KENTUCKY INDUSTRIAL ACCESS AND SAFETY IMPROVEMENT (KIASI) PROJECT APPLICATION

SECTION 1: PROJECT OVERVIEW

PROJECT TITLE	
Project Alta	
APPLICANT LEGAL NAME	**APPLICATION YEAR**
Wieland North America Recycling, LLC	2024
APPLICANT TYPE	**COUNTIES IMPACTED BY THE PROJECT**
Rail Served Industry	Shelby County
PROJECT PHYSICAL ADDRESS	
369 McConnell Way, Shelbyville, KY 40065	
RAILROAD(S) SERVING THE SITE	**ENTITY OWNING PROJECT SITE** (if different from applicant)
Norfolk Southern - Class 1	

TOTAL PROJECT COST	KIASI FUNDING REQUESTED	APPLICANT MATCH	APPLICANT MATCH % *(50% Minimum)*
$ 4,186,921	$ 1,875,000	$ 2,311,921	55 %

Will the proposed project be matching awarded federal funds? ☒ NO ☐ YES

Does applicant plan to use their own manpower, equipment, or materials on the project (Force Account) or competitively bid out all work related to the project? Bid Out

DESCRIPTION OF PROPOSED PROJECT *(Provide a brief project description and the proposed work to be completed. Text is limited to the space provided below.)*

The project will construct rail infrastructure connecting Norfolk Southern's (NS) existing Class 1 rail line to Wieland's facility at 369 McConnell Way, Shelbyville, KY 40065. This strategic rail connection will enable direct loading and unloading of bulk materials, significantly reducing transportation costs and truck traffic on local roads. Rail access is also critical to support a potential major expansion project at the Shelbyville facility that Wieland is in negotiations with the Department of Energy on. The rail infrastructure will strengthen Wieland's supply chain connectivity and enhance operational efficiency.

DESCRIPTION OF HOW PROJECT WOULD PROVIDE KENTUCKY COMMUNITIES AND INDUSTRIES WITH TRANSPORTATION OPTIONS, CONNECTIVITY AND OPPORTUNITIES *(Text is limited to the space provided below.)*

The proposed rail infrastructure project will enhance Kentucky's transportation network by providing critical rail connectivity to a major manufacturing facility in Shelbyville. This investment strengthens the Commonwealth's industrial infrastructure while reducing heavy truck traffic on local roads. The project supports Kentucky's growing manufacturing sector by improving freight transportation options and efficiency, which helps create and retain quality jobs while providing sustainable transportation options that support long-term economic growth in Shelbyville and surrounding communities.

DESCRIPTION OF HOW PROJECT WOULD ENHANCE RAIL LINE CORRIDORS TO INCREASE ON-TIME PERFORMANCE *(Text is limited to the space provided below.)*

The proposed rail infrastructure project will improve freight movement efficiency by providing direct rail access to a major manufacturing facility adjacent to NS's existing line. This dedicated industrial spur will enable efficient loading and unloading operations, reducing potential congestion on the main line and enhancing schedule reliability. The project's modern rail infrastructure design will ensure seamless integration with NS's operations, optimizing train movements throughout the corridor.

DESCRIPTION OF HOW PROJECT WOULD IMPROVE RAIL SERVICES TO EXISTING INDUSTRIES AND ENCOURAGE INVESTMENT IN THE COMMONWEALTH *(Text is limited to the space provided below.)*

The project would demonstrate Kentucky's commitment to providing robust transportation options for its existing industries. This strategic investment signals to other industries that Kentucky supports business growth through rail connectivity, potentially spurring follow-on investment in the region and helping existing businesses expand operations and improve efficiency. By enhancing the area's transportation infrastructure, the project strengthens Kentucky's competitive position for attracting new manufacturing and industrial development while supporting the growth of established businesses.

KENTUCKY INDUSTRIAL ACCESS AND SAFETY IMPROVEMENT (KIASI) PROJECT APPLICATION

SECTION 1: PROJECT OVERVIEW (CONTINUED)

DESCRIPTION OF PROPOSED PROJECT READINESS, OR HOW SOON AFTER AWARD CAN CONSTRUCTION BEGIN AND HOW LONG WILL THE PROJECT TAKE TO COMPLETE *(Text is limited to the space provided below.)*

The project is well-positioned for preliminary implementation upon award. Design and engineering work can begin promptly, with construction scheduled to begin in Q2 2027. The project's location adjacent to NS's existing rail line minimizes complexity, and the straightforward nature of the industrial spur design allows for efficient execution. With proper coordination between stakeholders, including NS and local authorities, construction will take ~7 months, targeting completion by October 2027. The project team will ensure necessary permits/agreements are secured in advance to prevent delays and maintain the construction timeline.

SECTION 2: CONTACT INFORMATION

APPLICATION PRIMARY CONTACT NAME & TITLE	PHONE	EMAIL	
Phil Moody	419 340 9332	phil.moody@wieland.com	
MAILING ADDRESS	**CITY**	**STATE**	**ZIP**
4803 Olympia Park Plz, Suite 3000	Louisville	KY	40241

If awarded, will signatory be different from the Primary Contact? ☐ NO ☒ YES *(Provide signatory information.)*

AGREEMENT SIGNATORY NAME & TITLE	PHONE	EMAIL	
Maclin Simpson	859 338 7229	maclin.simpson@wieland.com	
MAILING ADDRESS	**CITY**	**STATE**	**ZIP**
4803 Olympia Park Plz, Suite 3000	Louisville	KY	40241

If awarded, will Project Manager be different from the Primary Contact? ☐ NO ☒ YES *(Provide information.)*

PROJECT MANAGER NAME & TITLE	PHONE	EMAIL
Maclin Simpson	859 338 7229	maclin.simpson@wieland.com

SECTION 3: PROPOSED PROJECT PERMITS/APPROVALS/READINESS

A. Have consultations with state or federal agencies (US Army Corps of Engineers, US Coast Guard, US Fish and Wildlife Service, Kentucky Division of Water, Kentucky Heritage Council, or others) determined the need for permits? Yes

B. Have all required permits been obtained? In Progress

C. Will the proposed project have ANY impacts on a public road (City, County, State, US)? ☒ NO ☐ YES

D. Have all necessary roadway authorities been notified about the proposed project? ☐ YES ☐ NO ☒ N/A

E. Has preliminary engineering been completed for the proposed project? ☒ YES ☐ NO ☐ N/A

What engineering still needs to be done and when will it be completed? Much of the initial engineering and design for Project Alta has been done. However, there are still considerations to be made before finalization, including the

SECTION 4: ECONOMIC DEVELOPMENT INFORMATION

A. If the project will provide new and/or increased service to an industrial park:

Total acres of the industrial park	Total amount of developable acres	Acres that may be served by project

KENTUCKY INDUSTRIAL ACCESS AND SAFETY IMPROVEMENT (KIASI) PROJECT APPLICATION

SECTION 4: ECONOMIC DEVELOPMENT INFORMATION (CONTINUED)

B. Utility infrastructure in place to the site to be served by the project:

☒ Electric ☒ Natural Gas ☒ Water ☒ Wastewater ☒ Fiber/telecom

C. Provide listing of the names of existing companies currently served/to be served by the project along with the number of existing full-time jobs, potential new full-time jobs to be created, and potential capital investment to be incurred for each company.

	COMPANY NAME	FULL TIME JOBS		CAPITAL
		CURRENT	POTENTIAL	INVESTMENT
1	Wieland North America Recycling, LLC	68	200	$4,186,921
2				$
3				$
4				$
5				$
6				$
7				$
8				$
9				$
10				$

D. Are there any companies considering location or expansion of a potential project on the site to be served?

☒ YES ☐ NO ☐ N/A

If YES, provide details of the potential project for each company (e.g., potential new full-time jobs to be created and potential capital investment to be incurred). Wieland is currently in negotiations with the United States Department of Energy (DOE) on a 50% federal cost-share grant through the Industrial Demonstrations Program. If Wieland is to go through with the project and sign a grant agreement with the DOE, Wieland would commence a multi-hundred-million-dollar expansion project at its Shelbyville site. Project Alta's budget, schedule, and design reflect project development for Wieland's current site only and could change, should Wieland sign its grant agreement with the United States Department of Energy for site expansion. Wieland will have to enter into the NEPA process and receive a corresponding NEPA determination which would halt site activities and potentially

SECTION 5: SUBMISSION CHECKLIST *(See KIASI Guidance Document, Section VI, for details.)*

- ☒ Kentucky Industrial Access and Safety Improvement (KIASI) Project Application (TC 59-115)
- ☒ Statement of Work
- ☒ Scope of Work
- ☒ Project Schedule/Timeline
- ☒ Diagrams/maps depicting proposed project
- ☒ Rail Connectivity Letter (as applicable)
- ☐ Detailed engineering assessment report (as applicable)
- ☒ Aerial Photographs and photographic documentation of crossing location and condition
- ☒ Plans, schematics, details, drawings of the proposed project (as applicable)
- ☐ For equipment purchases, a price quote on letterhead from vendor OR for construction projects, a detailed estimate for the project
- ☐ Road authority consultation letters (as applicable)
- ☐ Public Interest Finding (as applicable)

KENTUCKY INDUSTRIAL ACCESS AND SAFETY IMPROVEMENT (KIASI) PROJECT APPLICATION

☒ Required Annual Affidavit for Bidders, Offerors and Contractors from applicant.

KENTUCKY INDUSTRIAL ACCESS AND SAFETY IMPROVEMENT (KIASI) PROJECT APPLICATION

SECTION 6: KENTUCKY RAILROAD ANNUAL REPORT COMPLIANCE (Required ***ONLY*** if applicant is a railroad.)

I hereby certify that as an applicant defined as a railroad in 603 KAR 7:090(1), my company has completed and submitted TC 59-102, *Kentucky Railroad Annual Report*, in compliance with the provisions of 603 KAR 7:090 at the time of this application to be considered an eligible applicant. If it is determined I am not an eligible applicant at the time of this submission, I agree that this application shall be immediately rejected without consideration and returned to me without review.

PRINTED NAME AND TITLE	SIGNATURE	DATE

SECTION 7: BUSINESS STANDING CERTIFICATION

I hereby certify that the applicant is a business entity in good standing with the Office of the Kentucky Secretary of State or under the laws of the jurisdiction(s) in which the entity is organized or authorized to conduct business and is not delinquent in taxes owed to any taxing entity. Applicants not in good standing or delinquent in taxes are subject to this application immediately being rejected without consideration and returned to me without further review.

PRINTED NAME AND TITLE	SIGNATURE	DATE
Maclin Simpson		12/02/2024

SECTION 8: APPLICANT CERTIFICATION

I have read the Kentucky Industrial Access and Safety Improvement (KIASI) Projects guidance document, and I understand and agree to abide by what is stated therein. I also hereby certify, subject to the provisions of KRS 523.100 (unsworn falsification to authorities), that the above information is true and correct to the best of my knowledge.

PRINTED NAME AND TITLE	SIGNATURE	DATE
Maclin Simpson		12/02/2024

KENTUCKY INDUSTRIAL ACCESS AND SAFETY IMPROVEMENT (KIASI) PROJECT APPLICATION

Submission Directions: Applicants must combine their completed application and all required attachments into a single PDF and submit it electronically via email to address provided in call for projects. It is the responsibility of the applicant to ensure delivery of the emailed submission.

Statement of Work – Project Alta

Project Purpose

Project Alta seeks to strengthen Kentucky's industrial rail infrastructure through the development of a rail spur at Wieland North America Recycling's Shelbyville facility. The following objectives outline the project's comprehensive approach to improving Kentucky's rail network while delivering tangible benefits to our local communities and businesses:

- **This project will establish critical rail connectivity for a major manufacturing facility in Shelbyville, KY through construction of a new industrial spur to Norfolk Southern's Class 1 line.** The connection will serve Wieland North America Recycling's facility at 369 McConnell Way, Shelbyville, KY 40065, enabling efficient bulk material transport while reducing regional truck traffic.
- **Project Alta would potentially support a major expansion at the project site, as Wieland is currently in negotiations with the United States Department of Energy (DOE) on a 50% federal cost-share grant through the Industrial Demonstrations Program**. If Wieland is to go through with the project and sign a grant agreement with the DOE, Wieland would commence a multi-hundred-million-dollar expansion project at its Shelbyville site that would support an additional 130 jobs. However, Project Alta's budget, schedule, and design only reflect project development for Wieland's current site only and could change, should Wieland sign its grant agreement with the United States Department of Energy for site expansion.
- **The rail spur will significantly reduce heavy truck traffic on local roads and highways, reducing the emissions associated with Wieland's supply chain considerably.** At current peak production capacity of (tens of thousands of metric tons), the facility requires approximately 1,200+ long-haul truck trips annually, generating nearly 2,000 metric tons of CO2 emissions. At future peak production, the expanded capacity and greater use of rail transport will reduce annual long-haul truck trips, substantially lower carbon emissions, enhance safety for residents, and reduce road maintenance costs.
- **Modern rail infrastructure will enhance the efficiency of freight movement throughout the region.** The project's design ensures seamless integration with Norfolk Southern's operations while optimizing material handling capabilities for Wieland.
- **Project Alta strengthens Kentucky's competitive position by enhancing rail infrastructure**, demonstrating the Commonwealth's commitment to industrial growth while establishing a model for future transportation development projects that will continue to attract and support manufacturing operations.

Project Scope

The rail infrastructure development project encompasses comprehensive design, engineering, permitting, and construction activities to establish safe and efficient rail service. The following components outline the project's full scope of work and technical requirements:

- **Construction industrial spur track connecting to Norfolk Southern's main line.** This track will be built to Class 1 railroad standards with appropriate turnouts, switches, and rail components to ensure seamless integration with existing operations and long-term durability.

- **Construct purpose-built loading and unloading areas with dedicated track infrastructure.** These facilities will include reinforced concrete pads, proper drainage systems, and all necessary equipment foundations to support efficient material handling operations.
- **Conduct site preparation work including earthwork, grading, and drainage improvements.** These modifications will ensure proper water management, erosion control, and long-term stability of the rail infrastructure while meeting all environmental requirements.
- **Modern safety and signaling systems will be installed to Norfolk Southern specifications.** This includes all required crossing protection, communication equipment, and operational safety features to protect both rail and facility operations.
- **Access road modifications will accommodate safe interaction between rail and vehicular traffic.** New paving, proper signage, and traffic control measures will be implemented to ensure efficient site circulation and operational safety.
- **The project includes all necessary utility relocations and protection measures.** This encompasses modification or replacement of affected infrastructure while maintaining service to existing facilities.
- **Obtain all necessary state and local permits.** The project team will secure all necessary approvals and ensure compliance throughout construction.
- **Final testing and commissioning will verify all systems meet operational requirements.** This includes comprehensive inspection and certification of all infrastructure components before handover to operations.

Rail System Enhancement

This project will strengthen Kentucky's rail infrastructure by:

Modernizing Rail Infrastructure	- Installing new, high-quality rail components meeting current industry standards - Implementing modern safety and operational systems - Enhancing the capacity and capabilities of the existing rail corridor
Preserving Rail Service	- Maintaining and improving the viability of existing rail lines - Supporting continued rail operations in the region - Ensuring long-term sustainability of rail infrastructure
Supporting Industrial Growth	- Providing essential rail access to a major manufacturing facility - Creating potential for additional rail-served development on adjacent properties - Demonstrating Kentucky's commitment to rail-served industrial development
Catalyzing Regional Development	- Creating opportunities for follow-on investment in Shelby County's industrial corridor - Enhancing the region's attractiveness for new manufacturing and logistics operations - Strengthening the local supply chain network through improved transportation infrastructure

Advancing Economic Resilience	- Diversifying transportation options for existing and future businesses - Supporting the growth of advanced manufacturing in the region - Building long-term economic stability through strategic infrastructure investment

Economic Benefits

The project will generate significant economic advantages by:

- **The rail infrastructure directly supports major private sector investment in Kentucky's manufacturing sector.** Wieland has already invested over $100 million to-date in Shelbyville and with additional investments in rail access it will be able to serve the region in a greater capacity. Moreover, Wieland's potential grant funding from the Department of Energy will increase production and capital investment by far more. This strategic alignment of public infrastructure with private investment maximizes economic impact and demonstrates Kentucky's commitment to industrial growth.
- **The project creates immediate opportunities for follow-on industrial development in Shelby County.** Adjacent properties become more attractive for investment due to potential rail access, creating a multiplier effect for regional economic growth. This includes Wieland's potential expansion as well as several adjacent lots to the West of Wieland's property that could be developed.
- **The rail infrastructure enhances the region's competitiveness and supply chain efficiency by providing manufacturers with cost-effective rail transport options**, strengthening logistics networks for both bulk materials and finished products while signaling Kentucky's readiness to support major industrial operations through modern transportation infrastructure.
- **The investment supports Wieland's job creation and retention, including the 68 current employees at the Shelbyville facility, with the potential for up to 200 with the DOE grant expansion.** By providing essential transportation infrastructure, the project helps secure existing manufacturing jobs while creating conditions for future employment growth.

Transportation Improvements

Implementation will enhance transportation efficiency through:

- **The project will significantly reduce heavy truck traffic on local roads and highways.** This reduction decreases wear on transportation infrastructure while improving traffic flow throughout the region. Rail access will eliminate hundreds of truck trips per year. Moreover, with potential expansion with the DOE grant Wieland is in negotiations for, thousands of truck trips would be avoided by utilizing rail.
- **The strategic connection to Norfolk Southern's main line optimizes existing rail infrastructure while providing Wieland with direct Class 1 rail access**, creating a more resilient and sustainable freight network through transportation mode diversification that offers cost-effective shipping alternatives and superior efficiency for large-volume materials movement.

- **Safety will be enhanced for local transportation system users through reduced truck traffic.** The shift from truck to rail transport decreases congestion and potential conflicts between industrial and passenger vehicles on local roads, including (but not limited to):
    - **Buck Creek Road** – Immediate access road;
    - **Freedom's Way** – Connects to facility from Buck Creek Road;
    - **McConnell Way** – The facility's direct frontage road;
    - **KY-55/Taylorsville Road** – Major route leading into Shelbyville;
    - **I-64** – Main interstate access.
- **All-weather shipping capability will be established through modern rail infrastructure.** Rail service provides reliable transportation options regardless of road conditions, enhancing year-round operational efficiency.

Environmental and Community Benefits

The project delivers important community benefits including:

- **Greenhouse gas emissions will be significantly reduced through the shift from truck to rail transportation.** Each rail car eliminates multiple truck trips, creating immediate and lasting environmental improvements for the community through the reduction in carbon emissions.
- **Local roadways will experience reduced wear and maintenance requirements due to decreased heavy truck traffic.** This preservation of infrastructure extends the life of public roads while reducing maintenance costs for local governments.
- **Road safety will be enhanced for residents through the reduction of industrial truck traffic.** Fewer trucks on local roads creates safer conditions for community members while improving overall traffic flow.
- **Project Alta supports Wieland's goal to establish the lowest carbon footprint globally for high-end copper applications**. By enabling efficient rail transportation that substantially reduces carbon emissions, the project demonstrates Kentucky's commitment to sustainable industrial development while strengthening domestic supply chains for critical materials needed in electric vehicles, semiconductors, and the broader electrification of industry.
- **Economic opportunities will be enhanced for local residents through industrial growth and job creation.** The rail infrastructure supports business expansion while creating conditions for new employment opportunities in the manufacturing sector.
- **Quality of life improvements will be realized through reduced traffic congestion and noise.** The shift of industrial transport to rail creates quieter, safer streets while improving community livability.

Scope of Work – Project Alta

Overview

Project Alta encompasses the design, construction, and commissioning of a new industrial rail spur connecting Norfolk Southern's Class 1 main line to Wieland North America Recycling's facility in Shelbyville, Kentucky.

Technical Requirements

The track infrastructure consists of an industrial spur track constructed to Class 1 railroad standards with a new turnout connection to the Norfolk Southern main line. Secondary track spurs will be installed for loading and unloading operations. All track components must meet NS engineering specifications, with appropriate curve radius and grade specifications to ensure safe operations.

For track components, the project requires rail sections meeting Norfolk Southern standards, along with railroad ties of standard dimensions. The construction will utilize appropriate quantities of track ballast and sub-ballast materials. The design includes standard turnout sets and all necessary track fasteners and accessories as specified by Norfolk Southern engineering requirements.

Site work encompasses substantial earthwork operations for proper grading and foundation preparation. The scope includes installation of drainage pipes throughout the site and comprehensive erosion control measures. The project requires access road paving and concrete installation for designated loading areas to support operational requirements.

Safety systems form a critical component of the infrastructure, incorporating crossing protection installations at key intersections. Multiple sets of warning devices will be installed along the track, complemented by signaling equipment that meets Norfolk Southern requirements. A comprehensive communication system will be integrated throughout the facility to ensure operational safety and efficiency.

Project Scope

The rail infrastructure development project encompasses comprehensive design, engineering, permitting, and construction activities to establish safe and efficient rail service. The following components outline the project's full scope of work and technical requirements:

- **Industrial Spur Track**: We will construct a Class 1 railroad-standard connection to Norfolk Southern's main line, complete with turnouts, switches, and rail components for seamless integration.
- **Loading and Unloading Areas**: We will build dedicated track infrastructure with reinforced concrete pads, drainage systems, and equipment foundations to support efficient material handling.
- **Site Preparation**: We will execute earthwork, grading, and drainage improvements to ensure proper water management and long-term infrastructure stability.
- **Safety and Signaling Systems**: We will install Norfolk Southern-specified crossing protection, communication equipment, and operational safety features.
- **Access Road Modifications**: We will implement new paving, signage, and traffic control measures to ensure safe interaction between rail and vehicular traffic.

