



**CALL NO. 405**

**CONTRACT ID. 252326**

**LEWIS - CARTER COUNTIES**

**FED/STATE PROJECT NUMBER 121GR25P081 - FD04**

**DESCRIPTION CARTER & LEWIS COUNTIES (US 60, KY 8, & KY 57)**

**WORK TYPE SLIDE REPAIR**

**PRIMARY COMPLETION DATE 6/30/2026**

**LETTING DATE: August 21,2025**

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME August 21,2025. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

**NO PLANS ASSOCIATED WITH THIS PROJECT.**

**REQUIRED BID PROPOSAL GUARANTY:** Not less than 5% of the total bid.

TABLE OF CONTENTS

PART I	SCOPE OF WORK <ul style="list-style-type: none"><li>PROJECT(S), COMPLETION DATE(S), &amp; LIQUIDATED DAMAGES</li><li>CONTRACT NOTES</li><li>STATE CONTRACT NOTES</li><li>SURFACING AREAS</li><li>ASPHALT MIXTURE</li><li>INCIDENTAL SURFACING</li><li>COMPACTION OPTION B</li><li>SPECIAL NOTE(S) APPLICABLE TO PROJECT</li><li>WASTE AND BORROW SITES</li><li>ELECTRONIC DELIVERY MANAGEMENT SYSTEM (E-TICKETING) ASPHALT</li><li>ELECTRONIC DELIVERY MANAGEMENT SYSTEM (E-TICKETING) AGGREGATE</li><li>RECYCLED ASPHALT PAVEMENT (RAP) STOCKPILE MANAGEMENT</li><li>ASPHALT MIX PAVEMENT WEDGE MONOLITHIC OPERATION</li><li>GUARDRAIL</li><li>ASPHALT MILLING AND TEXTURING</li><li>BASE FAILURE REPAIR</li><li>TRAFFIC CONTROL PLAN SLIDE REPAIR</li><li>EROSION CONTROL</li><li>SKETCH MAP(S)</li><li>MATERIAL SUMMARY</li><li>SUMMARY SHEET(S)</li><li>TYPICAL SECTION(S)</li><li>DETAIL SHEETS FOR SLIDE REPAIR</li></ul>
PART II	SPECIFICATIONS AND STANDARD DRAWINGS <ul style="list-style-type: none"><li>STANDARD AND SUPPLEMENTAL SPECIFICATIONS</li><li>2020 STANDARD DRAWINGS THAT APPLY</li><li>PAVEMENT STRIPING DETAILS FOR TWO LANE TWO WAY ROADWAYS</li><li>STANDARD RAIL AND CRIBBING INSTALL</li><li>PAVEMENT STRIPING DETAILS FOR TWO LANE TWO WAY ROADWAYS</li><li>GUARDRAIL END TREATMENT TYPE 1</li></ul>
PART III	EMPLOYMENT, WAGE AND RECORD REQUIREMENTS <ul style="list-style-type: none"><li>LABOR AND WAGE REQUIREMENTS</li><li>EXECUTIVE BRANCH CODE OF ETHICS</li><li>KENTUCKY EQUAL EMPLOYMENT OPPORTUNITY ACT OF 1978 LOCALITY / STATE</li><li>PROJECT WAGE RATES / STATE FUNDED</li></ul>
PART IV	BID ITEMS

# **PART I**

## **SCOPE OF WORK**

ADMINISTRATIVE DISTRICT - 09

CONTRACT ID - 252326  
121GR25P081 - FD04  
COUNTY - CARTER  
PCN - MP02200602501  
FD04 022 0060 000-001

MOREHEAD TO OLIVE HILL ROAD (US 60) (MP 0.8) BEGIN 676 FEET WEST OF DOUGLAS HOLLOW ROAD  
EXTENDING EAST TO 148 FEET WEST OF DOUGLAS HOLLOW ROAD (MP 0.9), A DISTANCE OF 0.10  
MILES.SLIDE REPAIR SYP NO. 99-511.04.  
GEOGRAPHIC COORDINATES LATITUDE 38:16:35.43 LONGITUDE 83:18:25.07  
ADT 2,589

COUNTY - LEWIS  
PCN - MP06800082501  
FD04 068 0008 000-002

CONCORD TO VANCEBURG ROAD (KY 8) (MP 0.980) BEGIN 0.624 MILES WEST OF CHANEY LANE EXTENDING  
EAST TO 0.164 MILES WEST OF CHANEY LANE (MP 1.460), A DISTANCE OF 0.48 MILES.SLIDE REPAIR SYP NO.  
99-511.04.  
GEOGRAPHIC COORDINATES LATITUDE 38:40:30.18 LONGITUDE 83:28:38.91  
ADT 89

PCN - MP06800572501  
FD04 068 0057 010-011

TOLLESBORO TO CONCORD ROAD (KY 57) (MP 10.720) BEGIN 0.267 MILES SOUTH OF QUICKS RUN ROAD  
EXTENDING NORTH TO 0.107 MILES SOUTH OF QUICKS RUN ROAD (MP 10.880), A DISTANCE OF 0.16  
MILES.SLIDE REPAIR SYP NO. 99-511.04.  
GEOGRAPHIC COORDINATES LATITUDE 38:37:12.39 LONGITUDE 83:31:09.93  
ADT 131

PCN - MP06800572502  
FD05 068 0057 013-015

TOLLESBORO TO CONCORD ROAD (KY 57) (MP 13.90) BEGIN 496 FEET NORTH OF PINE GROVE CHURCH  
ROAD EXTENDING NORTH TO 0.264 MILES NORTH OF PINE GROVE CHURCH ROAD (MP 14.2), A DISTANCE OF  
0.30 MILES.SLIDE REPAIR SYP NO. 99-511.04.  
GEOGRAPHIC COORDINATES LATITUDE 38:39:43.72 LONGITUDE 83:31:05.17  
ADT 131

COMPLETION DATE(S):  
COMPLETED BY 06/30/2026                      APPLIES TO ENTIRE CONTRACT

## **CONTRACT NOTES**

### **INSURANCE**

Refer to Kentucky Standard Specifications for Road and Bridge Construction, current edition.

### **PROPOSAL ADDENDA**

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

### **BID SUBMITTAL**

Bidder must use the Department's electronic bidding software. The Bidder must download the bid file located on the Bid Express website ([www.bidx.com](http://www.bidx.com)) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

### **JOINT VENTURE BIDDING**

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

### **UNDERGROUND FACILITY DAMAGE PROTECTION**

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. When prescribed in said directives, the contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom shall be contacted through their individual Protection Notification Center. Non-compliance with these directives can result in the enforcement of penalties.

### **REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY**

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth ("certificate") from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the

foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

**For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity's solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.**

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

### **SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT**

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by email to [kytc.projectquestions@ky.gov](mailto:kytc.projectquestions@ky.gov). The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading "Questions & Answers" on the Construction Procurement website ([www.transportation.ky.gov/construction-procurement](http://www.transportation.ky.gov/construction-procurement)). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

### **HARDWOOD REMOVAL RESTRICTIONS**

The US Department of Agriculture has imposed a quarantine in Kentucky and several surrounding states, to prevent the spread of an invasive insect, the emerald ash borer. Hardwood cut in conjunction with the project may not be removed from the state. Chipping or burning on site is the preferred method of disposal.

### **INSTRUCTIONS FOR EXCESS MATERIAL SITES AND BORROW SITES**

Identification of excess material sites and borrow sites shall be the responsibility of the Contractor. The Contractor shall be responsible for compliance with all applicable state and federal laws and may wish to consult with the US Fish and Wildlife Service to seek protection under Section 10 of the Endangered Species Act for these activities.

### **ACCESS TO RECORDS**

The state agency certifies that it is in compliance with the provisions of KRS 45A.150, "Access to contractor's books, documents, papers, records, or other evidence directly pertinent to the contract." The Contractor, as defined in KRS 45A.030, agrees that the contracting agency, the

Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this agreement for the purpose of financial audit or program review. The Contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884. Records and other prequalification information confidentially disclosed as part of the bid process shall not be deemed as directly pertinent to the agreement and shall be exempt from disclosure as provided in KRS 61.878(1)(c).

### **BOYCOTT PROVISIONS**

If applicable, the contractor represents that, pursuant to [KRS 45A.607](#), they are not currently engaged in, and will not for the duration of the contract engage in, the boycott of a person or an entity based in or doing business with a jurisdiction with which Kentucky can enjoy open trade. **Note:** The term Boycott does not include actions taken for bona fide business or economic reasons, or actions specifically required by federal or state law.

If applicable, the contractor verifies that, pursuant to KRS 41.480, they do not engage in, and will not for the duration of the contract engage in, in energy company boycotts as defined by KRS 41.472.

### **LOBBYING PROHIBITIONS**

The contractor represents that they, and any subcontractor performing work under the contract, have not violated the agency restrictions contained in [KRS 11A.236](#) during the previous ten (10) years, and pledges to abide by the restrictions set forth in such statute for the duration of the contract awarded.

The contractor further represents that, pursuant to [KRS 45A.328](#), they have not procured an original, subsequent, or similar contract while employing an executive agency lobbyist who was convicted of a crime related to the original, subsequent, or similar contract within five (5) years of the conviction of the lobbyist.

Revised: 1/1/2025

## 1.0 BUY AMERICA REQUIREMENT.

Follow the “Buy America” provisions as required by 23 U.S.C. § 313 and 23 C.F.R. § 635.410. Except as expressly provided herein all manufacturing processes of steel or iron materials including but not limited to structural steel, guardrail materials, corrugated steel, culvert pipe, structural plate, prestressing strands, and steel reinforcing bars shall occur in the United States of America, including the application of:

- Coating,
- Galvanizing,
- Painting, and
- Other coating that protects or enhances the value of steel or iron products.

The following are exempt, unless processed or refined to include substantial amounts of steel or iron material, and may be used regardless of source in the domestic manufacturing process for steel or iron material:

- Pig iron,
- Processed, pelletized, and reduced iron ore material, or
- Processed alloys.

The Contractor shall submit a certification stating that all manufacturing processes involved with the production of steel or iron materials occurred in the United States.

Produce, mill, fabricate, and manufacture in the United States of America all aluminum components of bridges, tunnels, and large sign support systems, for which either shop fabrication, shop inspection, or certified mill test reports are required as the basis of acceptance by the Department.

Use foreign materials only under the following conditions:

- 1) When the materials are not permanently incorporated into the project; or
- 2) When the delivered cost of such materials used does not exceed 0.1 percent of the total Contract amount or \$2,500.00, whichever is greater.

The Contractor shall submit to the Engineer the origin and value of any foreign material used.

## 2.0 – BUILD AMERICA, BUY AMERICA (BABA)

Contractor shall comply with the Federal Highway Administration (FHWA) Buy America Requirement in 23 C.F.R. § 635.410 and all relevant provisions of the Build America, Buy America Act (BABA), contained within the Infrastructure Investment and Jobs Act, Pub. L. No. 117-58, §§ 70901-52 enacted November 15, 2021. The BABA requires iron, steel, manufactured products, and construction materials used in infrastructure projects funded by federal financial assistance to be produced in the United States. Comply with 2 C.F.R § 184.

BABA permits FHWA participation in the Contract only if domestic steel and iron will be used on the Project. To be considered domestic, all steel and iron used, and all products manufactured from steel and iron must be produced in the United States and all manufacturing processes, including application of a coating, for these materials must occur in the United States. Coating includes all processes that protect or enhance the value of the material to which the coating is applied. This requirement does not preclude a minimal use of foreign steel and iron materials, provided the cost of such materials does not exceed 0.1% of the total contract amount under the Contract or \$2,500.00 whichever is greater.

BABA permits FHWA participation in the Contract only if all “construction materials” as defined in the Act are made in the United States. The Buy America preference applies to the following construction materials incorporated into infrastructure projects: non-ferrous metals; plastic and polymer-based products (including polyvinylchloride, composite building materials, and polymers used in fiber optic cables); glass (including optic glass); Fiber optic cable; optical fiber; lumber; engineered wood; and drywall. Contractor will be



required to use construction materials produced in the United States on this Project. The Contractor shall submit a certification stating that all construction materials are certified to be BABA compliant.

### **3.0 FINAL RULE – FHWA’S BUY AMERICA REGULATION TO TERMINATE GENERAL APPLICABILITY WAIVER FOR MANUFACTURED PRODUCTS**

- **March 17, 2025** (effective date): For all Federal-aid projects obligated on or after March 15, 2025, all iron or steel products, as defined in § 635.410(c)(1)(iii), must comply with FHWA’s Buy America requirements for steel and iron in § 635.410(b). In addition, for all Federal-aid projects obligated on or after March 15, 2025, per § 635.410(c)(2), articles, materials, and supplies should be classified as an iron or steel product, a manufactured product, or another product as specified by law or in 2 CFR part 184 (such other products specified by law or in 2 CFR part 184 include “excluded materials” and “construction materials”); an article, material, or supply must not be considered to fall into multiple categories.
- **October 1, 2025:** The final assembly requirement will become effective for Federal-aid projects obligated on or after October 1, 2025. This means that, for manufactured product to be Buy America compliant, for Federal-aid projects obligated on or after October 1, 2025, final assembly of the manufactured product must occur in the United States.
- **October 1, 2026:** The 55 percent requirement will become effective for Federal-aid projects obligated on or after October 1, 2026. This means that, for manufactured product to be Buy America-compliant, for Federal-aid projects obligated on or after October 1, 2026, all manufactured products permanently incorporated into the project must both be manufactured in the United States (satisfy the final assembly requirement) and have the cost of the components of the manufactured product that are mined, produced, or manufactured in the United States be greater than 55 percent of the total cost of all components of the manufactured product (satisfy the 55 percent requirement).

### **4.0 – ADDITIONAL REQUIREMENTS**

The Contractor has completed and submitted, or shall complete and submit, to the Cabinet a Buy America/Build America, Buy America Certificate prior to the Cabinet issuing the notice to proceed, in the format below. After submittal, the Contractor is bound by its original certification.

A false certification is a criminal act in violation of 18 U.S.C. § 1001. The Contractor has the burden of proof to establish that it’s in compliance.

At the Contractor’s request, the Cabinet may, but is not obligated to, seek a waiver of Buy America requirements if grounds for the waiver exist under 23 C.F.R. § 635.410(c) or will comply with the applicable Buy America requirements if a waiver of those requirements is not available or not pursued by the Cabinet.

Please refer to the Federal Highway Administration’s Buy America webpage for more information.

[Buy America - Construction Program Guide - Contract Administration - Construction - Federal Highway Administration \(dot.gov\)](#)

Effective - June 26, 2025, Letting

**BUY AMERICA / BUILD AMERICA, BUY AMERICA (ACT) MATERIALS CERTIFICATE OF COMPLIANCE**

The Contractor hereby certifies that it will comply with all relevant provisions of the Build America, Buy America Act, contained within the Infrastructure Investment and Jobs Act, Pub. L. NO. 117-58, §§ 70901-52, the requirements of 23 U.S.C. § 313, 23 C.F.R. § 635.410 and 2 C.F.R § 184.

Date Submitted:\_\_\_\_\_

Contractor:\_\_\_\_\_

Signature:\_\_\_\_\_

Printed Name:\_\_\_\_\_

Title:\_\_\_\_\_

**NOTE: THIS CERTIFICATION IS IN ADDITION TO ANY AND ALL REQUIREMENTS OUTLINED IN THE CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND/OR SPECIAL NOTES CONTAINED IN THE PROJECT PROPOSAL.**

## **SPECIAL NOTE FOR RECIPROCAL PREFERENCE**

### **RECIPROCAL PREFERENCE TO BE GIVEN BY PUBLIC AGENCIES TO RESIDENT BIDDERS**

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the electronic bidding software. Submittal of the Affidavit should be done along the bid in Bid Express.

April 30, 2018

### **US 60 SURFACING AREAS**

The Department estimates the mainline surfacing width to be 20 feet.

The Department estimates the total mainline area to be surfaced to be 1,420 square yards.

The Department estimates the shoulder width to be 1 foot on each side.

