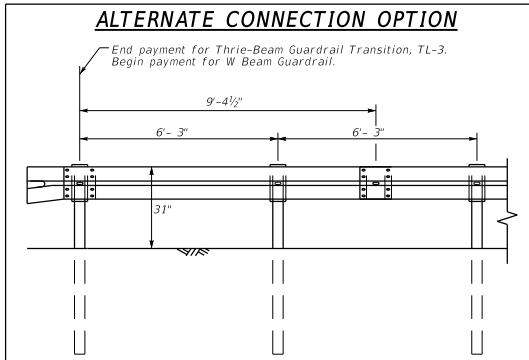
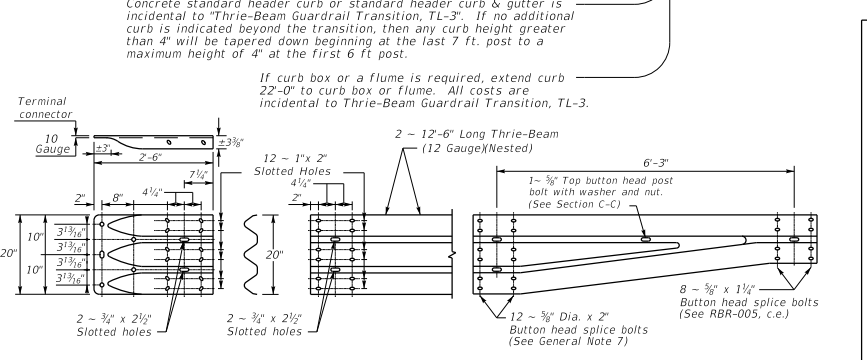


STEEL POST & BLOCK
(Showing Thrie-Beam Block)

GENERAL NOTES

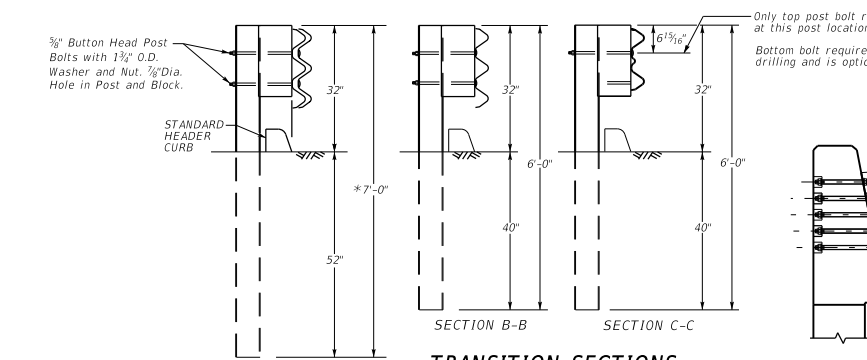
- Concrete curb shall be standard header curb or standard header curb and gutter as shown on Std. Dwg. RPM-100, c.e. and is required on all installations. At a minimum, concrete curb shall be continuous to the seventh post (first 6'-0" long post). Where a curb box or flume is not required, end curb at first 6'-0" long post as detailed. Where a curb box or flume is required extend the curb 22'-0" minimum to the curb box or flume. All costs are incidental to the lump sum price bid for Thrie-Beam Guardrail Transition, TL-3.
- Use steel posts as shown unless indicated otherwise in the plans.
- The post length shall be marked on all 7'-0" long posts by the Manufacturer. The mark shall be located within the top 1 ft. region of the post, at least 3/8" in height, and visible after installation. Steel posts shall be marked with a stencil before galvanizing.
- Rail element shall meet the requirements of AASHTO M180 except as modified on the plans. The thrie-beam terminal connector and the thrie-beam transition to w-beam shall be of the same material, but shall not be less than 10 gauge.
- Contractor shall verify that the locations of bolt holes match those in the thrie-beam terminal connector prior to ordering materials.
- Unless otherwise shown in the plans, transitions shall be placed with the block face in front of or directly above the curb face.
- Galvanized washers used with the 3/8" dia. post bolts shall be 1 3/4" O.D. washers. The (12) rectangular plate washers are required at the terminal connector splice.
- Button head "post" bolts (ASTM A307) shall be of sufficient length to extend through the full thickness of the nut (ASTM A563) and washer and not more than 1" beyond it. Button head "splice" bolts (ASTM A307) 3/8" dia. x 2" (at triple rail splices) with 3/8" double recessed nuts.
- Bolts, nuts, and washers shall be galvanized in accordance with AASHTO M232. Hardware shall be incidental to the bid item.
- If solid rock is encountered, see the standard specifications for the proper installation guidance.
- Posts shall not be set in concrete.
- This railing has been successfully evaluated by full scale crash test to meet MASH TL-3 criteria. This railing transition is to be used when speeds are over 45 mph.
- Method of measurement and basis of payment.
 - Thrie-Beam Guardrail Transition, TL-3 shall be paid at the contract unit price Each and includes the Thrie-beam terminal connector, nested Thrie-Beam elements, non symmetrical transition, posts, concrete standard header curb or standard header curb and gutter, hardware and all other incidentals necessary to complete the installation. This transition must tie into W Beam Guardrail, which is not included in this transition bid item.
 - Plastic pipe and cost of forming holes shall be included in the cost for the bridge railing.



ELEVATION VIEW

THRIE-BEAM TERMINAL CONNECTION
(See General Notes 6 & 7 for required hardware)

NON-SYMMETRICAL TRANSITION TO W-BEAM (10 Gauge)



SECTION A-A
Note: (All post types)
(See General Note 3)

CONNECTION TO CONCRETE BRIDGE RAIL AND TRAFFIC BARRIERS

(12) Galvanized rectangular washers (See Std. Dwg. RBR-005, c.e.) are required under the recessed nut at the terminal connector splice to nested thrie-beam. (See General Notes 6 & 7).

KENTUCKY DEPARTMENT OF HIGHWAYS	
THRIE-BEAM GUARDRAIL TRANSITION (TL-3)	
STANDARD DRAWING NO. BHS-014	
SUBMITTED DIRECTOR DIVISION OF STRUCTURAL DESIGN	02-26-20 DATE
APPROVED STATE ENGINEER	02-26-20 DATE