- **Utility Infrastructure**: We will relocate and protect affected utilities while maintaining service to existing facilities.
- **Regulatory Compliance**: We will secure all state and local permits and maintain compliance throughout construction.
- **Testing and Commissioning**: We will verify and certify all infrastructure components meet operational requirements before handover.

Work Scope

The Project Alta rail spur development encompasses a comprehensive scope spanning from initial planning through final commissioning of the rail infrastructure. The project will establish a new industrial rail connection between Norfolk Southern's main line and Wieland North America Recycling's Shelbyville facility, requiring extensive coordination with Norfolk Southern, environmental agencies, and local authorities. The work includes all necessary engineering, site preparation, construction, and systems integration to deliver a fully operational rail spur that meets Class 1 railroad standards.

- **Project Overview**: We will establish an industrial rail connection between Norfolk Southern's main line and Wieland North America Recycling's Shelbyville facility, coordinating with railroad, environmental, and local authorities.
- **Project Initiation**: We will award contracts, develop mobilization plans, and establish project execution procedures.
- **Engineering Services**: We will conduct geotechnical investigations and prepare complete engineering drawings, specifications, and construction documents.
- **Regulatory Approvals**: We will secure Norfolk Southern agreements, environmental permits, and compliance documentation.
- **Procurement**: We will source rail materials, select contractors, and procure necessary equipment.
- **Site Investigation**: We will perform site surveys, develop layouts, complete geotechnical studies, and identify utility locations.
- **Environmental Controls**: We will implement erosion, stormwater, dust, and noise control systems along with spill prevention measures.
- **Site Preparation**: We will clear the site, protect utilities, perform earthwork, and construct access roads.
- **Rail Infrastructure**: We will prepare the track bed, place sub-ballast, and install rail components and loading areas.
- **Support Infrastructure**: We will construct equipment foundations, maintenance roads, and concrete loading zones.
- **Safety Systems**: We will install signals, crossing protection, warning devices, and communication systems.
- **Quality Management**: We will conduct materials testing, construction inspection, and third-party verification.
- **Construction Administration**: We will manage site mobilization, contractor coordination, and project schedules.

- **Commissioning**: We will test systems, verify track geometry, and complete inspections.
- **Project Closeout**: We will train staff, compile documentation, and secure final certifications.

Quality Requirements

The Project Alta rail expansion demands rigorous quality control measures throughout all phases of construction. Every aspect of the work must meet or exceed Norfolk Southern's stringent engineering standards, with comprehensive materials testing and inspection protocols implemented throughout the construction process. Critical components will undergo third-party verification to ensure compliance with specifications. The project team will maintain strict adherence to all applicable regulations and permits, with thorough documentation maintained for all testing and commissioning activities. This systematic approach to quality management ensures the delivered infrastructure will meet the operational requirements of both Norfolk Southern and Wieland North America Recycling.

Environmental Controls

Environmental stewardship remains a key priority throughout the Project Alta construction process. A comprehensive erosion and sediment control plan will be implemented and maintained to protect surrounding areas, complemented by robust stormwater management practices during all construction activities. The project team will employ extensive dust control measures to minimize impact on adjacent properties, while implementing noise mitigation strategies during construction operations. All construction materials will be properly stored and handled according to environmental best practices, with detailed spill prevention and response procedures in place to address any potential environmental concerns promptly and effectively.

Project Materials and Cost

Project Alta – Rail Expansion Budget[1]	
Activity	**Cost Estimate**
Staffing and Engineering	
General Conditions - Staff & Expenses	$399,207
Geotech & materials testing Allowance	$37,580
Sitework	
Grading & storm	$911,654
Rock Allowance	$351,818
Lime Stabilization Allowance	$462,931
Rail construction	$1,631,469
Chain link fencing & gate	$122,800
Fees	
Engineering Fee including rail	$73,589
Construction Fee 5%	$195,873
Total	**$4,186,921**

[1] *Project Alta's budget, schedule, and design reflect project development for Wieland's current site only and could change, should Wieland sign its grant agreement with the United States Department of Energy for site expansion. Wieland will have to enter into the NEPA process and receive a corresponding NEPA determination which would halt site activities and potentially delay Project Alta.*

Project Schedule/Timeline – Project Alta

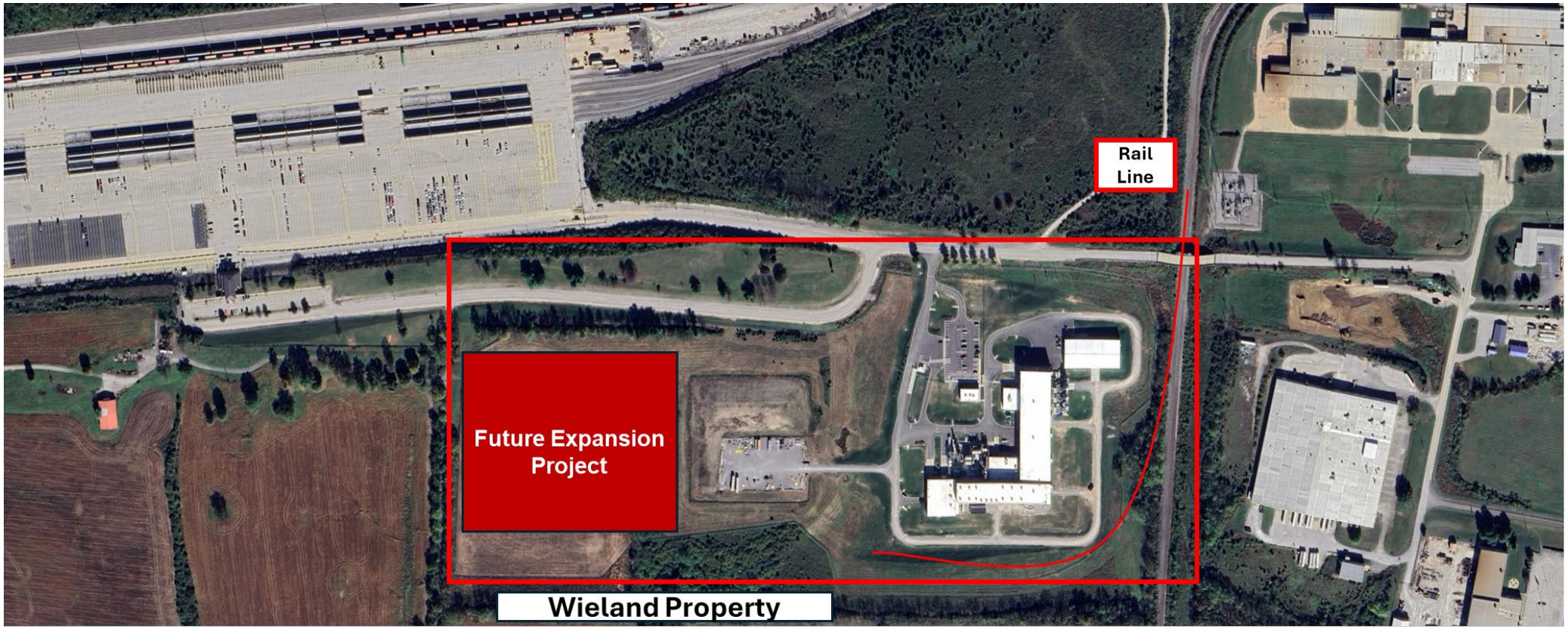
Project Alta's rail infrastructure development will follow a 7-month implementation plan from February 2027 through October 2027. The schedule prioritizes construction efficiency while maintaining quality and safety standards. The schedule incorporates built-in contingencies for weather delays and stakeholder coordination, targeting completion by October 2027. Specific tasks, durations, and critical path activities are detailed in the schedule and Gantt chart below.

Project Alta – Rail Expansion Schedule[1]			
Task	**Duration**	**Start**	**Finish**
Geotechnical Work	25 days	2/1/2027	3/7/2027
Rail Spur Design (Phase I)	30 days	2/15/2027	3/28/2027
Norfolk Southern Approval	40 days	3/29/2027	5/22/2027
Rail Material Procurement	40 days	3/29/2027	5/22/2027
Mobilize to Site	0 days	5/22/2027	5/22/2027
Erosion Control	10 days	6/1/2027	6/14/2027
Survey for Grading	10 days	6/1/2027	6/14/2027
Rail Grading (Mass Cuts and Fills)	50 days	6/8/2027	8/14/2027
Rail Spur Installation	35 days	7/19/2027	9/6/2027
Approval of Installation	20 days	9/7/2027	10/4/2027

Task	Feb 2027	Mar 2027	Apr 2027	May 2027	Jun 2027	Jul 2027	Aug 2027	Sep 2027	Oct 2027
Geotechnical									
Rail Spur Design (Phase I)									
Norfolk Southern Approval									
Rail Material Procurement									
Mobilize to Site									
Erosion Control									
Survey for Grading									
Rail Grading (Mass Cuts and Fills)									
Rail Spur Installation									
Approval of Installation									

■ **In Progress** ■ **Complete**

[1] *Project Alta's budget, schedule, and design reflect project development for Wieland's current site only and could change, should Wieland sign its grant agreement with the United States Department of Energy for site expansion. Wieland will have to enter into the NEPA process and receive a corresponding NEPA determination which would halt site activities and potentially delay Project Alta.*


Rail Line
Future Expansion Project
Wieland Property

Wieland Property Today

wieland
SHELBY COUNTY
Shelbyville
KENTUCKY

PROJECT ALTA SITE PLANS

LEGEND

- EXISTING TRACK
- RAILROAD OWNERSHIP
- INDUSTRY OWNERSHIP
- PROPERTY BOUNDARY
- ADJACENT PROPERTY BOUNDARY
- BUILDING SETBACK
- RIGHT OF WAY
- UTILITY EASEMENT
- ROADWAY CENTER LINE
- EXISTING OVERHEAD ELECTRIC
- EXISTING UNDERGROUND GAS
- EXISTING UNDERGROUND SANITARY SEWER LINE
- PIPE (DRAINAGE STRUCTURE)
- PROPOSED HEAVY DUTY ASPHALT
- PROPOSED LIGHT DUTY ASPHALT
- PROPOSED HEAVY DUTY CONCRETE
- PROPOSED LIGHT DUTY CONCRETE
- PROPOSED GRAVEL
- PROPOSED BUILDING

NOTES:

1. THESE NOTES ARE INTENDED TO SUPPLEMENT THE INFORMATION CONTAINED IN THE DESIGN DOCUMENTS AND SPECIFICATIONS. IF A CONFLICT EXISTS BETWEEN DESIGN DOCUMENTS AND SPECIFICATIONS AND THE NORFOLK SOUTHERN RAILWAY COMPANY SPECIFICATIONS FOR DESIGN AND CONSTRUCTION OF PRIVATELY OWNED INDUSTRY TRACKS, REVISED JULY 2020, THE MOST STRINGENT REQUIREMENT SHALL GOVERN.
2. CONTRACTOR SHALL VERIFY ALL EXISTING ELEVATIONS, DIMENSIONS, AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND SHALL NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPENCIES.
3. CONTRACTOR IS RESPONSIBLE FOR OBTAINING RIGHT-OF-ENTRY FROM RAILROAD PRIOR TO ENTERING THE RAILROAD RIGHT-OF-WAY.
4. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING STRICT CONTROL OF SITE RUN-OFF AT ALL TIMES TO PREVENT ANY FORM OF WATER POLLUTION.
5. NO WORK SHALL COMMENCE ON RAILROAD RIGHT-OF-WAY PRIOR TO NOTIFYING THE RAILROAD ENGINEER.
6. CONTRACTOR SHALL COMPLY WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS.
7. NO FIELD CHANGES SHALL BE PERMITTED WITHOUT WRITTEN APPROVAL FROM H+M ARCHITECTS/ENGINEERS, INC.
8. GRADING, DRAINAGE, AND PLACEMENT OF SUB-BALLAST SHALL BE AT THE EXPENSE OF THE OWNER.
9. PROPOSED DRAINAGE STRUCTURES SHALL COMPLY WITH A.R.E.M.A. PART 5 SPECIFICATIONS AND COMPLY WITH NORFOLK SOUTHERN STANDARDS.
10. EXISTING AND/OR PROPOSED UTILITIES SHALL BE RELOCATED, ADJUSTED, AND/OR PROTECTED IN COMPLIANCE WITH NORFOLK SOUTHERN STANDARDS AT THE OWNERS EXPENSE.
11. CLEARANCES TO EXISTING AND/OR PROPOSED STRUCTURES SHALL ADHERE TO NORFOLK SOUTHERN STANDARD PLANS 7-1.
12. DETAILED DESIGN PLANS FOR LOADING/UNLOADING FACILITIES, INCLUDING HORIZONTAL AND VERTICAL CLEARANCES, SHALL BE APPROVED BY THE RAILROAD PRIOR TO CONSTRUCTION OF SUCH FACILITIES.
13. BONDING OF TRACKS, IF REQUIRED, SHALL BE IN COMPLIANCE WITH NORFOLK SOUTHERN STANDARDS.
14. A BOW HANDLED THROW IS REQUIRED FOR ALL SWITCHES.
15. TIE AND RAIL AT CLEAR POINTS WITHOUT A DERAIL SHALL BE PAINTED FLUORESCENT GREEN. CLEAR POINT OF ADJACENT TRACK TO MINIMUM 14'.

OPERATIONS:

NORFOLK SOUTHERN SHALL DELIVER EMPTY 75' BOX CARS OR 70' FLAT CARS AND DROP THEM ON TRACK A OR TRACK C STORAGE LINES. EMPTY CARS SHALL BE MOVED TO THE LOADING DOCK BY WIELAND VIA INDUSTRY OWNED TRACKMOBILE AND FULL CARS SHALL BE STORED ON THE OPPOSITE LINE. NORFOLK SOUTHERN SHALL PICK UP LOADED CARS WHEN DROPPING EMPTY CARS. CARS SHALL ONLY BE STORED IN DESIGNATED STORAGE OR LOADING AREAS.

OWNER/ADDRESS: WIELAND NORTH AMERICA
4803 OLYMPIA PARK PLAZA, SUITE 300
LOUISVILLE, KENTUCKY 40241

CONTROL POINTS (KY STATE PLANE NAD 83, KY SINGLE ZONE, NAVD88)

POINT NUMBER	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	3962944.93	5054168.06	778.35	CP /IPC ⅝
2	3963175.09	5055144.06	800.08	CP /IPC ⅝
4	3962017.71	5054678.34	787.65	CP /IPC ⅝
5	3962015.14	5055078.03	773.75	CP /IPC ⅝

- ■ PLAN PREPARED BY OUTSIDE PARTY - SUBMITTED TO NS D&C FOR REVIEW
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- □ PRELIMINARY PLAN - NOT APPROVED FOR CONSTRUCTION - ISSUED BY NS D&C FOR TRANSPORTATION DEPARTMENT REVIEW ON:
- ■ APPROVED PLAN - ACCEPTED FOR CONSTRUCTION BY NS D&C ON: 1/4/23

STATE OF KENTUCKY
SHANE COCHRAN
32720
02/21/2024
LICENSED PROFESSIONAL ENGINEER

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E	09/18/2023	REISSUE FOR COMMENT RESPONSE
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G	02/21/2024	REISSUE FOR COMMENT RESPONSE

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H+M Architects/Engineers, Inc.
50 Security Drive, Jackson, Tennessee 38305
Telephone:(731)664-6330, Fax:(731)664-6339

SCALE 1" = 100'			PROJECT: NEW FACILITY FOR WIELAND NORTH AMERICA, SHELBYVILLE, SHELBY COUNTY, KY	PROJECT NUMBER 21030	
DRAWN BCF	A/E SSC	PM RB/BB		SHEET NUMBER CE-6.0	
SHEET TITLE: OVERALL HORIZONTAL RAILROAD LAYOUT				PLOT DATE 2/20/2024 4:35 PM	

NORFOLK SOUTHERN
NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY
MIDWEST
OPERATING DIVISION
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

REV	BY	DATE	DESCRIPTION
		1/16/24	

LOCATION: SHELBYVILLE, KENTUCKY
PID#: Id2720 | TITLE: PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
VRN: 0340701
VAL SEC / MAP: 90/8A | FILE: TRK1115240 | DRAWING NUMBER: AD-2024-0
MILE POST: 304.64 W | DATE: 1/16/24

PRELIMINARY - NOT FOR CONSTRUCTION

LEGEND

- EXISTING TRACK
- RAILROAD OWNERSHIP
- INDUSTRY OWNERSHIP
- PROPERTY BOUNDARY
- ADJACENT PROPERTY BOUNDARY
- BUILDING SETBACK
- RIGHT OF WAY
- UTILITY EASEMENT
- ROADWAY CENTER LINE
- EXISTING OVERHEAD ELECTRIC
- EXISTING UNDERGROUND GAS
- EXISTING UNDERGROUND SANITARY SEWER LINE
- PIPE (DRAINAGE STRUCTURE)
- PROPOSED HEAVY DUTY ASPHALT
- PROPOSED LIGHT DUTY ASPHALT
- PROPOSED HEAVY DUTY CONCRETE
- PROPOSED LIGHT DUTY CONCRETE
- PROPOSED GRAVEL
- PROPOSED BUILDING

NOTES:

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2. CONTRACTOR SHALL VERIFY ALL EXISTING ELEVATIONS, DIMENSIONS, AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND SHALL NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPENCIES.
3. CONTRACTOR IS RESPONSIBLE FOR OBTAINING RIGHT-OF-ENTRY FROM RAILROAD PRIOR TO ENTERING THE RAILROAD RIGHT-OF-WAY.
4. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING STRICT CONTROL OF SITE RUN-OFF AT ALL TIMES TO PREVENT ANY FORM OF WATER POLLUTION.
5. NO WORK SHALL COMMENCE ON RAILROAD RIGHT-OF-WAY PRIOR TO NOTIFYING THE RAILROAD ENGINEER.
6. CONTRACTOR SHALL COMPLY WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS.
7. NO FIELD CHANGES SHALL BE PERMITTED WITHOUT WRITTEN APPROVAL FROM H+M ARCHITECTS/ENGINEERS, INC.
8. GRADING, DRAINAGE, AND PLACEMENT OF SUB-BALLAST SHALL BE AT THE EXPENSE OF THE OWNER.
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SIDE TRACK AGREEMENT TABLE			*COORDINATE SYSTEM = UTM83-17F/OH83 NF*			
	TRACK A		TRACK B		TRACK C	
BEGIN TRACK	*STATION*		*STATION*		*STATION*	
	0+00.00		0+00.00		0+00.00	
DIVISION OF OWNERSHIP & MAINTENANCE	*STATION*	*COORDINATES*	*STATION*	*COORDINATES*	*STATION*	*COORDINATES*
	3+70.00	N47° 37' 58.49" W70° 31' 58.98"				
END TRACK	*STATION*		*STATION*		*STATION*	
	31+23.00		11+94.00		6+15.00	
CLEAR TRACK CAPACITY	349 TF		168.50 TF		349 TF	

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NORFOLK SOUTHERN

NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY
MIDWEST
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OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

REV	BY	DATE	DESCRIPTION
		1/16/24	

LOCATION: SHELBYVILLE, KENTUCKY
PID#: Id2720
VRN: 0340701
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VAL SEC / MAP: 90/8A
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DATE: 1/16/24
DRAWING NUMBER: AD-2024-0A

STATE OF KENTUCKY
SHANE COCHRAN
32720
02/21/2024
LICENSED PROFESSIONAL ENGINEER

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NO	DATE	RELEASE DESCRIPTION
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Architects/Engineers, Inc.
50 Security Drive, Jackson, Tennessee 38305
Telephone:(731)664-6330, Fax:(731)664-6339

SCALE: 1" = 100'
DRAWN: JHL | A/E: SSC | PM: RB/BB

PROJECT: NEW FACILITY FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE:
PROPOSED TRACK AGREEMENT PLAN

PROJECT NUMBER: 21030
SHEET NUMBER: CE-6.0A
PLOT DATE: 2/20/2024 4:35 PM

PRELIMINARY - NOT FOR CONSTRUCTION

SHELBY COUNTY, KENTUCKY

LEGEND

- EXISTING TRACK
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- INDUSTRY OWNERSHIP
- PROPERTY BOUNDARY
- ADJACENT PROPERTY BOUNDARY
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OWNER/ADDRESS: WIELAND NORTH AMERICA
4803 OLYMPIA PARK PLAZA, SUITE 300
LOUISVILLE, KENTUCKY 40241

CONTROL POINTS (KY STATE PLANE NAD 83, KY SINGLE ZONE, NAVD88)