The Department estimates the total shoulder area to be surfaced to be 120 square yards.

### **KY 8 SURFACING AREAS**

Surfacing to be done with separate project.

### **KY 57 MP 10.720 – 10.880 SURFACING AREAS**

The Department estimates the mainline surfacing width to be 18 feet.

The Department estimates the total mainline area to be surfaced to be 1,900 square yards.

The Department estimates the shoulder width to be 1 foot on each side.

The Department estimates the total shoulder area to be surfaced to be 190 square yards.

### **KY 57 (MP 13.900 – 14.200 SURFACING AREAS**

The Department estimates the mainline surfacing width to be 18 feet.

The Department estimates the total mainline area to be surfaced to be 2,350 square yards.

The Department estimates the shoulder width to be 1 foot on each side.

The Department estimates the total shoulder area to be surfaced to be 235 square yards.

### **ASPHALT MIXTURE**

Unless otherwise noted, the Department estimates the rate of application for all asphalt mixtures to be 110 lbs/sy per inch of depth.

### **INCIDENTAL SURFACING**

The Department has included in the quantities of asphalt mixtures established in the proposal estimated quantities required for resurfacing or surfacing mailbox turnouts, farm field entrances, residential and commercial entrances, curve widening, ramp gores and tapers, and road and street approaches, as applicable. Pave these areas to the limits as shown on Standard Drawing RPM-110-06 or as directed by the Engineer. In the event signal detectors are present in the intersecting streets or roads, pave the crossroads to the right of way limit or back of the signal detector, whichever is the farthest back of the mainline. Surface or resurface these areas as directed by the Engineer. The Department will not measure placing and compacting for separate payment but shall be incidental to the Contract unit price for the asphalt mixtures.

### **OPTION B**

Be advised that the Department will control and accept compaction of asphalt mixtures furnished on this project under OPTION B in accordance with Sections 402 and 403.

## **SPECIAL NOTES**

### **SLIDE REPAIR PROJECT**

#### **I. DESCRIPTION**

This work shall be performed in accordance with the Department's Current Standard Specifications and applicable Special Provisions except as hereafter specified. Article references are to the Standard Specifications.

This work shall consist of: (1) Clear and grub and perform necessary excavation; (2) Furnish and install railroad rails; (3) Furnish and install wall cribbing; (4) Excavate, place geotextile material, and backfill the area around the railroad rails and on the fill slope; (5) Reconstruct shoulder area; (6) Asphalt patching; (7) Install guardrail; (8) Maintain and control traffic; and (9) any other work as specified by this contract.

#### **II. MATERIALS**

All materials shall be sampled and tested in accordance with the Department's Sampling Manual and the materials shall be available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing unless otherwise specified in these Notes.

**A. Railroad Rails.** Use recycled (used) railroad rails classified with a nominal weight of 130 lb./yd (pounds per yard) size or greater. Use only visibly straight recycled railroad rails with no splices. The Engineer will verify rail nominal weights (Manufacturer's Stamp with lb./yd, date, etc.). Provide Certification for nominal weight if the Manufacturer's Stamp is unidentifiable.

**B. Wall Cribbing.** Use wall cribbing: recycled (used) steel "W" beam guardrail. Cribbing material will be furnished by the Contractor.

**C. Backfill material for Drilled Sockets.** Use the following for backfill material for Drilled sockets: concrete; free flowing sand; pea gravel, crushed limestone, or crushed sandstone. Use backfill material with one hundred percent (100%) passing a one-half (1/2) inch sieve. Do not use auger tailings. Engineer will use visual inspection and/or material testing, as applicable to determine acceptability.

**D. Fill Material for Cribbing.** Use Kentucky Aggregate Gradation No. 2's for backfill. Backfill material shall meet requirements of Section 805. The Engineer will use visual inspection and/or material testing, as applicable to determine acceptability.

**E. DGA.** Furnish Dense Graded Aggregate as per Section 805. Do not use Crushed Stone Base.

NOTES  
SLIDE REPAIR PROJECT  
Page 2

**F. Geotextile Fabric.** Furnish Geotextile Fabric Class 1 as per Section 843.

**G. Asphalt.** Furnish CL2 Asph Base 1.0D PG 64-22 for excavated areas and CL2 Asph Surf 0.38D PG 64-22 to overlay slide area as per Section 401.

**III. CONSTRUCTION METHODS**

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Staking.** Establish proper slope elevations and ratios, shoulder widths, existing ditch profile and final ditch profile to insure positive drainage. Be responsible for field layout. Positive drainage is required upon completion of the project and is the responsibility of the Contractor.

**C. Site Preparation.** Prepare slide repair sites. This includes clearing and grubbing, if necessary. Remove all obstructions. Sweep and remove debris, if necessary. The area to be cleared has not been measured by the Department and the bidder must draw his own conclusions. The Engineer shall approve all site preparation. The Department will not make direct payment for site preparation.

**D. Installation of Railroad Rails.** See attached summary for site locations and estimated quantities of materials required. Contrary to the attached tables and drawings for drilled railroad rails, install only 1 Row of RR Rails on 3 foot centers unless otherwise shown on the summary or mentioned in these notes.

NOTE TO ENGINEER AND CONTRACTOR: ABSOLUTELY NO CHANGE IN SCOPE OF WORK OR INCREASE IN QUANTITIES WILL BE ALLOWED ON THIS PROJECT WITHOUT PRIOR WRITTEN APPROVAL FROM THE TEBM (Transportation Engineering Branch Manager) FOR OPERATIONS OR HIS REPRESENTATIVE IN THE DISTRICT OFFICE.

THE DEPARTMENT SHALL NOT BE LIABLE FOR PAYMENTS DUE TO ADDITIONAL WORK THAT HAS NOT BEEN AUTHORIZED BY THE AFOREMENTIONED PERSONS.

Install used railroad rail piling in drilled sockets in rock or stable material under the landslides as project location dictates or as directed by the Engineer.

NOTES  
SLIDE REPAIR PROJECT  
Page 3

Drill the socket, furnish, and install the railroad rails into holes at slide locations. If the Engineer determines from the sounding obtained at a drilled socket that railroad rail piling cannot be used in that socket, the depth of the socket shall be measured and 50% of the depth shall be paid as "Railroad Rail-Drilled". Drill sockets into solid rock, if possible. The Department will monitor each hole, which will serve as a sounding for the rail to be installed in it. Embed the railroad rail into solid rock no less than one-half the free end length of the rail. (See figure 1 and figure 2). If solid rock cannot be obtained, the Engineer will determine the length of embedment required in other stable foundation. Allow adequate size of the drilled socket to allow free insertion of the railroad rail, but the maximum socket size is 1 foot in diameter.

After each hole is drilled, install railroad rail immediately with the flanges positioned perpendicular to the direction of the landslide or break (see figure 3). Determine the height of rail that is needed to reestablish pavement and shoulder typical section. Cut off excess rail flush with the proposed ground line that is not needed. Use cutoffs elsewhere in the project if possible; unusable cutoffs remain the property of the Contractor.

After railroad rail is installed, immediately backfill the drilled hole with the approved materials. Shovel the backfill material into the hole in small amounts. Avoid bridging between the rail and the sides of the hole. Do not use Auger tailings as backfill material.

When double or triple rows are required, stagger the rows to obtain the required spacing. Keep the spacing between the rows of rails as close as is practical; do not space between the rows of more than 2 feet, if possible. Select the spacing as per Table 1 for all 130 pound per yard rail or greater. The Department shall approve the selection prior to work being performed.

Crib any exposed portion of railroad rail before placing backfill.

**E. Excavation and Backfill for Embankment Repair.** Excavate each embankment/flood repair area to provide a platform for drilling the used railroad rails, if necessary. Excavate for roadway ditches as necessary for slope, shoulder and pavement drainage. Place geotextile fabric, then construct embankment behind railroad rails, cribbing and on slope, as per Section 206. Construct embankment up to the approximate existing pavement elevation.

Reconstruct the shoulder area with DGA up to the approximate existing elevation and width of the surrounding typical section or to a minimum width of 2 Feet at each slide

NOTES  
SLIDE REPAIR PROJECT  
Page 4

location. Do not pond water on the shoulder area or at the shoulder edge. Reconstruct the shoulder before installing guardrail.

**DO NOT USE EXCAVATED MATERIAL FROM THE SITE AS FILL MATERIAL.**

Excess excavation may be wasted at sites on the right-of-way, ONLY if approved by the Engineer. Material may NOT be wasted in flood prone areas or in streams.

If the Engineer deems no suitable sites are available within the right-of-way, the Contractor will be required to waste excess material off the right-of-way at sites obtained by the Contractor at no cost to the Department.

**F. Installation of Wall Cribbing.** Install Cribbing as shown on Figure 1 or Figure 2 as slide location dictates or as directed by the Engineer. Extend wall cribbing 2 feet below the existing ground line. If bedded rock is encountered, install the cribbing to the bedded rock only. If necessary, the Engineer will direct changes to this procedure. Furnish all labor and equipment to deliver and install wall cribbing on the recycled (used) railroad rail piling. Wall cribbing shall be lapped, bolted, and attached solid to the drilled railroad rails.

**G. On-Site Inspection.** Each Contractor submitting a bid for this work shall make a thorough inspection of the site prior to submitting his bid and shall thoroughly familiarize themselves with the existing conditions so that the work can be expeditiously performed after a contract is awarded. Submission of a bid will be considered evidence of this inspection having been made.

**H. Right-of-Way Limits.** Right-of-Way and easement limits shown on the plans are approximate only. The Contractor shall make every effort to limit his activities to obvious right-of-way and permanent or temporary easements and shall be responsible for encroachments onto private lands.

**I. Property Damage.** The Contractor will be responsible for all damage to public and/or private property resulting from his work.

**J. Utility Clearance** *NOTICE: Utility locations are not shown on plans or in the proposal for this project and have not been located by the Department. Locate all underground, above ground and overhead utilities prior to beginning construction. Be responsible for contacting and maintaining liaison with all utility companies that have utilities located within the project limits. Notify the Engineer and the utility owner(s) immediately when it is discovered or anticipated that any utility conflict could delay the Contractor's operations. Do not disturb existing overhead or underground utilities. Be responsible for repairing all utility damage that occurs because of the work* Working



NOTES  
SLIDE REPAIR PROJECT  
Page 5

*days will not be charged for those days on which work on the controlling item is delayed, as provided in the Specifications. If the total delay exceeds ten working days, an extension of the specified completion date will be negotiated with the Contractor for delay to the Contractor's work; however, no extension will be granted for any delay caused by the Contractor's failure to notify the Engineer and/or the utility company as specified above when a conflict is discovered or anticipated as specified. Comply with applicable sections of Chapter 107.*

**K. Asphalt Patching.** Except as specified herein, asphalt according to Section 403 and the Standard Drawings. Place 6" of CL2 Asph Base 1.0D PG 64-22 in excavated areas to reestablish roadway. Level and wedge as needed using CL2 Asph Surf 0.38D PG 64-22. Overlay entire roadway at the slide area with 1.5" of CL2 Asph Surf 0.38D PG 64-22 or as directed by the Engineer.

**L. Guardrail.** Except as specified herein, construct guardrail system according to Section 719 and the Standard Drawings. Locations listed on the summary and/or shown on the drawings are approximate only. The Engineer will determine the exact termini for individual guardrail installations at the time of construction. Unless directed otherwise by the Engineer, provide a minimum two (2) foot shoulder width. Construct radii at entrances and road intersections as directed by the Engineer.

Erect guardrail to the lines and grades shown on current Standard Drawings or as directed by the Engineer by any method approved by the Engineer which allows construction of the guardrail to the true grade without apparent sags.

When installing guardrail, do not leave the blunt end exposed where it would be hazardous to the public. When it is not practical to complete the construction of the guardrail and the permanent end treatments and terminal sections first, provide a temporary end by connecting at least 25 feet of rail to the last post, and by slightly flaring, and burying the end of the rail completely into the existing shoulder. If left overnight, place a drum with bridge panel in advance of the guardrail end and maintain during use.

#### IV. METHOD OF MEASUREMENT

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Railroad Rail-Drilled.** The Department will measure this item in Linear Feet finished in-place length: Laps, cutoffs, excess and waste will not be measured for payment. If the Engineer determines from the sounding obtained at a drilled socket that

NOTES  
SLIDE REPAIR PROJECT  
Page 6

railroad rail piling cannot be used in that socket, the depth of the socket shall be measured and 50% of the depth shall be paid as "Railroad Rail-Drilled".

**C. Wall Cribbing** The Department will measure this item in square feet finished in-place area. Laps, cutoffs, excess and waste will not be measured for payment.

**D. Excavation and Backfill.** The Department will measure "Roadway Excavation" in cubic yards. The Department will measure the quantity in the field as per Section 204 (Roadway Excavation) or other accepted methods of measurement as directed by the Engineer. The following items will not be measured directly by the Department: Site Preparation, Clearing and Grubbing, Seeding and Protection, Final Dressing, Temporary Erosion Control, Temporary Pollution Control, Waste removal, and Disposal, but will be incidental to "Roadway Excavation" as applicable to each project. The Department will measure "Crushed Aggregate Size No. 2" in tons for backfill.

**E. Site Preparation, Clearing and Grubbing, Temporary Pollution Control, Waste Disposal.** The Department will NOT MEASURE for payment these items. They include Site Preparation, Clearing and Grubbing, Temporary Pollution Control, Waste Disposal, and other items not expressly bid as individual items. These items shall be incidental to the bid item "Roadway Excavation" as applicable to each project.

**F. Asphalt Patching.** See Section 403.04.

**G. Guardrail.** See Section 719.04.

**V. BASIS OF PAYMENT**

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Railroad Rail-Drilled.** The Department will pay for the completed and accepted quantities under the bid item of "Railroad Rail-Drilled". The Department will consider payment full compensation for all work required in these notes and elsewhere in the Contract.

**C. Excavation and Backfill.** The Department will pay for the completed and accepted quantities under the bid items: "Roadway Excavation" and "Crushed Aggregate Size No. 2". Payment for "Roadway Excavation" will be based on quantity measured in the field. The following items are incidental to "Roadway Excavation" and will not be measured directly by the Department. These items include Site Preparation, Clearing and Grubbing, Seeding and Protection, Final Dressing, Temporary Erosion Control, Temporary Pollution Control, Waste removal and Disposal. Payment for "Crushed

NOTES  
SLIDE REPAIR PROJECT  
Page 7

Aggregate Size No. 2” will be based on quantity in tons weighed according to Section 109. The Department will consider payment full compensation for all work required on the project.

**D. Wall Cribbing.** The Department will pay for the completed and accepted quantities under the bid item of “Wall Cribbing”. Payment will be based on the quantity installed in the field. The Department will not make separate payment for the hauling of the wall cribbing to the slide site. The Department will consider payment full compensation for all work required on the project.

**E. Site Preparation, Clearing and Grubbing, Temporary Pollution Control, Waste Disposal.** The Department will NOT make direct payment for operations for which bid items do not exist. They include items listed here: Site Preparation, Clearing and Grubbing, Temporary Pollution Control, Waste removal and Disposal. These activities shall be incidental to the bid item “Roadway Excavation” as applicable to each project.

**F. Asphalt Patching.** See Section 403.05.

**G. Guardrail.** See Section 719.05.

## **SPECIAL PROVISION FOR WASTE AND BORROW SITES**

Obtain U.S. Army Corps of Engineer's approval before utilizing a waste or borrow site that involves "Waters of the United States". The Corps of Engineers defines "Waters of the United States" as perennial or intermittent streams, ponds or wetlands. The Corps of Engineers also considers ephemeral streams, typically dry except during rainfall but having a defined drainage channel, to be jurisdictional waters. Direct questions concerning any potential impacts to "Waters of the United States" to the attention of the appropriate District Office for the Corps of Engineers for a determination prior to disturbance. Be responsible for any fees associated with obtaining approval for waste and borrow sites from the U.S. Army Corps of Engineer or other appropriate regulatory agencies.

