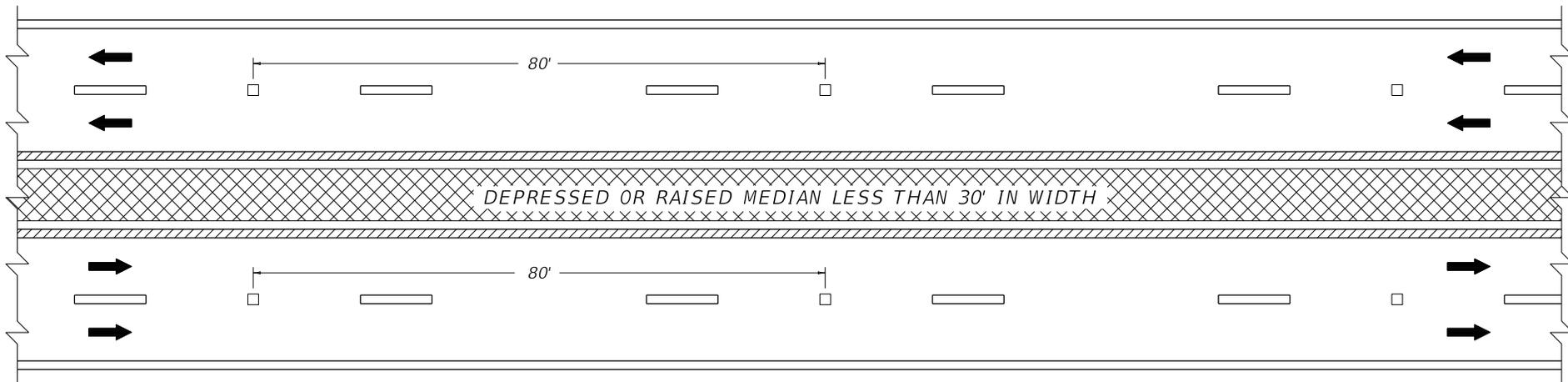


ARRANGEMENT "A" (UNDIVIDED HIGHWAY)



ARRANGEMENT "B" (DIVIDED HIGHWAY WITH DEPRESSED OR RAISED MEDIAN LESS THAN 30' IN WIDTH)

~ NOTES ~

1. MARKERS INSTALLED WITH DOUBLE YELLOW CENTERLINES SHOULD BE PLACED BETWEEN THE TWO LINES.
2. MARKERS INSTALLED ALONG LANE LINES SHOULD BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
3. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.
4. MARKERS SHALL BE INSTALLED AT 40' SPACING ALONG SOLID WHITE AUXILIARY LANES. MARKER COLOR SHALL MATCH THE MARKERS INSTALLED ALONG THE WHITE LANE LINES.

BID ITEMS AND UNIT TO BID:  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, MY)  
 INLAID PAVEMENT MARKER

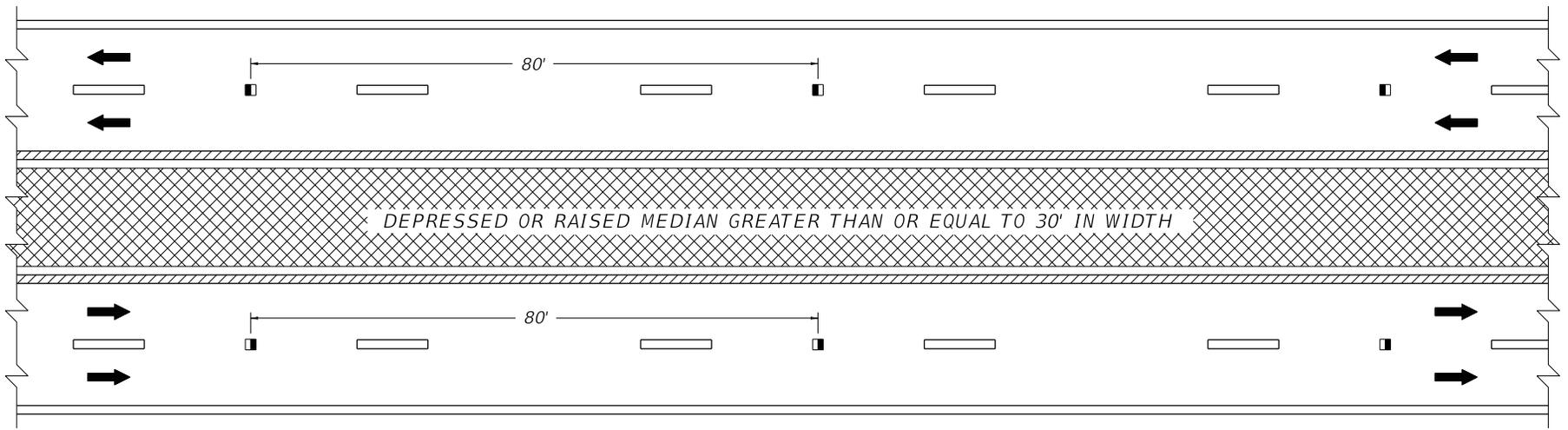
EACH  
 EACH

LEGEND	
⊗	BI-DIRECTIONAL (YELLOW)
□	MONO-DIRECTIONAL (WHITE)
▨	MARKINGS (YELLOW)
▭	MARKINGS (WHITE)
⊠	DEPRESSED OR RAISED MEDIAN

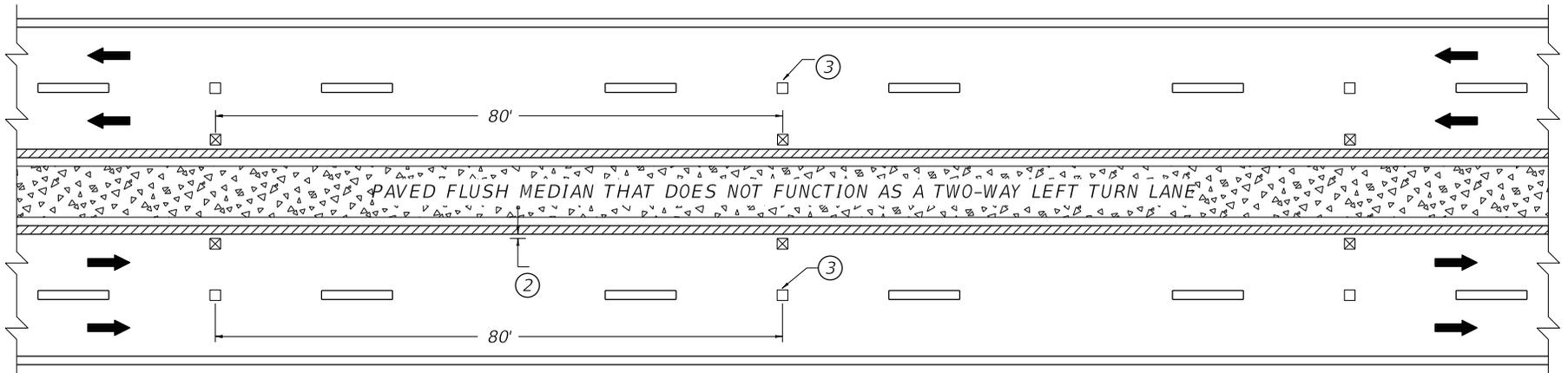
DRAWING NOT TO SCALE

KENTUCKY  
 DEPARTMENT OF HIGHWAYS  
 PAVEMENT MARKER  
 ARRANGEMENTS  
 MULTI-LANE ROADWAYS

STANDARD DRAWING NO. TPM-100-03  
 SUBMITTED *B. Allen Wolf* 02-26-20  
 DIRECTOR DIVISION OF TRAFFIC OPERATIONS DATE  
 APPROVED *[Signature]* 02-26-20  
 STATE HIGHWAY ENGINEER DATE



ARRANGEMENT "C" (DIVIDED HIGHWAY WITH DEPRESSED OR RAISED MEDIAN GREATER THAN OR EQUAL TO 30' IN WIDTH)



ARRANGEMENT "D" (DIVIDED HIGHWAY WITH PAVED FLUSH MEDIAN THAT DOES NOT FUNCTION AS A TWO-WAY LEFT TURN LANE)

~ NOTES ~

1. MARKERS INSTALLED ALONG LANE LINES SHOULD BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
- ② MARKERS INSTALLED ALONG EDGE LINES SHOULD BE PLACED SO THAT THE NEAR EDGE OF THE CASTING/GROOVE IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
- ③ IF WIDTH OF PAVED FLUSH MEDIAN IS GREATER THAN OR EQUAL TO 30', BI-DIRECTIONAL (WHITE-RED) MARKERS SHALL BE USED ALONG THE LANE LINES IN LIEU OF MONO-DIRECTIONAL (WHITE) MARKERS.
4. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.
5. MARKERS SHALL BE INSTALLED AT 40' SPACING ALONG SOLID WHITE AUXILIARY LANES. MARKER COLOR SHALL MATCH THE MARKERS INSTALLED ALONG THE WHITE LANE LINES.

BID ITEMS AND UNIT TO BID  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, MY)  
 INLAID PAVEMENT MARKER

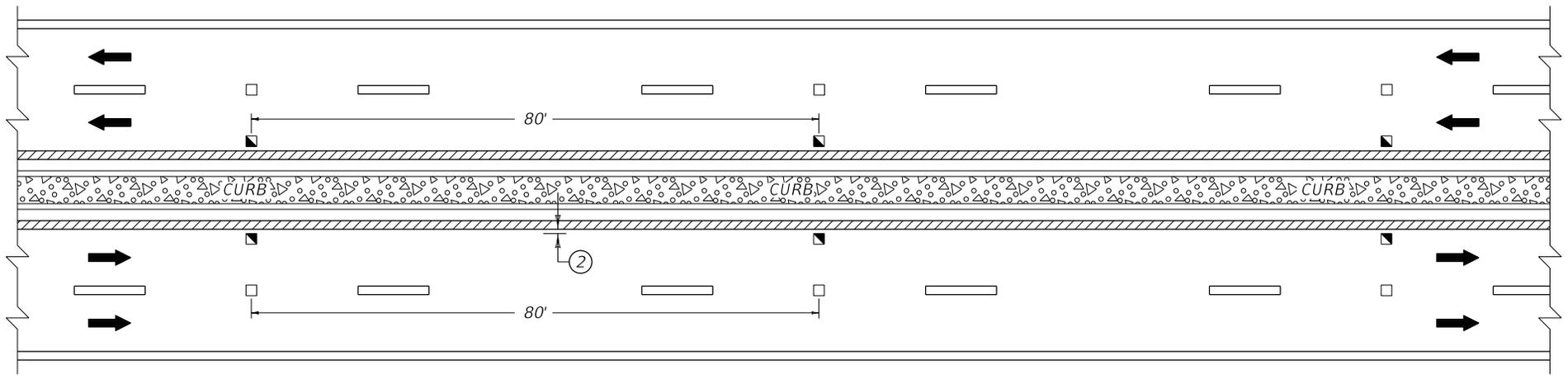
EACH  
 EACH

LEGEND	
⊗	BI-DIRECTIONAL (YELLOW)
■	BI-DIRECTIONAL (WHITE-RED)
□	MONO-DIRECTIONAL (WHITE)
▨	MARKINGS (YELLOW)
▭	MARKINGS (WHITE)
△	FLUSH MEDIAN
⊠	DEPRESSED OR RAISED MEDIAN

DRAWING NOT TO SCALE

KENTUCKY  
 DEPARTMENT OF HIGHWAYS  
 PAVEMENT MARKER  
 ARRANGEMENTS  
 MULTI-LANE ROADWAYS

STANDARD DRAWING NO. TPM-105-03  
 SUBMITTED *B. Allen Wolf* 12-01-15  
 DIRECTOR OF TRANSPORTATION OPERATIONS DATE  
 APPROVED *[Signature]* 12-01-15  
 STATE HIGHWAY ENGINEER DATE



ARRANGEMENT "E" (DIVIDED HIGHWAY WITH CURB WITHIN 8' OF DRIVING LANE)

~ NOTES ~

1. MARKERS INSTALLED ALONG LANE LINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
- ② MARKERS INSTALLED ALONG EDGE LINES SHALL BE PLACED SO THAT THE NEAR EDGE OF THE CASTING/GROOVE IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
3. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.
4. MARKERS SHALL BE INSTALLED AT 40' SPACING ALONG SOLID WHITE AUXILIARY LANES. MARKER COLOR SHALL MATCH THE MARKERS INSTALLED ALONG THE WHITE LANE LINES.

BID ITEMS AND UNIT TO BID:

PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, MY)  
INLAID PAVEMENT MARKER

EACH  
EACH

LEGEND

- MONO-DIRECTIONAL (YELLOW)
- MONO-DIRECTIONAL (WHITE)
- ▨ MARKINGS (YELLOW)
- ▭ MARKINGS (WHITE)

DRAWING NOT TO SCALE

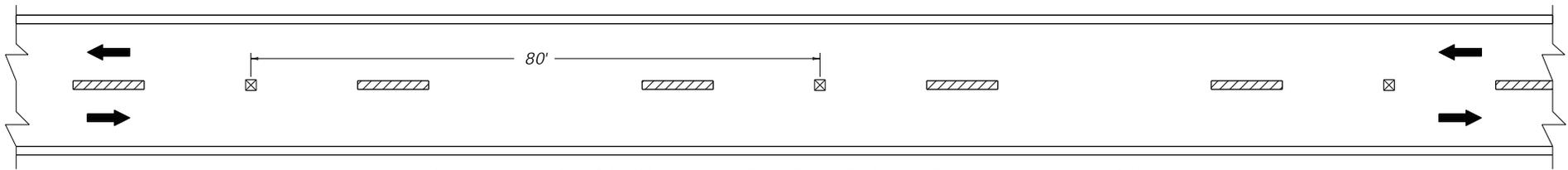
KENTUCKY  
DEPARTMENT OF HIGHWAYS

PAVEMENT MARKER  
ARRANGEMENTS  
MULTI-LANE ROADWAYS

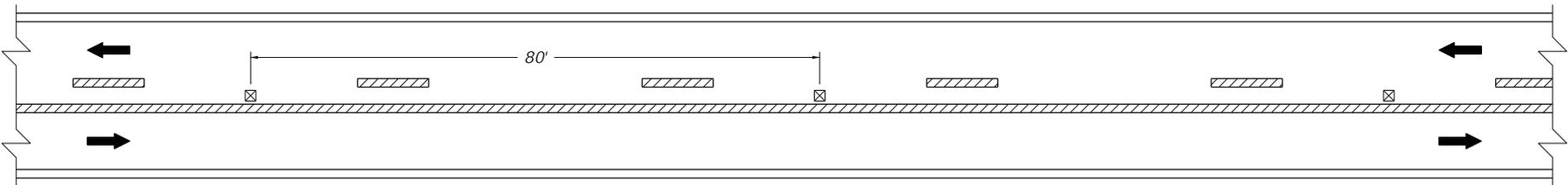
STANDARD DRAWING NO. TPM-110-03

SUBMITTED *B. Allen Wolf* 12-01-15  
DIRECTOR DISTRICT OPERATIONS DATE

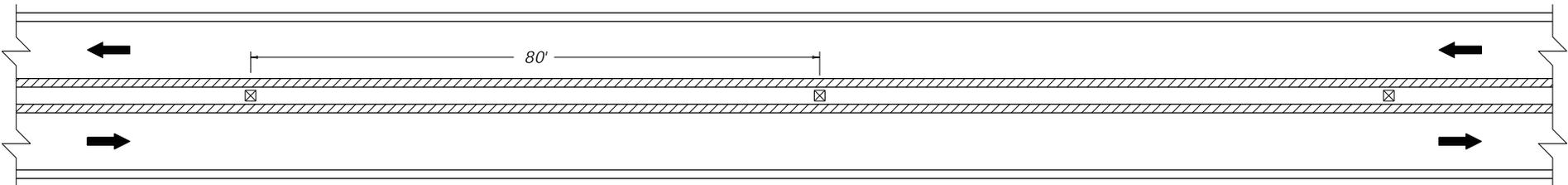
APPROVED *[Signature]* 12-01-15  
STATE HIGHWAY ENGINEER DATE



ARRANGEMENT "A" (PASSING PERMITTED FOR BOTH DIRECTIONS OF TRAVEL)



ARRANGEMENT "B" (PASSING PERMITTED FOR ONE DIRECTION OF TRAVEL)



ARRANGEMENT "C" (PASSING PROHIBITED FOR BOTH DIRECTIONS OF TRAVEL)

~ NOTES ~

1. MARKERS INSTALLED ALONG DASHED YELLOW CENTERLINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE DASHES.
2. MARKERS INSTALLED WITH DOUBLE YELLOW CENTERLINES SHALL BE PLACED BETWEEN THE TWO LINES.
3. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.

BID ITEMS AND UNIT TO BID  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, MY)  
 INLAID PAVEMENT MARKER

EACH  
 EACH

LEGEND	
☒	BI-DIRECTIONAL (YELLOW)
▨	MARKINGS (YELLOW)
▭	MARKINGS (WHITE)

DRAWING NOT TO SCALE

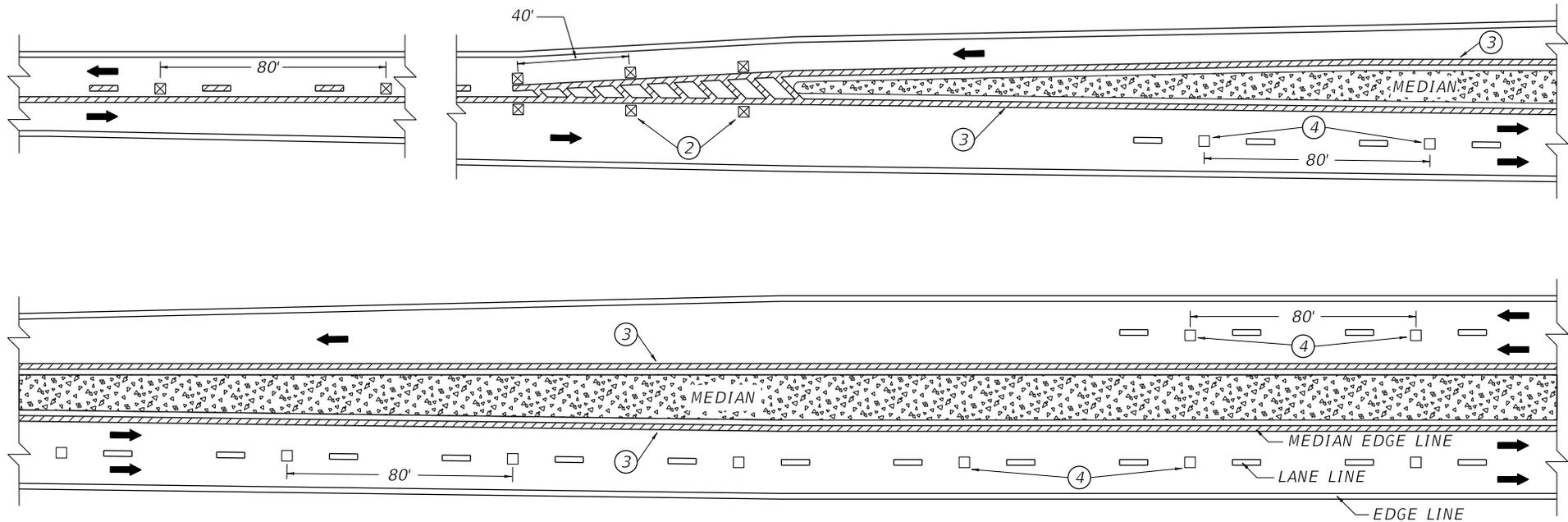
USE WITH CUR. STD. DWGS.  
 TPM-150 TPM-155 TPM-160

KENTUCKY  
 DEPARTMENT OF HIGHWAYS

PAVEMENT MARKER  
 ARRANGEMENTS  
 TWO-LANE, TWO-WAY  
 ROADWAYS

STANDARD DRAWING NO. TPM-115-03

SUBMITTED	<i>B. Allen Wolf</i>	12-01-15
DIRECTOR	DIRECTOR OF TRANSPORTATION OPERATIONS	DATE
APPROVED	<i>[Signature]</i>	12-01-15
	STATE HIGHWAY ENGINEER	DATE



TWO LANE TO FOUR LANE PAVEMENT TRANSITIONS

LEGEND

⊠	BI-DIRECTIONAL (YELLOW)
□	MONO-DIRECTIONAL (WHITE) ④
▨	MARKINGS (YELLOW)
▭	MARKINGS (WHITE)

~ NOTES ~

1. MARKERS INSTALLED ALONG LANE LINES OR DASHED YELLOW CENTERLINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE DASHES.
- ② MARKERS INSTALLED ALONG EDGE LINES SHALL BE PLACED SO THAT THE NEAR EDGE OF THE CASTING IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
- ③ MARKERS MAY BE REQUIRED ALONG THE MEDIAN EDGE LINES DEPENDING ON TYPE AND WIDTH OF MEDIAN. SEE [TPM-100](#), [TPM-105](#), AND [TPM-110](#) FOR GUIDANCE.
- ④ IF MEDIAN WIDTH IS GREATER THAN OR EQUAL TO 30', BI-DIRECTIONAL (WHITE-RED) MARKERS SHALL BE USED ALONG THE LANE LINES IN LIEU OF MONO-DIRECTIONAL (WHITE) MARKERS.
5. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED AT THE DISCRETION OF THE ENGINEER.