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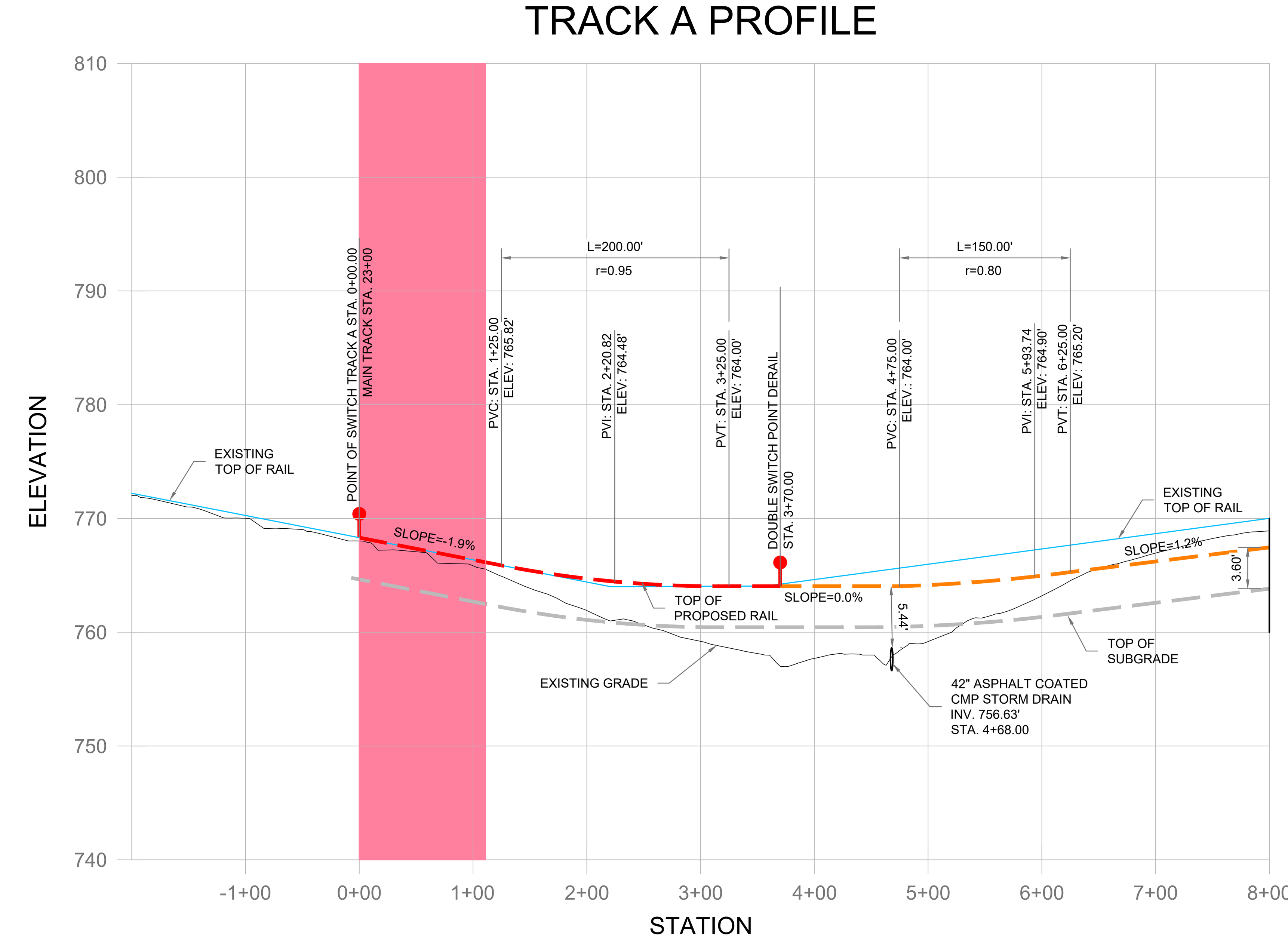
TRACK A PROFILE



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H+M
Architects/Engineers, Inc.
50 Security Drive, Jackson, Tennessee 38305
Telephone:(731)664-6330, Fax:(731)664-6339

SCALE 1" = 50'

DRAWN BCF | A/E SSC | PM RB/BB

PROJECT: NEW FACILITY FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE: ENLARGED RAILROAD LAYOUT
TRACK A PLAN & PROFILE FROM -2+00 TO 8+00

PROJECT NUMBER 21030

SHEET NUMBER CE-6.1

PLOT DATE 2/20/2024 4:36 PM

■ PLAN PREPARED BY OUTSIDE PARTY - SUBMITTED TO NS D&C FOR REVIEW
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NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY
MIDWEST
OPERATING DIVISION
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

REV	BY	DATE	DESCRIPTION

LOCATION: SHELBYVILLE, KENTUCKY

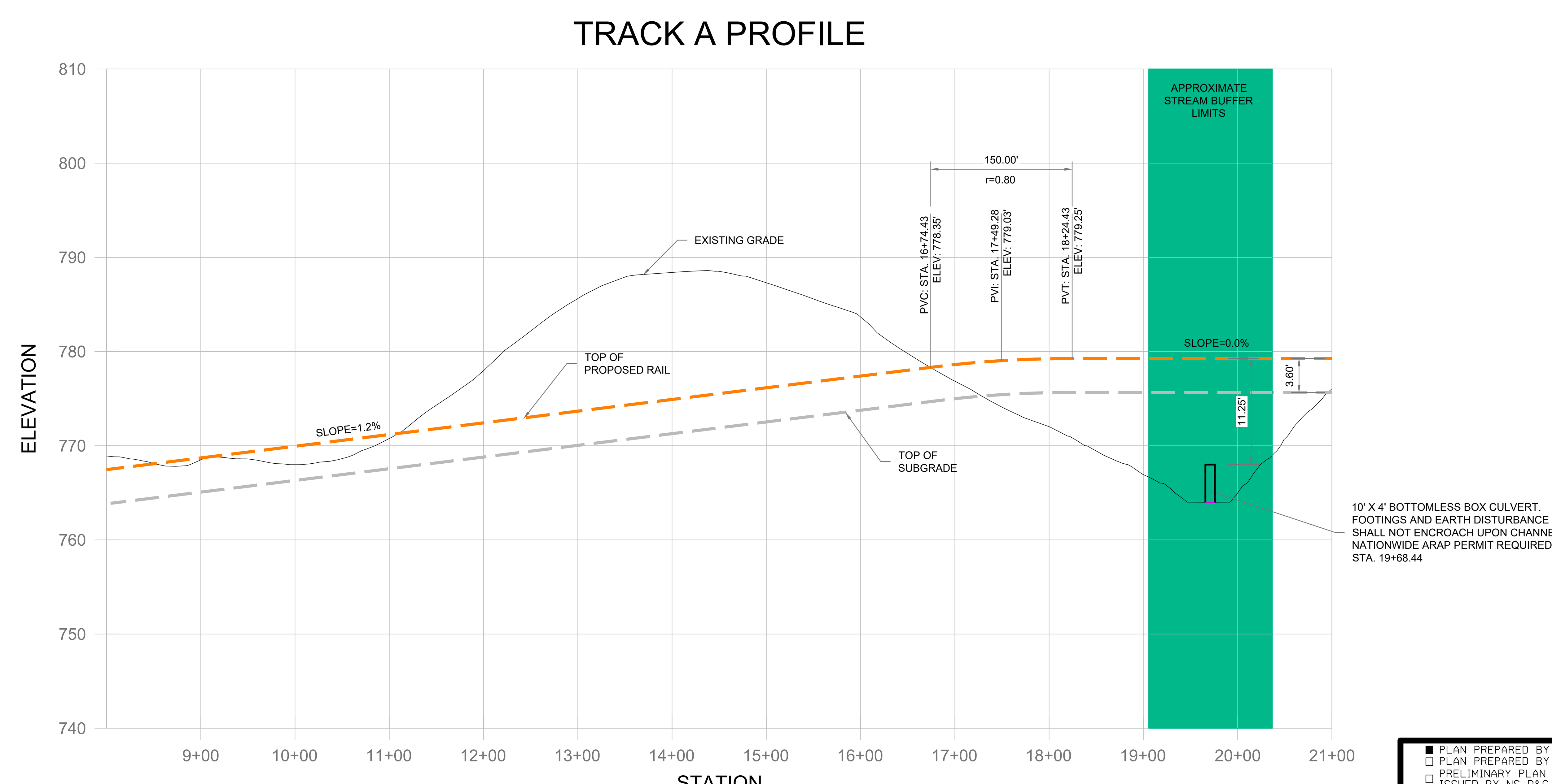
PID# Id2720 | VRN 0340701

TITLE: PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING

VAL SEC / MAP 90/8A | MILE POST 304.64 W | FILE TRK1115240 | DATE | DRAWING NUMBER AD-2024-0

PRELIMINARY - NOT FOR CONSTRUCTION

TRACK A PROFILE



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OWNER/ADDRESS: WIELAND NORTH AMERICA
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LOUISVILLE, KENTUCKY 40241

CONTROL POINTS (KY STATE PLANE NAD 83, KY SINGLE ZONE, NAVD88)

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STATE OF KENTUCKY SHANE COCHRAN 32720 02/21/2024 LICENSED PROFESSIONAL ENGINEER

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SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE: ENLARGED RAILROAD LAYOUT
TRACK A PLAN & PROFILE FROM 8+00 TO 21+00

PROJECT NUMBER 21030
SHEET NUMBER CE-6.2
PLOT DATE 2/20/2024 4:36 PM

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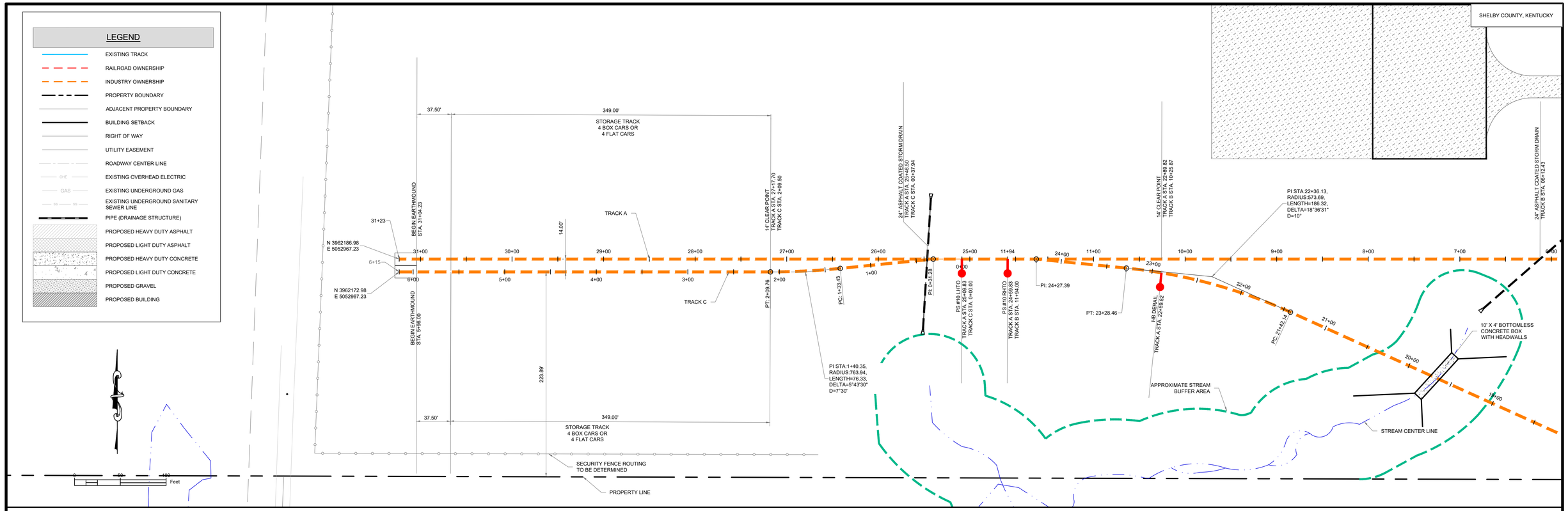
REV	BY	DATE	DESCRIPTION

LOCATION: SHELBYVILLE, KENTUCKY
PID# Id2720 | TITLE: PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
VRN 0340701
VAL SEC / MAP 90/8A | FILE TRK1115240 | DRAWING NUMBER AD-2024-0
MILE POST 304.64 W | DATE

PRELIMINARY - NOT FOR CONSTRUCTION



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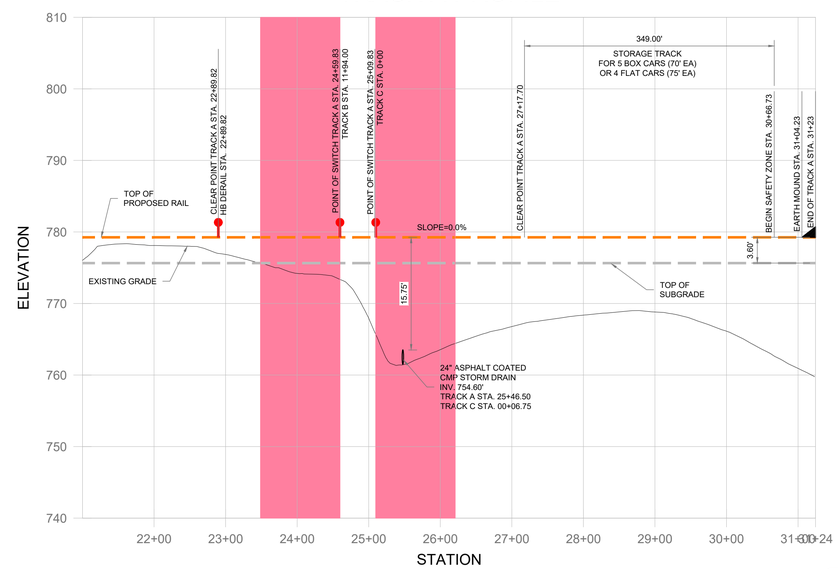
OWNER/ADDRESS: WIELAND NORTH AMERICA
4803 OLYMPIA PARK PLAZA, SUITE 300
LOUISVILLE, KENTUCKY 40241

CONTROL POINTS (KY STATE PLANE NAD 83, KY SINGLE ZONE, NAVD88)

POINT NUMBER	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	3962944.93	5054168.06	778.35	CP /IPC ⅝
2	3963175.09	5055144.06	800.08	CP /IPC ⅝
4	3962017.71	5054678.34	787.65	CP /IPC ⅝
5	3962015.14	5055078.03	773.75	CP /IPC ⅝

TRACK A PROFILE



STATE OF KENTUCKY
SHANE COCHRAN
32720
02/21/2024
LICENSED PROFESSIONAL ENGINEER

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G	02/21/2024	REISSUE FOR COMMENT RESPONSE

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50 Security Drive, Jackson, Tennessee 38305
Telephone:(731)664-6330, Fax:(731)664-6339

SCALE 1" = 100'

DRAWN	A/E	PM
BCF	SSC	RB/BB

PROJECT
NEW FACILITY FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE
ENLARGED RAILROAD LAYOUT
TRACK A PLAN & PROFILE FROM 21+00 TO END

PROJECT NUMBER 21030

SHEET NUMBER CE-6.3

PLOT DATE 2/20/2024 4:37 PM

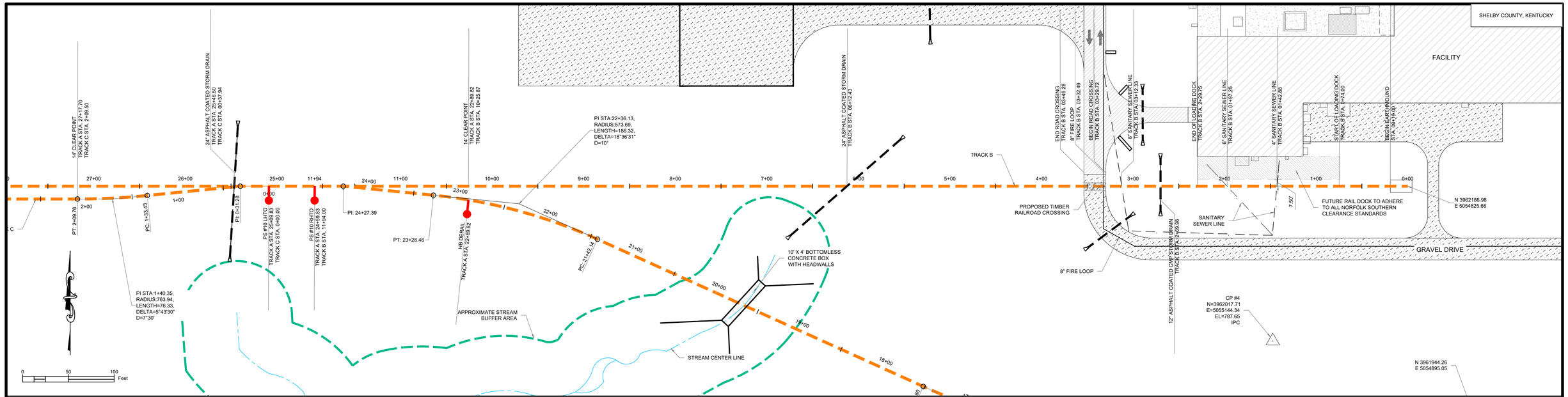
- ■ PLAN PREPARED BY OUTSIDE PARTY - SUBMITTED TO NS D&C FOR REVIEW
- □ PLAN PREPARED BY NS D&C USING INFORMATION PROVIDED BY OTHERS.
- □ PRELIMINARY PLAN - NOT APPROVED FOR CONSTRUCTION -
- □ ISSUED BY NS D&C FOR TRANSPORTATION DEPARTMENT REVIEW ON:
- ■ APPROVED PLAN - ACCEPTED FOR CONSTRUCTION BY NS D&C ON: 1/4/23

NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY
MIDWEST
OPERATING DIVISION
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

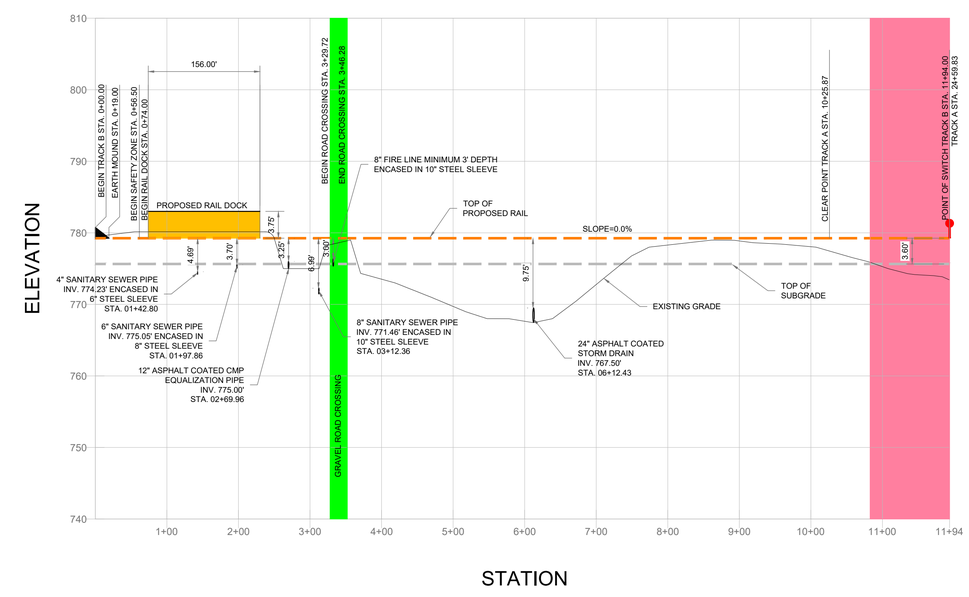
REV	BY	DATE	DESCRIPTION

LOCATION SHELBYVILLE, KENTUCKY
PID* 1d2720
TITLE PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
BRN 0340701
AL SEC / MAP 92/8A
MILE POST 304.64 W
FILE TRK1115240
DATE
DRAWING NUMBER AD-2024-0

PRELIMINARY - NOT FOR CONSTRUCTION



PROPOSED TRACK B PROFILE



LEGEND

- EXISTING TRACK
- RAILROAD OWNERSHIP
- INDUSTRY OWNERSHIP
- PROPERTY BOUNDARY
- ADJACENT PROPERTY BOUNDARY
- BUILDING SETBACK
- RIGHT OF WAY
- UTILITY EASEMENT
- ROADWAY CENTER LINE
- OHE — EXISTING OVERHEAD ELECTRIC
- GAS — EXISTING UNDERGROUND GAS
- SS — EXISTING UNDERGROUND SANITARY SEWER LINE
- PIPE (DRAINAGE STRUCTURE)
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- PROPOSED LIGHT DUTY CONCRETE
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STATE OF KENTUCKY — SHANE COCHRAN 32720 — 02/21/2024 — LICENSED PROFESSIONAL ENGINEER

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SCALE 1" = 50'
DRAWN BCF | A/E SSC | PM RB/BB