1-296 Waste & Borrow Sites  
01/02/2012

## **SPECIAL NOTE FOR ELECTRONIC DELIVERY MANAGEMENT SYSTEM (e-Ticketing) ASPHALT**

This Special Note will apply when indicated on the plans or in the proposal. Section references herein are to the Department's Standard Specifications for Road and Bridge Construction current edition.

**1.0 DESCRIPTION.** Incorporate an e-Ticketing Delivery Software for weighed asphalt material delivered to the project to report loads and provide daily running totals of weighed asphalt material for pay items and incidental work during the construction processes from the point of measurement and loading to the point of incorporation to the project.

**2.0 MATERIALS AND EQUIPMENT.** Contractor shall supply material data in JavaScript Object Notation (JSON) documents to the KYTC e-Ticketing Delivery Software (KYTC e-Ticketing Portal) via Application Programming Interface (API) or direct connection. Test and verify that ticket data can be shared from the original source no fewer than 30 days prior to material placement activities. An e-Ticketing Delivery Software supplier can provide a qualified representative for on-site technical assistance during the initial setup, pre-construction verifications, and data management and processing as needed during the Project to maintain material data delivery capabilities. Virtual meetings may be hosted in lieu of on-site meetings when deemed appropriate by the Engineer.

Provide e-Ticketing Delivery Software that will meet the following:

1. The e-Ticketing Delivery Software shall be fully integrated with the Contractor's Load Read-Out scale system at the material source location.
2. The e-Ticketing Delivery Software shall provide real-time delivery to KYTC e-Ticketing Portal.
3. Transmit any updates to the ticket data within 5 minutes of a change.

**3.0 CONSTRUCTION.** Provide the Engineer with the manufacturer's specifications and all required documentation for data access at the pre-construction conference.

### **A. Construction Requirements**

1. Install and operate software in accordance with the manufacturer's specifications.
2. Verify that all pertinent information is provided by the software within the requirements of this Special Note.

### **B. Data Deliverables**

Provide to the Engineer a means in which to gather report summaries by way of iOS apps, web pages, or any other method at the disposal of the Engineer. The Engineer may request data at any time during the project.

#### **1. Asphalt Material**

##### **a. Real-time Continuous Data Items**

Provide the Engineer access to JSON documents capable of being transmitted through the KYTC's e-Ticketing Portal that displays the following information in real-time with a web-based system compatible with iOS and Windows environments.

- Each Truck
  - Supplier Name
  - Supplier Address
  - Supplier Phone
  - Plant location
  - Date
  - Time at source
  - Project Location

- Contract ID#
- Carrier Name
- Unique Truck ID
- Description of Material
- Mix Design Number
- Gross, Tare and Net Weight
- Weighmaster

**4.0 MEASUREMENT.** The Department will not measure the electronic delivery management system.

**5.0 PAYMENT.** The Department will not measure this work for payment and will consider all items contained in this note to be incidental to the asphalt mixtures on the project, as applicable.

May 5, 2025

## **SPECIAL NOTE FOR ELECTRONIC DELIVERY MANAGEMENT SYSTEM (e-Ticketing) AGGREGATE**

This Special Note will apply when indicated on the plans or in the proposal. Section references herein are to the Department's Standard Specifications for Road and Bridge Construction current edition.

**1.0 DESCRIPTION.** Incorporate an e-Ticketing Delivery Software for weighed aggregate material delivered to the project to report loads and provide daily running totals of weighed aggregate material for pay items and incidental work during the construction processes from the point of measurement and loading to the point of incorporation to the project.

**2.0 MATERIALS AND EQUIPMENT.** Contractor shall supply material data in JavaScript Object Notation (JSON) documents to the KYTC e-Ticketing Delivery Software (KYTC e-Ticketing Portal) via Application Programming Interface (API) or direct connection. Test and verify that ticket data can be shared from the original source no fewer than 30 days prior to material placement activities. An e-Ticketing Delivery Software supplier can provide a qualified representative for on-site technical assistance during the initial setup, pre-construction verifications, and data management and processing as needed during the Project to maintain material data delivery capabilities. Virtual meetings may be hosted in lieu of on-site meetings when deemed appropriate by the Engineer.

Provide e-Ticketing Delivery Software that will meet the following:

1. The e-Ticketing Delivery Software shall be fully integrated with the Contractor's Load Read-Out scale system at the material source location.
2. The e-Ticketing Delivery Software shall provide real-time delivery to KYTC e-Ticketing Portal.
3. Transmit any updates to the ticket data within 5 minutes of a change.

**3.0 CONSTRUCTION.** Provide the Engineer with the manufacturer's specifications and all required documentation for data access at the pre-construction conference.

### **A. Construction Requirements**

1. Install and operate software in accordance with the manufacturer's specifications.
2. Verify that all pertinent information is provided by the software within the requirements of this Special Note.

### **B. Data Deliverables**

Provide to the Engineer a means in which to gather report summaries by way of iOS apps, web pages, or any other method at the disposal of the Engineer. The Engineer may request data at any time during the project.

#### **1. Aggregate Material**

##### **a. Real-time Continuous Data Items**

Provide the Engineer access to JSON documents capable of being transmitted through the KYTC's e-Ticketing Portal that displays the following information in real-time with a web-based system compatible with iOS and Windows environments.

- Each Truck
  - Supplier Name
  - Supplier Address
  - Supplier Phone
  - Plant location
  - Date
  - Time at source
  - Project Location

- Contract ID#
- Carrier Name
- Unique Truck ID
- Description of Material
- Load Number
- Gross, Tare and Net Weight
- Weighmaster

**4.0 MEASUREMENT.** The Department will measure the electronic delivery management system as a lump sum item.

**5.0 PAYMENT.** The Department will make payment for the completed and accepted quantities under the following:

1. Payment is full compensation for all work associated with providing all required equipment, training, and documentation.
2. Payment will be full compensation for costs related to providing the e-Ticketing Delivery Software, including integration with plant load-out systems, and report viewing/exporting process. All quality control procedures including the software representative’s technical support and on-site training shall be included in the Contract lump sum price.

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
26248EC	ELECTRONIC DELIVERY MGMT SYSTEM-AGG	LS

May 5, 2025



## **SPECIAL NOTE FOR RECYCLED ASPHALT PAVEMENT (RAP) STOCKPILE MANAGEMENT**

### **I. GENERAL**

The use of reclaimed asphalt pavement (RAP) from Department projects or other approved sources in hot mix asphalt (HMA) or warm mix asphalt (WMA) shall be subject to stockpile management and handling of material as described in this section.

The Department approves RAP on a stockpile basis, following the process set forth in this method. The contractor's responsibilities in the process are as follows:

- To obtain the Department's approval of all RAP prior to its use on a Department project and to deliver test data and samples as required
- To monitor and preserve the quality and uniformity of the approved material during storage and handling, adding no unapproved material to the existing stockpile
- To comply with the Department's requirements regarding replenishment of approved stockpiles

The Department will approve RAP based on its composition and variability in gradation and asphalt content, and on visual inspections of the stockpile, which the Department may conduct at its discretion. The Department may withdraw approval of a stockpile if the requirements of this specification are not followed in good faith.

The Maximum Percentage Allowed in a mix design will be based on these criteria and on the category of RAP source, as defined in this document.

### **II. APPROVAL PROCESS**

Qualified asphalt producers (listed in List of Approved Materials-Asphalt Mixing Plants) may submit requests for RAP stockpile approval to the Asphalt Branch, Division of Materials, in the Annual Certification for Previously Approved Asphalt Mixing Plants and Related Equipment. The requester shall provide test results as prescribed in Part IID. The Division of Materials may, at their discretion, collect samples or inspect a RAP stockpile consistent with Section IIE.

Upon completion of the review of testing results and, if applicable, visual inspection, the Division of Materials, Asphalt Branch will approve or disapprove the material by letter and will assign a Stockpile Identification Number for each approved RAP stockpile. Note: The contractor's average gradation and asphalt content, as listed in the approval letter, shall be the gradation used in subsequent mix designs. The approval letter will state the applicable limits on the use of the material in mix designs and will summarize the Department's findings, listing the average gradation and asphalt content from the contractor's tests and the corresponding values found by the Department. Where the Maximum Percentage Allowed is low due to variability, the contractor may elect to improve the uniformity of the material by further processing and may again sample, test, and request approval for the material.

No material shall be added to a stockpile after it has been approved, except as provided in Parts V, VI, and VII below.

#### **IIA. RAP Quality Management Plan**

For a contractor to receive approval to use RAP on any department project, a RAP Quality Management Plan must first be approved by the department. The RAP Quality Management Plan shall be submitted to the

Division of Materials annually for approval as part of the Contractor's Quality Control Plan/Checklist. The Quality Management Plan is required to demonstrate how the Contractor will provide consistency and quality of material utilized in all asphalt mixes produced for use on Department projects. The Quality Management Plan shall include:

- Unprocessed RAP Stockpiles
  - Designation of stockpile(s) as single or multiple source
  - Designation of stockpile(s) as classified or unclassified
  - Designation of stockpile(s) as captive or continuously replenishing
  - Plan for how stockpile(s) is built (layers, slope, etc.)
  - Plan to minimize stockpile(s) contamination
- Processing and Crushing
  - Equipment used to feed screener or crusher
  - Excavation process based on equipment type
- Processing Millings
  - Single Project or Source
    - Screening, Fractionation, or Crushing plan
  - Multiple Source
    - Process to achieve uniform material from stockpile
    - Screening, Fractionation, or Crushing plan
- Processed RAP Stockpiles
  - Minimization of segregation
  - Minimization of moisture

## **IIB. RAP Stockpile Placement**

All processed RAP stockpiles shall be placed on a sloped, paved surface. The requirement for a paved surface may be waived by the Cabinet if the Contractor's RAP Quality Management Plan demonstrates effective material handling that will minimize deleterious material from beneath the processed stockpile entering the plant. *No processed stockpile will be placed directly on grass or dirt.*

## **IIC. Stockpile Identification Signs**

RAP stockpiles shall be identified with posted signs displaying the gradation of material in the stockpile (course, intermediate, or fine). These signs shall be made of weatherproof material and shall be highly visible. Numerals shall be easily readable from outside the stockpile area. If a stockpile exists in two or more parts, each part must have its own sign.

## **IID. Standard Approval Procedure**

The Contractor shall obtain random samples representative of the entire stockpile and shall have each sample tested for gradation and asphalt content according to KM 64-426, KM 64-427, and AASHTO T308. The material samples must be in its final condition after all crushing and screening. At least one sample shall be obtained for each 1,000 tons of processed RAP, with a minimum of five samples per stockpile. Sampling shall be performed according to the method prescribed for asphalt mix aggregates in the Department's Materials Field Testing and Sampling Manual and KM 64-601. The minimum sampling size (after quartering) for tests of RAP samples is 1,500 g. except for samples containing particles more than one inch in diameter, for which the minimum is 2,000 g.

To request approval of a RAP stockpile, submit the following documents to the Division of Materials. It is the requester's responsibility to correctly address, label, and deliver these submittals:

- Submit request for approval at beginning of the paving season as part of the Annual Certification for Previously Approved Asphalt Mixing Plants and Related Equipment.
- If requesting approval after paving season begins, submit memo, including stockpile portion of the inspection list for Annual Certification for Previously Approved Asphalt Mixing Plants and Related Equipment, to Division of Materials.
- Reports of the tests prescribed above using the Stockpile <INSERT NAME> document.
- A drawing of the plant site showing the location of the stockpile to be approved *and all other stockpiles on the premises*

**Mail, deliver or email the request form**, with test reports and site drawing, to:

Kentucky Transportation Cabinet  
Division of Materials  
ATTN: Asphalt Branch Manager  
1227 Wilkinson Boulevard  
Frankfort, Kentucky 40601  
  
Robert.Semones@ky.gov

**III. Tests and inspections by the Department**

The Department shall have the right to observe the collection of samples, or to perform the sampling and testing as a verification of contractor submittal. As a condition of approval, the Department may at any time inspect and sample RAP stockpiles for which approval has been requested and may perform additional quality control tests to determine the consistency and quality of the material.

The approval letter issued by the Department will include any results of verification testing performed by the Cabinet. The approved contractor results should be used by mix design technicians in the design calculations.

**III. RAP STOCKPILE TIERED MANAGEMENT AND EFFECTIVE BINDER CONTENT**

The stockpile management and approval requirements will be tiered based on the maximum cold feed percentages as defined in this section and Table 1. below.

Table 1. Tiered Testing Requirements

Mix Type	0-≤12%	12-≤20%	20-≤35%
Surface	Tier 1	Tier 2	Tier 3
Base	Tier 1	Tier 2	Tier 3

**NOTE: All asphalt mixes and binder selection will be subject to Section 409 of the current Standard Specifications.**

The following requirements will apply based on the percentage of RAP in the mix.

Tier 1

Tier 1 mixes (less than or equal to 12% RAP) will be subject to the requirements of sections IIA, IIB, and IIC.

Tier 2

Tier 2 mixes (12% to less than 20% RAP) will be subject to the requirements of Section II in its entirety and Table 2 requirements.

Tier 3

Tier 3 Asphalt Base mixes with 20% to less than 35% RAP, Tier 3 Asphalt Surface mixes with 20% to less than 30% RAP will be subject to Section II in its entirety and Table 2 requirements.

**IV. MAXIMUM PERCENTAGE OF RAP ALLOWED**

The Maximum Percent of RAP allowed in mix designs shall be the lowest percentage determined by the gradation and asphalt content of the RAP, as established under the criteria below, and requirements listed in Section III.

Limits according to range in gradation and bitumen content

The Maximum Percent of RAP Allowed, based on gradation and asphalt content, shall be determined by the Department using the standard deviation of these values. This standard deviation will be calculated using data provided by the contractor from at least five samples. While the contractor is required to provide the data from these tested samples, the Department retains the discretion to perform its own sampling and testing to support or verify its findings. An apparent outlier shall not be considered in determining these ranges. Where one result appears to be unrepresentative of the whole, two or more additional samples shall be tested. The outlying value of all tests shall then be excluded from the range. The maximum percentage of RAP allowable shall be the lowest percentage determined according to Table 2 below.

**Table 2. Maximum Percent RAP According to Variability in Test Results**

	Standard Deviation as calculated above:		
Surface			
% asphalt content	< 0.4	< 0.5	
% passing No. 200 sieve	< 1.25	< 1.5	
% passing Median Sieve	< 4.0	< 5.0	
	Allowable RAP Cold Feed %		
	Tier 3 - 20%-30%	Tier 2 - 12%-20%	Tier 1 - 0%-12%
Base			
% asphalt content	< 0.5	< 0.75	
% passing No. 200 sieve	< 1.5	< 2.25	
% passing Median sieve	< 5.0	< 7.0	
	Allowable RAP Cold Feed %		
	Tier 3 - 20%-35%	Tier 2 - 12%-20%	Tier 1 - 0%-12%

**NOTE: These allowances notwithstanding, the Contractor is required to maintain the mixture within the Mixture Control Tolerances of Kentucky Method 443.**

The percentage allowable in mix designs shall be limited to meet the design criteria for viscosity established in the Standard Specifications.

## **V. GENERAL STOCKPILE REQUIREMENTS AND REPLENISHMENT**

### **V.A. Single Pavement Source**

**Early approval of material from a single pavement source.** When a new stockpile is to consist entirely of millings removed from a single existing pavement, the stockpile may be approved based on samples taken during the milling and processing operations, prior to completion of milling. The initial stockpile may be approved as either a new stockpile or a new stockpile in continual replenishment status.

For continual replenishment status, samples shall be taken from the processed stockpile after it reaches 1,000 tons. A total of five initial samples, plus one additional sample for every 1,000 tons, is required. As prescribed in Part II above, the contractor shall test all samples and deliver the test results, together with a letter request for approval in Continual Replenishment status, to the address indicated. The stockpile shall be subject to initial approval as prescribed above in Part II. Once approved, it may be replenished without further approvals as provided in Part VII below.

### **V.B. Heterogeneous or contaminated material**

Asphalt pavement millings containing traffic detection loops, raised pavement markers, or other debris must be separated and excluded before stockpiling RAP for approval for use in KYTC asphaltic concrete mixtures.