BID ITEMS AND UNIT TO BID  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, OR MY)  
 INLAID PAVEMENT MARKER

EACH  
 EACH

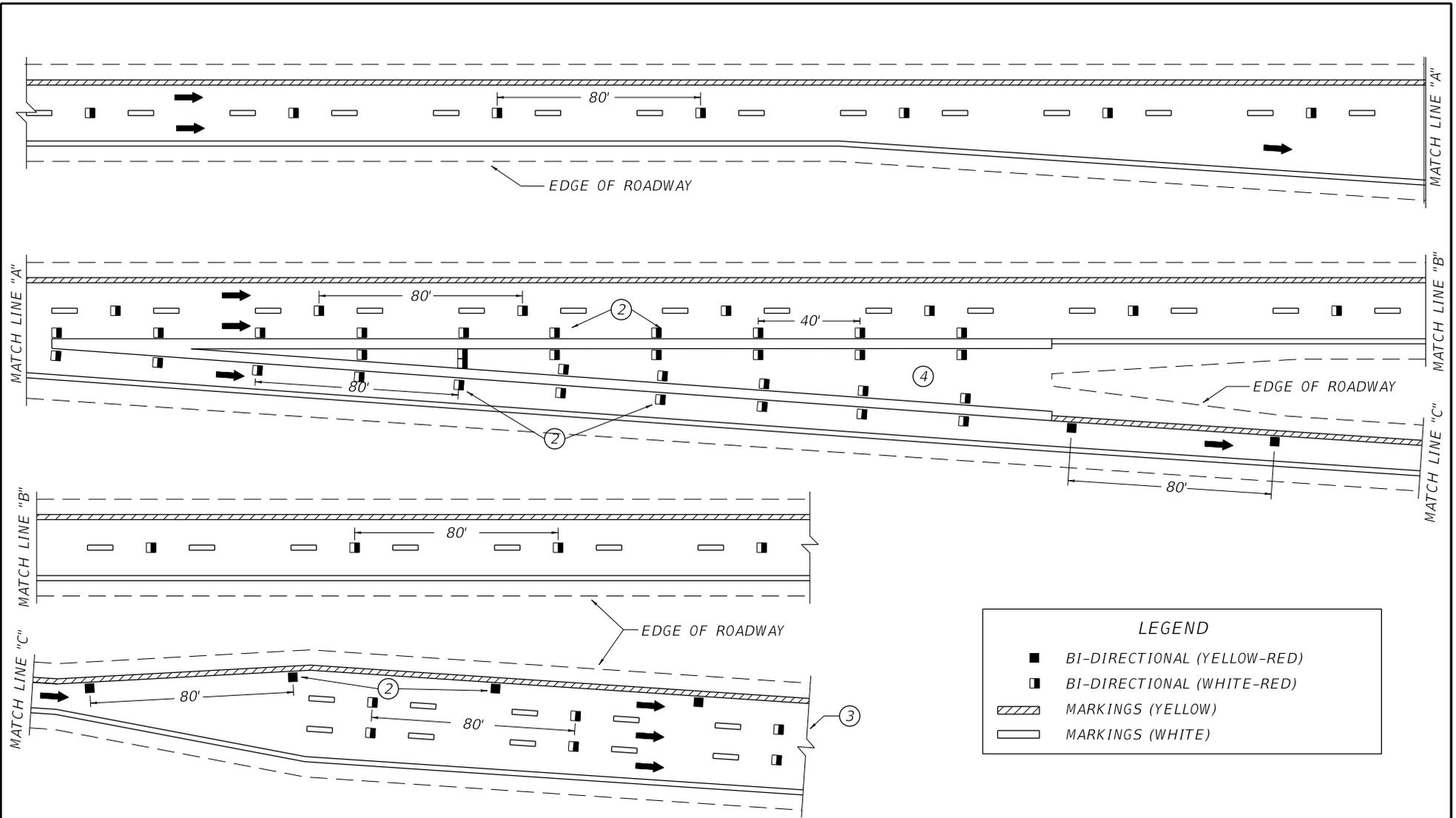
DRAWING NOT TO SCALE

USE WITH CUR. STD. DWGS.  
[TPM-100](#) [TPM-105](#) [TPM-110](#)

KENTUCKY  
 DEPARTMENT OF HIGHWAYS  
 PAVEMENT MARKER  
 ARRANGEMENT  
 TWO-LANE TO FOUR-LANE  
 TRANSITIONS

STANDARD DRAWING NO. [TPM-120-03](#)

SUBMITTED	<i>B. Allen Wolf</i>	12-01-15
DIRECTOR OF DESIGN & OPERATIONS		DATE
APPROVED	<i>[Signature]</i>	12-01-15
STATE HIGHWAY ENGINEER		DATE



~ NOTES ~

1. MARKERS INSTALLED ALONG LANE LINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
- ② MARKERS INSTALLED ALONG EDGE LINES SHALL BE PLACED SO THAT THE NEAR EDGE OF THE CASTING/GROOVE IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
- ③ MARKERS SHALL BE CONTINUED ALONG THE ENTIRE LENGTH OF THE RAMP UNTIL THE INTERSECTION WITH THE CROSS-STREET.
- ④ ON TWO-LANE, TWO-WAY HIGHWAYS, MARKERS INSTALLED ALONG GORE MARKINGS SHALL BE MONO-DIRECTIONAL (WHITE).
5. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.

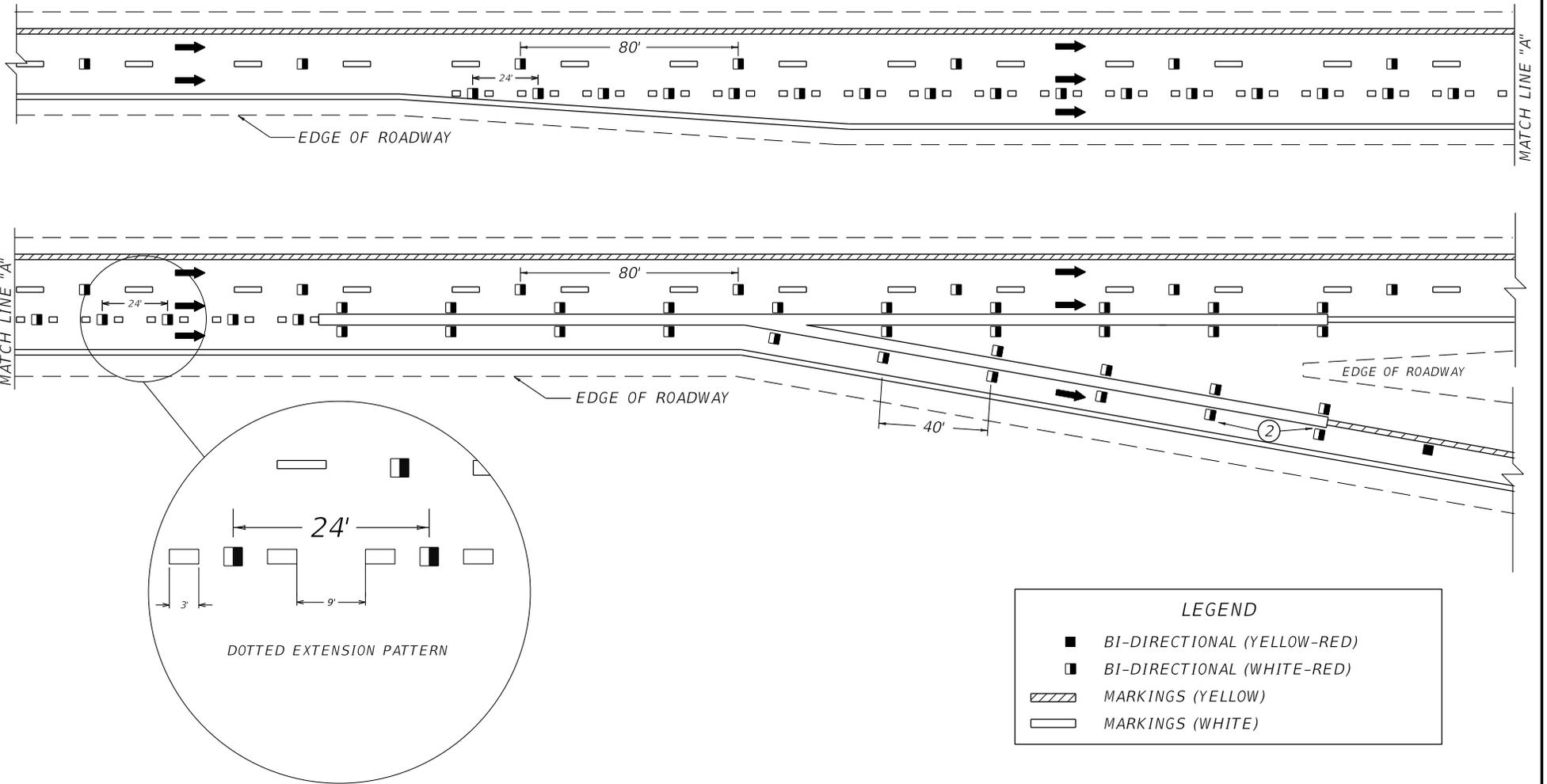
BID ITEMS AND UNIT TO BID  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, MY)  
 INLAID PAVEMENT MARKER

EACH  
 EACH

LEGEND	
■	BI-DIRECTIONAL (YELLOW-RED)
▨	BI-DIRECTIONAL (WHITE-RED)
▨	MARKINGS (YELLOW)
▭	MARKINGS (WHITE)

DRAWING NOT TO SCALE

<b>KENTUCKY</b> <b>DEPARTMENT OF HIGHWAYS</b>		
<b>PAVEMENT MARKER</b> <b>ARRANGEMENT</b> <b>EXIT GORE AND OFF-RAMP</b>		
STANDARD DRAWING NO. <b>TPM-125-03</b>		
SUBMITTED <i>B. Allen Wolf</i> <small>DIRECTOR OF TRANSPORTATION OPERATIONS</small>	DATE <b>12-01-15</b>	
APPROVED <i>[Signature]</i> <small>STATE HIGHWAY ENGINEER</small>	DATE <b>12-01-15</b>	



~ NOTES ~

1. MARKERS INSTALLED ALONG LANE LINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
- ② MARKERS INSTALLED ALONG EDGE LINES SHALL BE PLACED SO THAT THE NEAR EDGE OF THE CASTING IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
- ③ MARKERS SHALL BE CONTINUED ALONG THE ENTIRE LENGTH OF THE RAMP UNTIL THE INTERSECTION WITH THE CROSS-STREET.
- ④ ON TWO-LANE, TWO-WAY HIGHWAYS, MARKERS INSTALLED ALONG GORE MARKINGS SHALL BE MONO-DIRECTIONAL (WHITE).
5. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED AT THE DISCRETION OF THE ENGINEER.
6. MARKERS INSTALLED ALONG THE RAMP EDGELINE SHALL BE SPACED AT 80' INTERVALS.

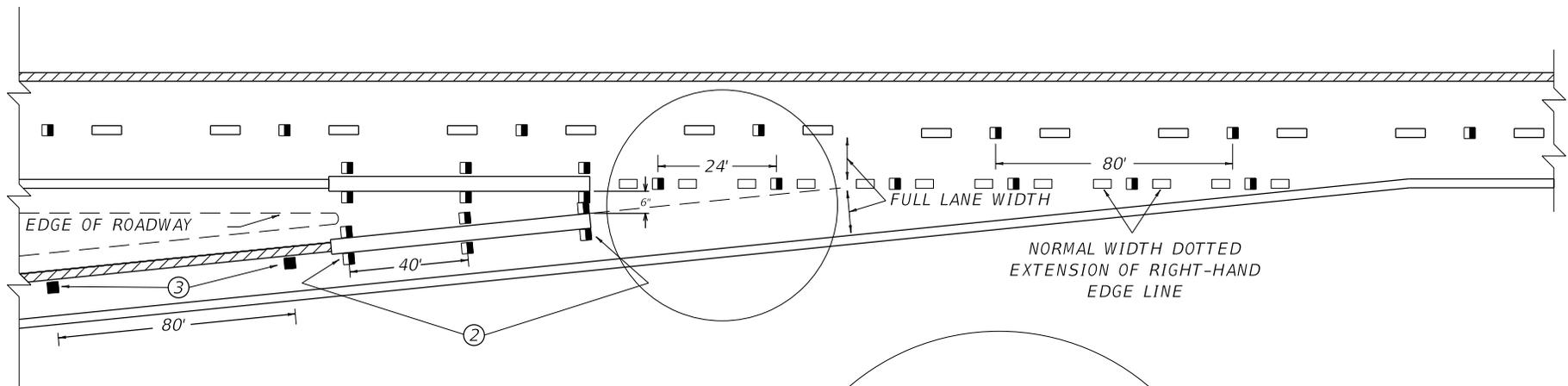
BID ITEMS AND UNIT TO BID  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, OR MY)  
 INLAID PAVEMENT MARKER

EACH  
 EACH

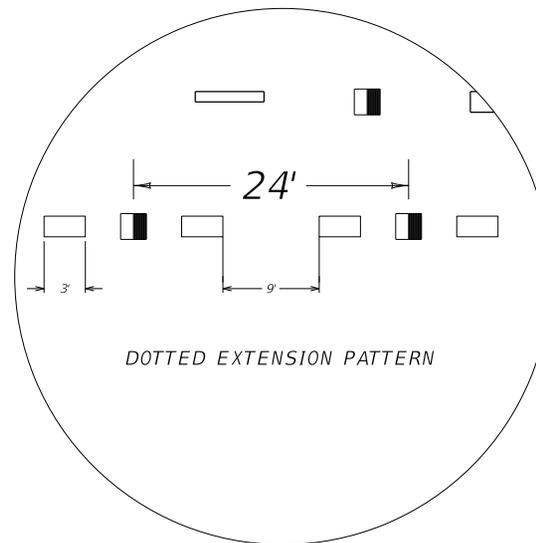
LEGEND	
■	BI-DIRECTIONAL (YELLOW-RED)
◼	BI-DIRECTIONAL (WHITE-RED)
▨	MARKINGS (YELLOW)
▭	MARKINGS (WHITE)

DRAWING NOT TO SCALE

<b>KENTUCKY</b> <b>DEPARTMENT OF HIGHWAYS</b>		
<b>PAVEMENT MARKER</b> <b>ARRANGEMENT</b> <b>FOR PARALLEL</b> <b>DECELERATION LANE</b>		
STANDARD DRAWING NO. TPM-126		
SUBMITTED <i>B. Allen Wolf</i> <small>DIRECTOR, DIVISION OF OPERATIONS</small>	DATE 12-01-15	
APPROVED <i>[Signature]</i> <small>STATE HIGHWAY ENGINEER</small>	DATE 12-01-15	



LEGEND	
■	BI-DIRECTIONAL (YELLOW-RED)
◻	BI-DIRECTIONAL (WHITE-RED)
▨	MARKINGS (YELLOW)
▭	MARKINGS (WHITE)



~ NOTES ~

- MARKERS INSTALLED ALONG LANE LINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
- MARKERS INSTALLED ALONG EDGE LINES SHALL BE PLACED SO THAT THE NEAR EDGE OF THE CASTING/GROOVE IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
- BI-DIRECTIONAL (YELLOW-RED) MARKERS ARE TO BE PLACED ALONG THE ENTIRE LENGTH OF THE YELLOW EDGE LINE FROM THE INTERSECTION OF THE CROSS-STREET TO THE BEGINNING OF THE GORE AREA.
- ON TWO-LANE, TWO-WAY HIGHWAYS, MARKERS INSTALLED ALONG GORE MARKINGS SHALL BE MONO-DIRECTIONAL (WHITE).
- MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.

BID ITEMS AND UNIT TO BID  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, OR MY)  
 INLAID PAVEMENT MARKER

EACH  
 EACH

DRAWING NOT TO SCALE

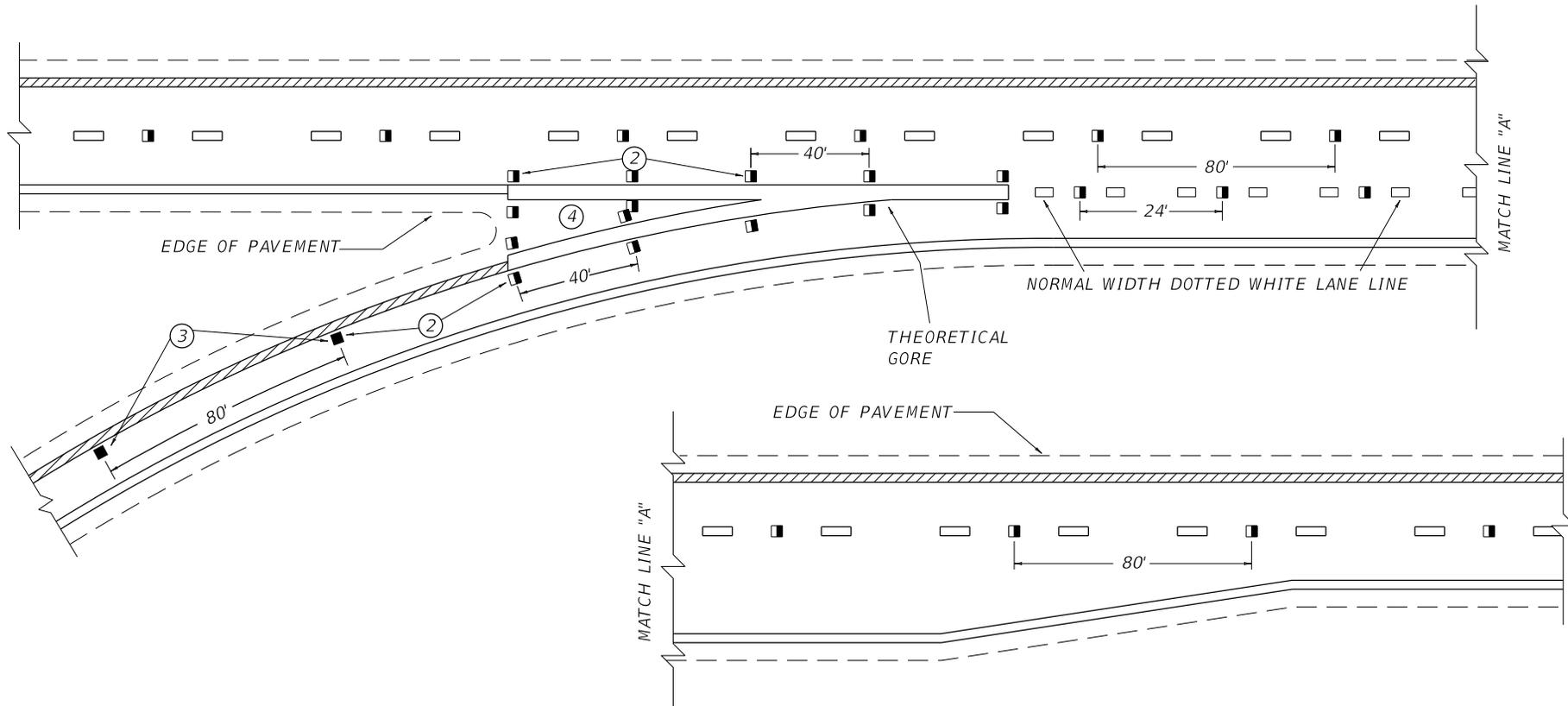
KENTUCKY  
 DEPARTMENT OF HIGHWAYS

PAVEMENT MARKER  
 ARRANGEMENT  
 ON-RAMP WITH TAPERED  
 ACCELERATION LANE

STANDARD DRAWING NO. TPM-130-03

SUBMITTED *B. Allen Wolf* 12-01-15  
DIRECTOR OF HIGHWAY OPERATIONS DATE

APPROVED *[Signature]* 12-01-15  
STATE HIGHWAY ENGINEER DATE



~ NOTES ~

1. MARKERS INSTALLED ALONG LANE LINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
- ② MARKERS INSTALLED ALONG EDGE LINES SHALL BE PLACED SO THAT THE NEAR EDGE OF THE CASTING IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
- ③ BI-DIRECTIONAL (YELLOW-RED) MARKERS ARE TO BE PLACED ALONG THE ENTIRE LENGTH OF THE YELLOW EDGE LINE FROM THE INTERSECTION OF THE CROSS-STREET TO THE BEGINNING OF THE GORE AREA.
- ④ ON TWO-LANE, TWO-WAY HIGHWAYS, MARKERS INSTALLED ALONG GORE MARKINGS SHALL BE MONO-DIRECTIONAL (WHITE).
5. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.
6. THE NORMAL WIDTH DOTTED WHITE LANE LINE SHALL EXTEND FOR AT LEAST HALF THE LENGTH OF THE FULL-WIDTH ACCELERATION LANE PLUS TAPER MEASURED FROM THE THEORETICAL GORE.

