and communities, to local, state, and federal officials. All participants are afforded an opportunity to provide input on project related concerns, alternatives, and solutions.

In addition to the traffic analysis, a project specific public involvement plan has been developed to aid the Project Team in making decisions and advising the community and stakeholders. Public engagement is a core component of KYTC's project development process. For the **I-65 Bridge Bundling Project**, KYTC has engaged and will continue to engage key community stakeholders during the design phase and into construction. Public outreach will be conducted in accordance with [KYTC's Public Involvement Plan](#).

Environmental Justice-Focused Engagement

As the design is finalized and construction begins, neighborhood community engagement will continue to be an important aspect of the project. While the public involvement for the project will adhere to KYTC's Public Involvement Plan, an enhanced Public Involvement Plan specific to the project will be developed to direct engagement activities through construction. Special emphasis will be placed on reaching traditionally underserved communities.

Targeting these communities will require additional efforts that include translated materials for non-English speakers and non-electronic communication for those without internet access. Supplementary efforts will include:

- Meetings with community leaders and organizations serving underserved communities
- Coordination with shelters, food banks, and groups like the Louisville Homeless Coalition that serve the homeless population and distribution of project tool kits containing project information
- Coordination with groups that serve non-English populations to ensure project information reaches those communities
- Placement of project information and displays, including documents in languages other than English, at local libraries and community centers in the surrounding neighborhoods

Community outreach like pop-up events at groceries, bus route visits and walking tours with community organizations will also be held. This will allow KYTC to interact with members of the public who may not attend traditional public meetings or may not be reached through channels such as social media.

These events will also allow for the team to share project information and the project timeline. They will be able to discuss needs such as the distinct challenges faced by homeless populations, communities in the denser northern section of the project corridor, and communities in the southern portion.

The project corridor is surrounded by deep-seated communities, several having established neighborhood associations. KYTC will meet with these organizations along with individual groups to discuss the bridge projects relevant to those neighborhoods, including reasons residents cross under the interstate, traffic detour concerns, and any other concerns they may bring up.

Public Transit

The Transit Authority of River City (TARC) is the federally designated Tier 1 transit provider serving Louisville. There are nine different TARC routes that use the surface streets below I-65 (23, 99, 93, 52, 21, 19, 28, 31, and 15). Transit lines that use this portion of I-65 include route 17X Bardstown Road, UPS Shuttle routes 93 UofL-JCTC and 99 West Louisville, and Route 28 (Preston). 72.6% of multimodal public transport trips on the corridor are taken via Route 28.

Early coordination with TARC representatives has indicated that while the MOT would have some effect on their operations, additional coordination would allow for service adjustments to ensure continued service for their ridership. Overall, because of the Louisville Metro area's robust roadway network, TARC riders in the area are not anticipated to experience notable travel time delays during construction.

Community and EJ Impacts

Temporary MOT options can translate to community and EJ impacts through increased traffic on local roads; increased traffic noise for communities adjacent to detour routes; changes in accessibility to businesses, jobs, schools, community facilities, goods, and services; and potential loss of business revenue as a result of changes. The following evaluation factors were used in the community and EJ evaluations:

EJ impacts were evaluated per FHWA Order 6640.23A (5)(g) that defines a "Disproportionately High and Adverse Effect on Minority and Low-Income Populations" as "an adverse effect that (1) is predominately borne by a minority population and/or a low-income population; or (2) will be suffered by the minority population and/or low-income population and is appreciably more severe or greater in magnitude than the adverse effect that will be suffered by the nonminority population and/or non-low-income population."

All of the MOT options can be expected to have some degree of EJ impacts, since each census tract is considered to have a higher percentage of low-income, minority, or both, as compared to the rest of Jefferson County. The type of impacts anticipated include the inability to cross under I-65 on cross-streets during construction, diversion of traffic from surface streets onto other surface streets, or the temporary loss of business activity by diverted traffic. However, the overall changes are anticipated to be minor, as the various closures will be staged, interstate traffic routed to other interstate(s) (not local roads), and of cross-interstate surface traffic maintained overall, even if temporarily rerouted. Because downtown Louisville is on a grid network, east-west travel will have multiple options while certain underpasses are temporarily closed.