No material other than RAP from an approved stockpile shall be included in mixtures for State projects. The following materials are specifically excluded:

- Material contaminated with foreign matter such as liquids, soil, concrete, or debris
- Plant waste, especially waste containing abnormal concentrations of bitumen, drum build-up, or material from spills or plant clean-up operations

The following materials shall not be added to or placed in proximity to an approved stockpile but may be accumulated in a separate stockpile and submitted for approval according to Part III:

- Production mixtures returned to the plant for any reason.
- Mis-proportioned mixtures, especially those generated at start-up.

## **VI. REPLENISHMENT OF STOCKPILES**

An approved RAP stockpile may be replenished with Department approval, provided the replenishment material meets all necessary requirements for approval and maintains uniformity in gradation and asphalt content as outlined in this document.

### **VI.A. Procedure and approval criteria**

The procedure for requesting approval of a stockpile replenishment, that is not in continual replenishment status, shall be the same as for approval of an original stockpile, and the material for the replenishment shall meet all criteria for approval as a new stockpile. RAP proposed for replenishment shall be sampled and tested by the Contractor for gradation and asphalt cement as prescribed in Section II above. The Laboratory shall

review these results and provide approval for use in Department asphalt mix designs, according to Table 2 above.

**VI.B. Effect of replenishment on existing approved mix designs**

Replenishment of a stockpile may render certain mix designs invalid, depending on the percent RAP allowed in the design and on the difference in average properties between the old and new stockpiles. A replenished stockpile may be used as the RAP ingredient in an existing approved design provided that:

1. The Maximum Percent Allowed for the replenishment stockpile equals or exceeds the percent RAP called for in the mix design. In no case may the Maximum Percent Allowed be exceeded.

However, if a mix design calls for up to 5.0 percent more than the Maximum Percent Allowed for the replenishment, the *design* may be adjusted, with approval, to use the lower percent allowed, provided that the production mixture continues to meet all acceptance criteria. For example, a design which calls for 20 percent RAP may be adjusted and produced with 15 percent if it continues to meet for acceptance.

**VII. CONTINUAL REPLENISHMENT WITHOUT RE-APPROVAL**

At the request of the contractor, a previously approved stockpile may be placed in Continual Replenishment Status and may be replenished any number of times without re-approval provided that:

1. Replenishment is within six months of the last stockpile addition.
2. The contractor shall continue to monitor and test the materials added to the stockpile and shall forward these results to the Division of Materials for every 1,000 tons of RAP added to the stockpile.
3. The contractor must certify that replenishment materials are free of contaminants.
4. The Department shall be notified by letter to the Director of the Division of Materials that the stockpile is being replenished on a continual basis.
5. The RAP Maximum Percent Allowed for continual replenishment shall be limited by Sections III and IV.

**Note: Upon request, one 20-pound sample bag of RAP for each Continual Replenishment Stockpile shall be submitted to the Division of Materials for petrographic analysis every 12 months.**

The Department may inspect, sample, and test such stockpiles at its discretion and may, upon determining that the stockpile is unsuitable, withdraw approval of the material and all mix designs which include it. Approval of the stockpile may be withdrawn at any time based upon extreme or erratic ingredient proportions, unsuitable ingredients, or poor performance, as determined by the Division of Materials, Asphalt Branch. The Department will conduct periodic comparison testing on the opposite quarters of samples submitted by the Contractor for special replenishment approval category. The approval of the stockpile may be withdrawn if

erroneous information was found on the contractor's testing and/or improper sampling procedures were involved after a thorough investigation.

## **VIII. DEPLETION OF STOCKPILE AND EXPIRATION OF APPROVAL**

When a stockpile has been fully depleted, the Contractor may replenish it within 24 months after the date of depletion; a depleted stockpile not replenished after 24 months will be removed from the approved list and may not be replenished.

Approval of a stockpile may be withdrawn if, in the finding of the Division of Materials, Asphalt Branch, the total amount of material used in new mixtures equals the total tonnage of the original stockpile plus all approved replenishments. Six years from the original approval of a stockpile or from its most recent replenishment, a stockpile shall be presumed to be depleted, and its approval shall expire. This shall apply to all stockpiles, regardless of status or history of use.

## **IX. RECORDS**

The Contractor shall maintain records at the plant site on all RAP stockpiles. These records shall be available for inspection by representatives of the Department and shall include the following:

- All test results.
- The Department's approval letter for each stockpile and replenishment, together with the Contractor's requests for approval and all data submitted therewith.
- A current drawing of all stockpile locations at the plant site, including unapproved stockpiles, showing stockpile numbers of all stockpiles approved for State work.

## **X. RELOCATION OF STOCKPILE**

If material from an approved RAP stockpile is to be moved to another location, the contractor shall seek approval from the Department prior to its further use on State projects. A letter request shall be submitted to the Division of Materials indicating the current stockpile location, the total quantity of material to be moved, and the amount, if any, to remain in the current location. The Division of Materials will issue an approval letter applicable to the new location.

June 18, 2025

## SPECIAL NOTE FOR PAVEMENT WEDGE AND SHOULDER MONOLITHIC OPERATION

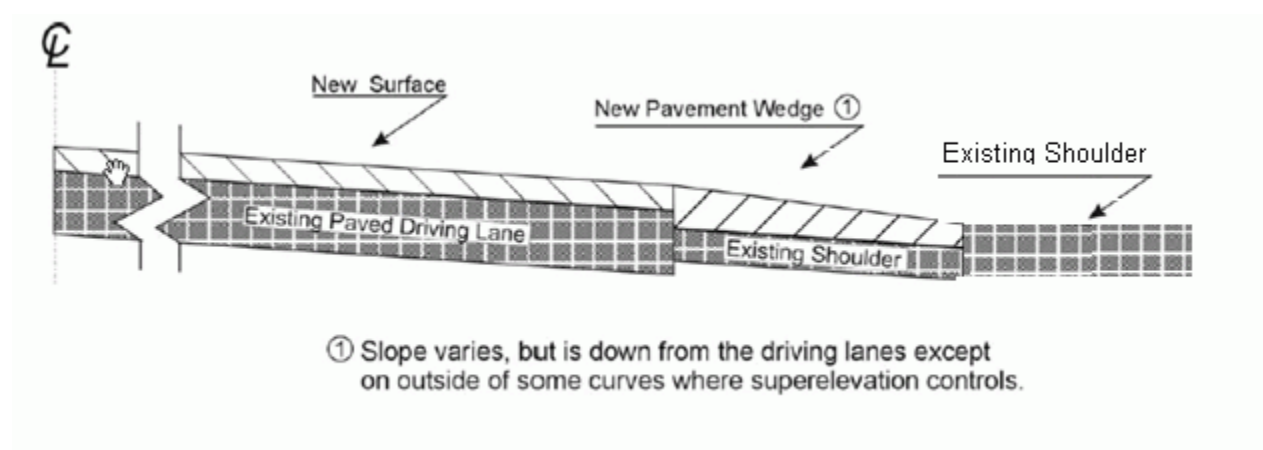
**1.0 MATERIALS.** Provide an Asphalt Surface Mixture conforming to Section 403 of the Standard Specifications, as applicable to the project, for the pavement wedge.

**2.0 CONSTRUCTION.** Place the specified Asphalt Surface Mixture on shoulders monolithically with the driving lane. Prime the existing shoulder with tack material as the Engineer directs before placing the wedge. Construct according to Section 403.03 of the Standard Specifications.

Equip the paver with a modified screed that extends the full width of the wedge being placed and is tapered to produce a wedge. Obtain the Engineer's approval of the modified screed before placing shoulder wedge monolithically with the driving lane.

The wedge may vary in thickness at the edge of the milled area in the shoulder. If the area to receive the shoulder wedge is milled prior to placement, during rolling operations pinch the outside edge of the new inlay wedge to match the existing shoulder elevation not being resurfaced. Unless required otherwise by the Contract, construct rolled or sawed rumble strips according to Section 403.03.08, as applicable.

The following sketch is primarily for the computation of quantities; however, the wedge will result in a similar cross-section where sufficient width exists. Do not construct a shoulder for placing the wedge unless specified elsewhere in the Contract.



**3.0 MEASUREMENT.** The Department will measure Asphalt Surface Mixture placed as the pavement wedge according to Section 403.

**4.0 PAYMENT.** The Department will make payment for the completed and accepted quantities of Asphalt Surface Mixtures on pavement wedges according to Section 403.



## SPECIAL NOTES FOR GUARDRAIL

---

### I. DESCRIPTION

Except as specified herein, perform all work in accordance with the Department's Standard and Supplemental Specifications and Standard and Sepia Drawings, current editions. Article references are to the Standard Specifications.

Furnish all equipment, labor, materials, and incidentals for the following work items:

(1) Site preparation; (2) Guardrail, End Treatments, Bridge End Connectors, and Terminal Sections, as applicable; (3) Delineators for guardrail; (4) Maintain and control traffic; and (5) all other work specified as part of this contract.

### II. MATERIALS

Except as specified herein, provide for all materials to be sampled and tested in accordance with the Department's Sampling Manual and make the materials available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing.

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Guardrail.** Furnish guardrail system components according to section 814 and the Standard and Sepia Drawings; except use steel posts only, no alternates.

**C. Delineators for Guardrail.** Furnish Delineators for Guardrail according to the Sepia Drawing.

**D. Erosion Control.** See Special Notes for Erosion Control.

### III. CONSTRUCTION METHODS

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Site Preparation.** Be responsible for all site preparation, including but not limited to, clearing and grubbing, excavation, embankment, and removal of all obstructions or any other items; regrading, reshaping, adding and compacting of suitable materials on the existing shoulders to provide proper template or foundation for the guardrail; temporary pollution and erosion control; disposal, of excess and waste materials and debris; and final dressing, cleanup, and seeding and protection. Perform all site preparation as approved or directed by the Engineer.

## Guardrail

### Page 2 of 3

**C. Guardrail.** Except as specified herein, construct guardrail system according to Section 719 and the Standard and Sepia Drawings. Locations listed on the summary and/or shown on the drawings are approximate only. The Engineer will determine the exact termini for individual guardrail installations at the time of construction. Unless directed otherwise by the Engineer, provide a minimum two (2) foot shoulder width. Construct radii at entrances and road intersections as directed by the Engineer.

Erect guardrail to the lines and grades shown on current Standard and Sepia Drawings or as directed by the Engineer by any method approved by the Engineer which allows construction of the guardrail to the true grade without apparent sags. Support cantilevered terminal sections with an additional post.

When installing guardrail, do not leave the blunt end exposed where it would be hazardous to the public. When it is not practical to complete the construction of the guardrail and the permanent end treatments and terminal sections first, provide a temporary end by connecting at least 25 feet of rail to the last post, and by slightly flaring, and burying the end of the rail completely into the existing shoulder. If left overnight, place a drum with bridge panel in advance of the guardrail end and maintain during use.

**D. Delineators for Guardrail.** Install delineators for guardrail according to the Standard and Sepia Drawings.

**E. Property Damage.** Be responsible for all damage to public and/or private property resulting from the work. Restore damaged roadway features and private property at no additional cost to the Department.

**F. Coordination with Utility Companies.** Locate all underground, above ground and overhead utilities prior to beginning construction. Be responsible for contacting and maintaining liaison with all utility companies that have utilities located within the project limits. Do not disturb existing overhead or underground utilities. It is not anticipated that any utility facilities will need to be relocated and/or adjusted; however, in the event that it is discovered that the work does require that utilities be relocated and/or adjusted, the utility companies will work concurrently with the Contractor while relocating their facilities. Be responsible for repairing all utility damage that occurs as a result of guardrail operations at no additional cost to the Department.

**G. Right of Way Limits.** The Department has not established exact limits of the Right-of-Way. Limit work activities to obvious Right-of-Way, permanent or temporary easements, and work areas secured by the Department through consent and release of the adjacent property owners. Be responsible for all encroachments onto private lands.

**H. Disposal of Waste.** Dispose of all removed concrete, debris, and other waste and debris off the Right-of-Way at sites obtained by the Contractor at no additional cost to the Department. See Special; Note for Waste and Borrow.

Guardrail  
Page 3 of 3

**I. Final Dressing, Clean Up, and Seeding and Protection.** Apply Class A Final Dressing to all disturbed areas, both on and off the Right-of-Way. Sow all disturbed earthen areas according to the Special Notes for Erosion Control.

**J. Erosion Control.** See Special Notes for Erosion Control.

**IV. METHOD OF MEASUREMENT**

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Site preparation.** Other than the bid items listed, the Department will not measure Site Preparation for separate payment but shall be incidental to Guardrail, End Treatments, Bridge End Connectors, and Terminal Sections as applicable.

**C. Guardrail.** See Section 719.04.

**D. Delineators for Guardrail.** See the Sepia Drawing.

**E. Erosion Control.** See Special Notes for Erosion Control.

**V. BASIS OF PAYMENT**

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Guardrail.** See Section 719.05.

**C. Delineators for Guardrail.** See the Sepia Drawing.

**D. Erosion Control.** See Special Notes for Erosion Control.

### **SPECIAL NOTE FOR ASPHALT MILLING AND TEXTURING**

Begin paving operations within **48 hours** of commencement of the milling operation. Continue paving operations continuously until completed. If paving operations are not begun within this time period, the Department will assess liquidated damages at the rate prescribed by Section 108.09 until such time as paving operations are begun.

Take possession of the millings and recycle the millings or dispose of the millings off the Right-of-Way at sites obtained by the Contractor at no additional cost to the Department.

1-3520 48 hours Contractor keeps millings  
01/2/2012

## **SPECIAL NOTE FOR BASE FAILURE REPAIR**

Repair locations listed on the summary are approximate only. The Engineer will determine actual repair locations and dimensions at the time of construction. Prior to overall milling and/or leveling and wedging, excavate the designated base failure areas by milling to a depth 6 inches below the existing asphalt pavement surface level. Dispose of the excavated materials at waste sites off the Right-of-Way obtained by the Contractor at no additional cost to the Department. See Special Note for Waste and Borrow.

Backfill the excavated areas with Class 2 Asphalt Base 1.00D PG64-22. Compact the asphalt base to the compaction required in Section 403.03.10. Seal the asphalt base with leveling and wedging. Perform all base failure repairs in such a manner that removal and replacement are completed on the same day. Do this work as one of the Contractor's first operations in order to allow further compaction by traffic. Do not mill or place new asphalt surface over repaired base failure areas until a minimum of 7 calendar days have elapsed after placement of the asphalt base. After a minimum of 7 calendar days and when the Engineer determines the base failure repair areas have sufficiently stabilized, begin milling and/or resurfacing operations. Prior to milling and/or constructing the new asphalt surface, level and wedge any settlement of the repair areas.

The bidder must draw conclusions as to the conditions encountered. The Department does not give any guarantee as to the accuracy of the data and will not consider any claim for additional compensation if the materials encountered that are not in accord with the classification shown.

Accept payment at the Contract unit prices per ton for Asphalt Milling and Texturing, Asphalt Base, and Leveling and Wedging as full compensation for all labor, materials, equipment, and incidentals for removing pavement and disposing of the materials, furnishing and placing asphalt base, leveling and wedging, and all other items necessary to complete the work according to these notes to the satisfaction of the Engineer.

1-3605 basefailurerepairmillinlaypayton  
01/02/2012

## **TRAFFIC CONTROL PLAN FOR SLIDE CORRECTION DRILLED RAILROAD RAIL PILING**

---

### **TRAFFIC CONTROL GENERAL**

Except as specified herein, maintain and control traffic in accordance with the Standard and Supplemental Specifications and the Standard and Sepia Drawings, current editions. Except for the roadway and traffic control bid items listed, furnish all other items necessary to maintain and control traffic incidental to the Contract lump sum price Maintain and Control Traffic.

Contrary to Section 106.01, furnish new, or used in like new condition, traffic control devices, at the beginning of the work and maintain the devices in like new condition until completion of the work.

### **PROJECT PHASING & CONSTRUCTION PROCEDURES**

At the discretion of the Engineer, the Department may specify days and hours when lane closures will not be allowed. Prior to beginning work, provide a proposed lane closure and work schedule for the approval of the Engineer. The Department will provide public notification. Notify the Engineer immediately and obtain prior approval of any proposed deviations from the approved schedule.