PROJECT
FACILITY EXPANSION FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE
RAILROAD PLAN AND PROFILE
TRACK B

PROJECT NUMBER 21030
SHEET NUMBER CE-6.4
PLOT DATE 2/20/2024 4:37 PM

- ■ PLAN PREPARED BY OUTSIDE PARTY - SUBMITTED TO NS D&C FOR REVIEW
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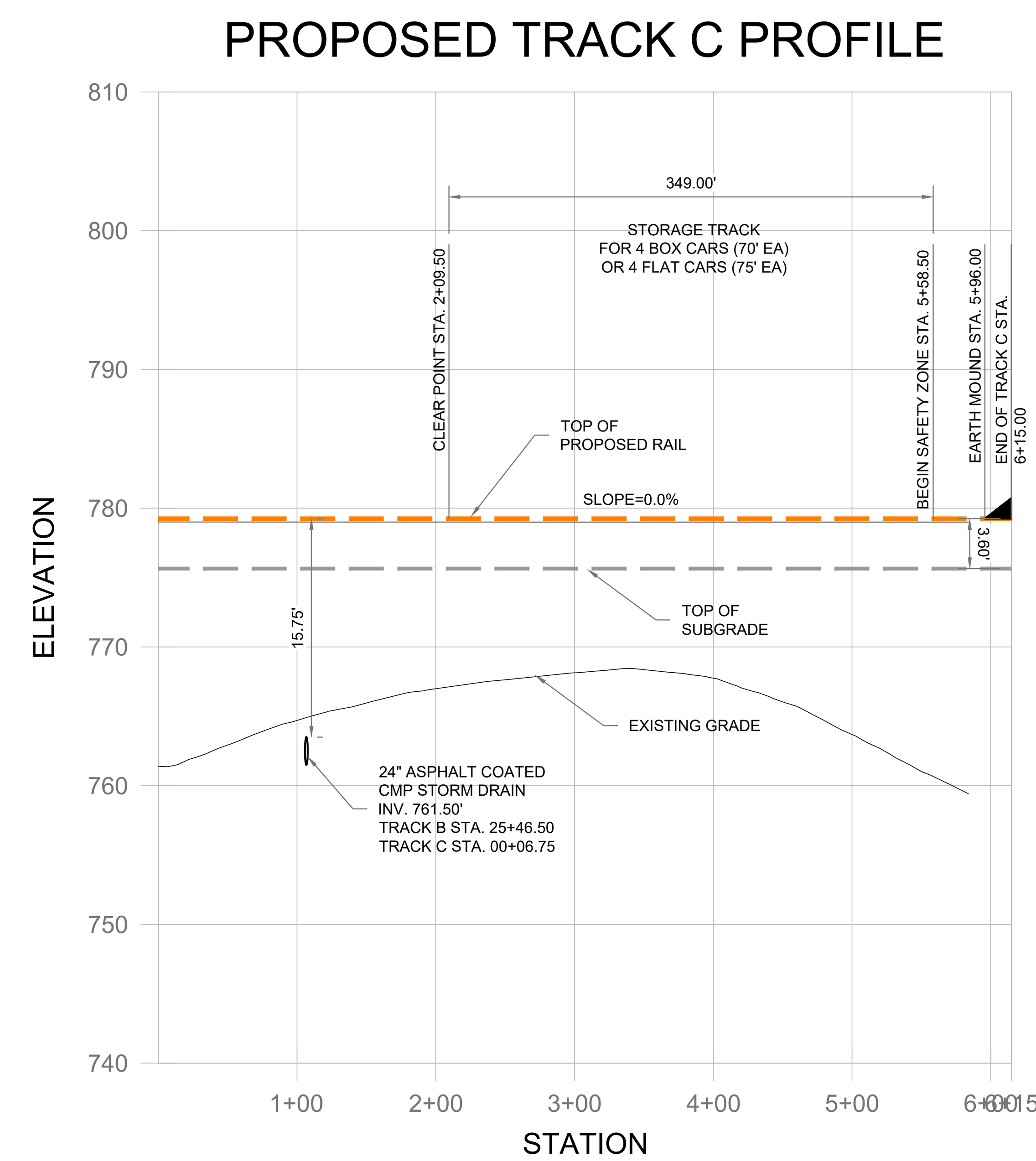
NORFOLK SOUTHERN
NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY
MIDWEST
OPERATING DIVISION
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

REV	BY	DATE	DESCRIPTION

LOCATION SHELBYVILLE, KENTUCKY
PID* 1d2720 — TITLE PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
DIV 8340701
AL SEC / MAP 92/8A — FILE TRK1115240 — DRAWING NUMBER AD-2024-0
MILE POST 384.64 W — DATE

PRELIMINARY - NOT FOR CONSTRUCTION



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STATE OF KENTUCKY
SHANE COCHRAN
32720
02/21/2024
LICENSED PROFESSIONAL ENGINEER

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50 Security Drive, Jackson, Tennessee 38305
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SCALE 1" = 50'
DRAWN BCF | A/E SSC | PM RB/BB

PROJECT: FACILITY EXPANSION FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE: RAILROAD PLAN AND PROFILE
TRACK C

PROJECT NUMBER 21030
SHEET NUMBER CE-6.5
PLOT DATE 2/20/2024 4:38 PM

■ PLAN PREPARED BY OUTSIDE PARTY - SUBMITTED TO NS D&C FOR REVIEW
□ PLAN PREPARED BY NS D&C USING INFORMATION PROVIDED BY OTHERS.
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NORFOLK SOUTHERN
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OWNING COMPANY
MIDWEST
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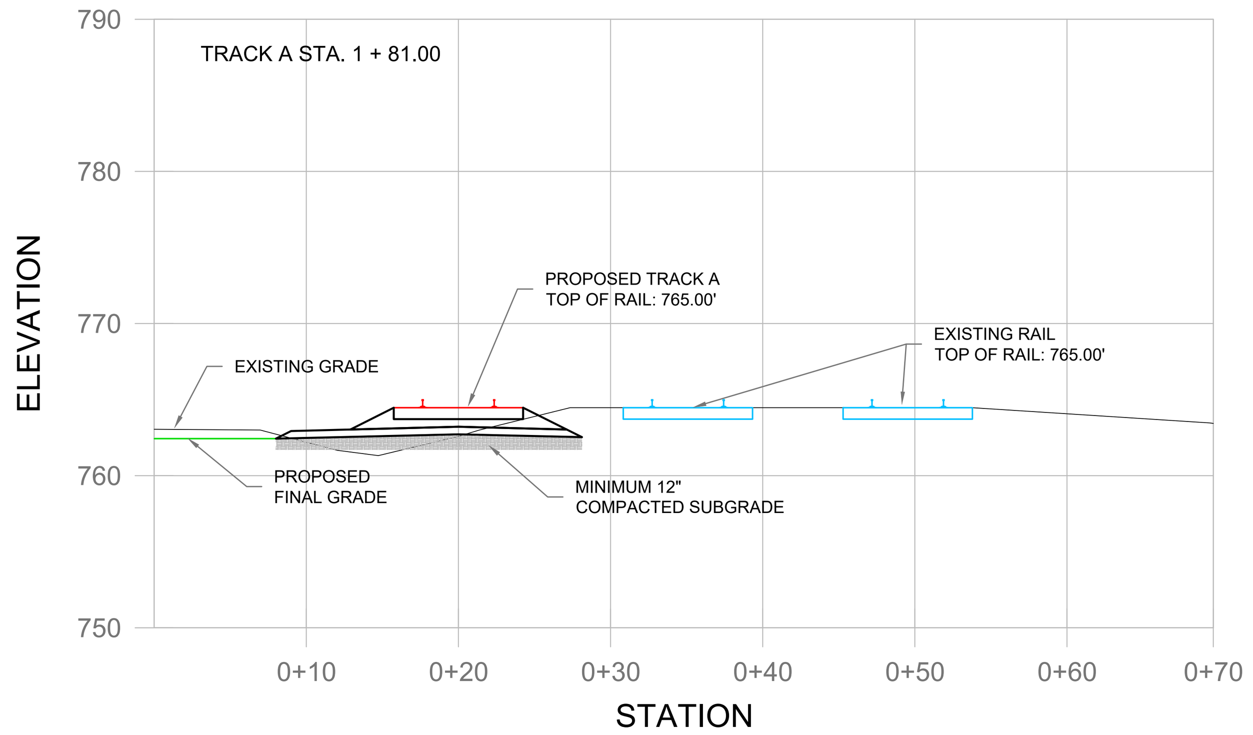
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

REV | BY | DATE | DESCRIPTION
LOCATION SHELBYVILLE, KENTUCKY
PID# Id2720 | TITLE PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
VRN 0340701
VAL SEC / MAP 90/8A | FILE TRK1115240 | DRAWING NUMBER AD-2024-0
MILE POST 304.64 W | DATE