BID ITEMS AND UNIT TO BID  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, MY)  
 INLAID PAVEMENT MARKER

EACH  
 EACH

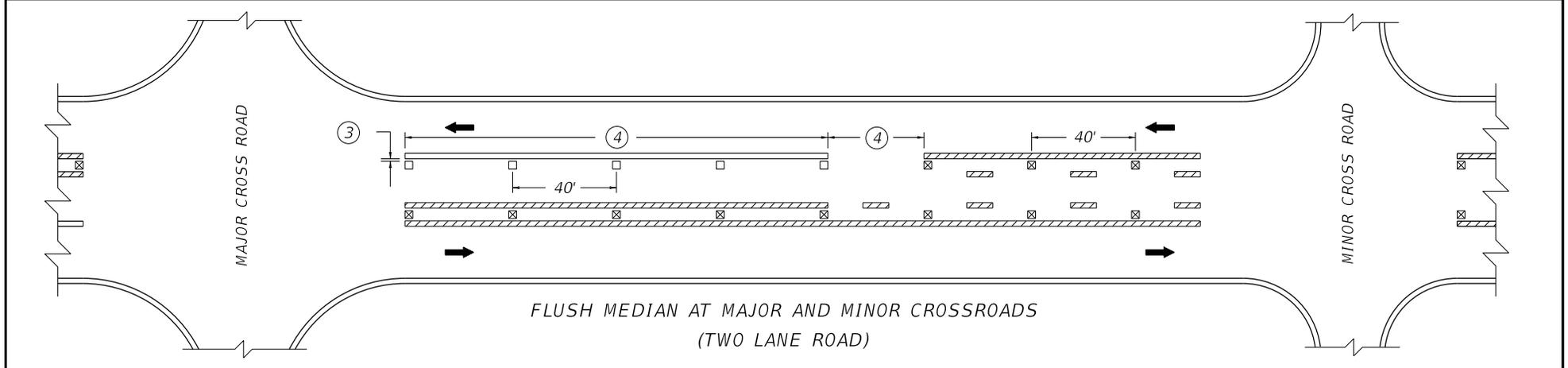
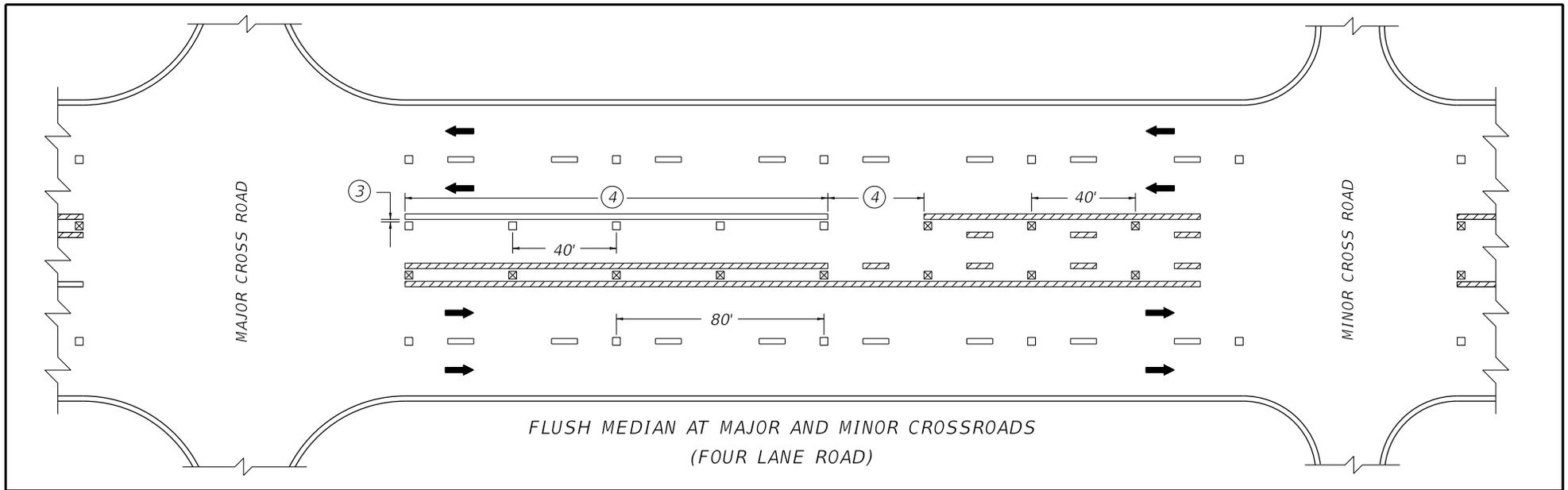
LEGEND	
■	BI-DIRECTIONAL (YELLOW-RED)
◻	BI-DIRECTIONAL (WHITE-RED)
▨	MARKINGS (YELLOW)
▭	MARKINGS (WHITE)

DRAWING NOT TO SCALE

KENTUCKY  
 DEPARTMENT OF HIGHWAYS  
 PAVEMENT MARKER  
 ARRANGEMENT  
 ON-RAMP WITH PARALLEL  
 ACCELERATION LANE

STANDARD DRAWING NO. TPM-135-03

SUBMITTED *B. Allen Wolf* 12-01-15  
DIRECTOR OF TRANSPORTATION OPERATIONS DATE  
 APPROVED *[Signature]* 12-01-15  
STATE HIGHWAY ENGINEER DATE



~ NOTES ~

1. MARKERS INSTALLED AT DOUBLE YELLOW CENTERLINES SHALL BE PLACED BETWEEN THE TWO LINES.
2. MARKERS INSTALLED ALONG LANE LINES OR DASHED YELLOW CENTERLINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
- ③ MARKERS INSTALLED ALONG EDGE LINES SHALL BE PLACED SO THAT THE NEAR EDGE OF THE CASTING/GROOVE IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
- ④ LENGTH TO BE DETERMINED ON A PROJECT BY PROJECT BASIS.
5. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.

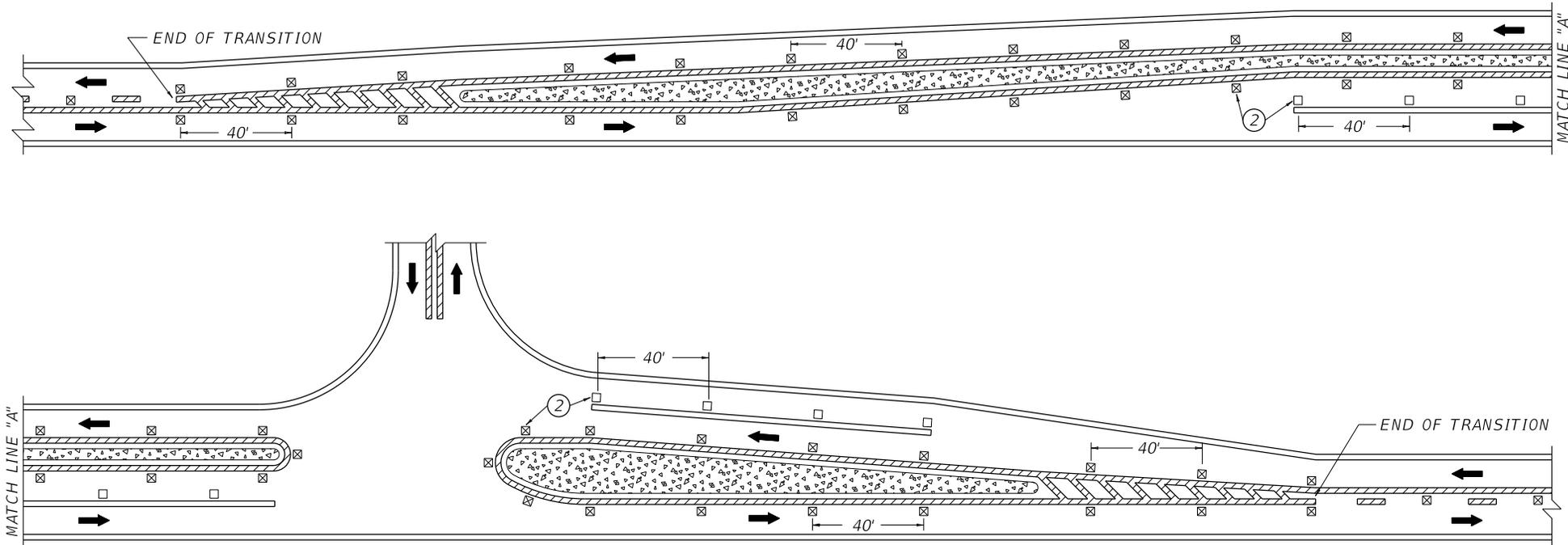
BID ITEMS AND UNIT TO BID  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, MY)  
 INLAID PAVEMENT MARKER

EACH  
 EACH

LEGEND	
☒	BI-DIRECTIONAL (YELLOW)
□	MONO-DIRECTIONAL (WHITE)
▨	MARKINGS (YELLOW)
▬	MARKINGS (WHITE)

DRAWING NOT TO SCALE

<b>KENTUCKY DEPARTMENT OF HIGHWAYS</b>	
<b>PAVEMENT MARKER ARRANGEMENTS TWO-WAY, LEFT TURN LANE</b>	
STANDARD DRAWING NO. <i>TPM-140-04</i>	
SUBMITTED <i>R. Allen Wolf</i>	DATE <i>02-26-20</i>
<small>DIRECTOR OF PUBLIC SAFETY OPERATIONS</small>	
APPROVED <i>[Signature]</i>	DATE <i>02-26-20</i>
<small>STATE HIGHWAY ENGINEER</small>	



CHANNELIZED INTERSECTION ON A TWO DIRECTIONAL TWO LANE FACILITY

LEGEND	
⊗	BI-DIRECTIONAL (YELLOW)
□	MONO-DIRECTIONAL (WHITE)
	CURBED OR FLUSH MEDIAN
	MARKINGS (YELLOW)
	MARKINGS (WHITE)

~ NOTES ~

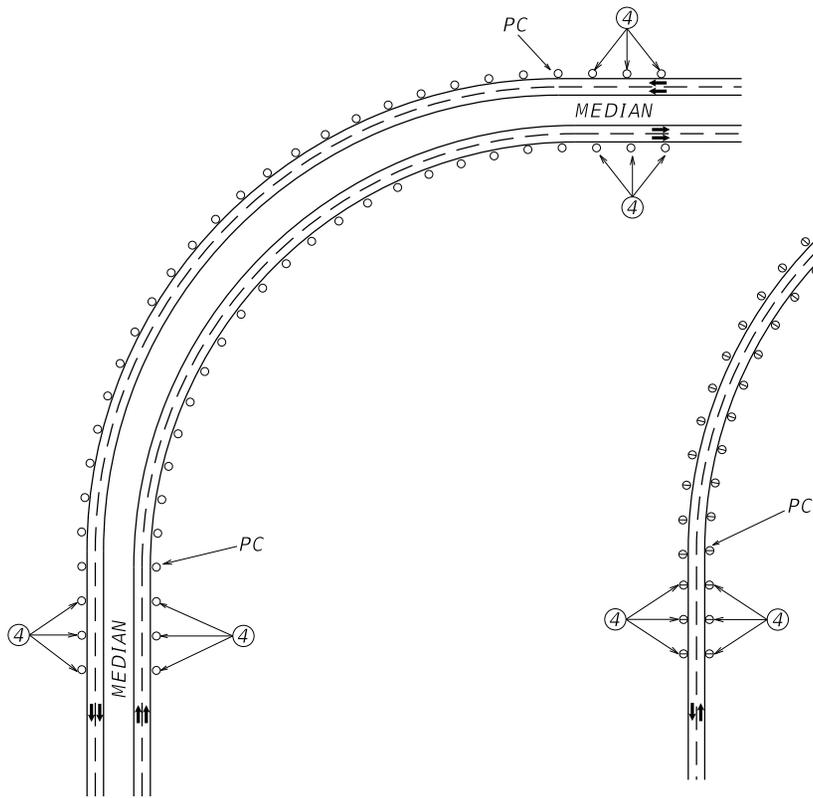
1. MARKERS INSTALLED ALONG DASHED YELLOW CENTERLINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
- ② MARKERS INSTALLED ALONG EDGE LINES OR CHANNELIZING LINES SHALL BE PLACED SO THAT THE NEAR EDGE OF THE CASTING/GROOVE IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
3. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2" FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.

BID ITEMS AND UNIT TO BID  
 PAVEMENT MARKER TYPE V (B-W/R, B-Y/R, BY, MW, MY)  
 INLAID PAVEMENT MARKER

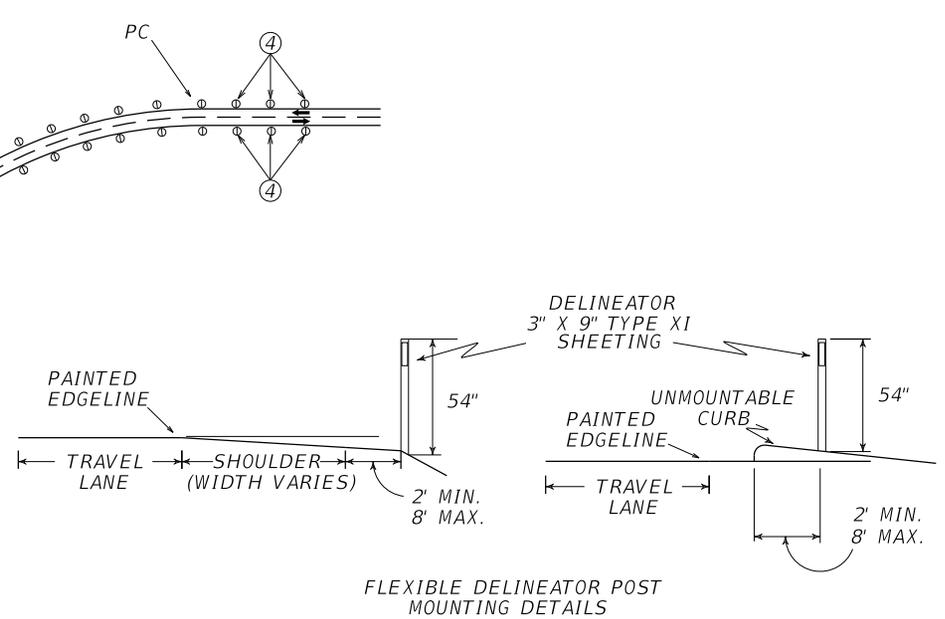
EACH  
 EACH

DRAWING NOT TO SCALE

<b>KENTUCKY          DEPARTMENT OF HIGHWAYS</b>		
<b>PAVEMENT MARKER          ARRANGEMENT          CHANNELIZED          INTERSECTION</b>		
STANDARD DRAWING NO. TPM-145-03		
SUBMITTED <i>B. Allen Wolf</i> <small>DIRECTOR OF TRANSPORTATION OPERATIONS</small>	DATE 12-01-15	
APPROVED <i>[Signature]</i> <small>STATE HIGHWAY ENGINEER</small>	DATE 12-01-15	



DELINEATION FOR HORIZONTAL CURVES ON MULTI-LANE EXPRESSWAYS/FREEWAYS



DELINEATION FOR HORIZONTAL CURVES ON TWO-LANE EXPRESSWAYS/FREEWAYS

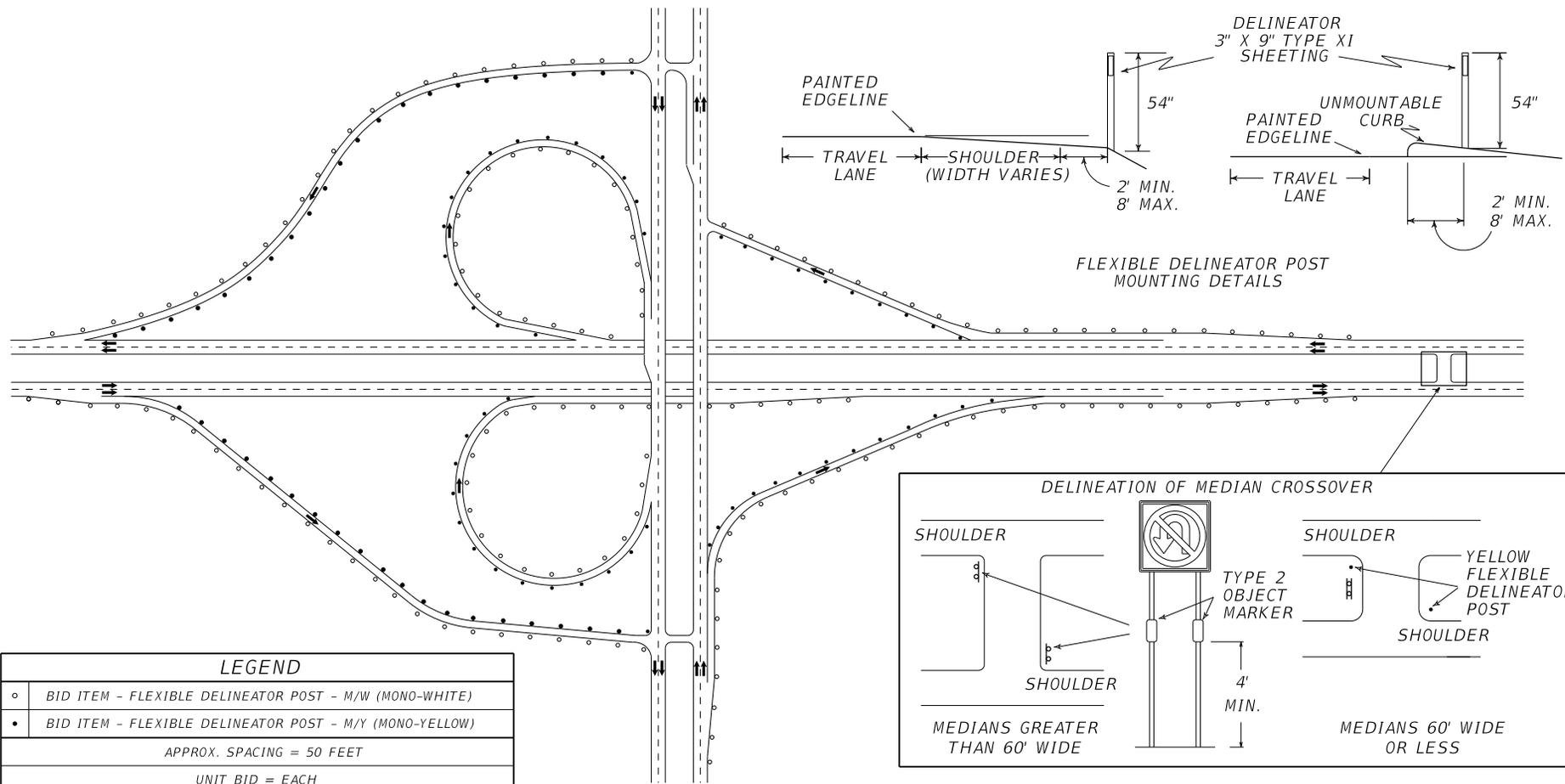
LEGEND	
○	BID ITEM - FLEXIBLE DELINEATOR POST - M/W (MONO-WHITE)
⊖	BID ITEM - FLEXIBLE DELINEATOR POST - B/W (BI-WHITE)
APPROX. SPACING = 50 FEET	
UNIT BID = EACH	