Maintain alternating one-way traffic during construction. Unless directed otherwise by the Engineer, provide a minimum clear lane width of 10 feet. If traffic should be stopped due to construction operations, and a school bus on an official run arrives on the scene, immediately make provisions for the passage of the bus.

If the Engineer determines that work site conditions require all traffic be stopped while drilling operations are in progress, stop traffic for the length of time required to drill and set one rail. Allow all waiting traffic to pass before starting the next hole. If an emergency vehicle or school bus is present in the queue, stop drilling and allow traffic to pass immediately.

### **LANE CLOSURES**

Except as permitted by the Engineer, do not leave lane closures in place during non-working hours. If permitted, the Engineer may require additional signing and/or traffic control devices.

### **CHANGEABLE MESSAGE SIGNS**

If deemed necessary by the Engineer, the Department will furnish, operate, and maintain

## Traffic Control Plan for Slide Correction

### Page 2 of 3

Changeable Message Signs.

## **SIGNS**

Signposts and splices shall be compliant with NCHRP 350 or MASH. Manufacturer's documentation validating this compliance shall be provided to the Engineer prior to installation. Signs, including any splices, shall be installed according to manufacturer's specifications and installation recommendations. Contrary to section 112.04.02, only long-term signs (signs intended to be continuously in place for more than 3 days) will be measured for payment. Short-term signs (signs intended to be left in place for 3 days or less) will not be measured for payment but will be incidental to Maintain and Control Traffic.

## **BARRICADES**

The Department will not measure Barricades used in lieu of barrels and cones for channelization or delineation but shall be incidental to Maintain and Control Traffic according to Section 112.04.01. The Department will measure Barricades used for protection of pavement removal areas according to Section 112.04.04. Retain possession of the barricades upon completion of the work.

## **PAVEMENT EDGE DROP-OFFS**

Do not allow a difference in elevation of a pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation greater than 1½". Place warning signs ((MUTCD W8-9, W8-9A, or W8-11) in advance of and at 1500 feet intervals throughout the drop-off area. Dual post the signs on both sides of the traveled way. Wedge transverse transitions between newly surfaced pavement and the existing pavement areas that traffic may cross with asphalt mixture for leveling and wedging. Remove wedges prior to placement of the final surface course.

Treat pavement edges that traffic is not expected to cross, except accidentally, as follows:

Less than 2" - No protection required.

2" to 4" - Place plastic drums, vertical panels, or barricades every 50 feet. The Engineer will allow cones to be used in lieu of plastic drums, panels, and barricades during daylight working hours only. Wedge drop-offs within 10 feet of traffic with DGA or asphalt mixture for leveling and wedging as directed or approved by the Engineer with a 1:1 or flatter slope in daylight working hours, or 3:1 or flatter slope during nighttime hours or when work is not active in the drop-off area.

Traffic Control Plan for Slide Correction  
Page 3 of 3

Greater than 4" – Protect drop-offs greater than 4 inches within 10 feet of traffic by placing drums, vertical panels, or barricades every 25 feet. The Engineer will not allow the use of cones in lieu of drums, vertical panels, or barricades for drop-offs greater than 4". Place Type III Barricades directly in front of the drop-off facing oncoming traffic in both directions of travel. Provide warning signs as shown on the Standard Drawings or as

13802trafficcontrolplanslidecorrection  
01/02/2012



## **SPECIAL NOTE FOR EROSION CONTROL**

---

### **I. DESCRIPTION**

Except as provided herein, perform all erosion and water pollution control work in accordance with the Department's Standard and Supplemental Specifications, and Standard and Sepia Drawings, current editions, and as directed by the Engineer. Section references are to the Standard Specifications. This work shall consist of:

(1) Developing and preparing a Best Management Practices Plan (BMP) tailored to suit the specific construction phasing for each site within the project; (2) Preparing the project site(s) for construction, including locating, furnishing, installing, and maintaining temporary and/or permanent erosion and water pollution control measures as required by the BMP prior to beginning any earth disturbing activity on the project site; (3) Clearing and grubbing and removal of all obstructions as required for construction; (4) Removing all erosion control devices when no longer needed; (5) Restoring all disturbed areas as nearly as possible to their original condition; (6) Preparing seedbeds and permanently seeding all disturbed areas; (7) Providing a Kentucky Erosion Prevention and Sediment Control Program (KEPSC) qualified inspector; and (8) Performing any other work to prevent erosion and/or water pollution as specified by this contract, required by the BMP, or as directed by the Engineer.

### **II. MATERIALS**

Except as provided herein, furnish all materials for erosion and water pollution control work in accordance with the Department's Standard and Supplemental Specifications, and Standard and Sepia Drawings, current editions, and as directed by the Engineer.

Provide for all materials to be sampled and tested in accordance with the Department's Sampling Manual. Unless directed otherwise by the Engineer, make the materials available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing.

### **III. CONSTRUCTION**

Except as provided herein, construct all erosion and water pollution control work in accordance with the Department's Standard and Supplemental Specifications, and Standard and Sepia Drawings, current editions, and as directed by the Engineer.

## Erosion Control

### Page 2 of 3

Be advised, these Erosion Control Plan Notes do not constitute a BMP plan for the project. Jointly with the Engineer, prepare a site-specific BMP plan for each drainage area within the project in accordance with Section 213 and the supplemental specifications. Provide a unique BMP at each project site using good engineering practices considering existing site conditions, the type of work to be performed, and the construction phasing, methods and techniques to be utilized to complete the work. Be responsible for all erosion prevention, sediment control, and water pollution prevention measures required by the BMP for each site. Represent and warrant compliance with the Clean Water Act (33 USC Section 1251 et seq.), the 404 Permit, the 401 Water Quality Certification, and applicable state and local government agency laws, regulations, rules, specifications, and permits. Contrary to Section 105.05, in case of discrepancy between these notes, the Standard Specifications, interim Supplemental Specifications, Special and Special Notes, Standard and Sepia Drawings, and such state and local government agency requirements, adhere to the most restrictive requirement.

Conduct operations in such a manner as to minimize the amount of disturbed ground during each phase of the construction and limit the haul roads to the minimum required to perform the work. Preserve existing vegetation not required to be removed by the work or the contract. Seed and/or mulch disturbed areas at the earliest opportunity. Use silt fence, silt traps, temporary ditches, brush barriers, erosion control blankets, sodding, channel lining, and other erosion control measures in a timely manner as required by the BMP and as directed or approved by the Engineer. Prevent sediment laden water from leaving the project, entering an existing drainage structure, or entering a stream.

Provide for erosion control measures to be in place and functioning prior to any earth disturbance within a drainage area. Compute the volume and size of silt control devices necessary to control sediment during each phase of construction. Remove sediment from silt traps before they become a maximum of ½ full. Maintain silt fence by removing accumulated trappings and/or replacing the geotextile fabric when it becomes clogged, damaged, or deteriorated, or when directed by the Engineer. Properly dispose of all materials trapped by erosion control devices at approved sites off the right of way obtained by the Contractor at no additional cost to the Department (See Special Note for Waste and Borrow).

As work progresses, add or remove erosion control measures as required by the BMP applicable to the Contractor's project phasing and construction methods and techniques. Update the volume calculations and modify the BMP as necessary throughout the duration of the project. Ensure that an updated BMP is kept on site and available for public inspection throughout the life of the project.

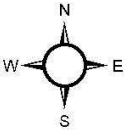
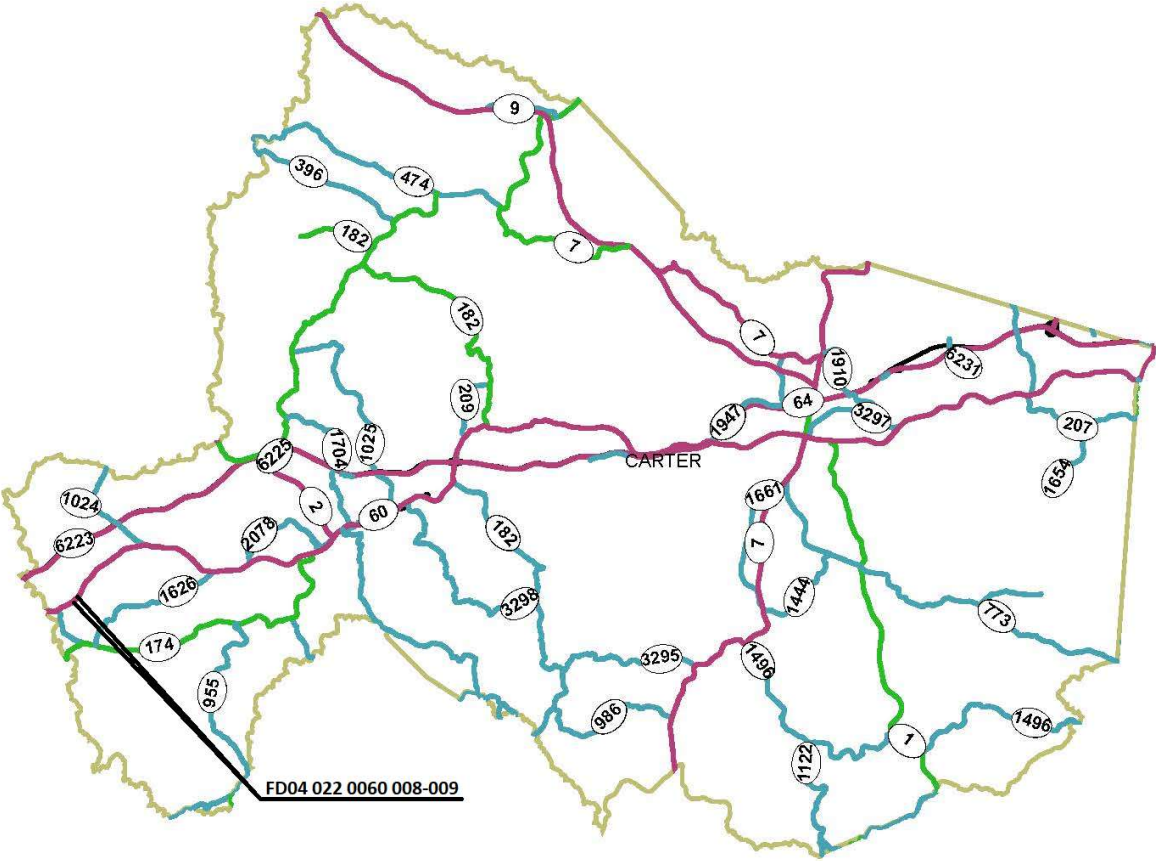
After all construction is complete, restore all disturbed areas in accordance with Section 212. Completely remove all temporary erosion control devices not required as part of the permanent erosion control from the construction site. Prior to removal, obtain the Engineer's concurrence of items to be removed. Grade the remaining exposed earth (both on and off the Right of-Way) as nearly as possible to its original condition, or as directed by the Engineer. Prepare the seed bed areas and sow all exposed earthen areas with the applicable seed mixture(s) according to Section 212.03.03.

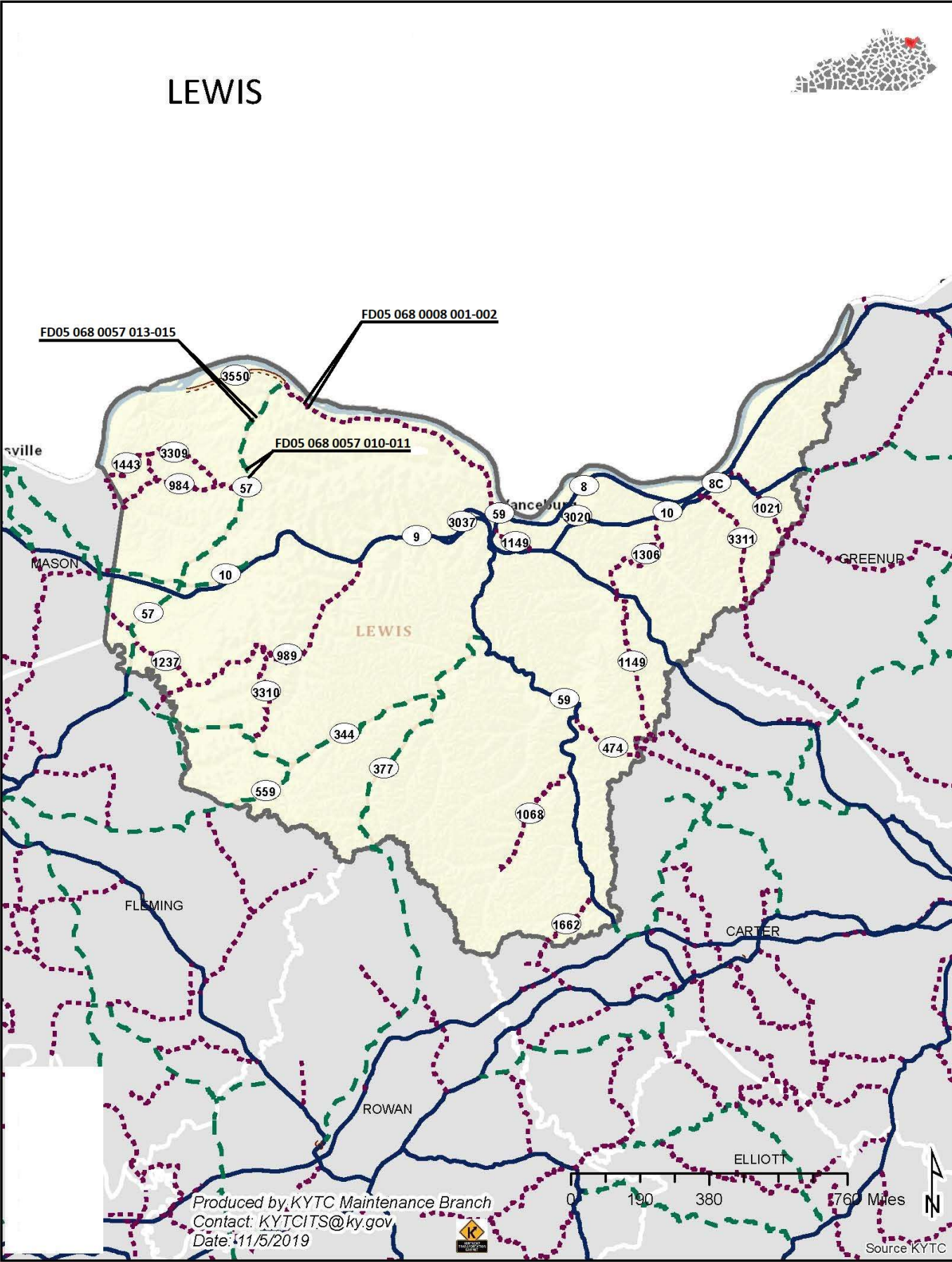
Erosion Control  
Page 3 of 3

#### **IV. MEASUREMENT**

Contrary to Section 212.04 and 213.04, the Department will not measure Erosion Control items for separate payment, but shall be incidental to Pavement Removal, DGA, Leveling and Wedging, and JPC Pavement as applicable