Because there would be no relocations, the closures will be minimal, phased, temporary, and the MOT will sign traffic to other available options, it is the conclusion that there will not be an overall adverse effect. While the effects of cross-interstate mobility would be disproportionate since the temporary inconvenience would be predominately borne by low-income and/or minority populations; it would not be adverse and it would be temporary. The public involvement plan would be geared to identifying methods to minimize impacts to these communities.

AVOIDANCE, MINIMIZATION, AND MITIGATION MEASURES

The following section includes examples of potential avoidance, minimization, and mitigation measures for consideration:

- Avoidance measures are limited, given the nature of the Project:
 - No property acquisitions or new Right-of-Way (ROW)

- No relocations or setback buffers
 - No increase in capacity (no added travel or access ramp lanes)
 - Rehabilitation of existing structures
- Coordination with local officials regarding potential policy and operational adjustments to local streets in downtown Louisville, that could include:
 - Signal timing
 - Limiting left turns during peak hour
 - Temporary conversion of street parking to travel lanes
 - Temporary conversion of one-way to two-way streets; or two-way to one-way streets
- Development of Project specific communication measures and public outreach such as:
 - Advance communication throughout construction
 - Construction activity schedules
 - Interactive website
 - Social media information
 - Realtime notification for incident and emergency management
- Intelligent transportation system (ITS) enhancements
- Additional traffic operation and safety measures:
 - Signage for lane shift, merging, and work zone areas
 - Traffic monitoring cameras
 - Real-time information and alternate recommendations
- Public transportation (TARC) coordination and outreach:
 - Encourage use of public transit
 - Route and operations adjustments to maintain service
- Considerations regarding heavy truck traffic:
 - Official truck detour and/or complete prohibition of heavy trucks during construction to improve traffic operations through the work zone
 - Local municipal action(s) regarding truck restrictions to reduce/prohibit/discourage heavy truck diversions on the local street network.
- Duration considerations:
 - Overnight
 - Weekends
 - Two-weeks

- 1-month
 - Other?
- Traffic management plans (both motorized and non-motorized traffic) prepared by the contractor in conjunction with KYTC, TRIMARC, and Louisville Metro.
 - Demonstrate how safe access will be provided
 - Identify closure times and locations
 - Specify notification and approval process

KYTC will continue public outreach in areas of construction to advise local officials, transit representatives, neighborhoods, businesses, and the traveling public of planned activities throughout the Project and the return of I-65 to full service.

SPECIAL NOTE

For Relocating Unhoused Populations Residing Beneath Bridges Scheduled for Repairs/Replacement

Owing to the presence of unhoused population residing beneath several structures scheduled for repairs/replacement, the following measures must be taken **prior to** initiating any construction:

- The Contractor will advise the KYTC Project Manager of the proposed construction schedules and locations where relocations will be required.
- KYTC will contact Louisville Office of Resilience and Community Services Homeless Services Division [**Jon Pilbean**, jon.pilbean@louisvilleky.gov (as of Feb 2024)] at **(502) 574-6967 (office)** or **502-377-3884 (cell)** regarding coordinating with resource agencies the relocation of unhoused populations under bridges scheduled for repairs/replacement.
- Louisville Metro will publish and enforce the City's Houseless Encampment Law requiring affected unhoused populations be given notice of the relocation **21 days** prior to the relocation date. The Contractor will adhere to the ordinance's requirements. After the 21-day notice expires, Louisville Metro will no longer be involved in the process. Resource agencies and volunteers will continue to monitor.

If there are any questions regarding this note, please contact Royce Meredith, PE, KYTC Project Manager, at Royce.Meredith@ky.gov.

Public Involvement Effort

Public engagement is a core component of KYTC's project development process. For the I-65 Bridge Bundling Project, KYTC has engaged and will continue to engage key community stakeholders during the design phase and through construction.

KYTC has already begun outreach with Louisville Metro Government and will continue to work closely with the city, and organizations like TRIMARC and TARC, throughout the duration of the project. KYTC has also been reaching out to various stakeholders and elected officials to inform them about the project goals.

Collaboration with stakeholders and the public will continue as the project progresses. Small meetings will be held with major stakeholders, such as local elected officials, hospitals, and universities. Key outreach goals will include informing stakeholders about the project, its expected timeline, and any potential impacts. The project team will also collect feedback to consider during final design.