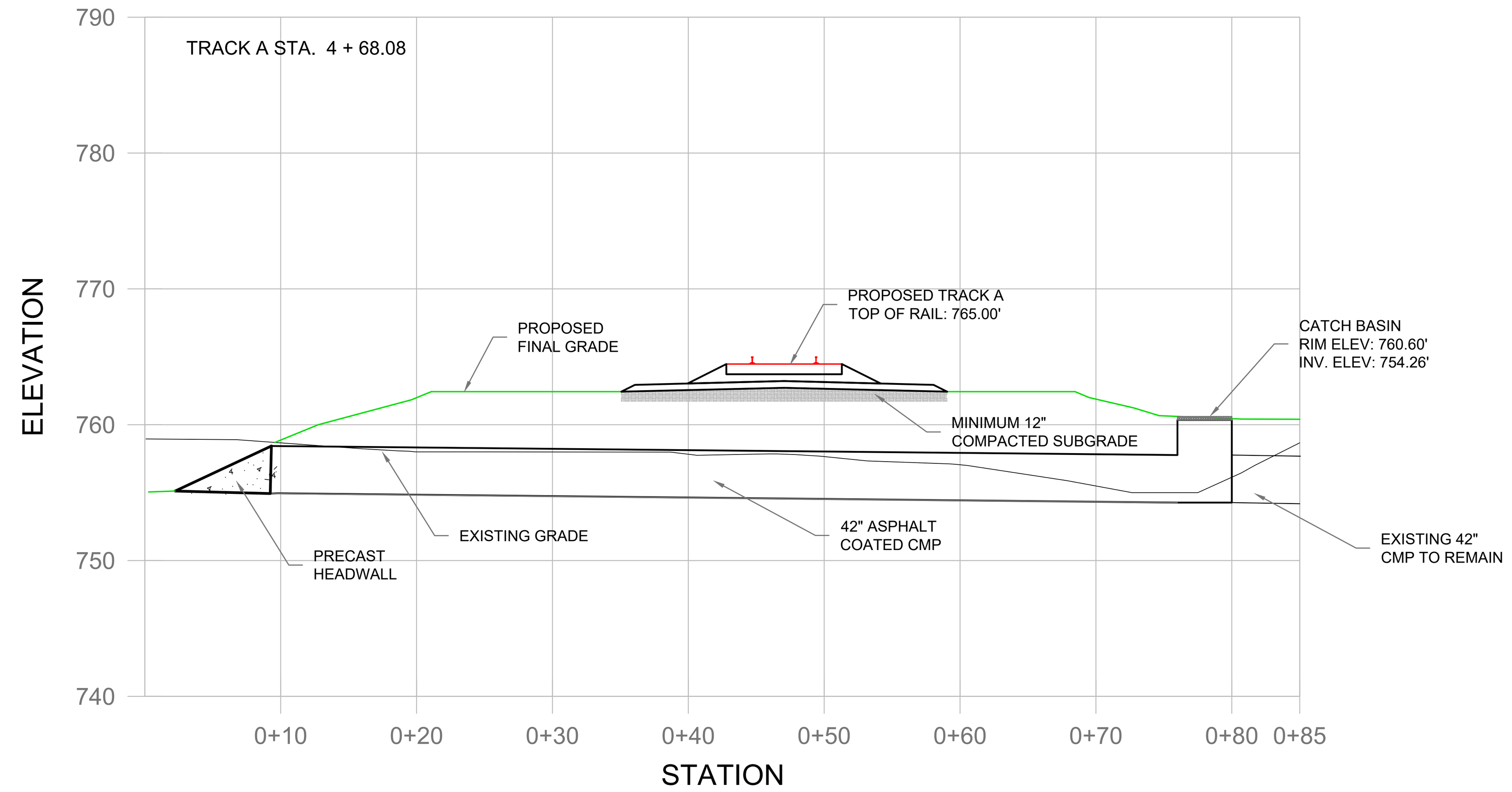
PRELIMINARY - NOT FOR CONSTRUCTION

SHELBY COUNTY, KENTUCKY

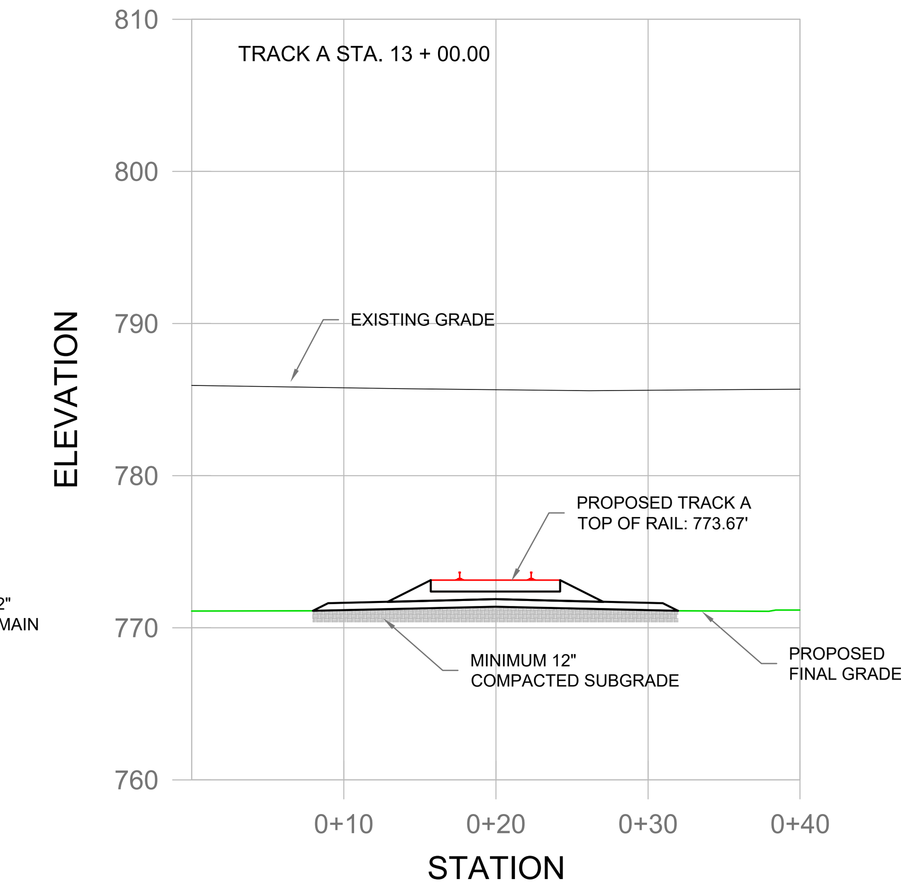
TRACK A 15' CLEAR POINT PROFILE



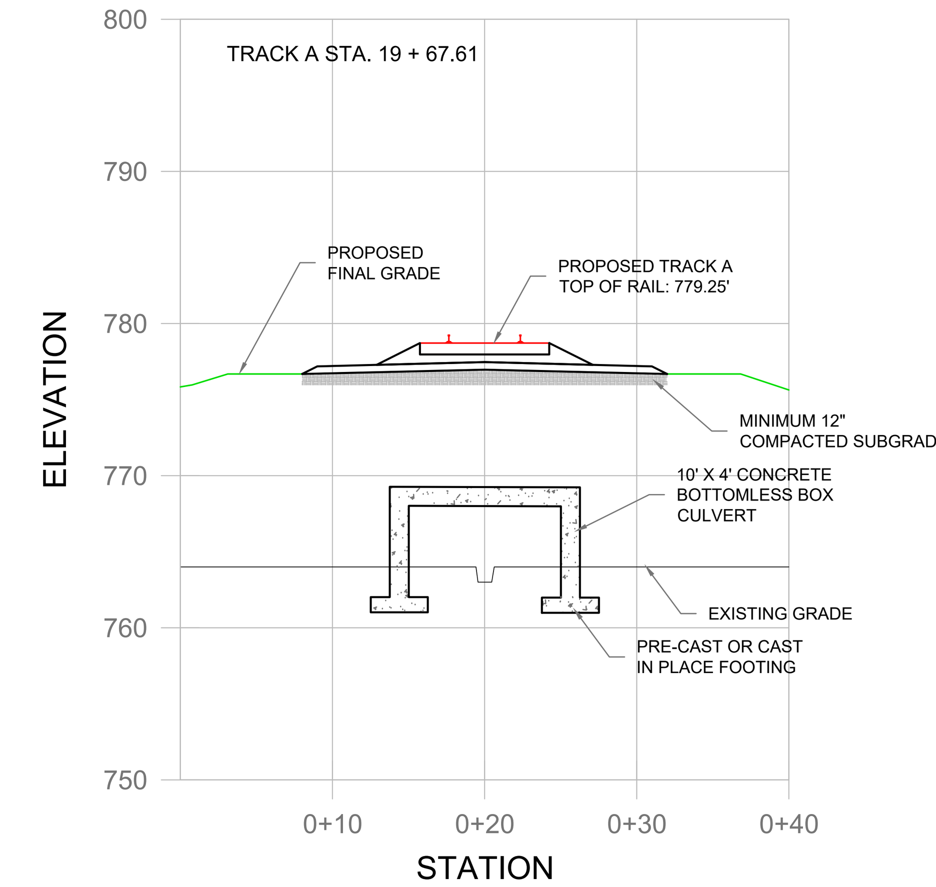
TRACK A SKEWED PIPE PROFILE



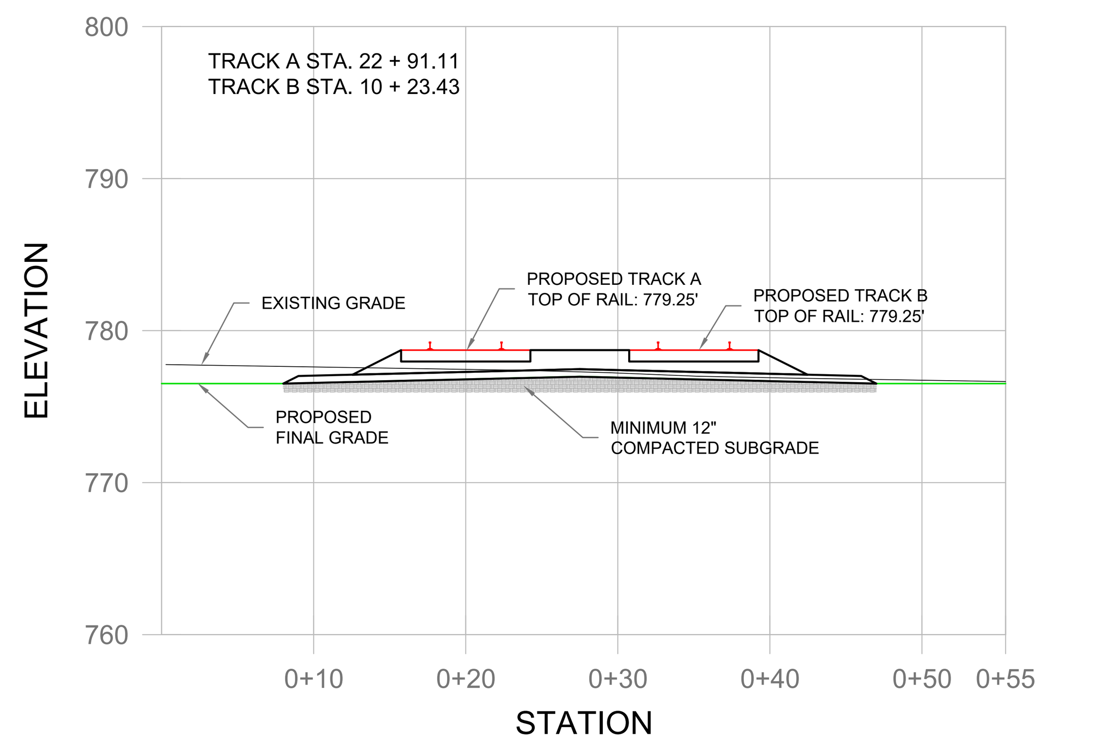
TRACK A TYPICAL CROSS SECTION PROFILE



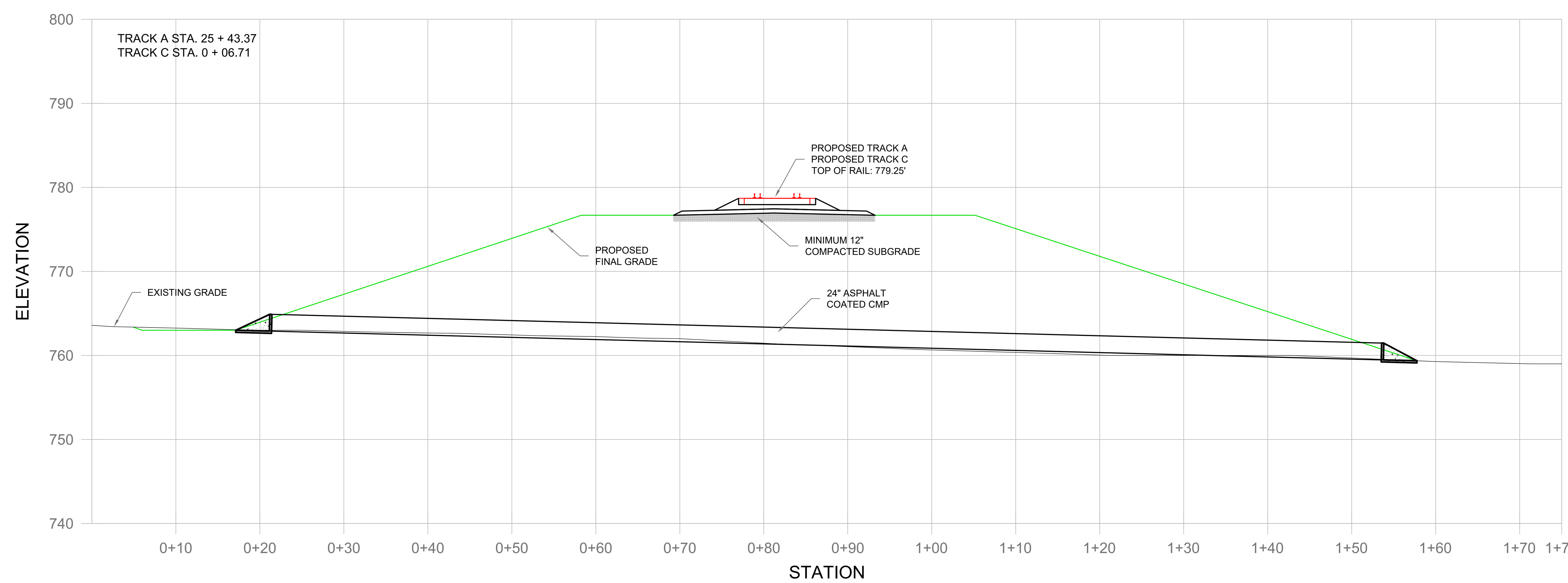
TRACK A STREAM CROSSING PROFILE



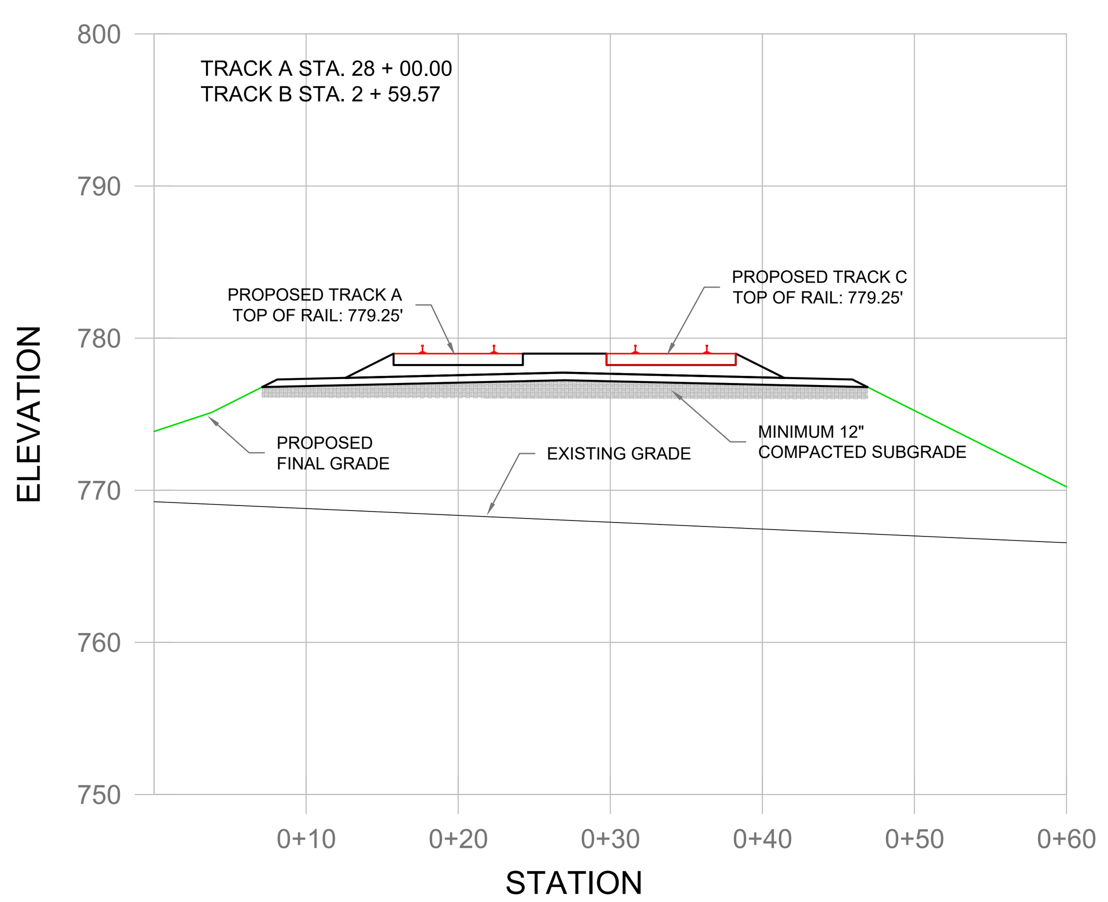
TRACK A AND TRACK B 15' CLEAR POINT PROFILE

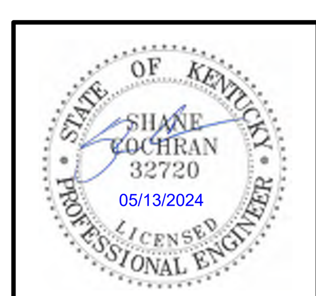


PIPE AT TRACK A AND TRACK C PROFILE



TRACK A AND TRACK C TYPICAL PROFILE





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PROJECT: FACILITY EXPANSION FOR WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE: RAILROAD CROSS SECTIONS

PROJECT NUMBER: 21030
SHEET NUMBER: CE-6.6
PLOT DATE: 5/13/2024 9:10 AM

■ PLAN PREPARED BY OUTSIDE PARTY - SUBMITTED TO NS D&C FOR REVIEW
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NORFOLK SOUTHERN RAILWAY COMPANY
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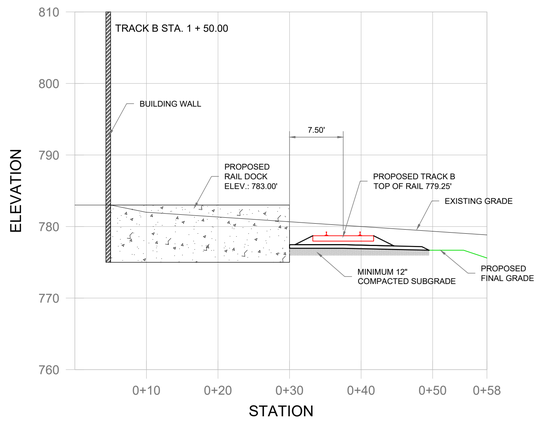
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

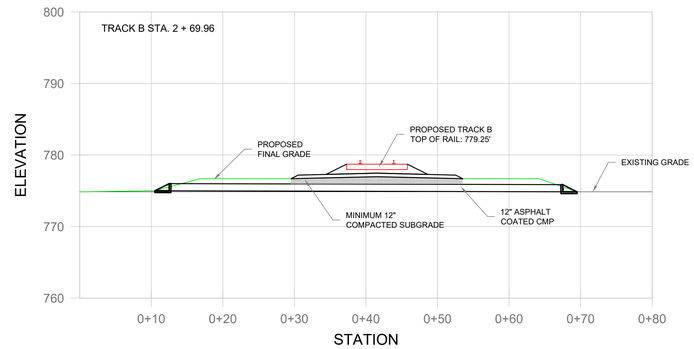
LOCATION: SHELBYVILLE, KENTUCKY
PID# 1d2720 | TITLE: PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
VRN 0340701
VAL SEC / MAP 90/8A | FILE TRK1115240
MILE POST 304.64 W | DATE | DRAWING NUMBER AD-2024-0

SHELBY COUNTY, KENTUCKY

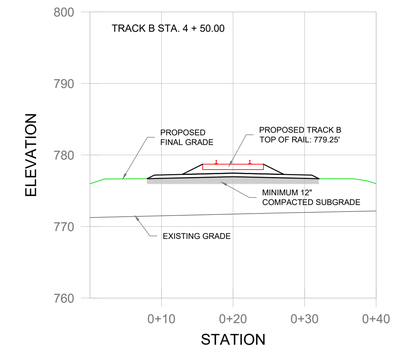
TRACK B RAIL DOCK PROFILE



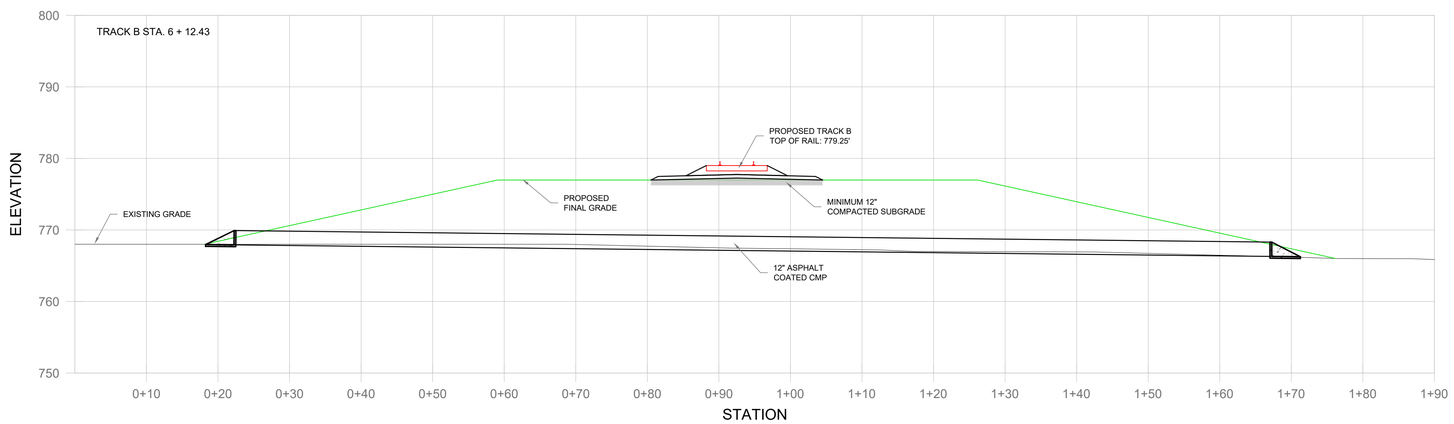
TRACK B EQUALIZATION PIPE PROFILE



TRACK B TYPICAL CROSS SECTION PROFILE



TRACK B SKEWED PIPE PROFILE



STATE OF KENTUCKY
SHANE COCHRAN
32720
02/21/2024
LICENSED PROFESSIONAL ENGINEER

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HM
Architects/Engineers, Inc.
50 Security Drive, Jackson, Tennessee 38305
Telephone:(731)664-6330, Fax:(731)664-6339

SCALE 1" = 10'

DRAWN	A/E	PM
BCF	SSC	RB/BB

SHEET TITLE

PROJECT
FACILITY EXPANSION FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

RAILROAD
CROSS SECTIONS

PROJECT NUMBER 21030

SHEET NUMBER CE-6.7

PLOT DATE 2/20/2024 4:38 PM

- ■ PLAN PREPARED BY OUTSIDE PARTY - SUBMITTED TO NS D&C FOR REVIEW
- ☐ PLAN PREPARED BY NS D&C USING INFORMATION PROVIDED BY OTHERS.
- ☐ PRELIMINARY PLAN - NOT APPROVED FOR CONSTRUCTION
- ISSUED BY NS D&C FOR TRANSPORTATION DEPARTMENT REVIEW ON:
- ■ APPROVED PLAN - ACCEPTED FOR CONSTRUCTION BY NS D&C ON:1/4/23

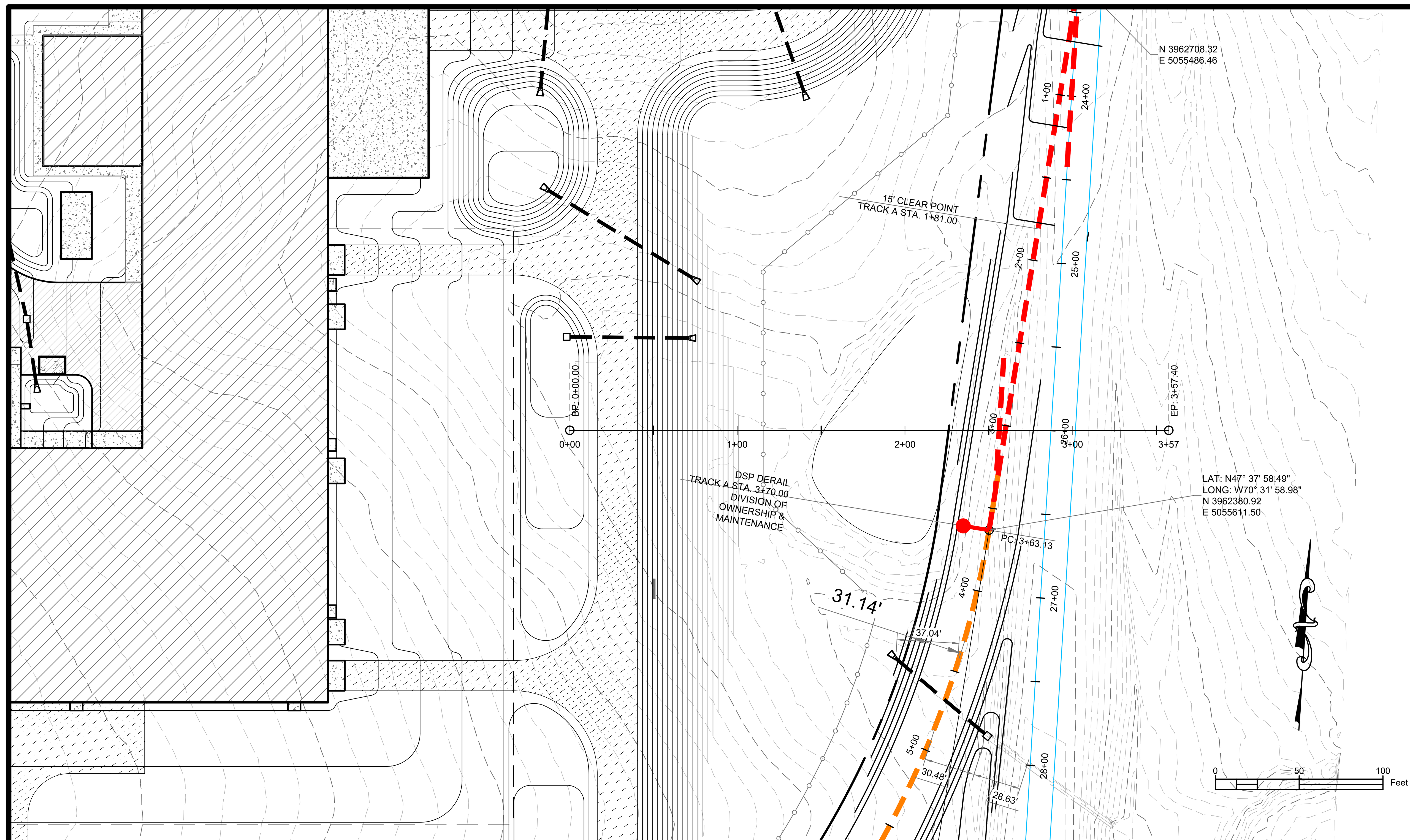
NORFOLK SOUTHERN
NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY
MIDWEST
OPERATING DIVISION
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

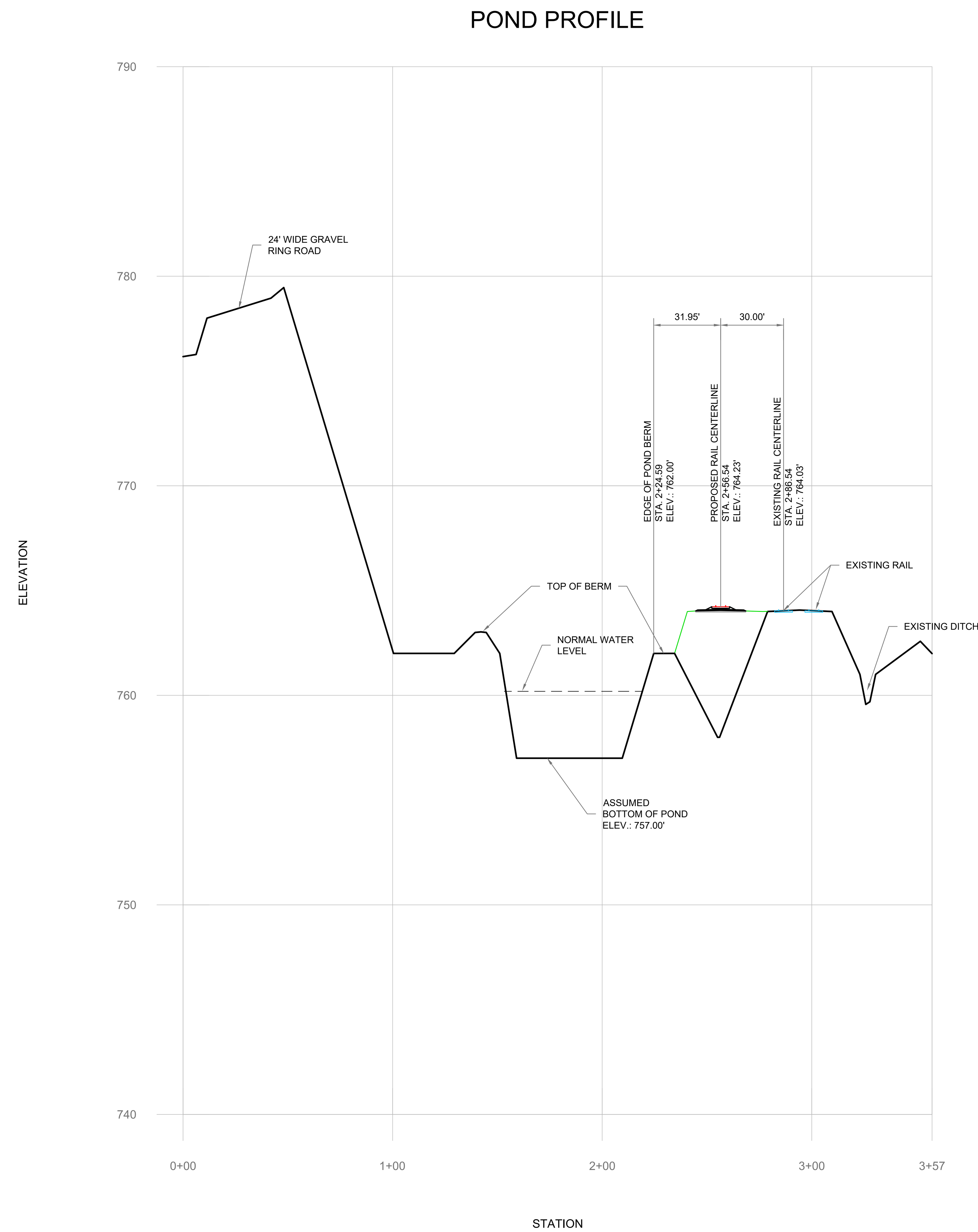
REV	BY	DATE	DESCRIPTION

LOCATION SHELBYVILLE, KENTUCKY
FID* Id2720
TITLE PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
PIN 0340701
AL SEC / MAP 90/8A
FILE TRK1115240
DRAWING NUMBER AD-2024-0
MILE POST 384.64 W
DATE

PRELIMINARY - NOT FOR CONSTRUCTION

SHELBY COUNTY, KENTUCKY





LEGEND

- EXISTING TRACK
- RAILROAD OWNERSHIP
- INDUSTRY OWNERSHIP
- PROPERTY BOUNDARY
- ADJACENT PROPERTY BOUNDARY
- BUILDING SETBACK
- RIGHT OF WAY
- UTILITY EASEMENT
- ROADWAY CENTER LINE
- EXISTING OVERHEAD ELECTRIC
- EXISTING UNDERGROUND GAS
- EXISTING UNDERGROUND SANITARY SEWER LINE
- PIPE (DRAINAGE STRUCTURE)
- PROPOSED HEAVY DUTY ASPHALT
- PROPOSED LIGHT DUTY ASPHALT
- PROPOSED HEAVY DUTY CONCRETE
- PROPOSED LIGHT DUTY CONCRETE
- PROPOSED GRAVEL
- PROPOSED BUILDING

NOTES:

1. THESE NOTES ARE INTENDED TO SUPPLEMENT THE INFORMATION CONTAINED IN THE DESIGN DOCUMENTS AND SPECIFICATIONS. IF A CONFLICT EXISTS BETWEEN DESIGN DOCUMENTS AND SPECIFICATIONS AND THE NORFOLK SOUTHERN RAILWAY COMPANY SPECIFICATIONS FOR DESIGN AND CONSTRUCTION OF PRIVATELY OWNED INDUSTRY TRACKS, REVISED JULY 2020, THE MOST STRINGENT REQUIREMENT SHALL GOVERN.
2. CONTRACTOR SHALL VERIFY ALL EXISTING ELEVATIONS, DIMENSIONS, AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND SHALL NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPENCIES.
3. CONTRACTOR IS RESPONSIBLE FOR OBTAINING RIGHT-OF-ENTRY FROM RAILROAD PRIOR TO ENTERING THE RAILROAD RIGHT-OF-WAY.
4. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING STRICT CONTROL OF SITE RUN-OFF AT ALL TIMES TO PREVENT ANY FORM OF WATER POLLUTION.
5. NO WORK SHALL COMMENCE ON RAILROAD RIGHT-OF-WAY PRIOR TO NOTIFYING THE RAILROAD ENGINEER.
6. CONTRACTOR SHALL COMPLY WITH ALL FEDERAL, STATE, AND LOCAL REGULATIONS.
7. NO FIELD CHANGES SHALL BE PERMITTED WITHOUT WRITTEN APPROVAL FROM H+M ARCHITECTS/ENGINEERS, INC.
8. GRADING, DRAINAGE, AND PLACEMENT OF SUB-BALLAST SHALL BE AT THE EXPENSE OF THE OWNER.
9. PROPOSED DRAINAGE STRUCTURES SHALL COMPLY WITH A.R.E.M.A. PART 5 SPECIFICATIONS AND COMPLY WITH NORFOLK SOUTHERN STANDARDS.
10. EXISTING AND/OR PROPOSED UTILITIES SHALL BE RELOCATED, ADJUSTED, AND/OR PROTECTED IN COMPLIANCE WITH NORFOLK SOUTHERN STANDARDS AT THE OWNERS EXPENSE.
11. CLEARANCES TO EXISTING AND/OR PROPOSED STRUCTURES SHALL ADHERE TO NORFOLK SOUTHERN STANDARD PLANS 7-1.
12. DETAILED DESIGN PLANS FOR LOADING/UNLOADING FACILITIES, INCLUDING HORIZONTAL AND VERTICAL CLEARANCES, SHALL BE APPROVED BY THE RAILROAD PRIOR TO CONSTRUCTION OF SUCH FACILITIES.
13. BONDING OF TRACKS, IF REQUIRED, SHALL BE IN COMPLIANCE WITH NORFOLK SOUTHERN STANDARDS.
14. A BOW HANDLED THROW IS REQUIRED FOR ALL SWITCHES.
15. TIE AND RAIL AT CLEAR POINTS WITHOUT A DERAIL SHALL BE PAINTED FLUORESCENT GREEN.