~ NOTES ~

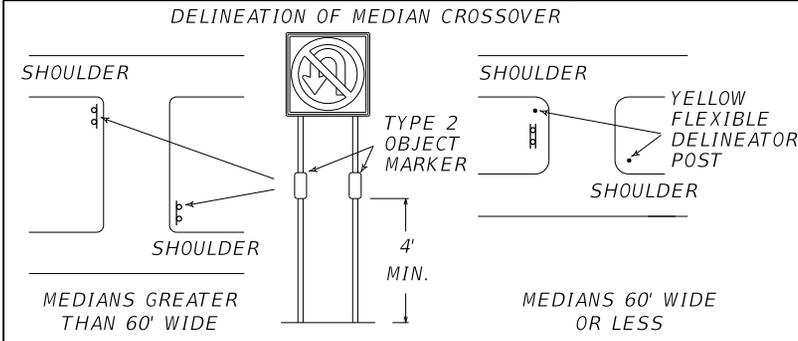
1. DELINEATORS SHALL BE FROM THE LIST OF APPROVED MATERIALS AND SHALL BE IN ACCORDANCE WITH SECTION 838 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION.
2. DELINEATORS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
3. THE DELINEATOR POST AND RETROREFLECTIVE SHEETING SHALL BE THE SAME COLOR AND SHALL MATCH THE COLOR OF THE EDGELINES THEY SUPPLEMENT.
- ④ THREE DELINEATORS SHALL BE INSTALLED IN ADVANCE OF THE BEGINNING AND PROCEEDING AWAY FROM THE END OF THE CURVE AT A SPACING OF APPROXIMATELY 50 FEET.
5. DELINEATORS SHALL BE PROVIDED ON THE RIGHT-HAND SIDE OF HORIZONTAL CURVES ON EXPRESSWAYS AND FREEWAYS. DELINEATORS ARE NOT REQUIRED ON TANGENT SECTIONS OF EXPRESSWAYS AND FREEWAYS.
6. A FREEWAY IS DEFINED AS A DIVIDED HIGHWAY WITH FULL CONTROL OF ACCESS. AN EXPRESSWAY IS DEFINED AS A DIVIDED HIGHWAY WITH PARTIAL CONTROL OF ACCESS.
7. DELINEATORS SHOULD BE PLACED AT A CONSTANT DISTANCE FROM THE ROADWAY EDGE. WHEN AN OBSTRUCTION EXISTS NEAR THE PAVEMENT EDGE, THE LINE OF DELINEATORS SHOULD BE TRANSITIONED TO THE INSIDE OF THE OBSTRUCTION OR TO THE BARRIER DELINEATION IF THE OBSTRUCTION IS GUARDRAIL OR BARRIER WALL. SUCH TRANSITIONS SHOULD BEGIN APPROXIMATELY 200 FT IN ADVANCE OF THE OBSTRUCTION/BARRIER.
8. DELINEATORS SHOULD BE INSTALLED AT AN APPROXIMATE RIGHT ANGLE TO APPROACHING TRAFFIC.
9. DELINEATORS MAY BE DISCONTINUED ALONG SECTIONS WITH BARRIER WALL OR GUARDRAIL DELINEATION.
10. SPACING SHOULD BE ADJUSTED IN CURVES SO THAT SEVERAL DELINEATORS ARE ALWAYS SIMULTANEOUSLY VISIBLE TO THE ROAD USER.

DRAWING NOT TO SCALE

<b>KENTUCKY DEPARTMENT OF HIGHWAYS</b>	
FLEXIBLE DELINEATOR POST ARRANGEMENTS FOR HORIZONTAL CURVES	
STANDARD DRAWING NO. TPM-170-01	
SUBMITTED <i>B. Allen Wolf</i>	DATE 02-26-20
<small>DIRECTOR OF TRANSPORTATION OPERATIONS</small>	
APPROVED <i>[Signature]</i>	DATE 02-26-20
<small>REGISTERED PROFESSIONAL ENGINEER</small>	



LEGEND	
○	BID ITEM - FLEXIBLE DELINEATOR POST - M/W (MONO-WHITE)
●	BID ITEM - FLEXIBLE DELINEATOR POST - M/Y (MONO-YELLOW)
APPROX. SPACING = 50 FEET	
UNIT BID = EACH	

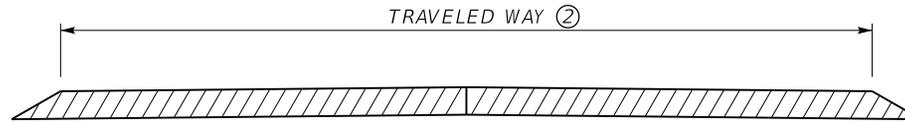


~ NOTES ~

1. DELINEATORS SHALL BE FROM THE LIST OF APPROVED MATERIALS AND SHALL BE IN ACCORDANCE WITH SECTION 838 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION.
2. DELINEATORS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
3. THE DELINEATOR POST AND RETROREFLECTIVE SHEETING SHALL BE THE SAME COLOR AND SHALL MATCH THE COLOR OF THE EDGELINES THEY SUPPLEMENT.
4. DELINEATORS SHALL BE PROVIDED ON BOTH SIDES OF INTERCHANGE RAMP. DELINEATORS SHOULD BE ERECTED AT APPROXIMATELY 50 FOOT INTERVALS ALONG RAMP AND ACCELERATION AND DECELERATION LANES ON THE MAINLINE AND CROSSROAD.
5. DELINEATORS SHOULD BE PLACED AT A CONSTANT DISTANCE FROM THE ROADWAY EDGE. WHEN AN OBSTRUCTION EXISTS NEAR THE PAVEMENT EDGE, THE LINE OF DELINEATORS SHOULD BE TRANSITIONED TO THE INSIDE OF THE OBSTRUCTION OR TO THE BARRIER DELINEATION IF THE OBSTRUCTION IS GUARDRAIL OR BARRIER WALL. SUCH TRANSITIONS SHOULD BEGIN APPROXIMATELY 200 FEET IN ADVANCE OF THE OBSTRUCTION/BARRIER.
6. DELINEATORS SHOULD BE INSTALLED AT AN APPROXIMATE RIGHT ANGLE TO APPROACHING TRAFFIC.
7. DELINEATORS MAY BE DISCONTINUED ALONG SECTIONS WITH BARRIER WALL OR GUARDRAIL DELINEATION.
8. SPACING SHOULD BE ADJUSTED IN CURVES SO THAT SEVERAL DELINEATORS ARE ALWAYS SIMULTANEOUSLY VISIBLE TO THE ROAD USER.

DRAWING NOT TO SCALE

<b>KENTUCKY DEPARTMENT OF HIGHWAYS</b>		
FLEXIBLE DELINEATOR POST ARRANGEMENTS FOR INTERCHANGE RAMP AND CROSSOVERS		
STANDARD DRAWING NO. TPM-171-01		
SUBMITTED	<i>B. Allen Wolf</i> DIRECTOR OF TRANSPORTATION OPERATIONS	DATE 02-26-20
APPROVED	<i>SM</i> SUPERVISOR ENGINEER	DATE 02-26-20



## TWO LANE ROADWAY PAVEMENT CROSS-SECTION

TRAVELED WAY ②	TYPE OF PAVEMENT STRIPING	NON-STATE PRIMARY ROUTES				STATE PRIMARY ROUTES	
		< 1000 ADT		≥ 1000 ADT		ANY ADT	
		WIDTH	MATERIAL	WIDTH	MATERIAL	WIDTH	MATERIAL
< 16' ④	EDGE LINE STRIPES ONLY	4"	PAINT	4"	PAINT	6"	DURABLE
16' TO < 20'	EDGE LINE STRIPES ONLY OR CENTERLINE STRIPE ONLY	4"	PAINT	4"	PAINT	6"	DURABLE
≥ 20' ③	CENTERLINE AND EDGE LINE STRIPES	4" ⑤	PAINT	6"	PAINT	6"	DURABLE

~ NOTES ~

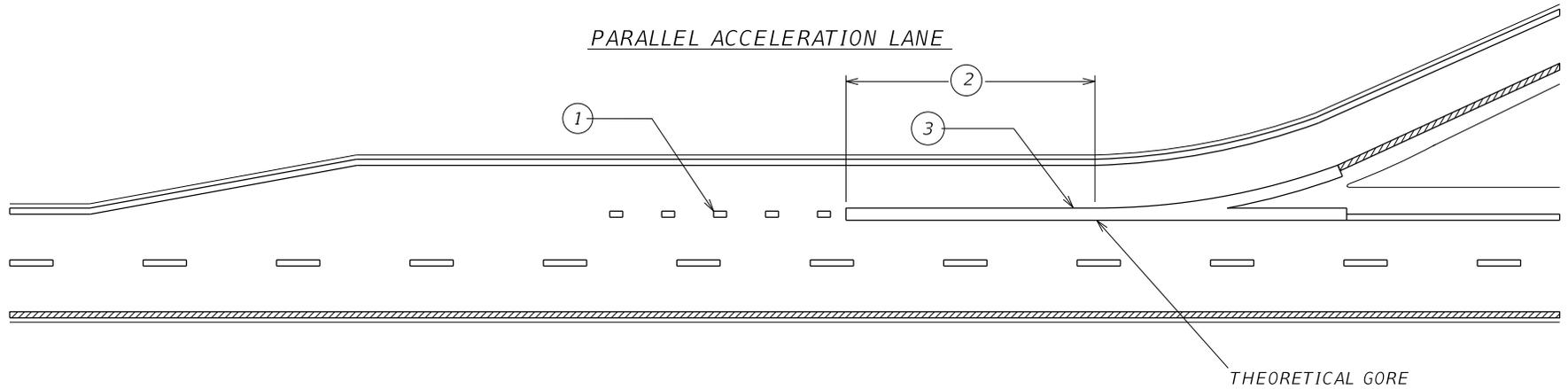
1. INSTALL PAVEMENT STRIPING ON TWO LANE, TWO WAY ROADWAYS AS DETAILED IN THE ABOVE TABLE AND IN ACCORDANCE WITH THE PAVEMENT MARKINGS AND DELINEATION CHAPTER OF THE TRAFFIC OPERATIONS GUIDANCE MANUAL. CONTACT THE DIVISION OF TRAFFIC OPERATIONS FOR ADDITIONAL GUIDANCE IF NECESSARY.
- ② THE TRAVELED WAY IS THE PORTION OF ROADWAY FOR THE MOVEMENT OF VEHICLES, EXCLUSIVE OF THE SHOULDERS.
- ③ ON TWO LANE, TWO WAY ROADWAYS THAT HAVE A TOTAL PAVEMENT WIDTH (W) THAT IS 20 FT OR GREATER, BUT LESS THAN 22 FT, EDGE LINE RUMBLE STRIPS ARE NOT A STANDARD APPLICATION, BUT THEY MAY BE INSTALLED. THE DIVISION OF TRAFFIC OPERATIONS IS AVAILABLE TO ASSIST WITH THE DETERMINATION OF WHETHER OR NOT TO INSTALL EDGE LINE RUMBLE STRIPS ON PAVEMENT WIDTHS LESS THAN 22 FT, AS WELL AS THE DIMENSION AND PLACEMENT DETAILS OF THE RUMBLE STRIPS AND PAVEMENT STRIPING.  
ON TWO LANE, TWO WAY ROADWAYS THAT HAVE A TOTAL PAVEMENT WIDTH (W) THAT IS 22 FT OR GREATER, BUT LESS THAN 34 FT, INSTALL PAVEMENT STRIPING AS DETAILED IN THE ABOVE TABLE AND IN CONJUNCTION WITH CENTERLINE AND EDGE LINE RUMBLE STRIPS AS DETAILED ON [TPR-120](#).
- ON TWO LANE, TWO WAY ROADWAYS THAT HAVE A TOTAL PAVEMENT WIDTH (W) THAT IS 34 FT OR GREATER, INSTALL PAVEMENT STRIPING AS DETAILED IN THE ABOVE TABLE AND IN CONJUNCTION WITH CENTERLINE AND SHOULDER RUMBLE STRIPS AS DETAILED ON [TPR-125](#).
- ④ EDGE LINES MAY BE OMITTED FROM ROADWAYS WITH A TRAVELED WAY WIDTH LESS THAN 16 FEET WITH THE APPROVAL OF THE DIVISION OF TRAFFIC OPERATIONS.
- ⑤ EDGE LINES MAY BE OMITTED ON NON-STATE PRIMARY ROUTES WITH A TRAVELED WAY WIDTH GREATER THAN OR EQUAL TO 20 FEET AND AN ADT LESS THAN 1,000.
6. EDGE LINES MAY BE OMITTED, BASED ON ENGINEERING JUDGMENT, IN AREAS WHERE THE PAVEMENT EDGE IS DELINEATED BY PHYSICAL OBJECTS SUCH AS CURBS, PARKING SPACES, OR OTHER MARKINGS. EDGE LINES SHOULD BE INSTALLED ON ROADWAYS WITH CURB AND GUTTER IF THE POSTED SPEED LIMIT IS 45 MPH OR GREATER.

DRAWING NOT TO SCALE

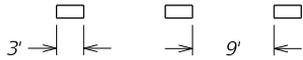
USE WITH CUR. STD. DWGS.  
[TPR-120](#) & [TPR-125](#)

<b>KENTUCKY DEPARTMENT OF HIGHWAYS</b>	
<b>PAVEMENT STRIPING DETAILS FOR TWO LANE TWO WAY ROADWAYS</b>	
STANDARD DRAWING NO. <b>TPM-175</b>	
SUBMITTED <i>B. Allen Wolfe</i> DIRECTOR DIVISION OF TRAFFIC OPERATIONS	DATE <b>02-26-20</b>
APPROVED <i>[Signature]</i> STATE HIGHWAY ENGINEER	DATE <b>02-26-20</b>

PARALLEL ACCELERATION LANE

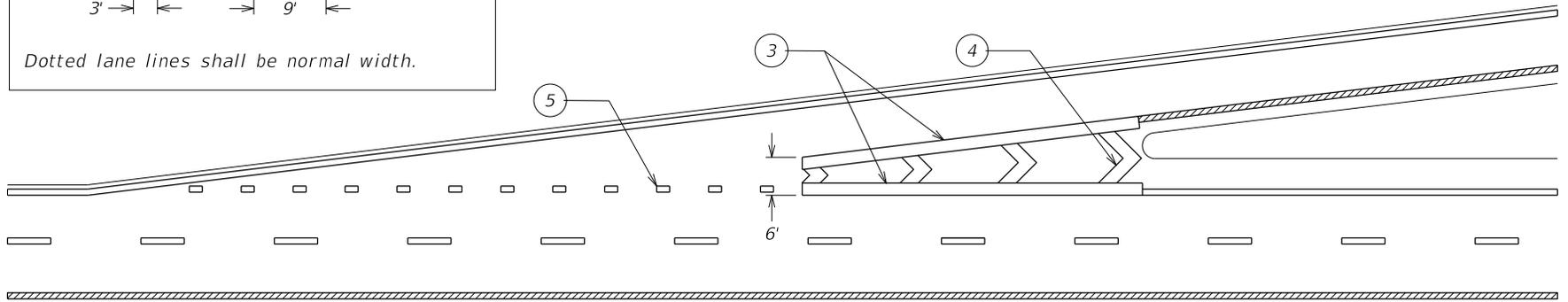


DOTTED LANE LINE DIMENSIONS:



Dotted lane lines shall be normal width.

TAPERED ACCELERATION LANE



- ① IN A PARALLEL ACCELERATION LANE, DOTTED LANE LINES SHALL BE INSTALLED FOR AT LEAST HALF THE LENGTH OF THE FULL-WIDTH ACCELERATION LANE PLUS TAPER MEASURED FROM THE THEORETICAL GORE. DOTTED EXTENSIONS MAY BE INSTALLED THROUGH THE TAPER IF DESIRED.
- ② WIDE SOLID LINE SHALL EXTEND DOWNSTREAM FROM THE THEORETICAL GORE FOR AT LEAST 1/4 THE LENGTH OF THE FULL-WIDTH ACCELERATION LANE PLUS TAPER.
- ③ 12" WIDE SOLID WHITE CHANNELIZATION LINES.
- ④ GORE AREA CHEVRON MARKINGS ARE OPTIONAL ON ENTRANCE RAMP. SEE [TPM-204](#) FOR DETAILS ON CHEVRON MARKINGS.
- ⑤ IN A TAPERED ACCELERATION LANE, DOTTED EXTENSIONS MAY BE INSTALLED AS SHOWN.

