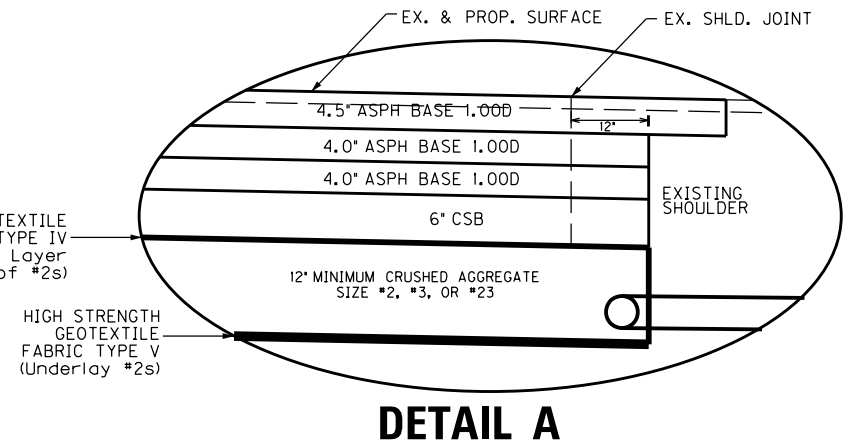
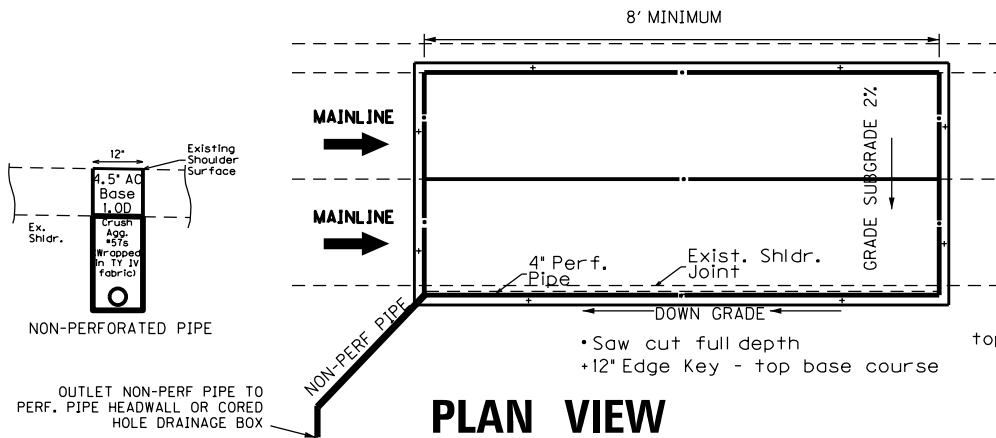
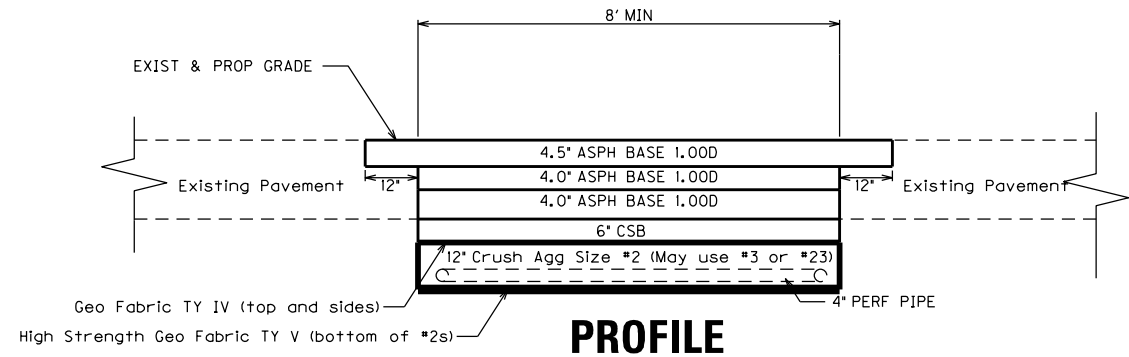
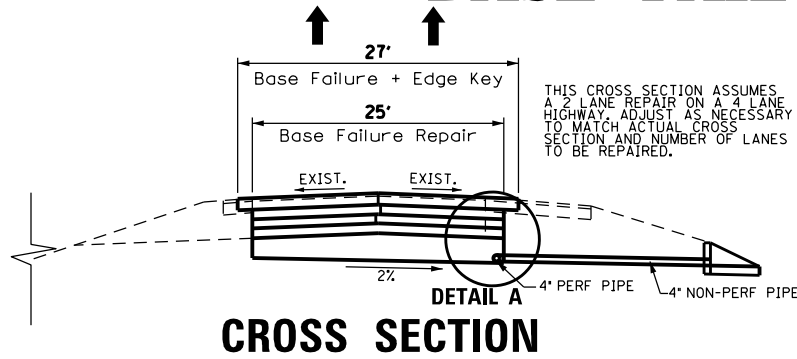