CARTER





MATERIAL SUMMARY

CONTRACT ID: 252326

121GR25P081 - FD04

MP02200602501

MOREHEAD TO OLIVE HILL ROAD (US 60) BEGIN 676 FEET WEST OF DOUGLAS HOLLOW ROAD EXTENDING EAST TO 148 FEET WEST OF DOUGLAS HOLLOW ROAD SLIDE REPAIR, A DISTANCE OF .1 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0005	00001	DGA BASE	50.00	TON
0010	00078	CRUSHED AGGREGATE SIZE NO 2	367.00	TON
0015	00190	LEVELING & WEDGING PG64-22	30.00	TON
0020	00212	CL2 ASPH BASE 1.00D PG64-22	60.00	TON
0025	00301	CL2 ASPH SURF 0.38D PG64-22	80.00	TON
0030	01987	DELINEATOR FOR GUARDRAIL BI DIRECTIONAL WHITE	9.00	EACH
0035	02200	ROADWAY EXCAVATION	244.00	CUYD
0040	02351	GUARDRAIL-STEEL W BEAM-S FACE	400.00	LF
0045	02367	GUARDRAIL END TREATMENT TYPE 1	2.00	EACH
0050	02562	TEMPORARY SIGNS	100.00	SQFT
0055	02602	FABRIC-GEOTEXTILE CLASS 1	196.00	SQYD
0060	02650	MAINTAIN & CONTROL TRAFFIC - (US 60 CARTER COUNTY)	1.00	LS
0065	02676	MOBILIZATION FOR MILL & TEXT - (US 60 CARTER COUNTY)	1.00	LS
0070	02677	ASPHALT PAVE MILLING & TEXTURING	70.00	TON
0075	03234	RAILROAD RAILS-DRILLED	1,650.00	LF
0080	03236	CRIBBING	1,650.00	SQFT
0085	06514	PAVE STRIPING-PERM PAINT-4 IN	2,000.00	LF
0090	26248EC	ELECTRONIC DELIVERY MGMT SYSTEM - AGG - (US 60)	1.00	LS
0095	02569	DEMOBILIZATION	1.00	LS

MATERIAL SUMMARY

CONTRACT ID: 252326

121GR25P081 - FD04

MP06800082501

CONCORD TO VANCEBURG ROAD (KY 8) BEGIN 0.624 MILES WEST OF CHANEY LANE EXTENDING EAST TO 0.164 MILES WEST OF CHANEY LANE SLIDE REPAIR, A DISTANCE OF .48 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0100	00001	DGA BASE	150.00	TON
0105	00078	CRUSHED AGGREGATE SIZE NO 2	1,767.00	TON
0110	00190	LEVELING & WEDGING PG64-22	50.00	TON
0115	00212	CL2 ASPH BASE 1.00D PG64-22	170.00	TON
0120	01987	DELINEATOR FOR GUARDRAIL BI DIRECTIONAL WHITE	15.00	EACH
0125	02200	ROADWAY EXCAVATION	1,178.00	CUYD
0130	02351	GUARDRAIL-STEEL W BEAM-S FACE	675.00	LF
0135	02371	GUARDRAIL END TREATMENT TYPE 7	4.00	EACH
0140	02562	TEMPORARY SIGNS	100.00	SQFT
0145	02602	FABRIC-GEOTEXTILE CLASS 1	628.00	SQYD
0150	02650	MAINTAIN & CONTROL TRAFFIC - (KY 8 LEWIS COUNTY)	1.00	LS
0155	02676	MOBILIZATION FOR MILL & TEXT - (KY 8 LEWIS COUNTY)	1.00	LS
0160	02677	ASPHALT PAVE MILLING & TEXTURING	170.00	TON
0165	03234	RAILROAD RAILS-DRILLED	3,210.00	LF
0170	03236	CRIBBING	5,300.00	SQFT
0175	26248EC	ELECTRONIC DELIVERY MGMT SYSTEM - AGG - (KY 8)	1.00	LS
0180	02569	DEMOBILIZATION	1.00	LS

MATERIAL SUMMARY

CONTRACT ID: 252326

121GR25P081 - FD04

MP06800572501

TOLLESBORO TO CONCORD ROAD (KY 57) BEGIN 0.267 MILES SOUTH OF QUICKS RUN ROAD EXTENDING NORTH TO 0.107 MILES SOUTH OF QUICKS RUN ROAD SLIDE REPAIR, A DISTANCE OF .16 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0185	00001	DGA BASE	100.00	TON
0190	00078	CRUSHED AGGREGATE SIZE NO 2	700.00	TON
0195	00190	LEVELING & WEDGING PG64-22	50.00	TON
0200	00212	CL2 ASPH BASE 1.00D PG64-22	65.00	TON
0205	00301	CL2 ASPH SURF 0.38D PG64-22	120.00	TON
0210	01987	DELINEATOR FOR GUARDRAIL BI DIRECTIONAL WHITE	15.00	EACH
0215	02200	ROADWAY EXCAVATION	467.00	CUYD
0220	02351	GUARDRAIL-STEEL W BEAM-S FACE	700.00	LF
0225	02371	GUARDRAIL END TREATMENT TYPE 7	2.00	EACH
0230	02562	TEMPORARY SIGNS	100.00	SQFT
0235	02602	FABRIC-GEOTEXTILE CLASS 1	249.00	SQYD
0240	02650	MAINTAIN & CONTROL TRAFFIC - (KY 57 LEWIS COUNTY 010-011)	1.00	LS
0245	02676	MOBILIZATION FOR MILL & TEXT - (KY 57 LEWIS COUNTY 010-011)	1.00	LS
0250	02677	ASPHALT PAVE MILLING & TEXTURING	75.00	TON
0255	03234	RAILROAD RAILS-DRILLED	2,100.00	LF
0260	03236	CRIBBING	2,100.00	SQFT
0265	06514	PAVE STRIPING-PERM PAINT-4 IN	1,700.00	LF
0270	26248EC	ELECTRONIC DELIVERY MGMT SYSTEM - AGG - (KY 57 MP 010-011)	1.00	LS
0275	02569	DEMOBILIZATION	1.00	LS



MATERIAL SUMMARY

CONTRACT ID: 252326

121GR25P081 - FD04

MP06800572502

TOLLESBORO TO CONCORD ROAD (KY 57) BEGIN 496 FEET NORTH OF PINE GROVE CHURCH ROAD  
EXTENDING NORTH TO 0.264 MILES NORTH OF PINE GROVE CHURCH ROAD SLIDE REPAIR, A DISTANCE  
OF .3 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0280	00001	DGA BASE	100.00	TON
0285	00078	CRUSHED AGGREGATE SIZE NO 2	1,680.00	TON
0290	00190	LEVELING & WEDGING PG64-22	75.00	TON
0295	00212	CL2 ASPH BASE 1.00D PG64-22	120.00	TON
0300	00301	CL2 ASPH SURF 0.38D PG64-22	200.00	TON
0305	01987	DELINEATOR FOR GUARDRAIL BI DIRECTIONAL WHITE	26.00	EACH
0310	02200	ROADWAY EXCAVATION	1,120.00	CUYD
0315	02351	GUARDRAIL-STEEL W BEAM-S FACE	1,200.00	LF
0320	02371	GUARDRAIL END TREATMENT TYPE 7	4.00	EACH
0325	02562	TEMPORARY SIGNS	100.00	SQFT
0330	02602	FABRIC-GEOTEXTILE CLASS 1	560.00	SQYD
0335	02650	MAINTAIN & CONTROL TRAFFIC - (KY 57 LEWIS COUNTY 013-015)	1.00	LS
0340	02676	MOBILIZATION FOR MILL & TEXT - (KY 57 LEWIS COUNTY 013-015)	1.00	LS
0345	02677	ASPHALT PAVE MILLING & TEXTURING	135.00	TON
0350	03234	RAILROAD RAILS-DRILLED	5,600.00	LF
0355	03236	CRIBBING	5,040.00	SQFT
0360	06514	PAVE STRIPING-PERM PAINT-4 IN	3,200.00	LF
0365	26248EC	ELECTRONIC DELIVERY MGMT SYSTEM - AGG - (KY 57 MP 013-015)	1.00	LS
0370	02569	DEMOBILIZATION	1.00	LS

Guardrail Summary Sheet

Project: FD04 022  
0060 000-001

County: Carter

District: D9 - Flemingsburg

Road Direction	Start MP	END MP	Comments	GUARDRAIL END TREATMENT TYPE 1
East Bound	0.816	0.890		2
Total:				2

Comments: End Treatment Type 7 @ MP 0.816 and Term Sect @ MP 0.89

Project: FD04 022  
0060 000-001

County: Carter

District: D9 - Flemingsburg

Mile Point	Notes	Length	Width	Avg Depth	Tons
0.800	EDGE KEY	100	24	0.5	7.3
0.850	TRENCHING FOR ASPHALT BASE	175	8	6	51.3
0.900	EDGE KEY	100	24	0.5	7.3
				Total:	65.9

Slide  
Repair Summary  
Carter County  
FD04 022 0060 000-001

Route	Milepoint	Length of Slide (FT)	Rail Spacing (FT)	# of Rail	Length of Rail (FT)	Depth to be Cribbed (FT)	RR Rail Quantity (LF)	Cribbing (SQ FT)	Roadway Excavation (CU YD)	Crushed Aggregate No. 2 (TONS)	Class 1 Geotextile Fabric (SQ YD)
US 60	0.850	165	3	55	30	10	1650	1650	244	367	196
Totals							1650	1650	244	367	196

Guardrail Summary Sheet

Project: FD04 068  
0008 001-002

County: Lewis

District: D9 - Flemingsburg

Road Direction	Start MP	END MP	Comments	NoBid Item
West Bound	0.980	1.078		0
West Bound	1.410	1.438		0
			Total:	0

Comments: End Treatment Type 7 at each end MP 0.980 - 1.078  
and End Treatment Type 7 at each end MP 1.410 - 1.438

Project: FD04 068  
0008 001-002

County: Lewis

District: D9 - Flemingsburg

Mile Point	Notes	Length	Width	Avg Depth	Tons
1.000	TRENCHING FOR ASPHALT BASE	410	8	6	120.3
1.410	TRENCHING FOR ASPHALT BASE	120	8	6	35.2
				Total:	155.5

Slide  
Repair Summary  
Lewis County  
FD04 068 0008 000-002

Route	Milepoint	Length of Slide (FT)	Rail Spacing (FT)	# of Rail	Length of Rail (FT)	Depth to be Cribbed (FT)	RR Rail Quantity (LF)	Cribbing (SQ FT)	Roadway Excavation (CU YD)	Crushed Aggregate No. 2 (TONS)	Class 1 Geotextile Fabric (SQ YD)
KY 8	1.000	410	3	67	30	10	2010	4100	911	1367	486
KY 8	1.410	120	3	40	30	10	1200	1200	267	400	142
Totals							3210	5300	1178	1767	628

\*\* At KY 8 MP 1.0, approximately 200 LF of the slide area contains existing railroad rail can be used and will only require cribbing.

Guardrail Summary Sheet

Project: FD04 068  
0057 010-011

County: Lewis

District: D9 - Flemingsburg

Road Direction	Start MP	END MP	Comments	GUARDRAIL END TREATMENT TYPE 7
South Bound	10.730	10.866		2
			Total:	2

Comments: End Treatment Type 7 at MP 10.730 and End Treatment Type 7 at MP 10.866



Project: FD04 068  
0057 010-011

County: Lewis

District: D9 - Flemingsburg

Mile Point	Notes	Length	Width	Avg Depth	Tons
10.800	EDGE KEY	100	18	0.5	5.5
10.830	TRENCHING FOR ASPHALT BASE	210	8	6	61.6
10.850	EDGE KEY	100	18	0.5	5.5
				Total:	72.6

Slide  
Repair Summary  
Lewis County  
FD04 068 0057 010-011

Route	Milepoint	Length of Slide (FT)	Rail Spacing (FT)	# of Rail	Length of Rail (FT)	Depth to be Cribbed (FT)	RR Rail Quantity (LF)	Cribbing (SQ FT)	Roadway Excavation (CU YD)	Crushed Aggregate No. 2 (TONS)	Class 1 Geotextile Fabric (SQ YD)
KY 57	10.830	210	3	70	30	10	2100	2100	467	700	249
Totals							2100	2100	467	700	249

Guardrail Summary Sheet

Project: FD04 068  
0057 01- 015

n ouLty: wes i9

Di9trict: Dg CFlemiLb9Ru

Moad DirectioL	Start E P	NUD E P	n ommeLt9	GAI MDM I TwNUD YMNI YE NUY YBPN 7
Uorth . ouLd	1- 350	143148		2
Uorth . ouLd	143150	143178		2
Total:				4

Comments: End Treatment Type 7 at each end MP 13.950 - MP 14.148 and End Treatment Type 7 at each end MP 14.150 - 14.178

Project: FD04 068  
0057 01- 015

n ouLty: wes i9

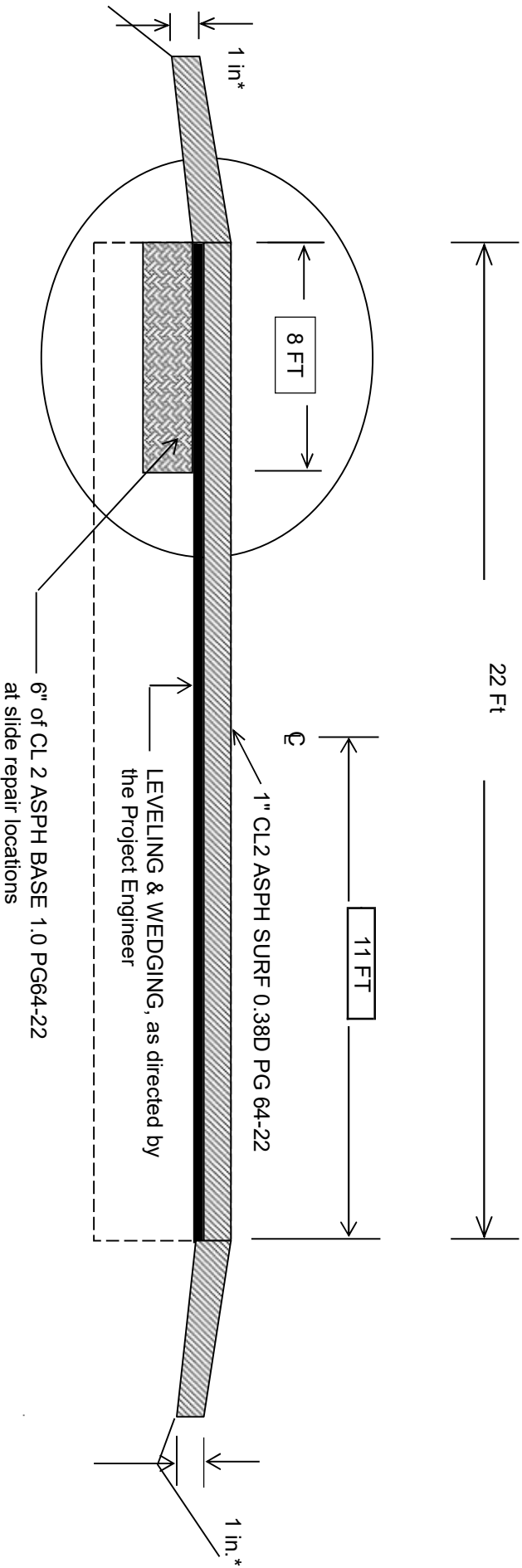
Di9trict: Dg CFlemiLb9Ru

E ile PoiLt	Uote9	weLbth	Width	I vb Depth	YoL9
14300	NDGN KNB	100	18	03	53
14300	YMNUnHTUG FOMI SPHI wY . I SN	180	8	6	523
143120	YMNUnHTUG FOMI SPHI wY . I SN	120	8	6	- 53
143150	YMNUnHTUG FOMI SPHI wY . I SN	120	8	6	- 53
143200	NDGN KNB	100	18	03	53
				Total:	134.2