DRAWING NOT TO SCALE

<u>LEGEND</u>	
MARKINGS	
	WHITE
	YELLOW

KENTUCKY  
DEPARTMENT OF HIGHWAYS

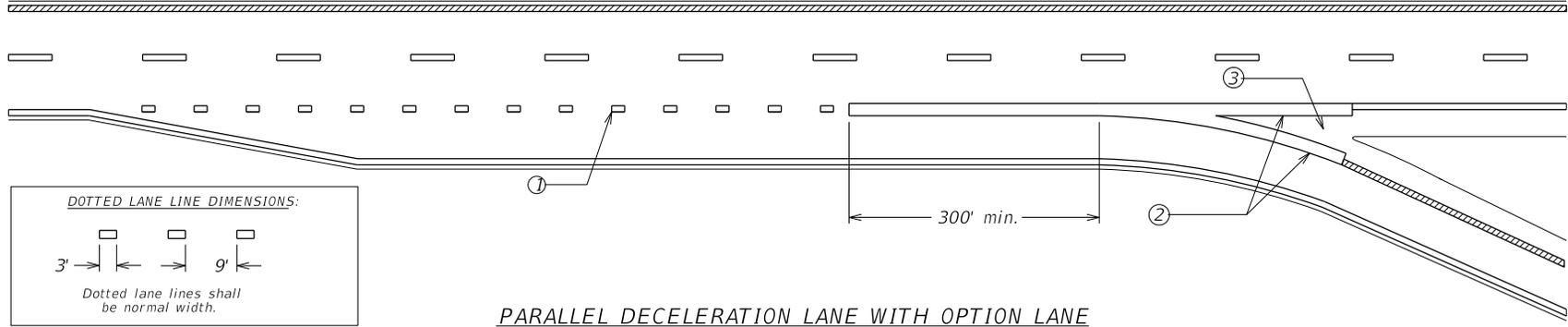
TYPICAL ENTRANCE  
RAMP MARKINGS  
FOR INTERSTATES  
AND PARKWAYS

STANDARD DRAWING NO. TPM-200

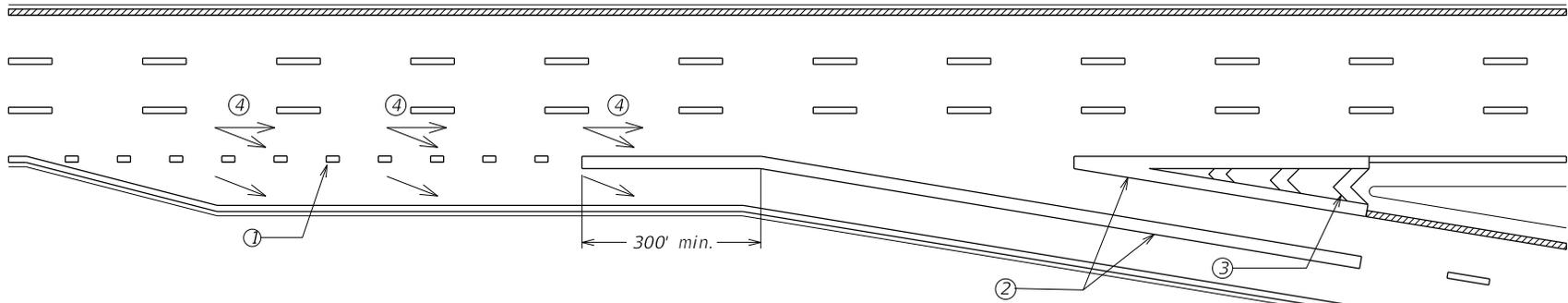
SUBMITTED *B. Allen Wolf* 02-26-20  
DIRECTOR, DIVISION OF TRAFFIC OPERATIONS DATE

APPROVED *[Signature]* 02-26-20  
TRAFFIC ENGINEER DATE

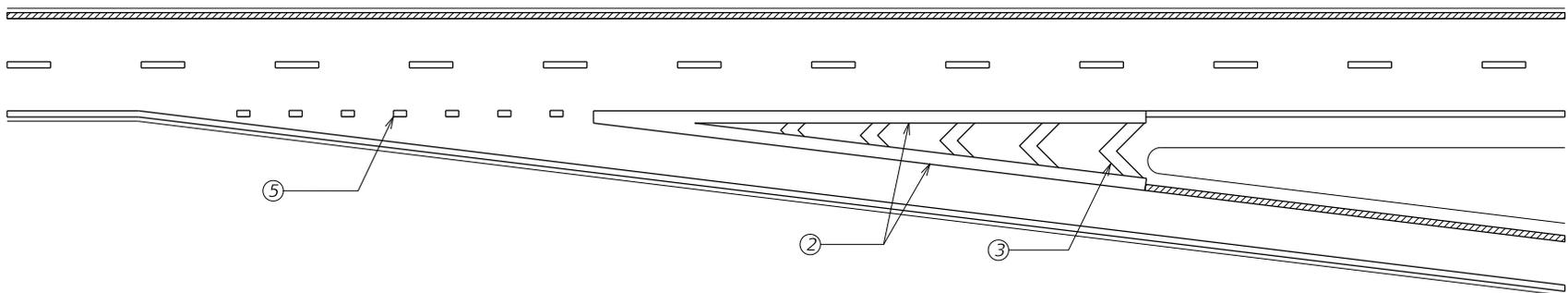
PARALLEL DECELERATION LANE



PARALLEL DECELERATION LANE WITH OPTION LANE



TAPERED DECELERATION LANE



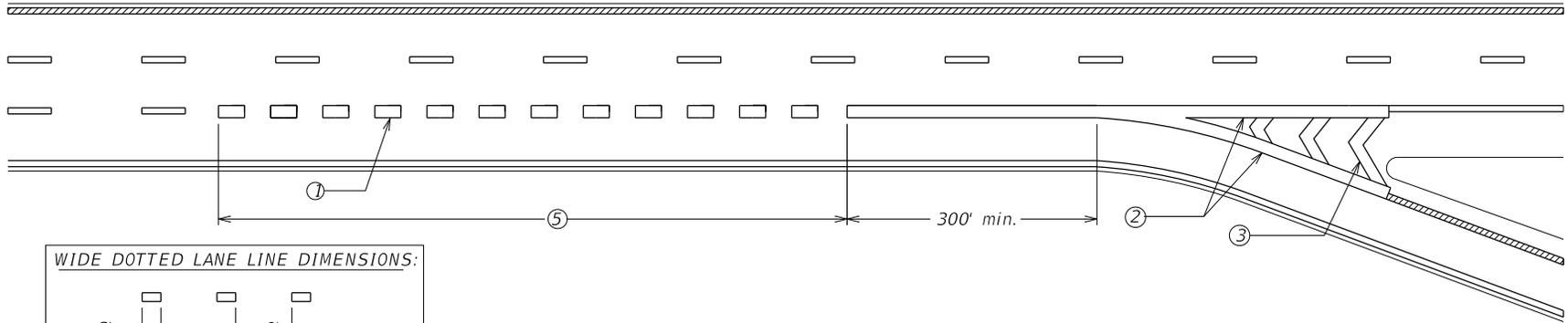
- ① NORMAL WIDTH DOTTED LANE LINES SHALL BE USED ALONG THE TAPER AND FULL-WIDTH SECTION OF A PARALLEL DECELERATION LANE.
- ② 12" WIDE SOLID CHANNELIZATION LINE.
- ③ GORE AREA CHEVRON MARKINGS SHOULD BE USED IN ACCORDANCE WITH *TPM-204*.
- ④ LANE USE ARROWS MAY BE USED WHEN THERE IS AN OPTION LANE. IF USED, A MINIMUM OF THREE ARROWS PER LANE SHOULD BE PLACED AS SHOWN AND SPACED AT APPROXIMATELY ¼ MILE.
- ⑤ FOR TAPERED DECELERATION LANES, NORMAL WIDTH DOTTED EXTENSIONS SHALL BE INSTALLED AS SHOWN.

Page 1 of 2  
 DRAWING NOT TO SCALE

LEGEND	
MARKINGS	
	WHITE
	YELLOW

KENTUCKY DEPARTMENT OF HIGHWAYS		
TYPICAL EXIT RAMP MARKINGS FOR INTERSTATES AND PARKWAYS		
STANDARD DRAWING NO. <i>TPM-201</i>		
SUBMITTED <i>R. Allen Wells</i> DIRECTOR DIVISION OF OPERATIONS	02-26-20 DATE	
APPROVED <i>[Signature]</i> STATE HIGHWAY ENGINEER	02-26-20 DATE	

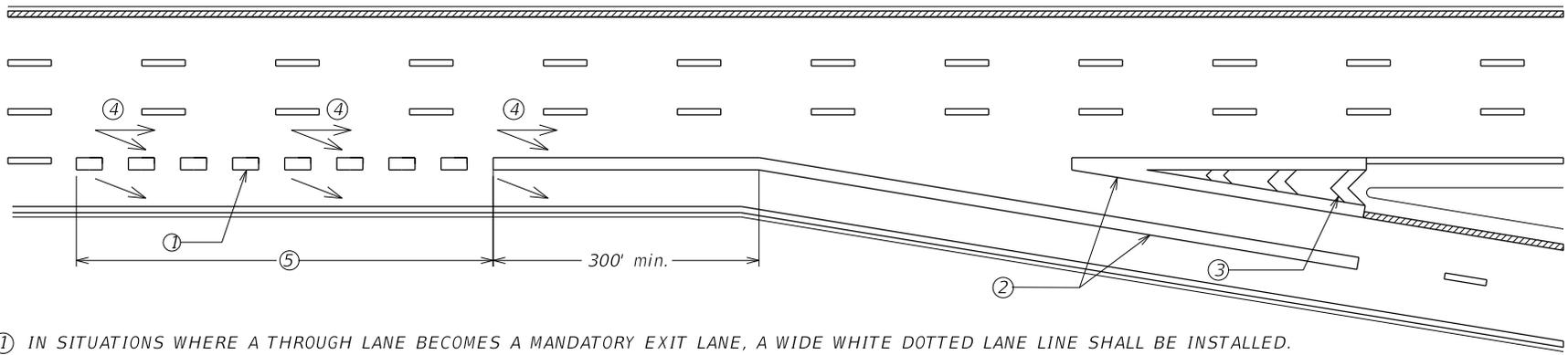
## SINGLE-LANE LANE DROP



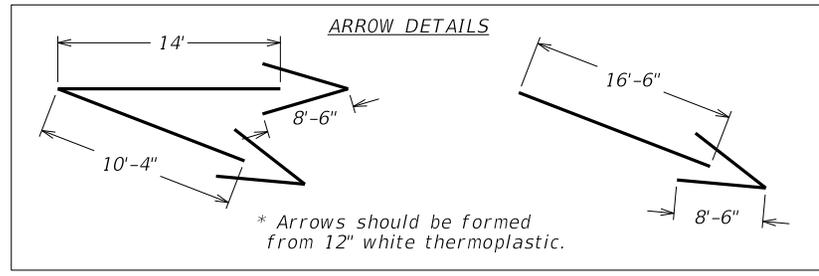
**WIDE DOTTED LANE LINE DIMENSIONS:**

Dotted lane lines shall be twice the normal width in lane drop scenarios.

## LANE DROP WITH AN OPTION LANE



- ① IN SITUATIONS WHERE A THROUGH LANE BECOMES A MANDATORY EXIT LANE, A WIDE WHITE DOTTED LANE LINE SHALL BE INSTALLED.
- ② 12" WIDE SOLID CHANNELIZATION LINE.
- ③ GORE AREA CHEVRON MARKINGS SHALL BE USED IN LANE DROP SCENARIOS AND WHERE AN INTERSTATE OR PARKWAY SPLITS. SEE [TPM-204](#) FOR DETAILS ON GORE AREA CHEVRON MARKINGS.
- ④ LANE USE ARROWS MAY BE USED WHEN THERE IS AN OPTION LANE. A MINIMUM OF THREE ARROWS PER LANE SHOULD BE PLACED AS SHOWN AND SPACED AT APPROXIMATELY 1/4 MILE.
- ⑤ DOTTED LANE LINES SHOULD EXTEND BACK AN APPROXIMATE DISTANCE OF 1/2 MILE FROM THE THEORETICAL GORE.



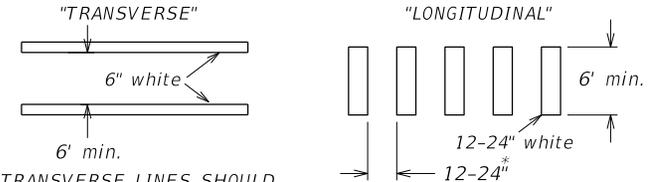
LEGEND	
MARKINGS	
	WHITE
	YELLOW

<b>KENTUCKY DEPARTMENT OF HIGHWAYS</b>		
<b>TYPICAL EXIT RAMP MARKINGS FOR INTERSTATES AND PARKWAYS</b>		
STANDARD DRAWING NO. <b>TPM-202</b>		
SUBMITTED	<i>B. Allen White</i>	02-26-20 DATE
<small>DIRECTOR, DIVISION OF TRAFFIC OPERATIONS</small>		
APPROVED	<i>[Signature]</i>	02-26-20 DATE
<small>STATE HIGHWAY ENGINEER</small>		

**STOP LINE NOTES**

1. STOP LINES SHALL BE 24" WHITE AND SHALL EXTEND ACROSS ALL APPROACH LANES.
2. STOP LINES SHOULD BE PLACED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF A MARKED CROSSWALK. IN THE ABSENCE OF A MARKED CROSSWALK, STOP LINES SHOULD BE PLACED NO MORE THAN 30' OR NO LESS THAN 4' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
3. STOP LINES IN LEFT TURN LANES MAY BE PULLED BACK SO AS NOT TO INTERFERE WITH THE WHEEL PATH OF TURNING VEHICLES.

**CROSSWALK DETAIL**

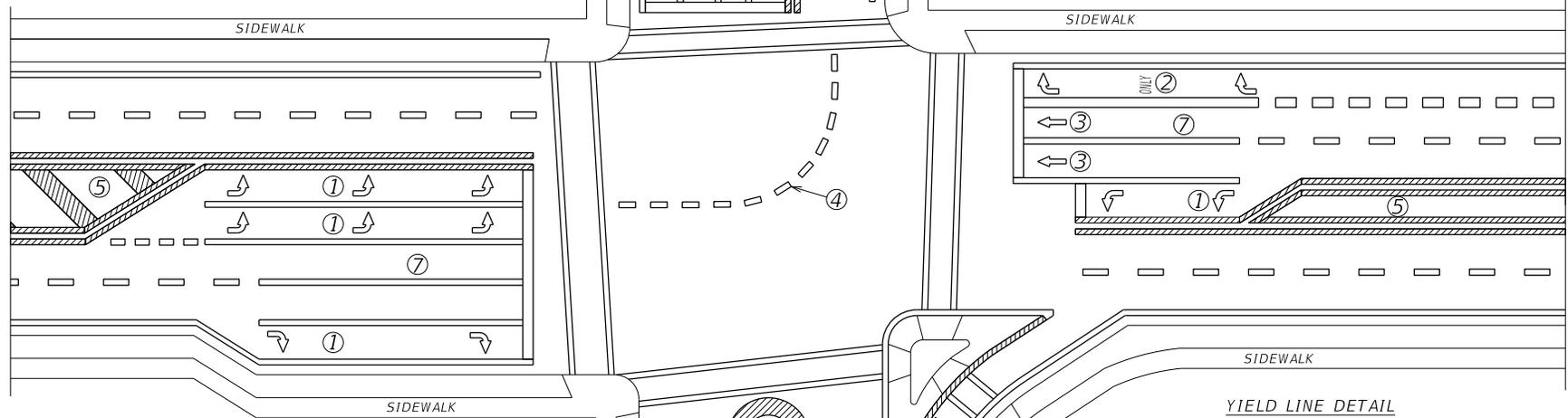


TRANSVERSE LINES SHOULD EXTEND ACROSS THE FULL WIDTH OF THE APPROACH PAVEMENT.

CROSSWALK BARS SHALL BE INSTALLED PARALLEL WITH ONCOMING TRAFFIC.

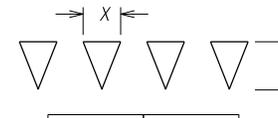
TRANSVERSE LINES MAY BE COMBINED WITH LONGITUDINAL LINES TO FORM A "LADDER-STYLE" CROSSWALK.

\*SPACING OF BARS SHOULD BE MODIFIED SO AS TO AVOID TIRE PATHS OF APPROACHING VEHICLES.



- ① ARROWS SHALL BE USED IN ALL DEDICATED TURN LANES AT SIGNALIZED INTERSECTIONS. REFER TO [TPM-206](#) FOR GUIDANCE ON SPACING.
- ② ARROWS, ALONG WITH THE WORD "ONLY", SHALL ONLY BE USED IN A LANE DROP SCENARIO. REFER TO [TPM-207](#) FOR GUIDANCE ON SPACING.
- ③ ARROWS ARE OPTIONAL IN THROUGH LANES.
- ④ DOTTED LANE LINE EXTENSIONS SHALL BE USED WITH DUAL TURN LANES. THEY SHALL BE THE SAME COLOR OF THE LINES WHICH THEY EXTEND AND SHOULD BE 6" IN WIDTH, 2' IN LENGTH, WITH A GAP OF 2-6" BETWEEN LINES.
- ⑤ REFER TO [TPM-205](#) FOR GUIDANCE ON, AND DIMENSIONS OF, CROSSHATCH AND CHEVRON MARKINGS.
- ⑥ REFER TO [TPM-205](#) FOR GUIDANCE ON MEDIAN NOSES.
- ⑦ SOLID LINE SEPARATING THROUGH LANES SHOULD EXTEND BACK A MINIMUM OF 50' FROM THE STOP BAR.

**YIELD LINE DETAIL**



NOTE: SPACING BETWEEN TRIANGLES SHOULD BE 3-12"

DRAWING NOT TO SCALE

LEGEND	
MARKINGS	
	WHITE
	YELLOW

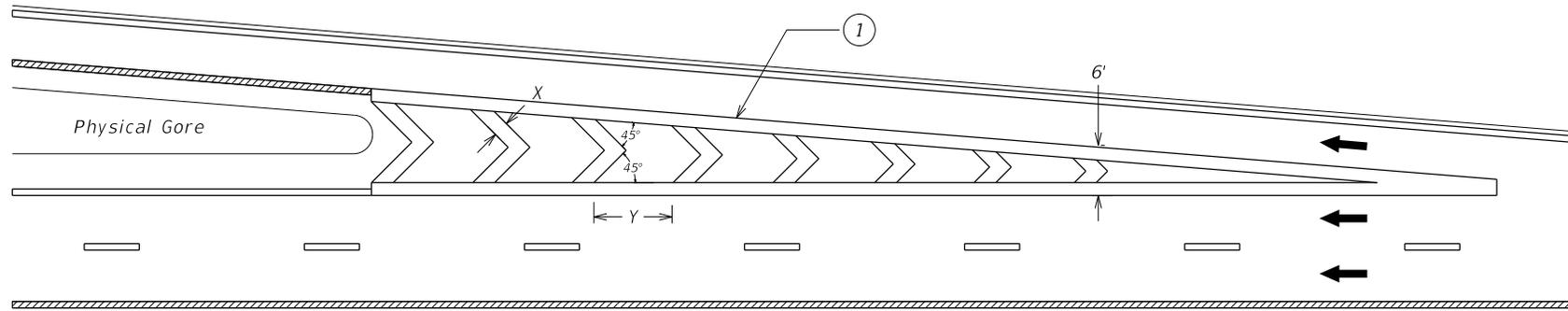
\*NOTE: OTHER THAN LONGITUDINAL STRIPING, ALL MARKINGS SHOWN ON THIS DRAWING SHOULD BE THERMOPLASTIC.