BASE FAILURE REPAIR DETAIL

Aug 6, 2019



NOTES

1. Caution: Existing concrete pavement may exist below the asphalt pavement.
2. Full depth base failure repairs shall be performed at locations selected by and as directed by the Engineer. The Engineer will assess, select, and mark areas for treatment. The full lane width will be removed and replaced. The Engineer may elect to perform repairs on one lane or multiple lanes. An edge key 12" into existing pavement is required for the top course of base. If only one lane is being repaired, carry top base course 12" into adjacent lane when MOT allows.
3. When replacing the outside lane, perforated pipe should be placed under the shoulder by extending the repair area 12 inches into the existing shoulder.
4. Select an appropriate outlet source which may include a perf pipe headwall or cored hole in an existing drop box inlet. Non-perf outlet pipe may require installation at significant length or adjusted alignment to provide positive drainage. Grade subgrade to the outlet side of the excavation at 2%, install a longitudinal perforated pipe on the low side of subgrade connecting to the down grade outlet.
5. Complete base failure operations in one continuous operation or protect with barrier wall. Do not leave an unprotected hole with no workers present. If barrier wall must be used for base failure repairs, it will be considered incidental to other items of work and not be considered for payment.
6. After completing base failure repair operations, open to traffic for a minimum of 14 days before resurfacing. Monitor pavement for settlement during this 14+ days and repair by leveling and wedging, as approved by the Engineer, until placement of final surface course.
7. The item REMOVE PAVEMENT includes removal of all asphalt and concrete pavement to the top of #2 Stone layer. The item ROADWAY EXCAVATION will include all excavation from the top of stone base layer to the bottom of excavation. Removal of the existing edge drain system (if present) and saw cutting will be considered incidental to other items of work.
8. Perforated pipe, non-perforated pipe, crushed aggregate #57s, cored hole drainage structures, and perforated pipe headwalls will not be measured for payment and will be considered incidental to other items of work.
9. Perform typical mill and inlay operations with resurfacing items subject to payment as part of the resurfacing operation.

* QUANTITIES TO BID

00003	CRUSHED STONE BASE	???	TONS
00078	CRUSHED AGGREGATE #2	???	TONS
02091	REMOVE PAVEMENT	???	SQ. YD.
02200	ROADWAY EXCAVATION	???	CU. YD.
02599	FABRIC-GEOTEXTILE TYPE IV	???	SQ. YD.
22861EN	HIGH STRENGTH GEO FAB TY V	???	SQ. YD.
XXXXX	ASPH BASE 1.00D	???	TONS

* Only items listed will be considered for payment and will be considered full compensation for the work required. Any other items of work not listed for payment will be considered incidental to other items of work.

Asph base course class and binder grade to be chosen by designer based on current asphalt warrants and/or to remain consistent with mainline asphalt surface used on the project.

NOT TO SCALE