Slide  
Repair Summary  
Lewis County  
FD04 068 0057 013-015

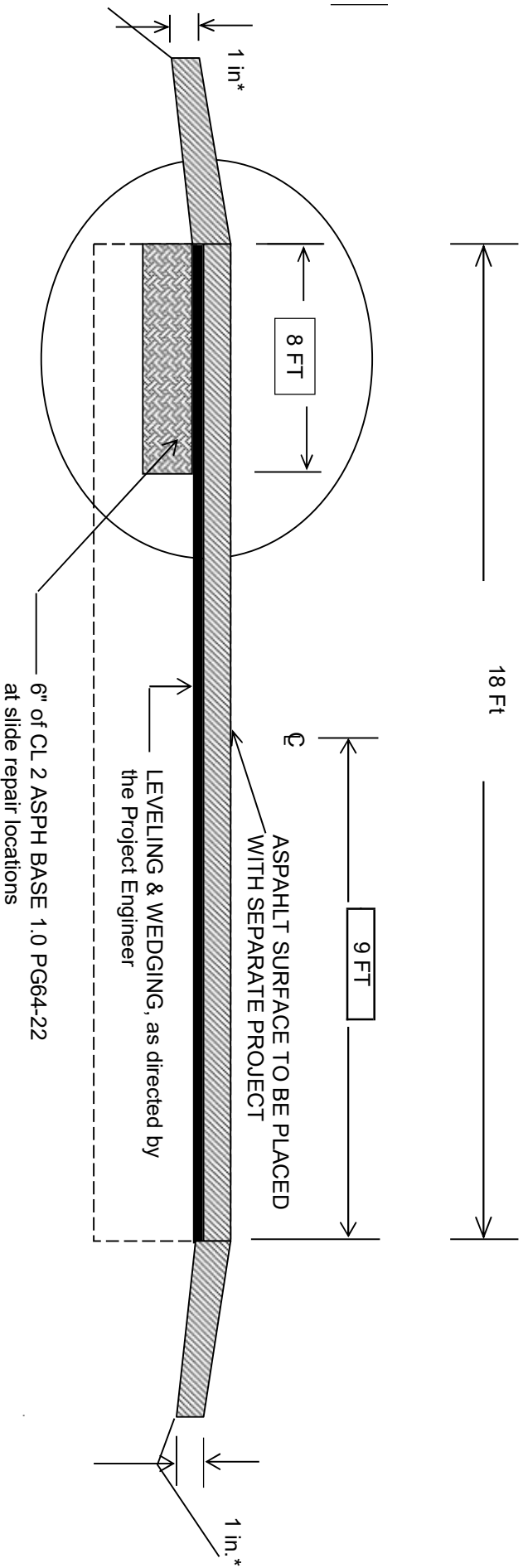
Route	Milepoint	Length of Slide (FT)	Rail Spacing (FT)	# of Rail	Length of Rail (FT)	Depth to be Cribbed (FT)	RR Rail Quantity (LF)	Cribbing (SQ FT)	Roadway Excavation (CU YD)	Crushed Aggregate No. 2 (TONS)	Class 1 Geotextile Fabric (SQ YD)
KY 57	14.000	180	3	60	40	12	2400	2160	480	720	240
KY 57	14.120	120	3	40	40	12	1600	1440	320	480	160
KY 57	14.150	120	3	40	40	12	1600	1440	320	480	160
Totals							5600	5040	1120	1680	560

Carter County  
TYPICAL SECTION  
FD04 022 0060 000-001  
MP's 0.800 - 0.900



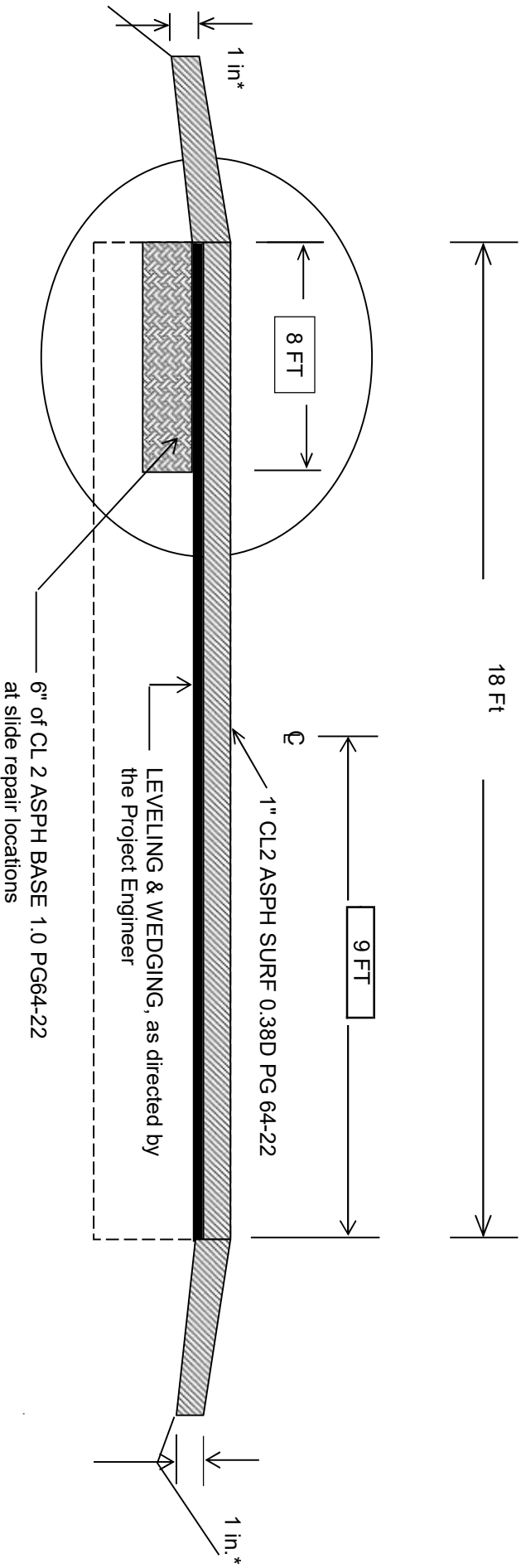
\*Where Existing Site Conditions Permit

Lewis County  
TYPICAL SECTION  
FD04 068 0008 000-002  
MP's 0.980 - 1.460



\*Where Existing Site Conditions Permit

Lewis County  
TYPICAL SECTION  
FD04 068 0057 010-011  
MP's 10.720 - 10.880



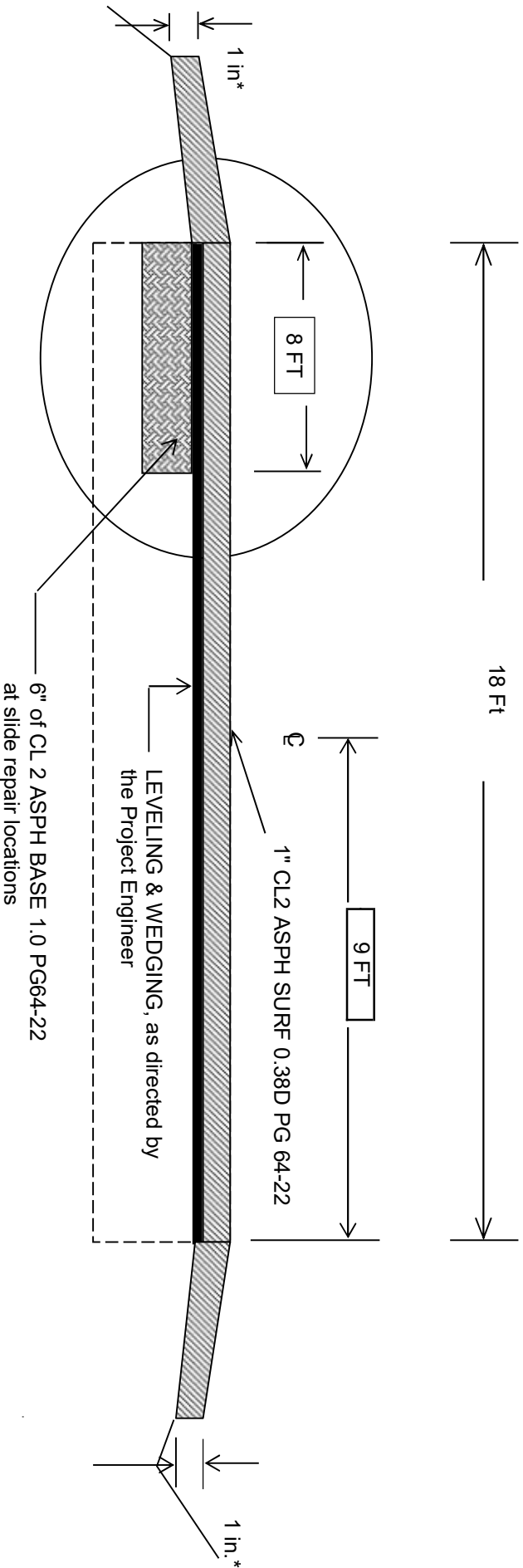
\*Where Existing Site Conditions Permit

TS1BASEFAILURE



**TYPICAL SECTION**  
**LEWIS COUNTY**

FD04 068 0057 014-015  
MP 13.900 TO MP 14.200



**\*Where Existing Site Conditions Permit**

TYPICAL SECTION DEPICTING INSTALLATION OF  
RECYCLED RAILROAD RAIL PLACED IN DRILLED  
SOCKET FOR LANDSLIDE CORRECTION

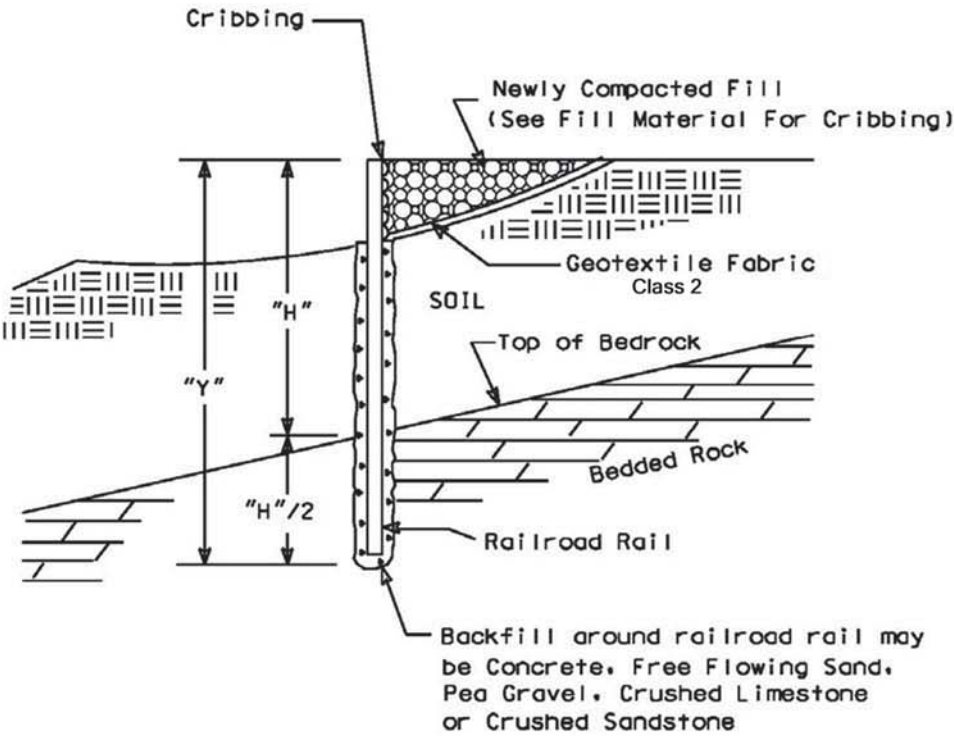
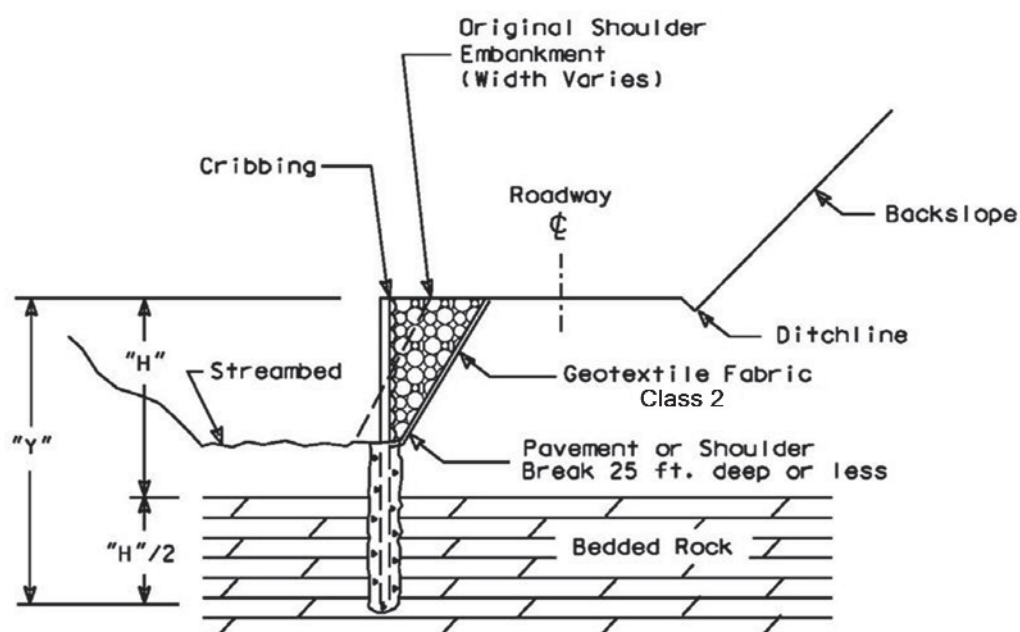


Figure 1

NOTE:  
Spacing from edge to  
edge of drilled  
socket : 3 ft. max.



NOTE :  
 "H"/2 Depth of Rail into bedded rock =  
 1/3 total length where rock is present.

Figure 2

ALTERNATE SCHEMES FOR INSTALLING RAILROAD RAILS  
IN DRILLED SOCKETS

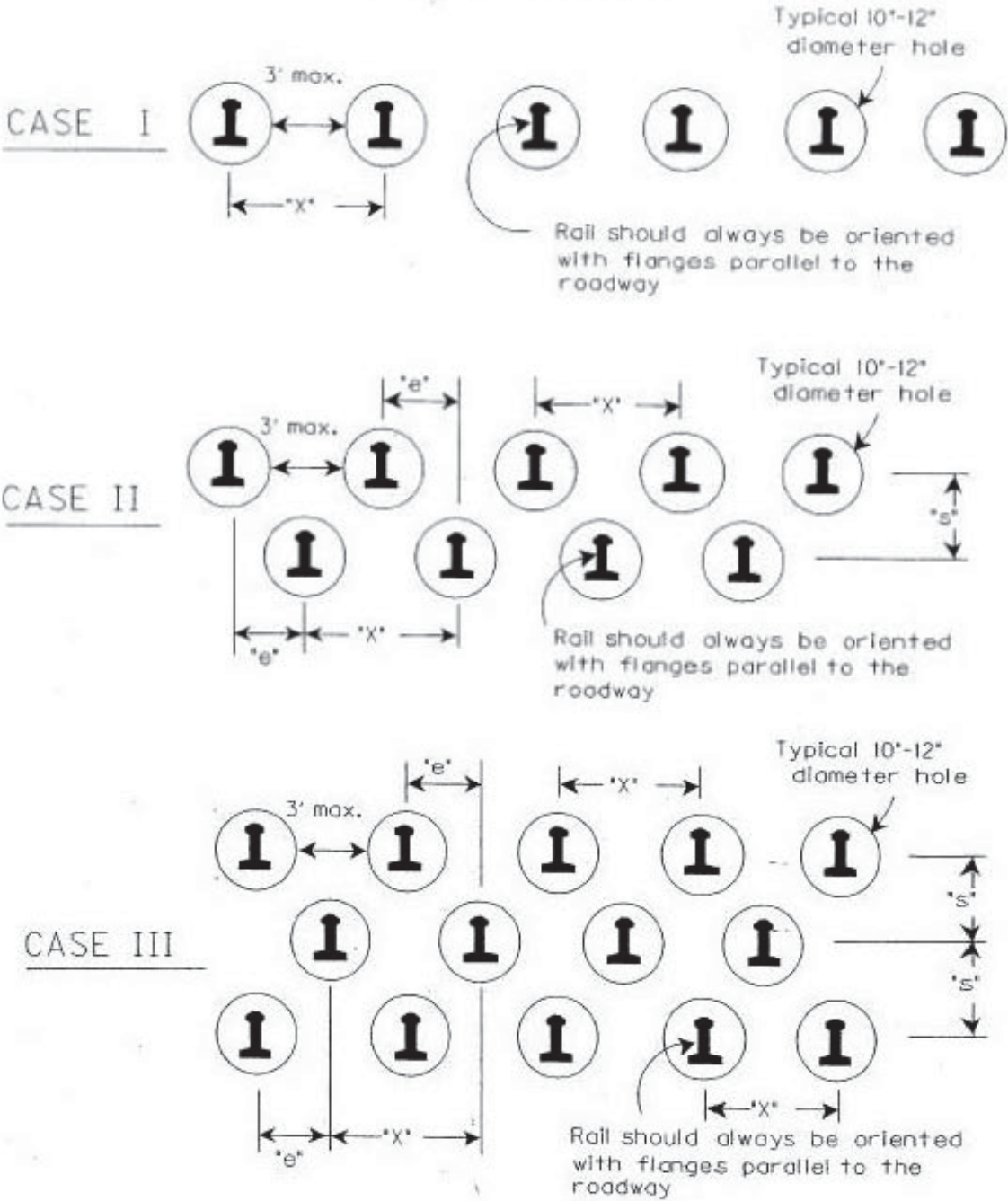


FIGURE 3

DESIGN CHART FOR 130LBS/YD TO 133 LBS/YD RECYCLED (USED) RAILROAD RAILS  
FACTOR OF SAFETY = 1

Soil Depth to Bedded Rock "H" (Feet)	Minimum Embedment into Bedded Rock "H/2" (Feet)	Total Length of Installed Railroad Rail "Y" (Feet)	Required Number of Rows	Maximum Spacing Between Rails "X" (Max. 48") (Inches)	Effective Spacing Between Rows of Rails "e" (Inches)
8	4	12	1	48	N/A
9	4.5	13.5	1	48	N/A
10	5	15	1	48	N/A
11	5.5	16.5	1	48	N/A
12	6	18	1	48	N/A
13	6.5	19.5	1	48	N/A
14	7	21	1	32	N/A
15	7.5	22.5	2	48	24
16	8	24	2	44	22
17	8.5	25.5	2	36	18
18	9	27	2	28	14
19	9.5	28.5	2	24	12
20	10	30	3	33	11
21	10.5	31.5	3	28.5	9.5
>21	N/A	N/A	N/A	N/A	N/A

- NOTES:
- 1. REFER TO FIGURES 1, 2, & 3 FOR DIMENSIONS SHOWN
  - 2. FOR SOIL DEPTHS "H" GREATER THAN 21 FEET CONTACT THE ENGINEER.