OPERATIONS:

NORFOLK SOUTHERN SHALL DELIVER EMPTY 75' BOX CARS OR 70' FLAT CARS AND DROP THEM ON TRACK A OR TRACK C STORAGE LINES. EMPTY CARS SHALL BE MOVED TO THE LOADING DOCK BY WIELAND VIA INDUSTRY OWNED TRACKMOBILE AND FULL CARS SHALL BE STORED ON THE OPPOSITE LINE. NORFOLK SOUTHERN SHALL PICK UP LOADED CARS WHEN DROPPING EMPTY CARS. CARS SHALL ONLY BE STORED IN DESIGNATED STORAGE OR LOADING AREAS.

OWNER/ADDRESS: WIELAND NORTH AMERICA
4803 OLYMPIA PARK PLAZA, SUITE 300
LOUISVILLE, KENTUCKY 40241

CONTROL POINTS (KY STATE PLANE NAD 83, KY SINGLE ZONE, NAVD88)

POINT NUMBER	NORTHING	EASTING	ELEVATION	DESCRIPTION
1	3962944.93	5054168.06	778.35	CP /IPC ⅝
2	3963175.09	5055144.06	800.08	CP /IPC ⅝
4	3962017.71	5054678.34	787.65	CP /IPC ⅝
5	3962015.14	5055078.03	773.75	CP /IPC ⅝

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SCALE	1" = 50'	
DRAWN	A/E	PM
BCF	SSC	RB/BB

PROJECT: NEW FACILITY FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE: FARM POND CROSS-SECTION

PROJECT NUMBER: 21030

SHEET NUMBER: CE-6.7A

PLOT DATE: 2/20/2024 4:39 PM

NORFOLK SOUTHERN
NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY
MIDWEST
OPERATING DIVISION
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

REV	BY	DATE	DESCRIPTION

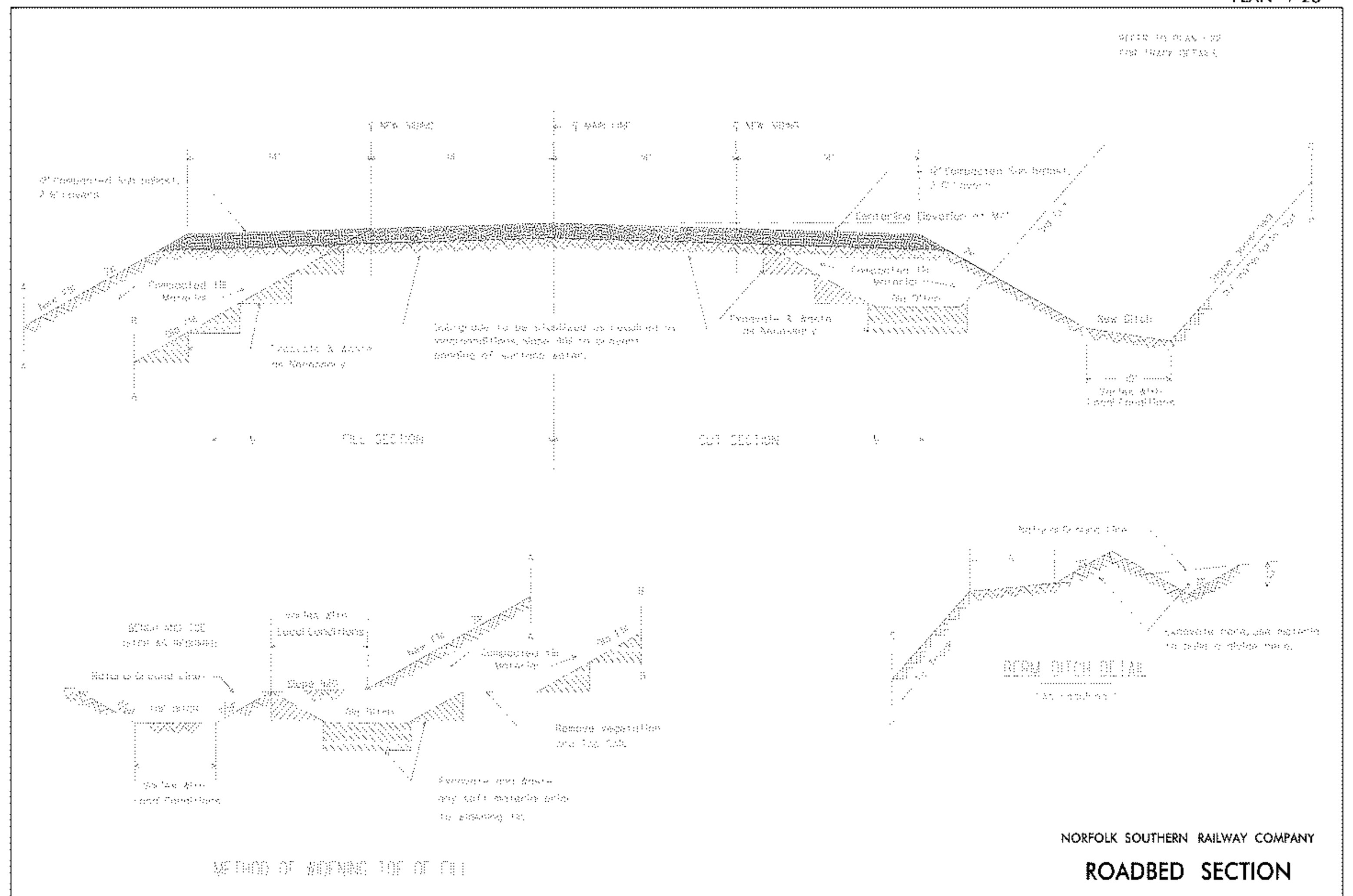
LOCATION: SHELBYVILLE, KENTUCKY

PID#: Id2720
VRN: 0340701
TITLE: PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
VAL SEC / MAP: 90/8A
MILE POST: 304.64 W
FILE: TRK1115240
DATE:
DRAWING NUMBER: AD-2024-0

PRELIMINARY - NOT FOR CONSTRUCTION

SHELBY COUNTY, KENTUCKY

PLAN 1-23



NORFOLK SOUTHERN RAILWAY COMPANY

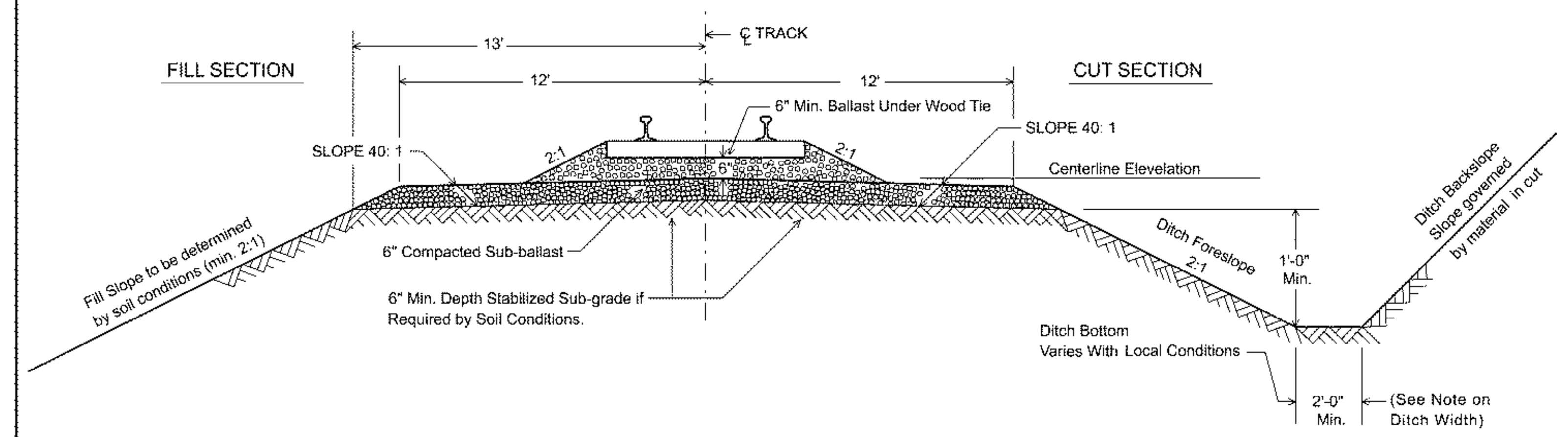
ROADBED SECTION

NEW SIDING PARALLEL TO EXISTING SINGLE MAIN LINE
JANUARY 1989
Atlanta, Georgia

PLAN 1-22

INDUSTRIAL TRACKS

PLAN NOT TO SCALE



GENERAL NOTES

(1) Sub-grade may be stabilized with lime, lime-fly ash, cement or stone.

(2) Tamping of ballast must not disturb compacted sub-ballast.

(3) Top of sub-grade is to be crowned.

DITCH WIDTH NOTES

(1) On NORFOLK SOUTHERN right-of-way, minimum ditch width should be calculated to carry volume of water for a 100 year storm event, or be at a minimum 5' wide, whichever is greater.

(2) Where ditch back slopes on NORFOLK SOUTHERN right-of-way are in cut sections greater than 25' in height, ditch width should be increased to a minimum of 10' wide.

NOTE:
Roadbed Sections Proposed Near Structures And/Or In Pavement Locations Will Need To Be Designed And Submitted For Review And Approval.

DATE	REVISION
11/06	ADDED CUT SECTION NOTES
03/10	ADD NOTE & 1' TO DITCH BOTTOM

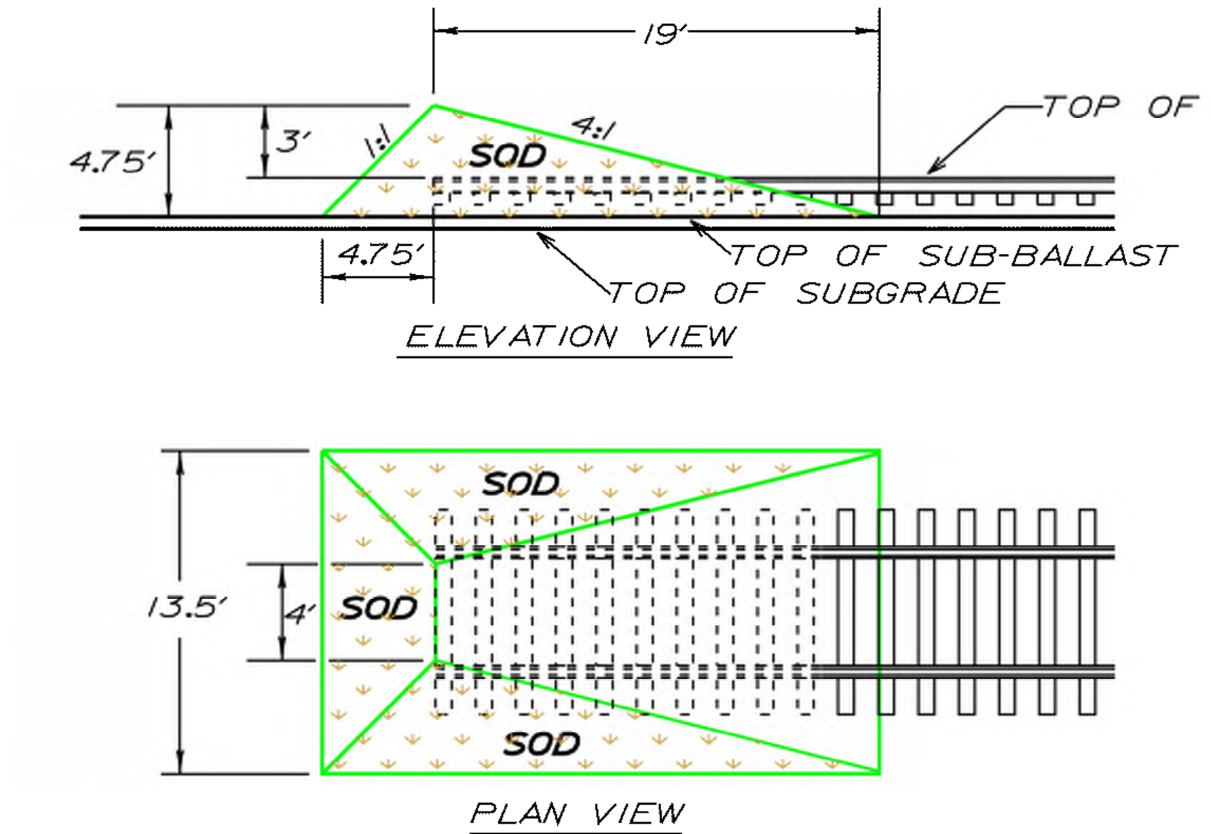
NORFOLK SOUTHERN RAILWAY COMPANY

ROADBED SECTION

FOR INDUSTRIAL TRACKS

JANUARY 1989
Atlanta, Georgia

DETAIL

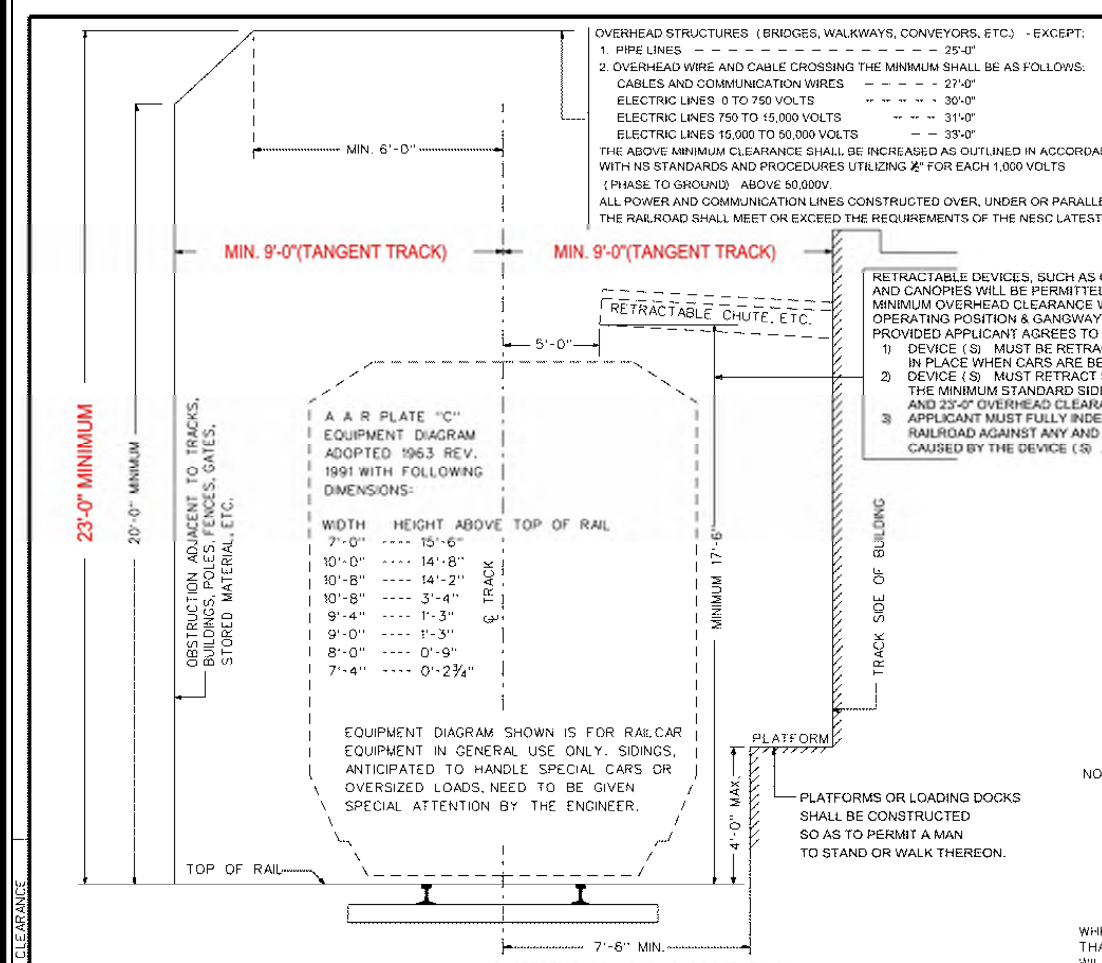


NORFOLK SOUTHERN RAILWAY COMPANY

EARTH MOUND DETAIL

AUGUST 2000
Atlanta, Georgia

PLAN 7-1



OVERHEAD STRUCTURES (BRIDGES, WALKWAYS, CONVEYORS, ETC.) - EXCEPT:
1. PIPE LINES ---------------- 25'-0"
2. OVERHEAD WIRE AND CABLE CROSSING THE MINIMUM SHALL BE AS FOLLOWS:
CABLES AND COMMUNICATION WIRES ----- 27'-0"
ELECTRIC LINES 0 TO 750 VOLTS ----- 30'-0"
ELECTRIC LINES 750 TO 15,000 VOLTS --- 31'-0"
ELECTRIC LINES 15,000 TO 50,000 VOLTS -- 33'-0"
THE ABOVE MINIMUM CLEARANCE SHALL BE INCREASED AS OUTLINED IN ACCORDANCE WITH NS STANDARDS AND PROCEDURES UTILIZING ½" FOR EACH 1,000 VOLTS (PHASE TO GROUND) ABOVE 50,000V.
ALL POWER AND COMMUNICATION LINES CONSTRUCTED OVER, UNDER OR PARALLEL TO THE RAILROAD SHALL MEET OR EXCEED THE REQUIREMENTS OF THE NESC LATEST REVISION

RETRACTABLE DEVICES, SUCH AS CHUTES, SPOUTS AND CANOPIES WILL BE PERMITTED WITH 17'-6" MINIMUM OVERHEAD CLEARANCE WHEN IN THE OPERATING POSITION & GANGWAYS WILL BE PERMITTED PROVIDED APPLICANT AGREES TO THE FOLLOWING:
1) DEVICE (S) MUST BE RETRACTED AND LOCKED IN PLACE WHEN CARS ARE BEING MOVED.
2) DEVICE (S) MUST RETRACT SO AS TO PROVIDE THE MINIMUM STANDARD SIDE CLEARANCE AND 23'-0" OVERHEAD CLEARANCE.
3) APPLICANT MUST FULLY INDEMNIFY THE RAILROAD AGAINST ANY AND ALL DAMAGES CAUSED BY THE DEVICE (S).