KENTUCKY  
DEPARTMENT OF HIGHWAYS

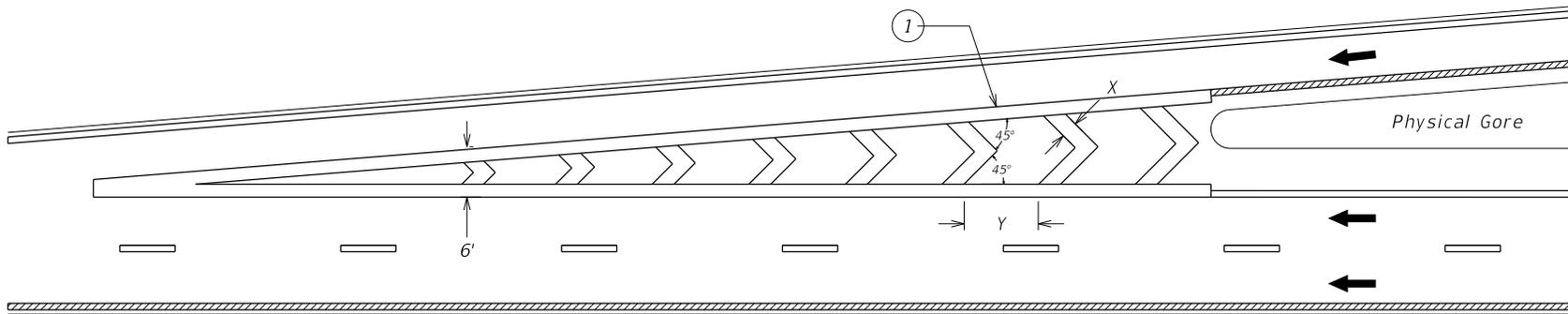
TYPICAL MARKINGS  
AT SIGNALIZED  
INTERSECTIONS

STANDARD DRAWING NO. [TPM-203](#)  
 SUBMITTED BY *B. Allen Wolf* DATE [02-26-20](#)  
DIRECTOR OF PUBLIC SAFETY OPERATIONS  
 APPROVED BY *[Signature]* DATE [02-26-20](#)  
STATE ENGINEER

TYPICAL EXIT RAMP GORE MARKINGS



TYPICAL ENTRANCE RAMP GORE MARKINGS



- ① 12" SOLID WHITE LINE TO BE INSTALLED AS SHOWN. THIS LINE SHOULD TERMINATE AT THE PHYSICAL GORE.
- 2 CHEVRON MARKINGS SHOULD BEGIN WHEN THE 12" WHITE CHANNELIZING LINES ARE APPROXIMATELY 6' APART.
- 3 A MINIMUM OF THREE CHEVRON MARKINGS SHOULD BE USED. IF AT LEAST THREE MARKINGS WILL NOT FIT INTO THE GORE AREA, NO CHEVRON MARKINGS SHOULD BE PLACED.
- 4 THE MINIMUM CHEVRON MARKING WIDTH (X) SHOULD BE 2'. THE MINIMUM SPACING BETWEEN CHEVRON MARKINGS (Y) SHOULD BE 20'. THESE DIMENSIONS MAY BE INCREASED. FOR EACH ADDITIONAL 1' OF WIDTH (X), INCREASE THE SPACING (Y) BY 10'.
- 5 GORE AREA CHEVRON MARKINGS SHALL BE THERMOPLASTIC.

DRAWING NOT TO SCALE

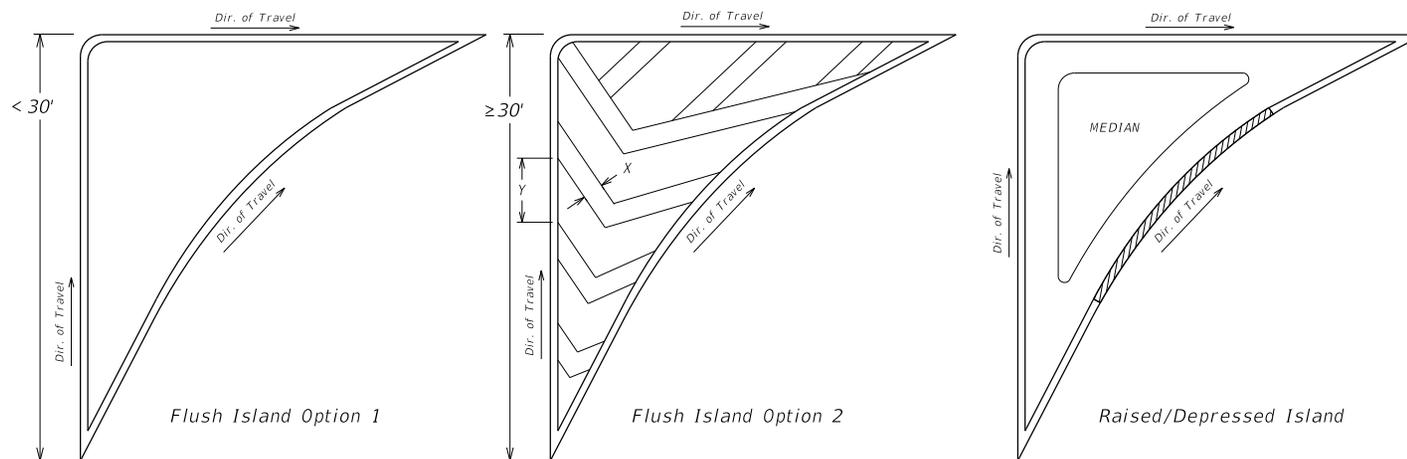
LEGEND	
	WHITE
	YELLOW

KENTUCKY  
DEPARTMENT OF HIGHWAYS

TYPICAL MARKINGS  
FOR GORE AREAS

STANDARD DRAWING NO. **TPM-204**  
 SUBMITTED *B. Allen Wolf* 02-26-20  
DIRECTOR, DIVISION OF TRAFFIC OPERATIONS DATE  
 APPROVED *[Signature]* 02-26-20  
STATE HIGHWAY ENGINEER DATE

TYPICAL RIGHT-TURN CHANNELIZING ISLAND MARKINGS



GENERAL NOTES

THE MINIMUM WIDTH (X) OF CROSSHATCH MARKINGS IS 12" FOR LOW SPEED ROADS ( $< 45$  MPH) AND 24" FOR HIGH SPEED ROADS ( $\geq 45$  MPH).

THE SPACE BETWEEN ADJACENT CROSSHATCH MARKINGS (Y) SHOULD BE SET AT 10 TIMES THE WIDTH (X) OF THE CROSSHATCH MARKINGS.

CROSSHATCH MARKINGS SHOULD NOT BE PLACED IN A MEDIAN LESS THAN 6' IN WIDTH.

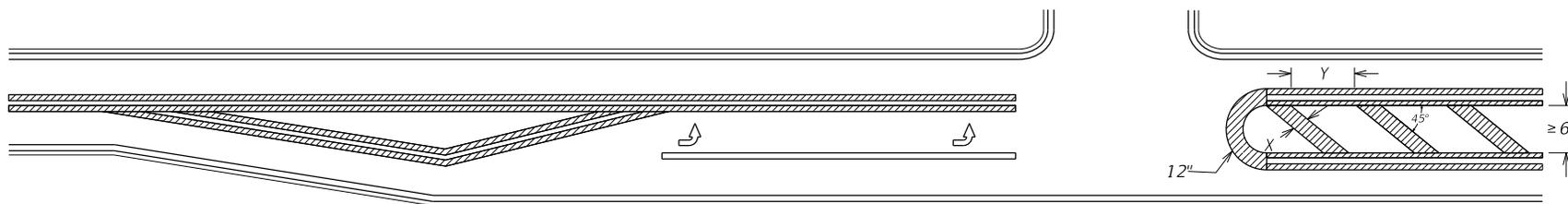
CROSSHATCH MARKINGS SHOULD NOT BE PLACED IN AN ISLAND IF ANY SIDE OF AN ISLAND IS LESS THAN 30' IN LENGTH.

THE OUTLINE OF AN ISLAND SHOULD BE EITHER 8" OR 12".

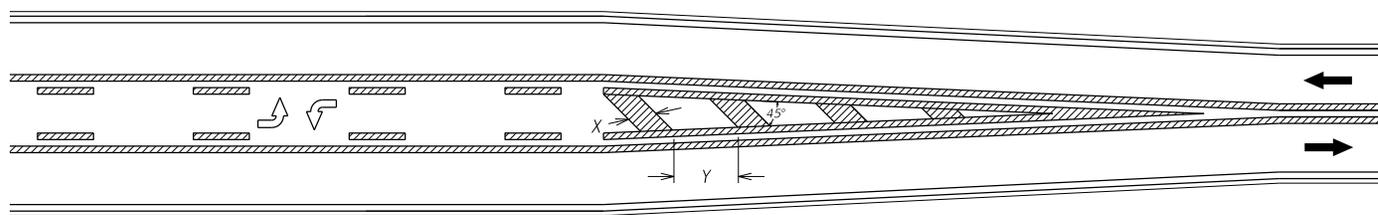
MEDIAN NOSES MAY BE EITHER A SINGLE 12" LINE OR A SOLID SEMI-CIRCLE.

CROSSHATCH AND CHEVRON MARKINGS SHOULD BE THERMOPLASTIC.

TYPICAL LEFT-TURN LANE / FLUSH MEDIAN CROSSHATCH MARKINGS



TYPICAL TWO-WAY LEFT-TURN LANE (TWLTL) TRANSITION MARKINGS



Note: Crosshatch markings shall be placed in the transition area of a TWLTL.

DRAWING NOT TO SCALE

LEGEND	
MARKINGS	
	WHITE
	YELLOW

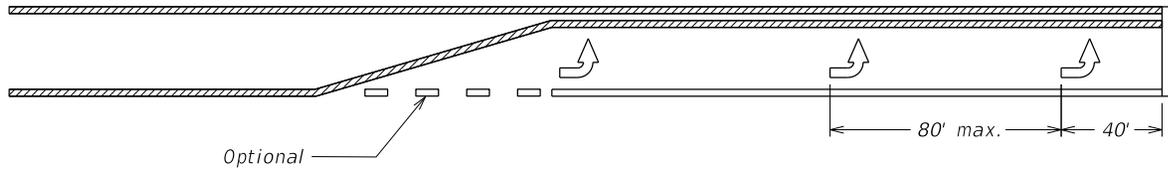
KENTUCKY  
DEPARTMENT OF HIGHWAYS

TYPICAL MARKINGS  
FOR ISLANDS  
AND MEDIANS

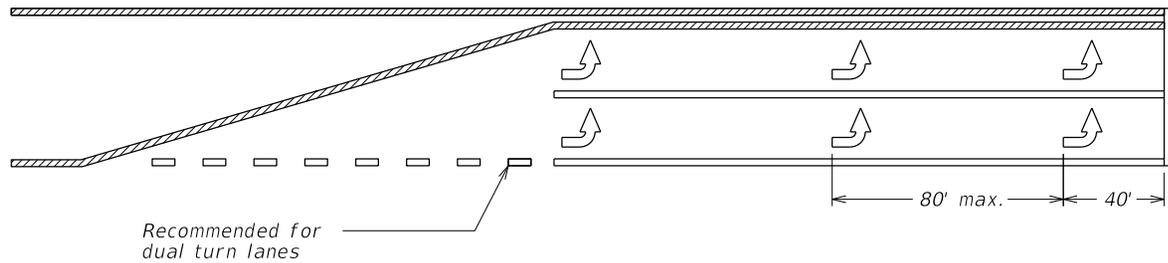
STANDARD DRAWING NO. **TPM-205**

SUBMITTED	<i>R. Allen White</i>	02-26-20
DIRECTOR, DIVISION OF	OPERATIONS	DATE
APPROVED	<i>[Signature]</i>	02-26-20
SINCE	REGISTERED ENGINEER	DATE

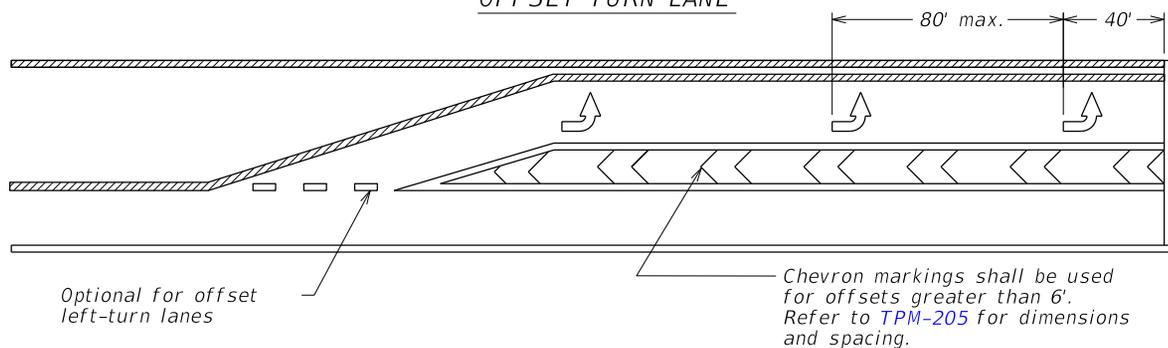
SINGLE TURN LANE



DUAL TURN LANE



OFFSET TURN LANE



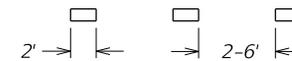
STRIPING NOTES:

- ARROWS SHALL BE USED IN ANY EXCLUSIVE TURN LANES.
- IN A SINGLE TURN LANE, DOTTED WHITE LANE LINE EXTENSIONS MAY BE USED THROUGH THE TAPER OF THE TURN LANE.
- IF USED, DOTTED WHITE LANE LINE EXTENSIONS SHALL BE NORMAL WIDTH, AND SHOULD BE 2' LONG, WITH A GAP OF 2-6' BETWEEN EACH LINE.
- IN DUAL TURN LANES, DOTTED WHITE LANE LINE EXTENSIONS SHOULD BE USED THROUGH THE TAPER OF THE TURN LANE. BOTH SOLID LINES FORMING THE TURN LANES SHALL BEGIN AT THE DOWNSTREAM END OF THE TAPER.

ARROW SPACING NOTES:

- IN SINGLE-DIRECTION TURN LANES, ARROWS SHOULD BE SPACED AS FOLLOWS:
- AT LEAST TWO ARROWS SHOULD BE USED IN EACH TURN LANE. HOWEVER, IF A TURN LANE IS LESS THAN 80' IN LENGTH, THE DOWNSTREAM ARROW MAY BE ELIMINATED.
  - THE FIRST UPSTREAM ARROW SHALL BE PLACED AT THE BEGINNING OF THE SOLID LINE FOR THE TURN LANE.
  - THE LAST DOWNSTREAM ARROW SHOULD BE PLACED 40' FROM THE STOP BAR.
  - ANY ADDITIONAL ARROWS SHOULD BE EVENLY SPACED. SPACING SHOULD NOT EXCEED 80'.
  - ARROW SPACING AND NUMBER OF ARROWS MAY VARY BASED ON SITE CONDITIONS.

DOTTED EXTENSION DIMENSIONS:



Dotted extensions shall be normal width.

Page 1 of 2

DRAWING NOT TO SCALE

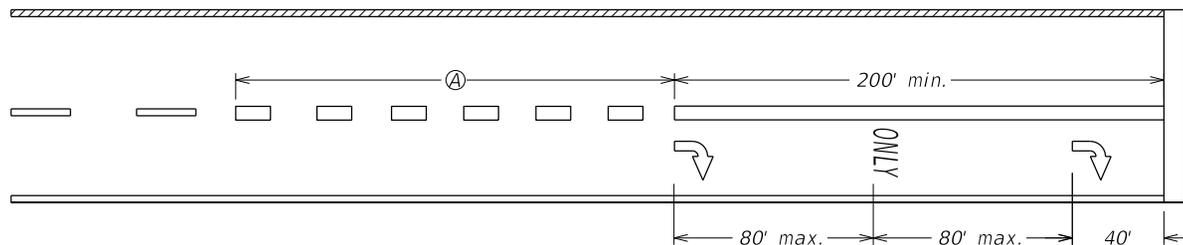
LEGEND	
MARKINGS	
	WHITE
	YELLOW

KENTUCKY  
DEPARTMENT OF HIGHWAYS

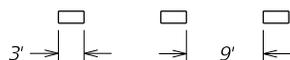
TYPICAL MARKINGS  
FOR TURN LANES

STANDARD DRAWING NO. TPM-206  
 SUBMITTED B. Allen Wolf 02-26-20  
 DIRECTOR OF TRANSPORTATION OPERATIONS DATE  
 APPROVED [Signature] 02-26-20  
 STATE HIGHWAY ENGINEER DATE

### LANE DROP SCENARIO



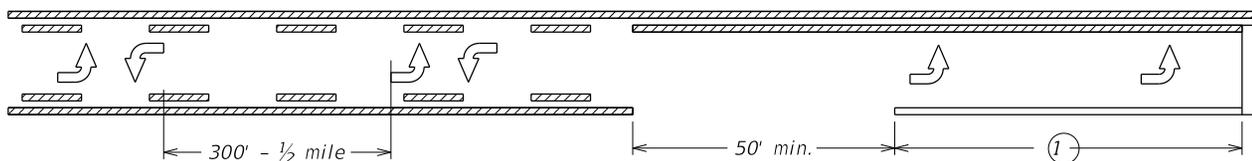
#### WIDE DOTTED LANE LINE DIMENSIONS:



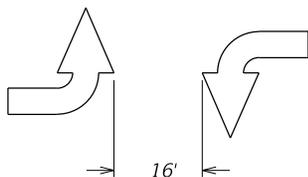
Dotted lane lines shall be twice the normal width in lane drop scenarios.

Speed Limit	(A)
25	min. 125'
35	min. 245'
45	min. 540'
55	min. 660'
65	min. 780'

### TWO-WAY LEFT-TURN LANE



#### TWLTL ARROW DETAILS:



#### LANE DROP MARKINGS NOTES:

IN SITUATIONS WHERE A THROUGH LANE BECOMES A MANDATORY TURN LANE, THE FOLLOWING GUIDELINES APPLY:

- A WIDE SOLID LINE SHOULD EXTEND BACK A MINIMUM OF 200' FROM THE STOP BAR.
- A WIDE, DOTTED LINE SHALL EXTEND FROM THE END OF THE SOLID LINE BACK A MINIMUM OF THE DISTANCE SHOWN IN THE CHART (A). THESE LINES SHALL BE 3' LONG, WITH A SPACE OF 9' BETWEEN LINES.
- ALTERNATING ARROWS AND "ONLY" WORD MESSAGES SHALL BE USED, WITH THE FIRST AND LAST MARKING BEING AN ARROW.
- ALTERNATING ARROWS AND "ONLY" WORD MESSAGES SHOULD BE SPACED EVENLY, FOLLOWING GUIDELINES FOR ARROW SPACING. THESE SYMBOLS SHALL EXTEND BACK AT LEAST TO THE END OF THE SOLID STRIPE, BUT MAY BE EXTENDED BACK FARTHER IF ADDITIONAL GUIDANCE IS NEEDED.

#### TWO-WAY LEFT-TURN LANE NOTES:

IN A TWO-WAY LEFT-TURN LANE, THE FOLLOWING GUIDELINES APPLY:

- CONTACT TRAFFIC ENGINEER FOR RECOMMENDED DISTANCE FOR LEFT TURN STORAGE AT INTERSECTIONS.
- ONE SET OF ARROWS SHOULD BE PLACED AT OR NEAR THE BEGINNING OF THE TWO-WAY LEFT-TURN LANE.
- ADDITIONAL SETS OF ARROWS SHOULD BE PLACED THROUGHOUT THE TWO-WAY LEFT-TURN LANE IF LEFT TURN MOVEMENTS ARE EXPECTED. THEY SHOULD BE SPACED NO LESS THAN 300' AND NO MORE THAN 1/2 MILE.
- THE SPACING BETWEEN EACH ARROW IN A SINGLE ARROW SET SHOULD BE 16 FEET.
- TWO-WAY LEFT-TURN LANES SHALL TERMINATE IN A DEDICATED LEFT-TURN LANE AT A SIGNALIZED INTERSECTION. THEY MAY TERMINATE IN A DEDICATED LEFT-TURN LANE AT OTHER LOCATIONS IF DEEMED NECESSARY.
- REFER TO THE TRAFFIC OPERATIONS GUIDANCE MANUAL SECTION T0-504 FOR MORE GUIDANCE ON TWO-WAY LEFT-TURN LANES.

