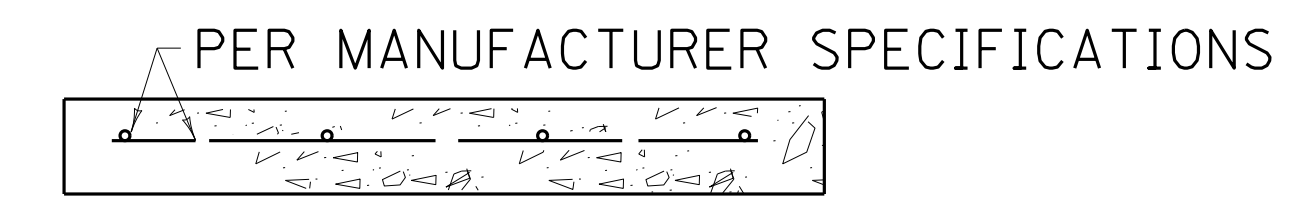


~ PICTORIAL VIEW ~



~ CONCRETE PAD SECTION ~
(PER MANUFACTURER SPECIFICATIONS)
REFER TO NOTES 11 15

~ NOTES ~

Additional Concrete Pad Construction may be Required Per Manufacturer Specifications

- ① NOSE ASSEMBLY (OBJECT MARKER TYPE 1 AS NECESSARY)
- ② CONSTRUCTION ZONE BACKUP
- 3. CRASH CUSHION TYPE VI, CLASS B, ☆, △
 - ☆ EITHER TEST LEVEL 2 (TL2) OR TEST LEVEL 3 (TL3), AS REQUIRED.
 - △ SEE "CONNECTION DETAILS OF CRASH CUSHION TYPE VI TO DOUBLE FACE GUARDRAIL".
- 4. CRASH CUSHION TYPE VI-BT IS DEPICTED ATTACHED TO A CONCRETE BARRIER (TEMPORARY).
- 5. WHEN CRASH CUSHION TYPE VI-BT IS ATTACHED TO STEEL "W" BEAM GUARDRAIL (DOUBLE FACE), ALL APPLICABLE DETAILS SHOWN ON CUR. SEPIA 018, "CONNECTION DETAIL OF CRASH CUSHION TYPE VI TO DOUBLE FACE GUARDRAIL" SHALL BE REQUIRED.
- 6. WHEN CRASH CUSHION TYPE VI-BT IS ATTACHED TO STEEL "W" BEAM GUARDRAIL (DOUBLE FACE), THE TRANSITION PANEL SHALL BE ELIMINATED.
- 7. IN A TWO-WAY TRAFFIC SITUATION FOR A 6" OR 9" TOP WIDTH WALL THE UNIT SHALL BE OFFSET FROM THE CENTERLINE OF THE WALL AS SHOWN IN THE PLAN VIEW. FOR A 12" TOP WIDTH WALL, THE UNIT SHALL BE CENTERED ON THE END OF THE BARRIER.
- 8. FOR ONE-WAY APPROACH TRAFFIC THE UNIT SHALL BE CENTERED ON THE END OF THE BARRIER.
- 9. THE COMPLETE INSTALLATION SHALL MEET ALL APPLICABLE REQUIREMENTS OF ENERGY ABSORPTIONS INC. OR TRINITY INDUSTRIES INC.
- 10. ANCHORAGE DEVICES TO SECURE THE CRASH CUSHION TO THE EXISTING SURFACE SHALL BE SHOWN ON APPROVED SHOP DRAWINGS.
- 11. THE CONCRETE PAD, PAD EXCAVATION AND STEEL REINFORCEMENT, INSTALLED IN PLACE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR CRASH CUSHION TYPE VI. DIMENSION AND REINFORCEMENT SPECIFICATIONS FOR CONCRETE PADS ARE TO BE PROVIDED BY THE MANUFACTURER. THE PAD WILL NOT BE REQUIRED WHEN UNIT IS CONSTRUCTED ON RIGID PAVEMENT.
- 12. THE PAD WILL NOT BE REQUIRED WHEN THE UNIT IS CONSTRUCTED ON EXISTING PAVEMENT OR BRIDGES AND THE COST OF ANCHORING SHALL BE INCLUDED IN THE UNIT PRICE OF THE CRASH CUSHION.
- 13. USE WITH CUR. SEPIA 018 WHEN CONNECTING TO DOUBLE FACE GUARDRAIL.
- 14. PERMISSIBLE ALTERNATES FOR CRASH CUSHION TYPE VI-BT ARE PATENTED ITEMS: ENERGY ABSORPTION SYSTEMS, INC. OF CHICAGO, IL., TRINITY INDUSTRIES, INC. OF DALLAS, TX.
- 15. CRASH CUSHIONS ARE TO BE INSTALLED PER MANUFACTURER SPECIFICATIONS, INCLUDING THE CONCRETE PAD. THE MANUFACTURER SHALL FURNISH TWO (2) SETS OF SHOP DRAWINGS TO THE CONTRACTOR WITH EACH INSTALLATION.
- 16. NCHRP-350-COMPLIANT CRASH CUSHIONS WILL BE ACCEPTED IN INSTANCES WHERE MASH-COMPLIANT DEVICES ARE NOT YET AVAILABLE.

CLASS	SPEED (MPH)	ATTENUATOR		
		MODEL	PRODUCT NAME	LENGTH
B	45 & LESS	TL2	SHORTRACC	14'-0"
			3-BAY QUADGUARD M10	12'-0"
	OVER 45	TL3	TRACC	21'-0"
			5-BAY QUADGUARD M10	18'-0"

A TYPE VI-CLASS C CAN BE USED AT THE CONTRACTOR'S DISCRETION.

USE WITH CUR. SEPIA 018 & SEPIA 020

KENTUCKY
DEPARTMENT OF HIGHWAYS

CRASH CUSHION
TYPE VI-BT

SUBMITTED  1-02-19
ACTING DIRECTOR DIVISION OF HIGHWAY DESIGN DATE