ADDED SIDE CLEARANCES, REQUIRED FOR STRAIGHT TRACK AT END OF CURVE

DISTANCE FROM ADJACENT OBSTRUCTION TO NEAR END OF CURVED TRACK	INCREASE IN SIDE CLEARANCE PER DEGREE OF CURVATURE
0 - 22 FT.	1½ INCHES
23 - 45 FT.	1 INCH
46 - 68 FT.	½ INCH
69 - 90 FT.	¼ INCH

TABLE OF CURRENT STATE LEGAL CLEARANCE REQUIREMENTS

	TRACK CENTERS		CLEARANCE							
			VERTICAL		HORIZONTAL					
	BETWEEN INDUSTRIAL TRACKS	BETWEEN MAIN & INDUSTRIAL TRACK	GENERAL	BUILDING DOORS	BUILDING DOORS	℄ OF TRACK A	B	C	POLES	GENERAL
VIRGINIA •										
ALABAMA •	N	N	N	N	N	N	N	N	N	N
FLORIDA	13'-6"	13'-6"	22'	17'	8'	5'-9"	CFH	8'	10'	8'
GEORGIA •										
ILLINOIS	13'-6"	15'	23'-0"	H	7'	6'-2"	CFH	8'	9'	8'
INDIANA	N	N	22'	22'	8'	*	N	8'	8'	8'
KENTUCKY	N	N	N	N	N	N	N	N	N	N
LOUISIANA •										
MISSISSIPPI •										
N. CAROLINA •										
OHIO	14'	15'	23'	21'	8'	*	N	8'	8'	8'
S. CAROLINA	N	N	N	N	N	N	N	N	N	N
TENNESSEE	13'	14'	22'	17'	8'	7'-6"	4'	8'	8'	8'
IOWA •										
MARYLAND	13'-6"	15'	22'	18'	8'	5'-7"	4'	8'-6"	8'-6"	8'-6"
MICHIGAN	14'	14'	22'-6"	22'-6"	8'-6"	N	N	N	8'-6"	9'-0"
MISSOURI	14'	14'	22'	22'	8'-6"	5'-9"	CFH	8'-6"	8'-6"	8'-6"
NEW YORK	13'-6"	15'	22'	18'	8'	8'-6"	4'	8'-6"	8'-6"	8'-6"
PENNSYLVANIA	13'-6"	15'	22'	18'	8'	8'-6"	4'	12'	12'	12'
W. VIRGINIA	14'	15'	22'	22'	8'	6'-4"	4'	8'	10'	8'
DELAWARE**										
NEW JERSEY	N	N	N	N	N	N	N	N	N	N

NOTE: ABOVE EXCERPTED FROM AREMA CHART SHOWING LEGAL CLEARANCE REQUIREMENTS LAST REVISED 2004
• NO CURRENT REGULATIONS
CFH - CAR FLOOR HEIGHT
H - HEIGHT OF CAR GOVERNS
N - NO REGULATION GIVEN
* - MUST APPLY TO STATE PUBLIC SERVICE COMMISSION FOR LESS THAN 8'-0" LATERAL CLEARANCE TO PLATFORMS.
** - AREMA STANDARDS APPLY

WHERE PUBLIC LAW OR REGULATION REQUIRES CLEARANCE GREATER THAN RAILWAY COMPANY STANDARD, SUCH LAWS AND REGULATIONS WILL GOVERN. SEE ABOVE TABLE.

WHERE NORFOLK SOUTHERN STANDARDS SPECIFY CLEARANCES GREATER THAN STATE STANDARDS, NORFOLK SOUTHERN CLEARANCE SHALL APPLY.

NOTE:
1. CLEARANCES APPLY BOTH TO STUB END TRACK AND TRACK CONNECTED AT BOTH ENDS.
2. CLEARANCES APPLY BOTH TO OUTSIDE AND INSIDE OF BUILDING.
3. DOORWAY FOR TRACK ENTERING BUILDING MUST HAVE 8'-0" SIDE CLEARANCE AND 22'-0" OVERHEAD CLEARANCE.
4. SIGN MUST BE PLACED ON EACH SIDE OF DOORWAY TO WARN OF CLOSE SIDE CLEARANCE.
5. INDUSTRY TRACK MUST BE NOT LESS THAN 15'-0" CENTER TO CENTER WITH MAIN AND RUNNING TRACK OF RAILWAY COMPANY. MINIMUM DISTANCE, CENTER TO CENTER, FOR ALL OTHER TRACKS TO BE 14'-0".
6. WHERE THE TRACK IS CURVED ALL SIDE CLEARANCES TO AN ADJACENT OBSTRUCTION MUST BE INCREASED 1½ INCHES FOR EACH DEGREE OF TRACK CURVATURE.
7. WHERE THE TRACK IS CURVED WITHIN 90 FEET OF AN ADJACENT OBSTRUCTION, THE SIDE CLEARANCES MUST BE INCREASED AS PER THE TABLE IN THE UPPER RIGHT HAND CORNER.
8. FOR ANY STRUCTURE CROSSING OR PROJECTING OVER TRACK, INDUSTRY MUST SUBMIT PLANS BEARING THE SEAL OF A REGISTERED PROFESSIONAL ENGINEER TO THE DIVISION SUPERINTENDENT FOR FORWARDING TO THE CHIEF ENGINEER BRIDGES AND STRUCTURES FOR APPROVAL.

7-20-18 REVISE OVERHEAD MIN. CLEARANCE

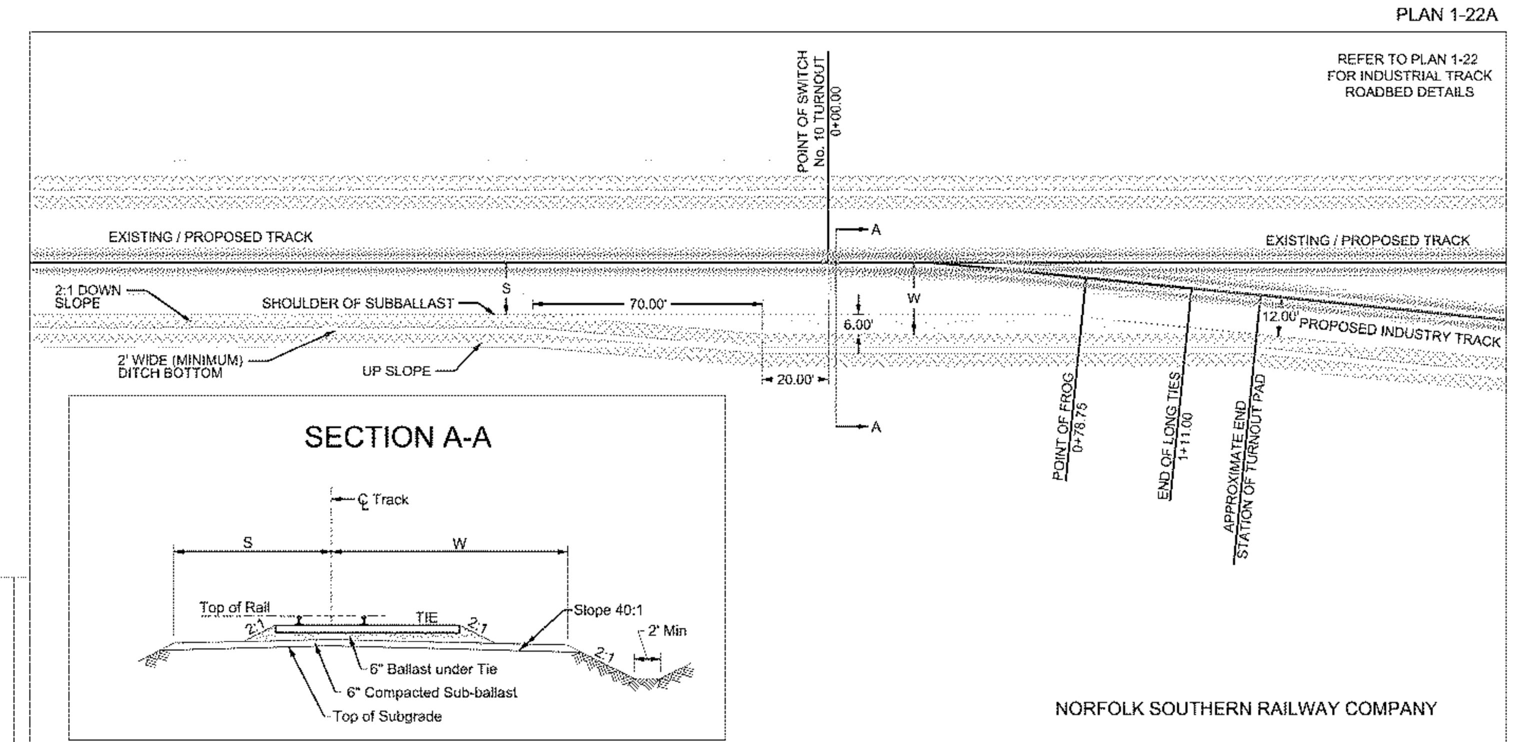
NORFOLK SOUTHERN RAILWAY COMPANY

CLEARANCES FOR TRACKS LOCATED ON INDUSTRY PROPERTY

MARCH 1994
Atlanta, Georgia

PLAN 1-22A

REFER TO PLAN 1-22 FOR INDUSTRIAL TRACK ROADBED DETAILS



	SUBBALLAST WIDTH FROM C/L (S)	WALKWAY WIDTH FROM C/L (W)	APPROXIMATE END STATION OF TURNOUT PAD
MAIN TRACK	14 FT	20 FT	1+12
LEAD TRACK	14 FT	20 FT	1+12
INDUSTRY TRACK	12 FT	18 FT	0+92

DATE	REVISION
5/7/15	REVISED DISTANCES / ADDED PROP. TRACK

NORFOLK SOUTHERN RAILWAY COMPANY

TURNOUT PAD

FOR No. 10 INDUSTRY TURNOUTS
MAY 2010
ATLANTA, GA

■ PLAN PREPARED BY OUTSIDE PARTY - SUBMITTED TO NS D&C FOR REVIEW
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NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY
MIDWEST
OPERATING DIVISION
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

REV	BY	DATE	DESCRIPTION

LOCATION: SHELBYVILLE, KENTUCKY
PID#: Id2720 | TITLE: PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
VRN: 0340701
VAL SEC / MAP: 90/8A | FILE: TRK1115240 | DRAWING NUMBER: AD-2024-0
MILE POST: 304.64 W | DATE:

STATE OF KENTUCKY
SHANE COCHRAN
32720
02/21/2024
LICENSED PROFESSIONAL ENGINEER

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50 Security Drive, Jackson, Tennessee 38305
Telephone:(731)664-6330, Fax:(731)664-6339

SCALE: NONE
DRAWN: BCF | A/E: SSC | PM: RB/BB

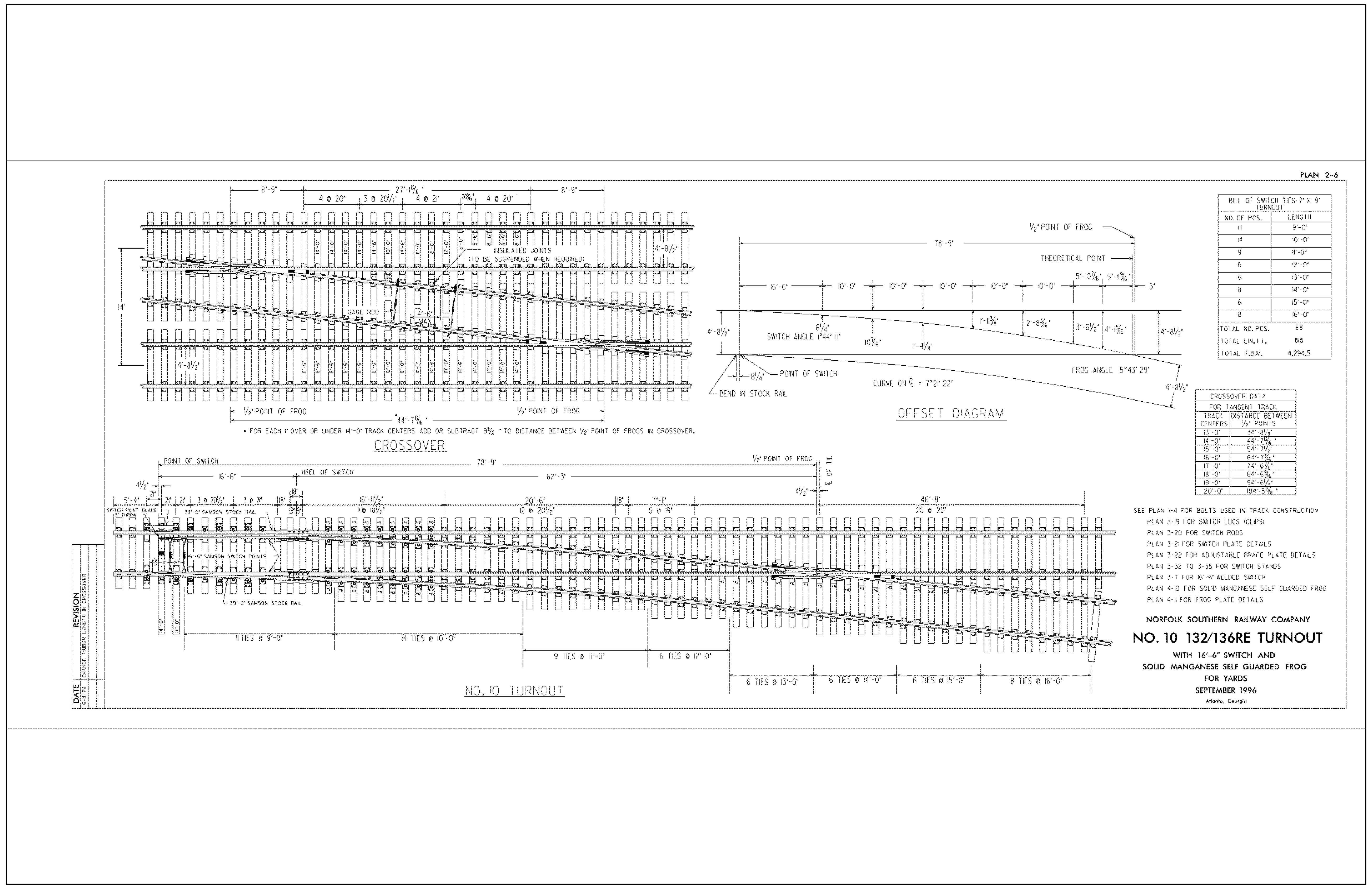
PROJECT: FACILITY EXPANSION FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

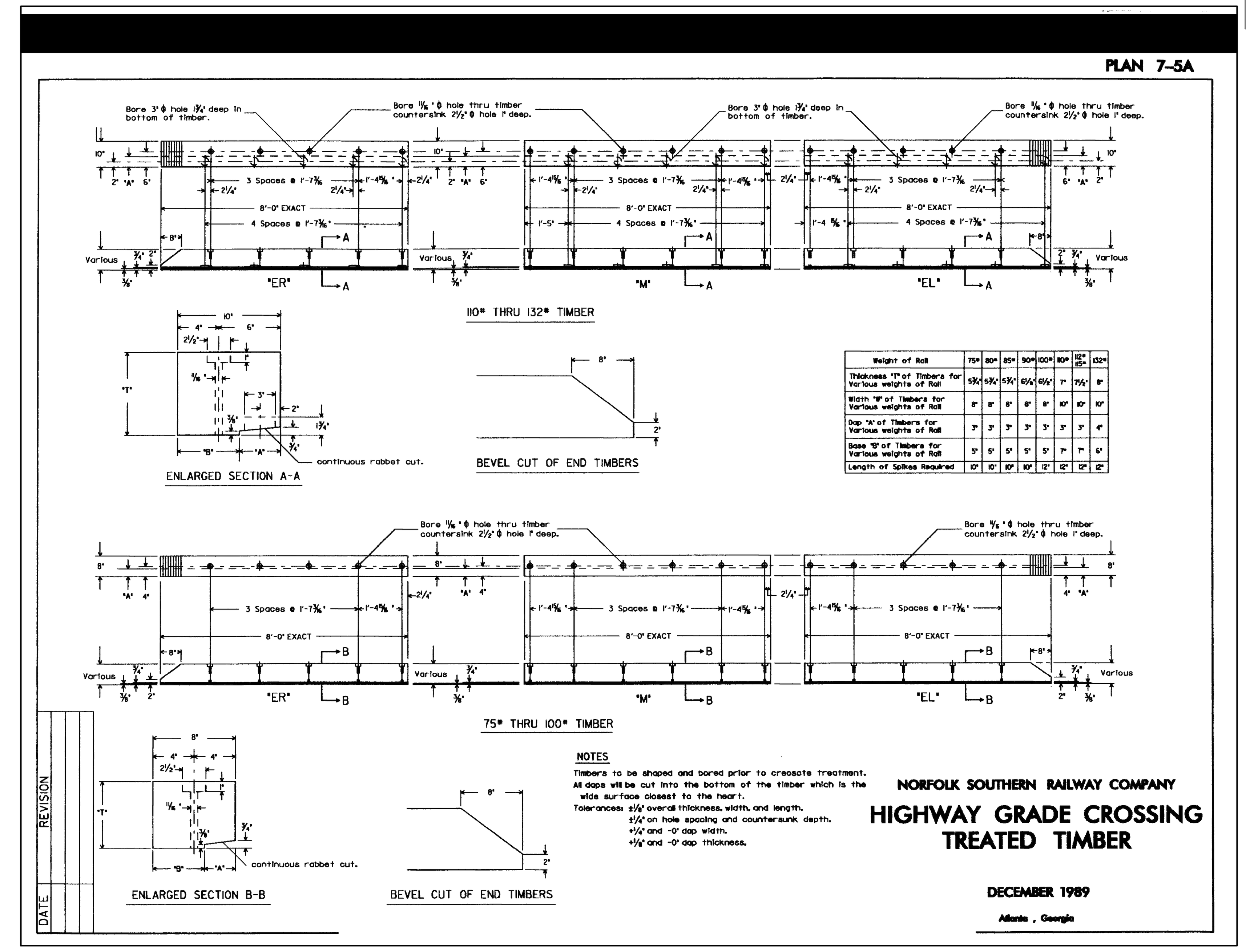
SHEET TITLE: RAILROAD DETAIL SHEET

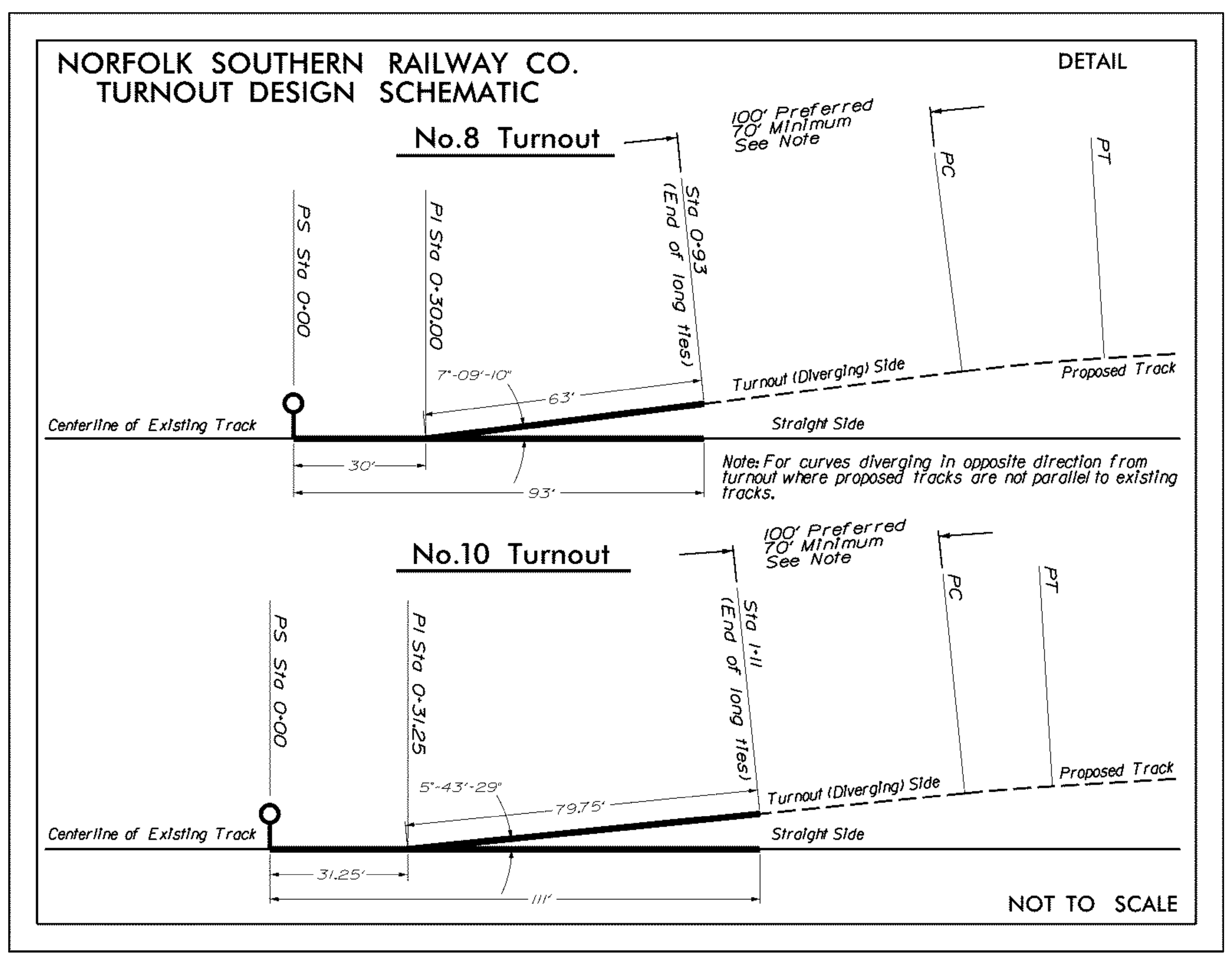
PROJECT NUMBER: 21030
SHEET NUMBER: CE-6.8
PLOT DATE: 2/20/2024 4:39 PM