Page 2 of 2

DRAWING NOT TO SCALE

#### LEGEND

MARKINGS

- WHITE
- YELLOW

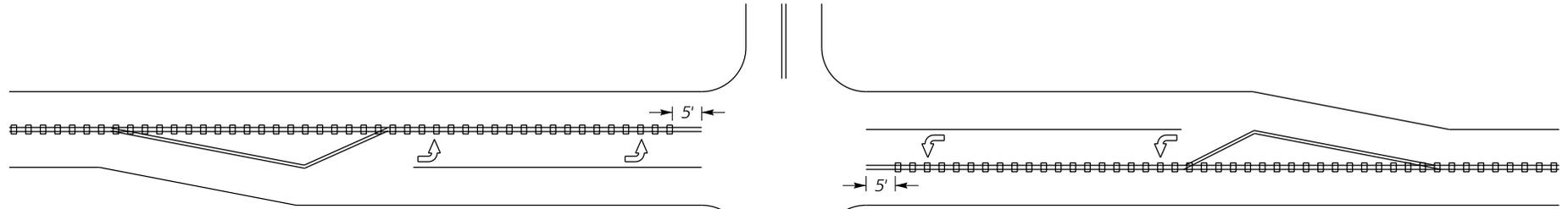
KENTUCKY  
DEPARTMENT OF HIGHWAYS

TYPICAL MARKINGS  
FOR TURN LANES

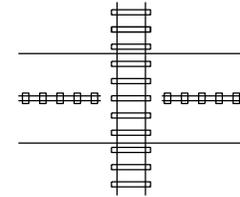
STANDARD DRAWING NO. TPM-207

SUBMITTED *B. Allen Wolf* 02-26-20  
DATE DIRECTOR OF PUBLIC OPERATIONS

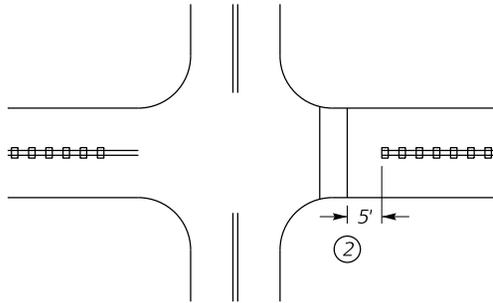
APPROVED *[Signature]* 02-26-20  
DATE ENGINEER



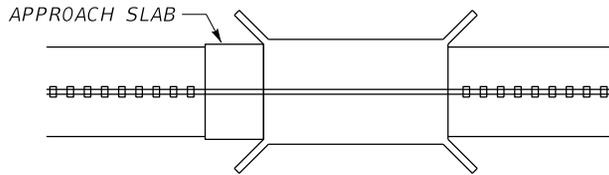
INTERSECTIONS WITH LEFT-TURN LANES ①



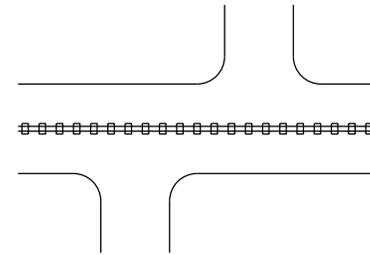
HIGHWAY-RAIL GRADE CROSSINGS ③



INTERSECTIONS WITHOUT LEFT-TURN LANES ①



BRIDGE DECK/APPROACH SLAB ④



DRIVEWAYS/MINOR COMMERCIAL ENTRANCES ⑤

~ NOTES ~

- ① CENTERLINE RUMBLE STRIPS SHALL BE OMITTED THROUGH MAJOR INTERSECTIONS WITH, OR WITHOUT, LEFT-TURN LANES. OMIT THE CENTERLINE RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF THE AREA WHERE THE CENTERLINE PAVEMENT MARKINGS HAVE BEEN OMITTED (NORMALLY WHERE SIDE STREET RADIUS INTERSECTS MAINLINE).
- ② CENTERLINE RUMBLE STRIPS SHALL NOT BE INSTALLED THROUGH MARKED CROSSWALKS. OMIT THE CENTERLINE RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF MARKED CROSSWALKS.
- ③ CENTERLINE RUMBLE STRIPS SHALL NOT BE INSTALLED ACROSS HIGHWAY-RAIL GRADE CROSSINGS.
- ④ CENTERLINE RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS OR APPROACH SLABS.
- ⑤ CENTERLINE RUMBLE STRIPS SHALL BE INSTALLED THROUGH DRIVEWAYS & MINOR COMMERCIAL ENTRANCES.
6. CENTERLINE RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS, OR WHERE LANE WIDTHS ARE LESS THAN 11 FT.

DRAWING NOT TO SCALE

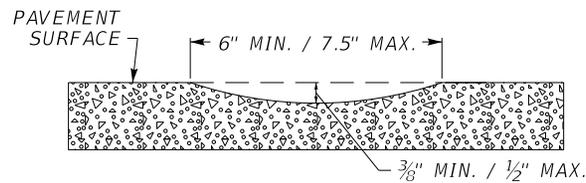
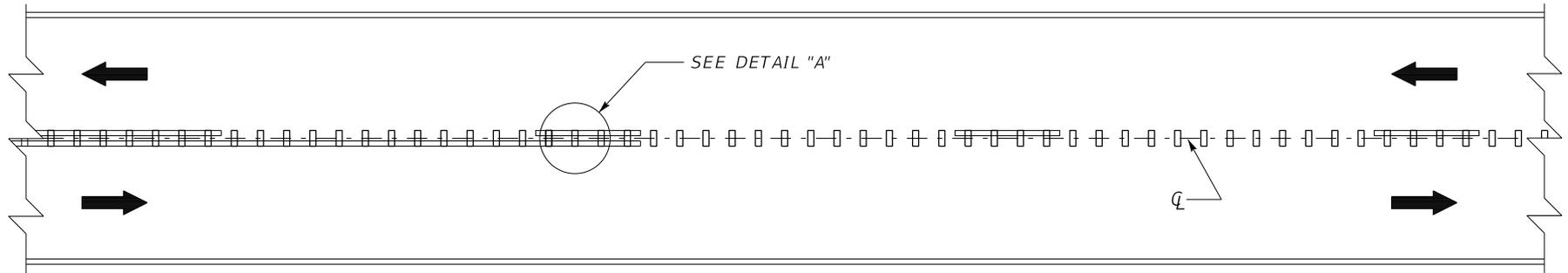
USE WITH CUR. STD. DWGS.  
 TPR-105, TPR-110, TPR-120,  
 AND TPR-125

KENTUCKY DEPARTMENT OF HIGHWAYS	
CENTERLINE RUMBLE STRIPS PLACEMENT DETAILS	
STANDARD DRAWING NO. <b>TPR-100</b>	
SUBMITTED <i>B. Allen Wolfe</i>	DATE 02-26-20
DIRECTOR, KY HIGHWAY TRAFFIC OPERATIONS	
APPROVED <i>[Signature]</i>	DATE 02-26-20
ENGINEER	

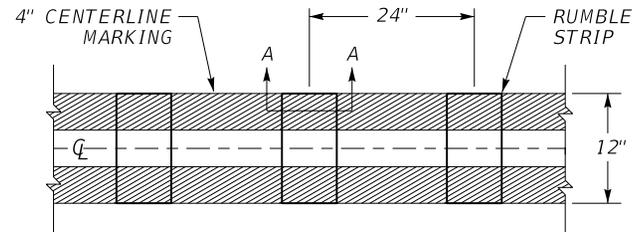
BID ITEM AND UNIT TO BID  
 CENTERLINE RUMBLE STRIPS

LF

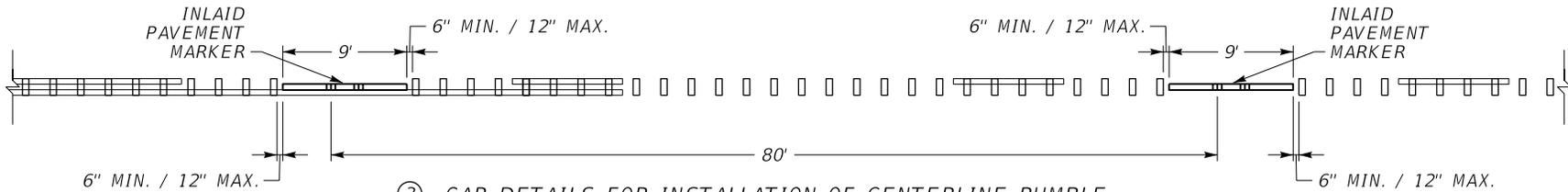
PLAN VIEW



SECTION A-A



DETAIL "A"



③ GAP DETAILS FOR INSTALLATION OF CENTERLINE RUMBLE STRIPS IN CONJUNCTION WITH INLAID PAVEMENT MARKERS

~ NOTES ~

1. DISTANCES SHOWN ARE APPROXIMATE. MAINTAIN RUMBLE STRIP DIMENSIONS AND SPACING AS MUCH AS POSSIBLE.
2. CENTERLINE RUMBLE STRIPS SHALL BE INSTALLED IN LINE WITH THE CENTER OF THE ROADWAY AS MUCH AS POSSIBLE.
- ③ FOR ROADWAYS WHERE BOTH INLAID PAVEMENT MARKERS AND CENTERLINE RUMBLE STRIPS ARE TO BE INSTALLED, DISCONTINUE THE CENTERLINE RUMBLE STRIPS 6" MIN. / 12" MAX. BEFORE AND AFTER THE GROOVE FOR EACH INLAID PAVEMENT MARKER. INSTALL AS MANY RUMBLE STRIPS AS POSSIBLE BETWEEN ADJACENT PAVEMENT MARKERS WHILE MAINTAINING THE 24" CYCLE.
4. DO NOT INSTALL CENTERLINE RUMBLE STRIPS IN AREAS INDICATED ON TPR-100.
5. CENTERLINE RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS, OR WHERE LANE WIDTHS ARE LESS THAN 11 FT.

BID ITEM AND UNIT TO BID  
CENTERLINE RUMBLE STRIPS

LF

DRAWING NOT TO SCALE

USE WITH CUR. STD. DWGS.  
TPR-100, TPR-120, AND  
TPR-125

KENTUCKY  
DEPARTMENT OF HIGHWAYS

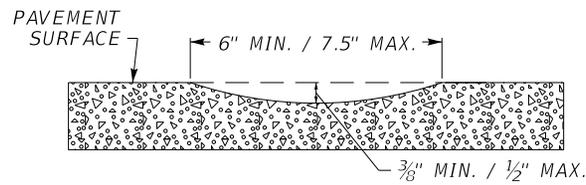
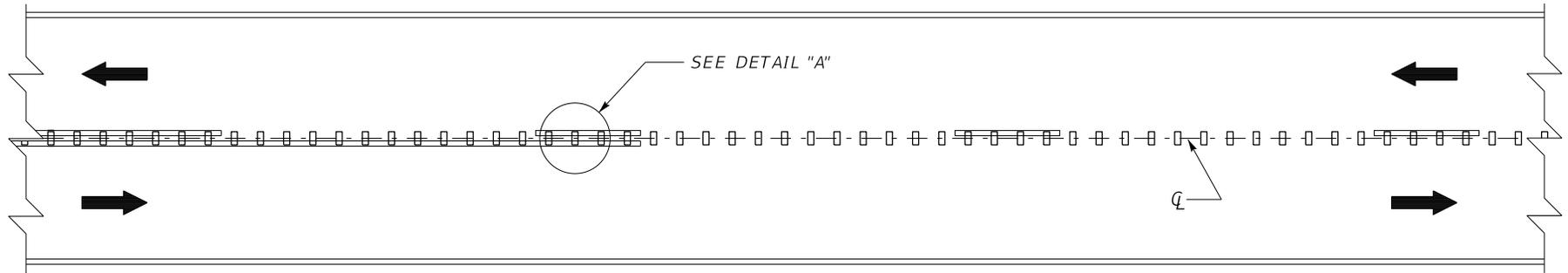
CENTERLINE  
RUMBLE STRIPS  
4 INCH STRIPING

STANDARD DRAWING NO. TPR-105

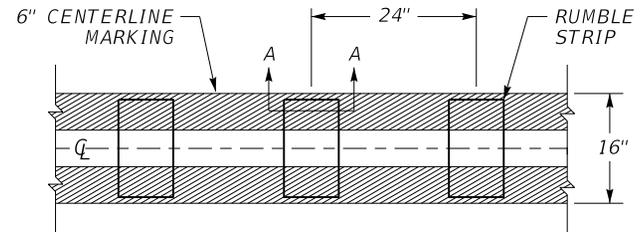
SUBMITTED *B. Allen Wolf* 02-26-20  
DIRECTOR, KENTUCKY DEPARTMENT OF TRANSPORTATION DATE

APPROVED *[Signature]* 02-26-20  
SPRINT ENGINEER DATE

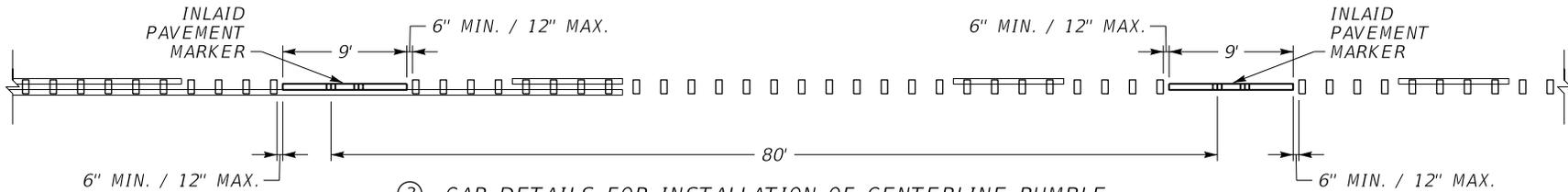
PLAN VIEW



SECTION A-A



DETAIL "A"



③ GAP DETAILS FOR INSTALLATION OF CENTERLINE RUMBLE STRIPS IN CONJUNCTION WITH INLAID PAVEMENT MARKERS

~ NOTES ~

1. DISTANCES SHOWN ARE APPROXIMATE. MAINTAIN RUMBLE STRIP DIMENSIONS AND SPACING AS MUCH AS POSSIBLE.
2. CENTERLINE RUMBLE STRIPS SHALL BE INSTALLED IN LINE WITH THE CENTER OF THE ROADWAY AS MUCH AS POSSIBLE.
- ③ FOR ROADWAYS WHERE BOTH INLAID PAVEMENT MARKERS AND CENTERLINE RUMBLE STRIPS ARE TO BE INSTALLED, DISCONTINUE THE CENTERLINE RUMBLE STRIPS 6" MIN. / 12" MAX. BEFORE AND AFTER THE GROOVE FOR EACH INLAID PAVEMENT MARKER. INSTALL AS MANY RUMBLE STRIPS AS POSSIBLE BETWEEN ADJACENT PAVEMENT MARKERS WHILE MAINTAINING THE 24" CYCLE.
4. DO NOT INSTALL CENTERLINE RUMBLE STRIPS IN AREAS INDICATED ON TPR-100.
5. CENTERLINE RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS, OR WHERE LANE WIDTHS ARE LESS THAN 11 FT.

BID ITEM AND UNIT TO BID  
CENTERLINE RUMBLE STRIPS

LF

DRAWING NOT TO SCALE

USE WITH CUR. STD. DWGS.  
TPR-100, TPR-120, AND  
TPR-125

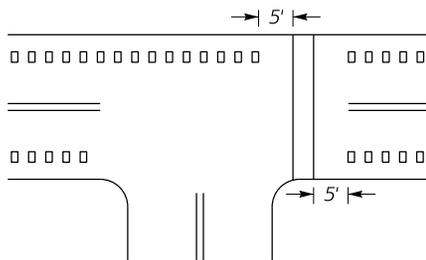
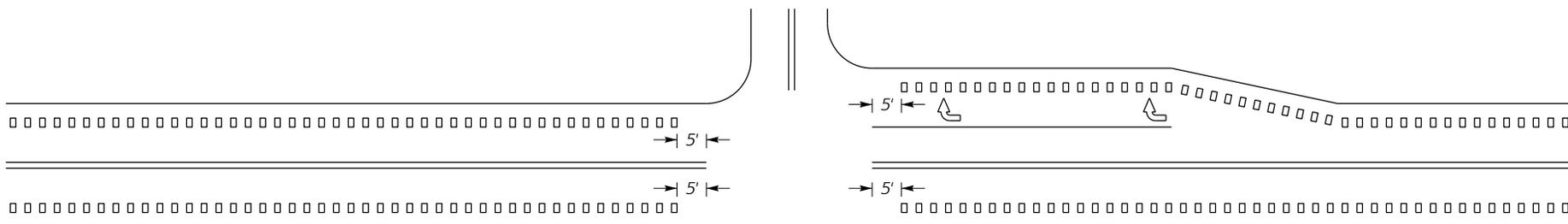
KENTUCKY  
DEPARTMENT OF HIGHWAYS

CENTERLINE  
RUMBLE STRIPS  
6 INCH STRIPING

STANDARD DRAWING NO. TPR-110

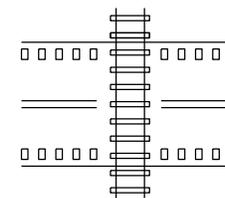
SUBMITTED *B. Allen Wolf* DATE 02-26-20  
DIRECTOR, DIVISION OF SAFETY OPERATIONS

APPROVED *[Signature]* DATE 02-26-20  
ENGINEER



MARKED CROSSWALK ②

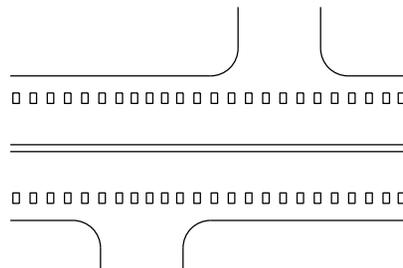
INTERSECTIONS WITH OR WITHOUT RIGHT TURN LANES ①



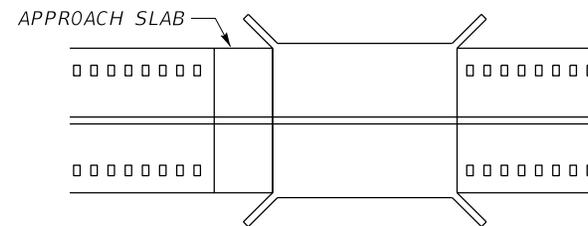
HIGHWAY-RAIL GRADE CROSSINGS ③



MAILBOX TURNOUTS ⑥



DRIVEWAYS/MINOR COMMERCIAL ENTRANCES ⑤



BRIDGE DECK/APPROACH SLAB ④

~ NOTES ~

- ① RUMBLE STRIPS SHALL BE OMITTED THROUGH MAJOR INTERSECTIONS WITH, OR WITHOUT, RIGHT-TURN LANES. OMIT RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF THE AREA WHERE EDGELINE PAVEMENT MARKINGS HAVE BEEN OMITTED (NORMALLY WHERE SIDE STREET RADIUS INTERSECTS MAINLINE).
- ② RUMBLE STRIPS SHALL NOT BE INSTALLED THROUGH MARKED CROSSWALKS. OMIT RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF MARKED CROSSWALKS.
- ③ RUMBLE STRIPS SHALL NOT BE INSTALLED ACROSS HIGHWAY-RAIL GRADE CROSSINGS.
- ④ RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS OR APPROACH SLABS.
- ⑤ RUMBLE STRIPS SHALL BE INSTALLED THROUGH DRIVEWAYS & MINOR COMMERCIAL ENTRANCES.
- ⑥ RUMBLE STRIPS SHALL BE INSTALLED THROUGH MAILBOX TURNOUTS.
7. RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS.