Project information, including project updates, will be available on the project website to help keep the general public informed. Lane closures and maintenance of traffic information will also be posted online during construction. The project website will also include a corridor map with detailed information about each of the 18 bridges.

In addition to the project website, project social media channels will be launched to share important project news, construction updates, and any traffic impacts. Updates will also be shared electronically, through text and email alerts, to directly communicate with stakeholders. Translations of all project materials will be available for non-English speakers.

An enhanced Public Involvement Plan has been developed that is specific to the project and directs engagement activities through construction. Special emphasis will be placed on reaching traditionally underserved communities through outreach events including pop-up events at groceries, bus route visits and walking tours with community organizations, driving engagement with members of the public who may not attend traditional public meetings or may not be reached through channels such as social media.

Public involvement, while key to the overall success of any transportation project, is an essential element of the process. Public involvement provides an opportunity for the full spectrum of public participation, from individuals, organizations, businesses, neighborhoods, and communities, to local, state, and federal officials. All participants are afforded an opportunity to provide input on project related concerns, alternatives, and solutions.

In addition to the traffic analysis, this project specific public involvement plan has been developed to aid the Project Team in making decisions and advising the community and stakeholders. Public engagement is a core component of KYTC's project development process. For the **I-65 Bridge Bundling Project**, KYTC has engaged and will continue to engage key community stakeholders during the design phase and into construction. Public outreach will be conducted in accordance with [KYTC's Public Involvement Plan](#).

Environmental Justice-Focused Engagement

As the design is finalized and construction begins, neighborhood community engagement will continue to be an important aspect of the project. While the public involvement for the project will adhere to KYTC's Public Involvement Plan, once the exact details are known regarding road closures, delays, and construction sequencing, an enhanced Public Involvement Plan specific to the project will be developed to direct engagement activities through construction. Special emphasis will be placed on reaching traditionally underserved communities.

Targeting these communities will require additional efforts that include translated materials for non-English speakers and non-electronic communication for those without internet access. Supplementary efforts will include:

- Meetings with community leaders and organizations serving underserved communities
- Coordination with shelters, food banks, and groups like the Louisville Homeless Coalition that serve the homeless population and distribution of project tool kits containing project information
- Coordination with groups that serve non-English populations to ensure project information reaches those communities
- Placement of project information and displays, including documents in languages other than English, at local libraries and community centers in the surrounding neighborhoods

Community outreach like pop-up events at groceries, bus route visits and walking tours with community organizations will also be held. This will allow KYTC to interact with members of the public who may not attend traditional public meetings or may not be reached through channels such as social media.

These events will also allow for the team to share project information and the project timeline. They will be able to discuss needs such as the distinct challenges faced by homeless populations, communities in the denser northern section of the project corridor, and communities in the southern portion.

The project corridor is surrounded by deep-seated communities, several having established neighborhood associations. KYTC will meet with these organizations along with individual groups to discuss the bridge projects relevant to those neighborhoods, including reasons residents cross under the interstate, traffic detour concerns, and any other concerns they may bring up.

I-65 Central Corridor Bridge Replacement Project MSAT Analysis

The purpose of this project is to address the deterioration of structural elements of the 18 bridges and improve safety at areas of concern. The goal of the bridge improvements is to extend the service life of the I-65 Interstate bridges by up to 70 years by reconstructing each bridge. The goal of the minor safety improvements is to reduce crashes by implementing minor restriping and ramp extensions at areas with known high crash rates. This project has been determined to generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special mobile source air toxic (MSAT) concerns. As such, this project will not result in changes in traffic volumes, vehicle mix, basic project location, or any other factor that would cause a meaningful increase in MSAT impacts of the project from that of the no-build alternative.

Moreover, Environmental Protection Agency (EPA) regulations for vehicle engines and fuels will cause overall MSAT emissions to decline significantly over the next several decades. Based on regulations now in effect, an analysis of national trends with EPA's MOVES3 model forecasts a combined reduction of over 76 percent in the total annual emissions rate for the priority MSAT from 2020 to 2060 while vehicle-miles of travel are projected to increase by 31 percent (Updated Interim Guidance on Mobile Source Air Toxic Analysis in NEPA Documents, Federal Highway Administration, January 18, 2023). This will both reduce the background level of MSAT as well as the possibility of even minor MSAT emissions from this project.