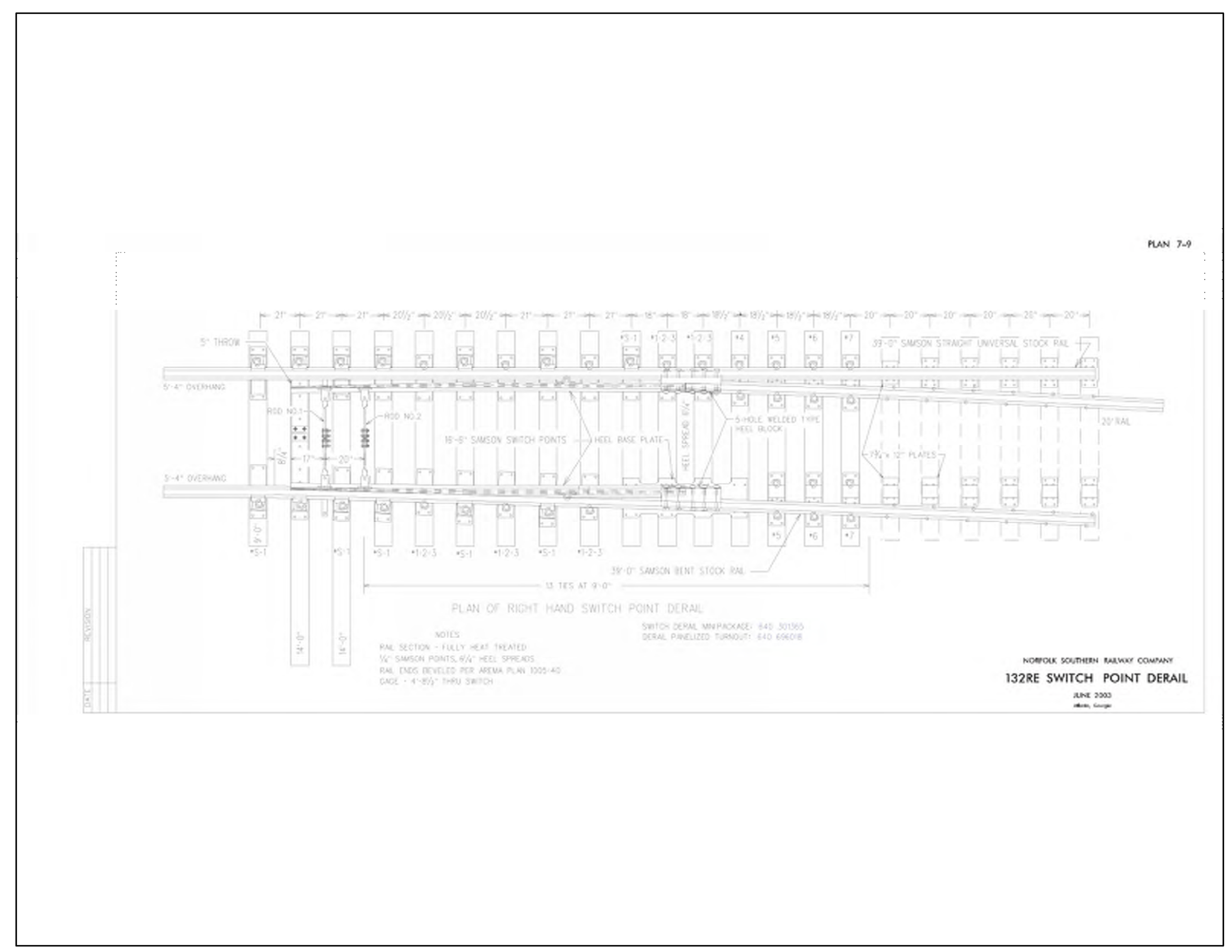
PRELIMINARY - NOT FOR CONSTRUCTION

SHELBY COUNTY, KENTUCKY









SHANE COCHRAN 32720 — STATE OF KENTUCKY LICENSED PROFESSIONAL ENGINEER — 02/21/2024

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H+M Architects/Engineers, Inc.
50 Security Drive, Jackson, Tennessee 38305
Telephone:(731)664-6330, Fax:(731)664-6339

SCALE: NONE
DRAWN: LAB | A/E: SSC | PM: RB/BB

PROJECT: FACILITY EXPANSION FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE: RAILROAD DETAIL SHEET

PROJECT NUMBER: 21030
SHEET NUMBER: CE-6.9
PLOT DATE: 2/20/2024 4:40 PM

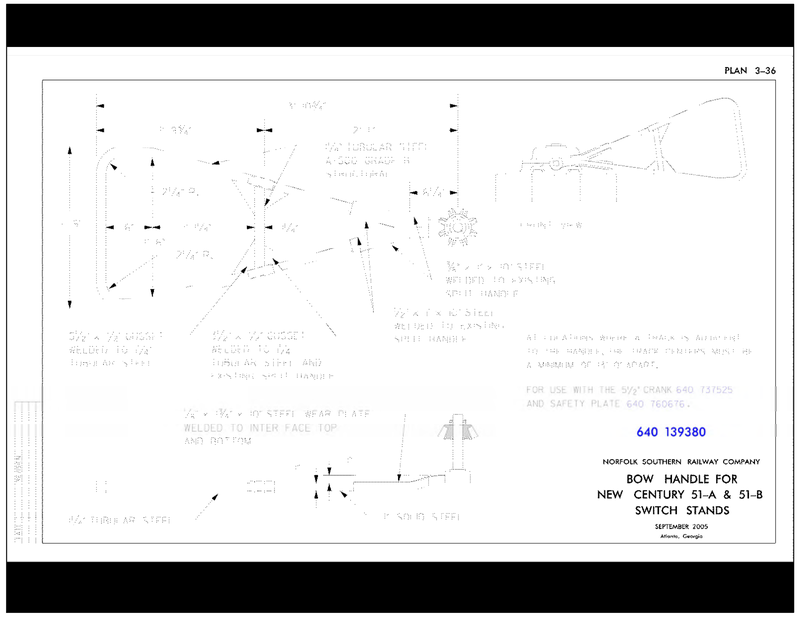
■ PLAN PREPARED BY OUTSIDE PARTY - SUBMITTED TO NS D&C FOR REVIEW
□ PLAN PREPARED BY NS D&C USING INFORMATION PROVIDED BY OTHERS.
□ PRELIMINARY PLAN - NOT APPROVED FOR CONSTRUCTION - ISSUED BY NS D&C FOR TRANSPORTATION DEPARTMENT REVIEW ON:
■ APPROVED PLAN - ACCEPTED FOR CONSTRUCTION BY NS D&C ON: 1/4/23

NORFOLK SOUTHERN
NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY
MIDWEST
OPERATING DIVISION
OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

REV	BY	DATE	DESCRIPTION

LOCATION: SHELBYVILLE, KENTUCKY
PID#: Id2720
VRN: 0340701
TITLE: PROPOSED TRACKS TO SERVE WIELAND COPPER RECYCLING
VAL SEC / MAP: 90/8A
FILE: TRK1115240
MILE POST: 304.64 W
DATE:
DRAWING NUMBER: AD-2024-0

PRELIMINARY - NOT FOR CONSTRUCTION



CORRUGATED METAL CULVERT GAGE SELECTION AND INSTALLATION UNDER NS-OWNED TRACKS

CORRUGATED METAL PIPE
GAGE TABLE FOR 2 2/3" X 1/2" CORRUGATIONS

AREA (SQ.FT.)	DIA. (IN.)	HEIGHT OF COVER (FEET) 1-10	11-20	21-30	31-40	41-50	51-60	61-70	71-80	81-90	91-100
1.8	18										
2.4	21	12 GAGE								10	10
3.1	24								10	10	10
4.9	30					10	10	10	8	8	8
7.1	36				10	8	8	8	8	8	8
9.6	42			10	8	8					
12.6	48	USE ELONGATED CORRUGATED METAL PIPE FOR THESE TABULAR VALUES									
13.9	54										
19.6	60										
23.8	66										
28.3	72										

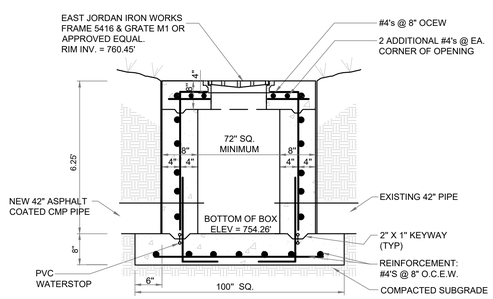
ELONGATED CORRUGATED METAL PIPE
GAGE TABLE FOR 2 2/3" X 1/2" CORRUGATIONS

DIA. (IN.)	HEIGHT OF COVER (FEET) 1-10	11-20	21-30	31-40	41-50	51-60	61-70	71-80	81-90	91-100
18	USE ROUND CORRUGATED METAL PIPE FOR THESE TABULAR VALUES									
21										
24										
30										
36										
42						8	8	8	8	8
48	10	10	8	8	8	8	8	8	8	8
54	8	8	8	8	8	8	8	8		
60	8	8	8	8	8					
66	8	8	8							
72	8	8								

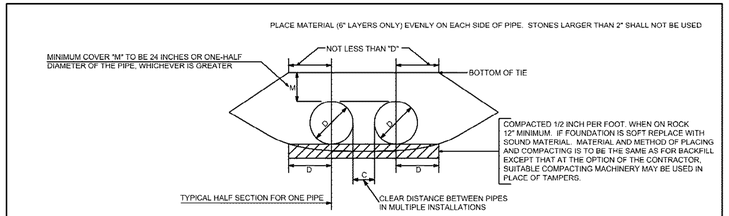
NOTES

1. MINIMUM DIAMETER : TO BE 36" UNLESS APPROVED BY BRIDGE OFFICE.
2. MATERIAL : TO BE GALVANIZED STEEL, ASTM A760 TYPE 1 PIPE, WITH 2 2/3" X 1/2" ANNULAR CORRUGATION.
3. COATING : PIPE AND BANDS TO BE FULLY ASPHALT COATED, CONFORMING TO AASHTO M190 TYPE A.
4. CONNECTING BANDS : MAY BE ONE GAGE LIGHTER THAN CULVERT MATERIAL. ALL BANDS UNDER COMPANY OWNED TRACKS ARE TO BE ONE PIECE. TWO FEET WIDE. CULVERTS 48" DIAMETER AND LARGER TO USE TWO FOOT BAND WITH FOUR - 1/2" DIAMETER RODS AND SILO LUGS.
5. INSTALLATION : WHERE CULVERTS ARE INSTALLED IN OPEN CUT TO REPLACE FAILED CULVERTS, BEDDING AND BACKFILL TO BE DENSE GRADED AGGREGATE.
6. ELONGATED PIPE : SHALL NOT HAVE LESS THAN 5% VERTICAL ELONGATION. ELONGATED PIPE 42" IN DIAMETER AND LARGER SHALL BE SHOP WIRE STRUTTED.
7. EXCEPTIONS : THE GAGE TABLES ARE TO PROVIDE ADEQUATE STRENGTH. EXCEPTIONS MUST BE CLEARED BY BRIDGE OFFICE. WHERE KNOWN CONDITIONS WILL AFFECT THE SERVICE LIFE OF THE CULVERT, ADDITIONAL GAGE REQUIREMENTS WILL BE FURNISHED BY THE BRIDGE OFFICE.
8. MINIMUM COVER : SHALL BE 24" OR 1/2 PIPE DIAMETER, WHICHEVER IS GREATER, AS MEASURED BETWEEN BOTTOM OF TIE AND TOP OF PIPE. EXCEPTIONS TO THIS CRITERIA WILL BE CONSIDERED ON AN INDIVIDUAL BASIS.

REVISED MAY 2, 2017



EXISTING PIPE EXTENSION DETAIL



DETAILS FOR BEDDING AND BACKFILLING PIPE
INSTALLATION INSTRUCTIONS FOR CORRUGATED METAL DRAINAGE STRUCTURES

PROTECTION: A REASONABLE AMOUNT OF CARE MUST BE TAKEN IN HANDLING CORRUGATED METAL DRAINAGE PRODUCTS TO PROTECT AGAINST CRACKING OR BREAKING THE PIPE OR DAMAGING THE GALVANIZED AND/OR OTHER COATING. HEAVY CONSTRUCTION EQUIPMENT MUST NOT BE RUN DIRECTLY OVER STRUCTURE UNTIL IT HAS BEEN BACKFILLED AND EMBANKMENT PLACED TO THE MAXIMUM HEIGHT ABOVE TOP OF PIPE AS SHOWN ON THIS DRAWING.

STRUTTING: PIPES SHOWN TO BE SHOP STRUTTED WILL HAVE TWISTED WIRE OR ROD STRUTS. STRUTS SHALL BE HORIZONTAL WHEN PIPE IS PLACED.

CAMBER: UNDER HIGH FILLS, WHERE THE FOUNDATION IS SUBJECT TO SUBSIDENCE, THE GRADE LINE WILL BE CAMBERED TO PRODUCE A STRAIGHT GRADE AFTER SUSIDENCE.

HAND TAMPED: WHEN APPROVED BY THE ENGINEER, A HAND TAMPER SHALL WEIGH NOT LESS THAN TWENTY (20) POUNDS AND HAVE A TAMPING FACE NOT LESS THAN 6" X 6".

ASSEMBLY OF STRUCTURE: CORRUGATED METAL PIPES MUST BE PLACED WITH INSIDE CIRCUMFERENTIAL LAPS POINTING DOWNSTREAM. THE LONGITUDINAL LAPS SHOULD BE AT THE SIDES OR QUARTER POINTS BUT NOT IN THE BOTTOM. STRUCTURAL PLATE PIPES WILL BE ASSEMBLED FROM DETAILED ERECTION INSTRUCTIONS WHICH ACCOMPANY EACH PIPE WHEN SHIPPED.

BEDDING AND BACKFILLING: BEDDING AND BACKFILL MATERIAL SHALL BE DENSE GRADED CRUSHED STONE MAXIMUM SIZE 2 INCHES TO DUST. BACKFILL IS TO BE PLACED IN LAYERS NOT LESS THAN 4" OR MORE THAN 6" AND COMPACTED WITH MECHANICAL TAMPERS TO NOT LESS THAN 95% MODIFIED PROCTOR. HEAVY EARTH COMPACTING EQUIPMENT SHALL NOT BE USED OVER THE PIPE UNTIL BACKFILL HAS BEEN COMPLETED AS SHOWN AND SPECIFIED ON THE PLAN. CARE MUST BE TAKEN NOT TO STRIKE THE PIPE WITH THE TAMPER.

MULTIPLE LINES OF PIPE: PIPE DIAMETERS MINIMUM CLEARANCE "C"
UP TO 72" ONE HALF DIAMETER OF THE PIPE
72" TO 180" 36"

NORFOLK SOUTHERN RAILWAYS
INSTALLATION INSTRUCTIONS
CORRUGATED METAL PIPE CULVERTS
REVISED 5/2/2017

STATE OF KENTUCKY
SHANE COCHRAN
32720
02/21/2024
LICENSED PROFESSIONAL ENGINEER

WARNING: It is a violation of the law for any person, unless, acting under the direction of a licensed architect/engineer, to alter an item in any way.

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PROJECT: FACILITY EXPANSION FOR
WIELAND NORTH AMERICA
SHELBYVILLE, SHELBY COUNTY, KY

SHEET TITLE: RAILROAD DETAIL SHEET

PROJECT NUMBER: 21030
SHEET NUMBER: CE-6.10
PLOT DATE: 2/20/2024 4:41 PM

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NORFOLK SOUTHERN
NORFOLK SOUTHERN RAILWAY COMPANY
OWNING COMPANY: MIDWEST
OPERATING DIVISION: OFFICE OF THE CHIEF ENGINEER - DESIGN AND CONSTRUCTION - ATLANTA, GA.

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PRELIMINARY - NOT FOR CONSTRUCTION

Norfolk Southern Corporation
One Constitution Avenue, NE Suite 300
Washington, DC 20002
Telephone: (202) 265-7330 (cell)
Email: rick.crawford@nscorp.com

Rick Crawford
Public Projects
Government Relations

November 21, 2024

Secretary Jim Gray
Kentucky Transportation Cabinet
200 Mero Street
Frankfort, KY 40622

Dear Secretary Gray:

I am writing in support of the application of Wieland North America Recycling ("Wieland Recycling") to the Kentucky Transportation Cabinet for the FY25 Kentucky Industrial Access and Safety Improvement (KIASI) Program.

Wieland Recycling seeks to establish critical rail connectivity for their advanced recycling facility in Shelbyville through the construction of a new industrial rail spur connecting to Norfolk Southern's freight rail network. This strategic infrastructure project will strengthen Kentucky's industrial rail network while delivering substantial economic and environmental benefits to the local community.

We at Norfolk Southern believe the project will have a significant impact on regional transportation efficiency by reducing heavy truck traffic on local roads and highways. This reduction will enhance safety for residents, decrease road maintenance costs, and reduce transportation-related emissions. The modern rail infrastructure will optimize freight movement through seamless integration with Norfolk Southern's operations while providing Wieland Recycling with efficient bulk material transport capabilities.

Norfolk Southern is proud to work with Wieland on this major initiative and confirm our willingness to provide rail service to Wieland North America Recycling's facility at 369 McConnell Way, Shelbyville, KY 40065. Norfolk Southern commits to working with Wieland to ensure the new rail infrastructure meets our Class 1 railroad standards and operational requirements.

Kentucky has demonstrated extraordinary recent economic growth, and we believe this project will catalyze further investment in the Commonwealth's advanced manufacturing sector while strengthening regional supply chains. Through reduced transportation costs, improved logistics

efficiency, and enhanced environmental sustainability, the project will deliver lasting benefits to the Shelbyville community.

Sincerely,

Rick Crawford

Rick Crawford

Shelby County Industrial & Development Foundation

Secretary Jim Gray
Kentucky Transportation Cabinet
200 Mero Street
Frankfort, KY 40622

Dear Secretary Gray:

I am writing in support of the application of Wieland North America Recycling, LLC ("Wieland Recycling") to the Kentucky Transportation Cabinet for the FY25 Kentucky Industrial Access and Safety Improvement (KIASI) Program.

Wieland Recycling seeks to establish critical rail connectivity for their advanced recycling facility at 369 McConnell Way, Shelbyville, KY 40065 through the construction of a new industrial rail spur connecting to Norfolk Southern's freight rail network. This strategic infrastructure project will strengthen Kentucky's industrial rail network while delivering substantial economic and environmental benefits to the local community.

The project will have a transformative impact on regional transportation efficiency by significantly reducing heavy truck traffic on local roads and highways. This reduction will enhance safety for residents, decrease road maintenance costs, and reduce transportation-related emissions. The modern rail infrastructure will optimize freight movement through seamless integration with Norfolk Southern's operations while providing Wieland Recycling with efficient bulk material transport capabilities.

Kentucky has demonstrated extraordinary recent economic growth, and we believe this project will catalyze further investment in the Commonwealth's advanced manufacturing sector while strengthening regional supply chains. Through reduced transportation costs, improved logistics efficiency, and enhanced environmental sustainability, the project will deliver lasting benefits to both industry and community.

Please give their application full and fair consideration. Should you have any questions, please do not hesitate to contact me.

Sincerely,

Ray Leathers
President/CEO
Phone: 502-633-5068
Cell: 502-664-1535
ray@scidf.com

Rev. 9-16-22

Required Affidavit for Bidders, Offerors and Contractors
(KRS 45A.110 & 45A.115)

Affidavit Effective for One (1) Year from Date of Execution

Instructions: Pursuant to KRS 45A.110 and 45A.115, a bidder, offeror, or contractor ("Contractor") is required to submit a Required Affidavit for Bidders, Offerors, and Contractors to be awarded a contract, or for the renewal of a contract. An authorized representative of the contracting party must complete the attestation below, have the attestation notarized, and return the completed affidavit to the Commonwealth.

Attestation

As a duly authorized representative for the Contractor, I swear and affirm under penalty of perjury, that that the Contractor has not knowingly violated campaign finance laws of the Commonwealth of Kentucky and that the award of a contract will not violate any provision of the campaign finance laws of the Commonwealth. For purposes of this attestation, "Knowingly" means that the bidder or offeror is aware or should have been aware of the existence of a violation. The bidder or offer understands that the Commonwealth retains the right to request an updated affidavit at any time.

Signature: _____ Printed Name: Maclin Simpson

Title: Manager, Strategic Development Date: 12/2/2024

Bidder or Offeror Name: _____

Address: _____

Commonwealth of Kentucky Vendor Code (If known): _____

Subscribed and sworn to before me this 2 day of December, 2024

State of: Kentucky Notary: Susan Marie Philpott

County of: Jefferson My Commission Expires: April 21, 2025

SUSAN MARIE PHILPOTT
NOTARY PUBLIC
Comm. # KYNP25700
COMMISSION EXPIRES 4/21/2025
COMMONWEALTH OF KENTUCKY