DRAWING NOT TO SCALE

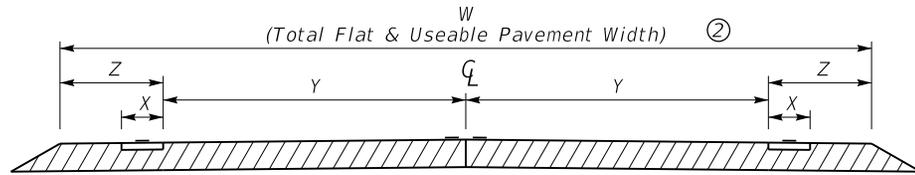
USE WITH CUR. STD. DWGS.  
TPR-120, TPR-125, AND  
TPR-130

BID ITEMS AND UNIT TO BID  
EDGELINE RUMBLE STRIPS  
SHOULDER RUMBLE STRIPS

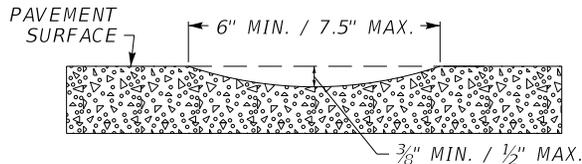
LF  
LF

<b>KENTUCKY DEPARTMENT OF HIGHWAYS</b>		
<b>SHOULDER &amp; EDGELINE RUMBLE STRIP PLACEMENT DETAILS</b>		
STANDARD DRAWING NO. <b>TPR-115</b>		
SUBMITTED <i>B. Allen Wolfe</i> <small>DIRECTOR, DIVISION OF TRAFFIC OPERATIONS</small>	DATE 02-26-20	
APPROVED <i>[Signature]</i> <small>STATE HIGHWAY ENGINEER</small>	DATE 02-26-20	

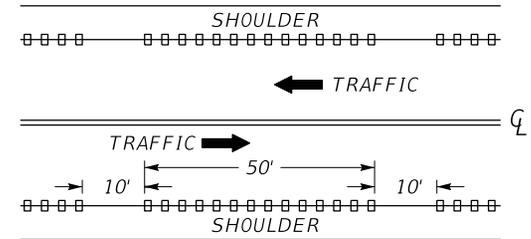
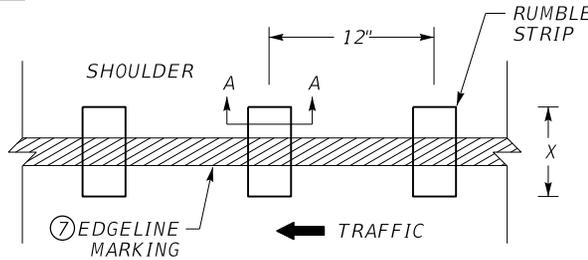
PAVEMENT WIDTH (W) ②	TYPES OF RUMBLE STRIPS TO INSTALL ⑦	LANE WIDTH (Y) ③	SHOULDER WIDTH (Z) ④	LENGTH OF EDGELINE RUMBLE (X) ⑥
22'	INSTALL ONLY EDGELINE RUMBLE STRIPS	10'	1'	8"
23'		10'	1.5'	8"
24'		10.5'	1.5'	8"
25'	INSTALL BOTH EDGELINE AND CENTERLINE RUMBLE STRIPS	11'	1.5'	8"
26'		11'	2'	8"
27'		11.5'	2'	8"
28'		12'	2'	8"
29'		12'	2.5'	8"
30'		12'	3'	8"
31'		12'	3.5'	8"
32'		12'	4'	8"
33'		12'	4.5'	8"
>33'	REFER TO TPR-125			



PAVEMENT CROSS-SECTION



SECTION A-A



BICYCLE GAPS ⑧

~ NOTES ~

APPLICATION OF THE TABLE ABOVE: THE TOTAL PAVEMENT WIDTH (W) IS THE STARTING POINT IN USING THE TABLE. THE TOTAL PAVEMENT WIDTH IS TO BE USED TO DETERMINE THE TYPE(S) OF RUMBLE STRIPS TO INSTALL AND THE RECOMMENDED LANE WIDTH (Y) AND SHOULDER WIDTH (Z).

- EDGELINE RUMBLE STRIPS, AND CENTERLINE RUMBLE STRIPS WHEN APPLICABLE, SHOULD BE INSTALLED TO CREATE THE LANE AND SHOULDER WIDTHS SHOWN ABOVE, UNLESS THERE IS A REASON THAT SUPPORTS A CHANGE IN DIMENSION. FOR EXAMPLE, IF THE EXISTING LANE WIDTH IS NARROWER THAN THE LANE WIDTH PROPOSED IN THIS DRAWING AND THE EXISTING SHOULDER PAVEMENT DEPTH IS NOT SUITABLE TO BE CONVERTED INTO A PORTION OF THE PROPOSED LANE WIDTH, THEN THE EXISTING LANE AND SHOULDER WIDTHS SHOULD BE USED INSTEAD OF THE LANE AND SHOULDER WIDTHS PROPOSED IN THIS DRAWING.
- PAVEMENT WIDTH (W) IS THE TOTAL WIDTH OF PAVEMENT THAT IS FLAT AND USEABLE FOR DRIVING. WHEN MEASURING THE PAVEMENT WIDTH (W), DO NOT INCLUDE THE WIDTH OF ANY PAVEMENT THAT IS NOT FLAT AND USEABLE, SUCH AS PAVEMENT WEDGES.
- LANE WIDTH (Y) TO BE MEASURED FROM CENTER OF ROAD TO LANE SIDE EDGE OF THE EDGELINE RUMBLE STRIP.
- PAVED SHOULDER WIDTH (Z) TO BE MEASURED FROM LANE SIDE EDGE OF THE EDGELINE RUMBLE STRIP TO OUTSIDE EDGE OF FLAT & USEABLE PAVEMENT.
- DIMENSIONS SHOWN ARE APPROXIMATE. MAINTAIN RUMBLE STRIP DIMENSIONS AND SPACING AS MUCH AS POSSIBLE. IF THE TYPICAL SECTION SHOWS A LANE WIDTH (Y) AND/OR SHOULDER WIDTH (Z) THAT DIFFERS FROM THE WIDTHS LISTED IN THIS DRAWING, THE ENGINEER SHALL DETERMINE THE LANE WIDTH (Y) AND/OR SHOULDER WIDTH (Z) AT THE TIME OF CONSTRUCTION.  
NOTE: CENTERLINE RUMBLE STRIPS SHOULD BE OMITTED IF THE DECISION IS TO INSTALL A LANE WIDTH (Y) THAT IS LESS THAN 11 FT.
- RUMBLE LENGTH (X) MAY BE MODIFIED AS THE ENGINEER DIRECTS, IF THE SHOULDER WIDTH (Z) IS EQUAL TO OR LESS THAN THE PROPOSED RUMBLE LENGTH (X).
- PLACE THE EDGELINE MARKING IN THE CENTER OF THE RUMBLE STRIP. ON NON-STATE PRIMARY ROUTES WITH LESS THAN 1000 ADT, THE EDGELINE MARKING MAY BE OMITTED, LEAVING THE RUMBLE STRIP AS THE ONLY COMPONENT INSTALLED.
- ALL EDGELINE RUMBLE STRIPS ALONG SHOULDERS THAT ARE 3' OR WIDER SHOULD INCLUDE BICYCLE GAPS AS DETAILED.
- RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS.

DRAWING NOT TO SCALE

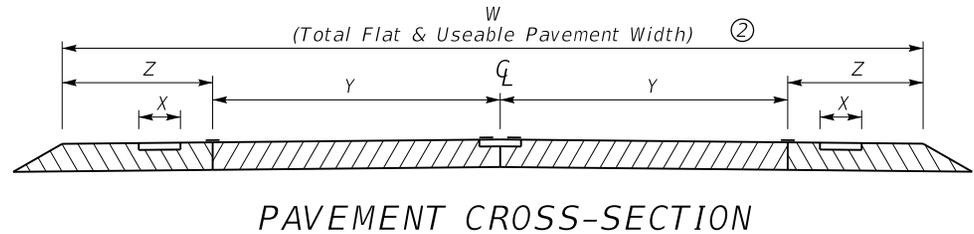
USE WITH CUR. STD. DWGS.  
TPR-100, TPR-105, TPR-110,  
AND TPR-115

BID ITEM AND UNIT TO BID  
EDGELINE RUMBLE STRIPS

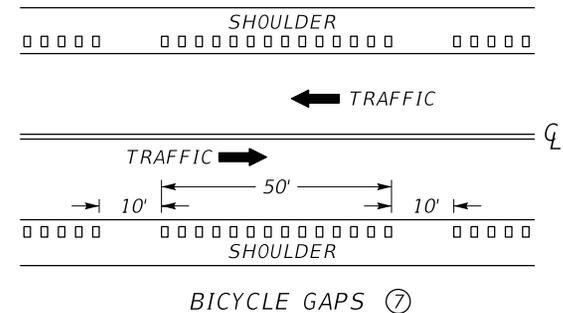
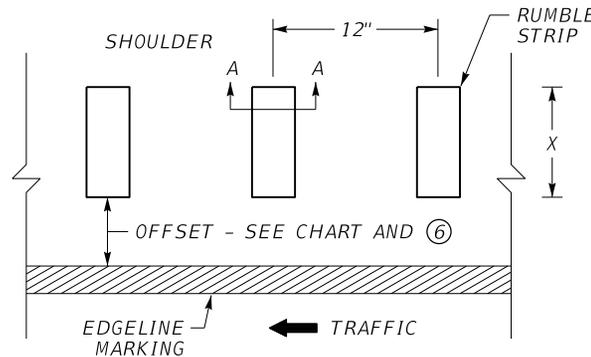
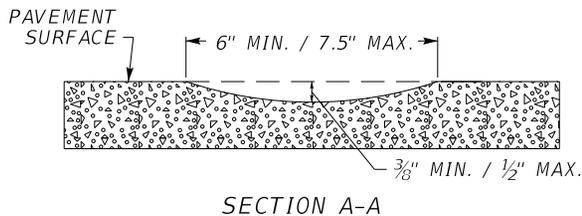
LF

KENTUCKY DEPARTMENT OF HIGHWAYS	
EDGELINE RUMBLE STRIP DETAILS TWO LANE ROADWAYS	
STANDARD DRAWING NO. TPR-120	DATE
SUBMITTED <i>B. Allen Wolf</i>	02-26-20
DIRECTOR OF HIGHWAYS	
APPROVED <i>[Signature]</i>	DATE
SPRINT ENGINEER	02-26-20

PAVEMENT WIDTH (W) ②	TYPES OF RUMBLE STRIPS TO INSTALL	LANE WIDTH (Y) ③	SHOULDER WIDTH (Z) ④	SHOULDER RUMBLE LENGTH (X) ⑥	SHOULDER RUMBLE OFFSET ⑥
<34'	REFER TO TPR-120				
34'	INSTALL BOTH SHOULDER AND CENTERLINE RUMBLE STRIPS	12'	5'	8"	6"
35'		12'	5.5'	8"	6"
36'		12'	6'	8"	6"
37'		12'	6.5'	12"	12"
38'		12'	7'	12"	12"
39'		12'	7.5'	12"	12"
>=40'		12'	>=8'	16"	12"



PAVEMENT CROSS-SECTION



~ NOTES ~

APPLICATION OF THE TABLE ABOVE: THE TOTAL PAVEMENT WIDTH (W) IS THE STARTING POINT IN USING THE TABLE. THE TOTAL PAVEMENT WIDTH IS TO BE USED TO DETERMINE THE TYPE(S) OF RUMBLE STRIPS TO INSTALL AND THE RECOMMENDED LANE WIDTH (Y) AND SHOULDER WIDTH (Z).

- SHOULDER AND CENTERLINE RUMBLE STRIPS SHOULD BE INSTALLED TO CREATE THE LANE AND SHOULDER WIDTHS SHOWN ABOVE, UNLESS THERE IS A REASON THAT SUPPORTS A CHANGE IN DIMENSION. FOR EXAMPLE, IF THE EXISTING LANE WIDTH IS NARROWER THAN THE LANE WIDTH PROPOSED IN THIS DRAWING AND THE EXISTING SHOULDER PAVEMENT DEPTH IS NOT SUITABLE TO BE CONVERTED INTO A PORTION OF THE PROPOSED LANE WIDTH, THEN THE EXISTING LANE AND SHOULDER WIDTHS SHOULD BE USED INSTEAD OF THE LANE AND SHOULDER WIDTHS PROPOSED IN THIS DRAWING.
- PAVEMENT WIDTH (W) IS THE TOTAL WIDTH OF PAVEMENT THAT IS FLAT AND USEABLE FOR DRIVING. WHEN MEASURING THE PAVEMENT WIDTH (W) DO NOT INCLUDE THE WIDTH OF ANY PAVEMENT THAT IS NOT FLAT AND USEABLE, SUCH AS PAVEMENT WEDGES.
- LANE WIDTH (Y) TO BE MEASURED FROM CENTER OF ROAD TO CENTER OF EDGELINE STRIPE.
- PAVED SHOULDER WIDTH (Z) TO BE MEASURED FROM CENTER OF EDGELINE STRIPE TO OUTSIDE EDGE OF FLAT & USEABLE PAVEMENT.
- DIMENSIONS SHOWN ARE APPROXIMATE. MAINTAIN RUMBLE STRIP DIMENSIONS AND SPACING AS MUCH AS POSSIBLE. IF THE TYPICAL SECTION SHOWS A LANE WIDTH (Y) AND/OR SHOULDER WIDTH (Z) THAT DIFFERS FROM THE WIDTHS LISTED IN THIS DRAWING, THE ENGINEER SHALL DETERMINE THE LANE WIDTH (Y) AND/OR SHOULDER WIDTH (Z) AT THE TIME OF CONSTRUCTION.  
NOTE: CENTERLINE RUMBLE STRIPS SHOULD BE OMITTED IF THE DECISION IS TO INSTALL A LANE WIDTH (Y) THAT IS LESS THAN 11 FT.
- RUMBLE LENGTH (X) AND/OR OFFSET DISTANCE MAY BE MODIFIED AS THE ENGINEER DIRECTS.
- ALL SHOULDER RUMBLE STRIPS SHOULD INCLUDE BICYCLE GAPS AS DETAILED.
- RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS.

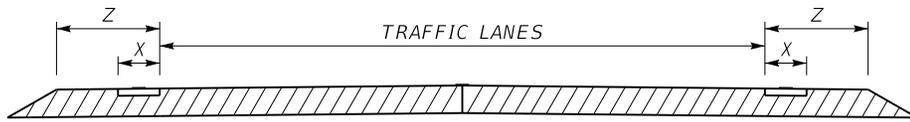
DRAWING NOT TO SCALE

USE WITH CUR. STD. DWGS.  
TPR-100, TPR-105, TPR-110,  
AND TPR-115

KENTUCKY DEPARTMENT OF HIGHWAYS	
SHOULDER RUMBLE STRIP DETAILS TWO LANE ROADWAYS	
STANDARD DRAWING NO. TPR-125	
SUBMITTED BY <i>B. Allen Wolf</i> DIRECTOR OF OPERATIONS	DATE 02-26-20
APPROVED BY <i>[Signature]</i> ENGINEER	DATE 02-26-20

BID ITEM AND UNIT TO BID  
SHOULDER RUMBLE STRIPS

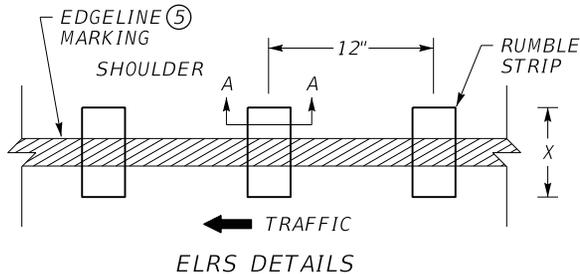
LF



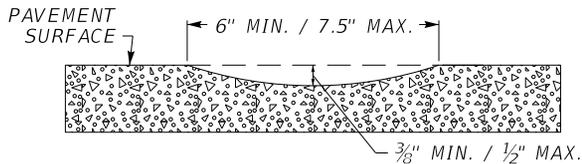
PAVEMENT CROSS-SECTION  
(WHEN ELRS ARE SPECIFIED)



PAVEMENT CROSS-SECTION  
(WHEN SRS ARE SPECIFIED)

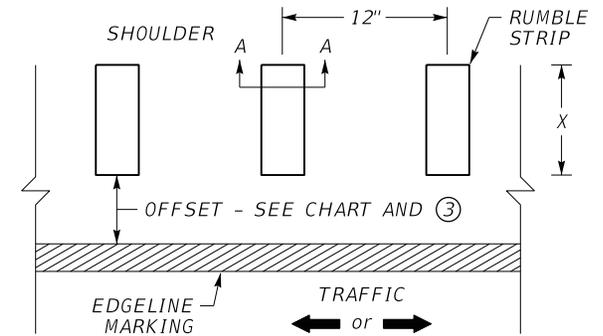


ELRS DETAILS

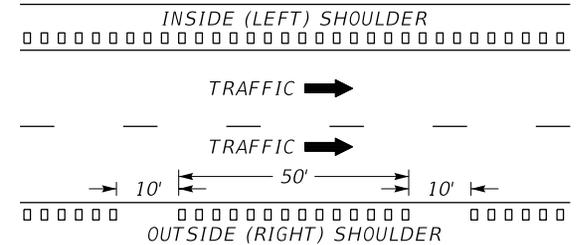


SECTION A-A

SHOULDER WIDTH (Z) ②	RUMBLE TYPE ①	RUMBLE LENGTH (X) ③	OFFSET ③
>=1'	ELRS	8"	N/A
2'	ELRS or SRS	8"	ELRS-N/A SRS-6"
3'	ELRS or SRS ⑥	8"	ELRS-N/A SRS-6"
4'	ELRS or SRS ⑥	8"	ELRS-N/A SRS-6"
5'	SRS ⑥	8"	6"
6'	SRS ⑥	8"	6"
7'	SRS ⑥	12"	12"
>=8'	SRS ⑥	16"	12"



SRS DETAILS



BICYCLE GAPS ⑥

~ NOTES ~

- FOR MULTI-LANE ROADWAYS, THE RUMBLE TYPE TO BE INSTALLED IS BASED ON SHOULDER WIDTH (Z). FOR SHOULDER WIDTHS OF 2', 3', AND 4' THE RUMBLE TYPE MAY BE SPECIFIED AS EITHER EDGELINE RUMBLE STRIPS (ELRS) OR SHOULDER RUMBLE STRIPS (SRS). IN THESE SITUATIONS, THE RUMBLE TYPE TO BE INSTALLED WILL BE SPECIFIED IN THE PLANS, PROPOSAL, AND/OR BID ITEMS, OR AS DIRECTED BY THE ENGINEER.
- WHEN ELRS ARE SPECIFIED, SHOULDER WIDTH (Z) IS FROM LANE SIDE EDGE OF THE EDGELINE RUMBLE STRIP TO OUTSIDE EDGE OF TRAVERSABLE PAVEMENT. WHEN SRS ARE SPECIFIED, SHOULDER WIDTH (Z) IS FROM CENTER OF EDGELINE STRIPE TO OUTSIDE EDGE OF TRAVERSABLE PAVEMENT.
- RUMBLE LENGTH (X) AND/OR OFFSET DISTANCE MAY BE MODIFIED AS THE ENGINEER DIRECTS.
- DISTANCES SHOWN ARE APPROXIMATE. MAINTAIN RUMBLE STRIP DIMENSIONS AND SPACING AS MUCH AS POSSIBLE.
- WHEN ELRS ARE SPECIFIED, THE EDGELINE MARKING SHALL BE PLACED IN THE CENTER OF THE RUMBLE STRIP.
- SHOULDER RUMBLE STRIPS (SRS) ALONG OUTSIDE (RIGHT) SHOULDERS THAT ARE 3' OR WIDER SHOULD INCLUDE BICYCLE GAPS AS DETAILED. BICYCLE GAPS ARE NOT REQUIRED ON INSIDE (LEFT) SHOULDERS. BICYCLE GAPS SHALL NOT BE USED ON INTERSTATES AND PARKWAYS.
- RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS, OR WHEN THE SHOULDER WIDTH IS LESS THAN 1 FT.

DRAWING NOT TO SCALE

USE WITH CUR. STD. DWG.  
TPR-125

KENTUCKY  
DEPARTMENT OF HIGHWAYS

RUMBLE STRIP DETAILS  
MULTI-LANE ROADWAYS  
AND RAMPS

STANDARD DRAWING NO. TPR-130  
SUBMITTED BY *B. Allen Wolf* DATE 02-26-20  
DIRECTOR, DIVISION OF TRAFFIC OPERATIONS  
APPROVED BY *[Signature]* DATE 02-26-20  
STATE HIGHWAY ENGINEER

BID ITEMS AND UNIT TO BID  
SHOULDER RUMBLE STRIPS  
EDGELINE RUMBLE STRIPS

LF  
LF