TABLE I

# IDENTIFICATION OF RAILROAD RAIL SIZES

- 1. Typically classified in units of lbs-per-yard.  
Examples :  
155 lbs/yd, 140 lbs/yd, 132 lbs/yd, 90 lbs/yd
- 2. Each rail has a classification stamped in web:  
Example :  
112 25 RE OH ILLINOIS USA 1935 IIIII  
↑  
Weight in lbs/yd

## **PART II**

### **SPECIFICATIONS AND STANDARD DRAWINGS**

### **STANDARD SPECIFICATIONS**

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2019* and *Standard Drawings, Edition of 2020*.

### **SUPPLEMENTAL SPECIFICATIONS**

The contractor shall use the Supplemental Specifications that are effective at the time of letting. The Supplemental Specifications can be found at the following link:  
<http://transportation.ky.gov/Construction/Pages/Kentucky-Standard-Specifications.aspx>



2020 KENTUCKY STANDARD DRAWINGS

---

CURVE WIDENING AND SUPERELEVATION TRANSITIONS .....	RGS-001-07
SUPERELEVATION FOR MULTILANE PAVEMENT .....	RGS-002-06
MISCELLANEOUS STANDARDS .....	RGX-001-06
APPROACHES, ENTRANCES, AND MAIL BOX TURNOUT .....	RPM-110-07
LANE CLOSURE TWO-LANE HIGHWAY .....	TTC-100-05
SHOULDER CLOSURE .....	TTC-135-03
PAVEMENT CONDITION WARNING SIGNS.....	TTD-125-02
MOBILE OPERATION FOR PAINT STRIPING CASE I .....	TTS-100-02
MOBILE OPERATION FOR PAINT STRIPING CASE II.....	TTS-105-02
TYPICAL GUARDRAIL INSTALLATIONS .....	RBI-001-12
TYPICAL GUARDRAIL INSTALLATIONS .....	RBI-002-07
STEEL BEAM GUARDRAIL (“W”-BEAM).....	RBR-001-13
INSTALLATION OF GUARDRAIL END TREATMENT TYPE 1 .....	RBI-004-06
GUARDRAIL END TREATMENT TYPE 7 .....	RBR-050-08
DELINEATORS FOR GUARDRAIL.....	RBR-055-01



TWO LANE ROADWAY  
PAVEMENT CROSS-SECTION

TRAVELED WAY	TYPE OF PAVEMENT STRIPING	NON-STATE PRIMARY ROUTES			STATE PRIMARY ROUTES	
		< 1000 ADT		>= 1000 ADT	ANY ADT	
		WIDTH	MATERIAL	WIDTH	MATERIAL	MATERIAL*
< 16' ④	EDGE LINE STRIPES ONLY	4"	PAINT	4"	PAINT	THERMO (ASHPALT) TYPE I TAPE (CONCRETE)
16' TO < 20'	EDGE LINE STRIPES ONLY OR CENTERLINE STRIPE ONLY	4"	PAINT	4"	PAINT	THERMO (ASHPALT) TYPE I TAPE (CONCRETE)
>=20' ③	CENTERLINE AND EDGE LINE STRIPES	4" ⑤	PAINT	6"	PAINT	THERMO (ASHPALT) TYPE I TAPE (CONCRETE)

\*OTHER DURABLE NON-WATERBORNE MARKINGS MAY BE USED WITH APPROVAL FROM THE DIVISION OF TRAFFIC OPERATIONS.


~ NOTES ~

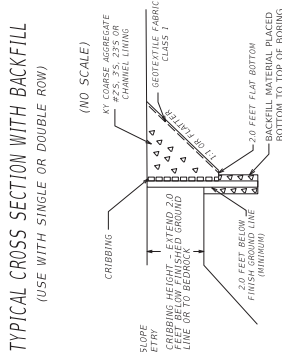
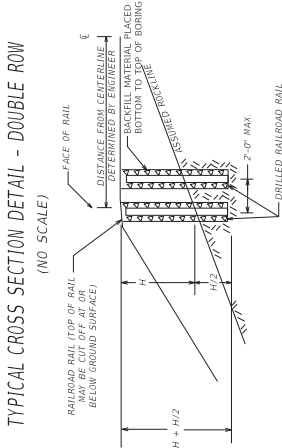
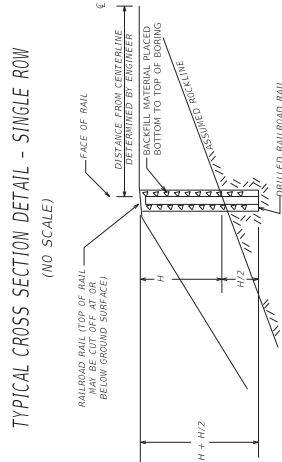
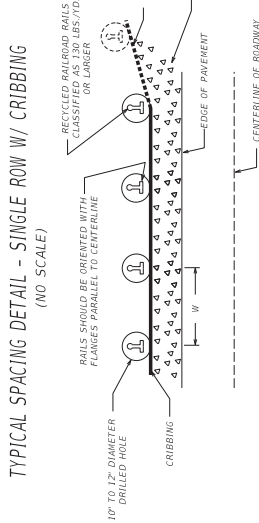
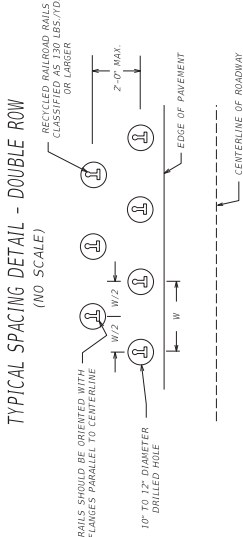
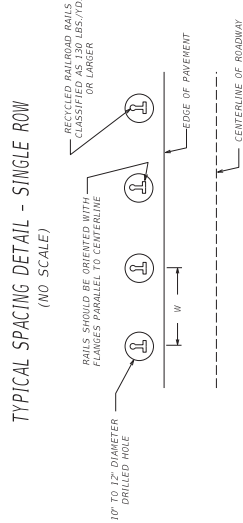
1. INSTALL PAVEMENT STRIPING ON TWO LANE, TWO WAY ROADWAYS AS DETAILED IN THE ABOVE TABLE AND IN ACCORDANCE WITH THE PAVEMENT MARKINGS AND DELINEATION CHAPTER OF THE TRAFFIC OPERATIONS GUIDANCE MANUAL. CONTACT THE DIVISION OF TRAFFIC OPERATIONS FOR ADDITIONAL GUIDANCE IF NECESSARY.
- ② THE TRAVELED WAY IS THE PORTION OF ROADWAY FOR THE MOVEMENT OF VEHICLES, EXCLUSIVE OF THE SHOULDERS.
- ③ ON TWO LANE, TWO WAY ROADWAYS THAT HAVE A TOTAL PAVEMENT WIDTH (W) THAT IS 20 FT OR GREATER, BUT LESS THAN 22 FT, EDGE LINE RUMBLE STRIPS ARE NOT A STANDARD APPLICATION, BUT THEY MAY BE INSTALLED. THE DIVISION OF TRAFFIC OPERATIONS IS AVAILABLE TO ASSIST WITH THE DETERMINATION OF WHETHER OR NOT TO INSTALL EDGE LINE RUMBLE STRIPS ON PAVEMENT WIDTHS LESS THAN 22 FT, AS WELL AS THE DIMENSION AND PLACEMENT DETAILS OF THE RUMBLE STRIPS AND PAVEMENT STRIPING.
- ON TWO LANE, TWO WAY ROADWAYS THAT HAVE A TOTAL PAVEMENT WIDTH (W) THAT IS 22 FT OR GREATER, BUT LESS THAN 34 FT, INSTALL PAVEMENT STRIPING AS DETAILED IN THE ABOVE TABLE AND IN CONJUNCTION WITH CENTERLINE AND EDGE LINE RUMBLE STRIPS AS DETAILED ON [TPR-120](#).
- ON TWO LANE, TWO WAY ROADWAYS THAT HAVE A TOTAL PAVEMENT WIDTH (W) THAT IS 34 FT OR GREATER, INSTALL PAVEMENT STRIPING AS DETAILED IN THE ABOVE TABLE AND IN CONJUNCTION WITH CENTERLINE AND SHOULDER RUMBLE STRIPS AS DETAILED ON [TPR-125](#).
- ④ EDGE LINES MAY BE OMITTED FROM ROADWAYS WITH A TRAVELED WAY WIDTH LESS THAN 16 FEET WITH THE APPROVAL OF THE DIVISION OF TRAFFIC OPERATIONS.
- ⑤ EDGE LINES MAY BE OMITTED ON NON-STATE PRIMARY ROUTES WITH A TRAVELED WAY WIDTH GREATER THAN OR EQUAL TO 20 FEET AND AN ADT LESS THAN 1,000.
6. EDGE LINES MAY BE OMITTED, BASED ON ENGINEERING JUDGMENT, IN AREAS WHERE THE PAVEMENT EDGE IS DELINEATED BY PHYSICAL OBJECTS SUCH AS CURBS, PARKING SPACES, OR OTHER MARKINGS. EDGE LINES SHOULD BE INSTALLED ON ROADWAYS WITH CURB AND GUTTER IF THE POSTED SPEED LIMIT IS 45 MPH OR GREATER.

DRAWING NOT TO SCALE  
USE WITH CUR. STD. DWGS.  
[TPR-120](#) & [TPR-125](#)

KENTUCKY  
DEPARTMENT OF HIGHWAYS

PAVEMENT STRIPING  
DETAILS FOR TWO LANE  
TWO WAY ROADWAYS

SUBMITTED  06-09-21  
DIVISION DIRECTOR DATE  
017



DEPTH TO ROCKLINE (H)	RAIL SPACING (W)	
	SINGLE ROW	DOUBLE ROW
<8 FT	4 FT	NA
8 - 12 FT	3 FT	NA
12 - 15 FT	2-3 FT*	3 FT
15 - 21 FT	NA	2-3 FT*
>21 FT	SPECIAL DESIGN REQUIREMENTS	

\* 2 FEET SPACING PREFERRED - CAN EXTEND UP TO 3 FEET PROVIDED DEEPER RAILS (UP TO 40 FEET LONG) ARE USED

GEOTECHNICAL NOTES

- H IS THE HEIGHT OF MATERIAL FROM ROCKLINE TO ROADWAY GRADE
- MINIMUM EMBEDMENT INTO BEDROCK IS H/2 OR 1/3 OF TOTAL LENGTH OF THE INSTALLED RAIL. THESE LENGTHS MAY BE EXCEEDED AT THE FIELD ENGINEER'S DISCRETION DUE TO FIELD OR MATERIAL CONSIDERATIONS BUT SHALL NOT BE REDUCED.
- BACKFILL IS TO BE DEGRAVEL NO. 8 OR 9 CRUSHED. BACKFILL SHOULD BE ELONGATED, CLEAN, AND FREE OF FILL OF BORINGS SHALL NOT BE OMITTED BY CONTRACTOR/ENGINEER AND SHALL NOT BE SOIL CUTTINGS FROM THE BORING.
- BACKFILL SHALL BE PLACED FROM THE BOTTOM TO THE TOP OF THE BORING
- BACKFILL GRAVEL OR CRUSHED STONE) IS TO BE SHOVELED INTO BORE HOLE TO THE TOP OF THE BORING TO PREVENT VOIDS
- RAIL IS TO BE INSTALLED PERPENDICULAR TO THE ROADWAY WITH WIDE FLANGE FACING ROADWAY
- SPACING BETWEEN ROWS IS TO BE 2.0 FEET
- CRIBBING SHALL BE MINIMUM 2.0 FEET BELOW FINISHED GROUND LINE OR TO BEDROCK BASED ON LOCAL CONDITIONS
- BACKFILL BEHIND THE WALL IS TO BE 2'S, 2'SS, 3'S, OR CHANNEL LINING
- ALL RAILS SHALL BE MINIMUM OF 130 LB/YD UNIT WEIGHT
- CRIBBING SHALL CONSIST OF USED W BEAM GUARDRAIL SECTIONS, CONCRETE PANELS, OR OTHER MATERIALS SPECIFIED BY THE ENGINEER
- RAILS 40.0 FEET IN LENGTH MAY BE REQUIRED WHEN LOCAL CONDITIONS WARRANT SUCH AS WHEN SPACING BETWEEN RAILS HAS TO BE WIDENED MORE THAN RECOMMENDED OR WHEN LOCALIZED, SMALL SECTIONS OF THE REPAIR EXCEED 21.0 FEET DEPTH TO BEDROCK.
- OPTIONAL: TIE CRIBBING INTO EMBANKMENT SLOPE OR HILLSIDE AS PERMITTED BY FIELD GEOMETRY (BOTH SIDES)

SUBMITTED Mahel Crest 09/18/23  
DIVISION DIRECTOR DATE

DRAWING TITLE: SEPIA 031 - STANDARD RAIL AND CRIBBING INSTALL



FILE NAME: C:\P\WORK\KTC\COMMON\ENGINE\RD27626\SEPA\N01.DSN

USER: collinsd@go

OpenRoads Designer (10.12.02.4)

ITEM NO.	COUNTY OF
SHEET NO.	



TWO LANE ROADWAY  
PAVEMENT CROSS-SECTION

Traveled Way	Type of Pavement Striping	Non-State Primary Routes				State Primary Routes		
		< 1000 ADT		>= 1000 ADT		Any ADT		
16' to < 20'	Edgeline Stripes Only or Centerline Stripe Only	Width	Material	Width	Material	Width	Material*	
		4"	PAINT	4"	PAINT	6"	THERMO (ASPHALT) TYPE I TAPE (CONCRETE)	
		4"	PAINT	4"	PAINT	6"	THERMO (ASPHALT) TYPE I TAPE (CONCRETE)	
>= 20'	Centerline and Edgeline Stripes	4"	PAINT	6"	PAINT	6"	THERMO (ASPHALT) TYPE I TAPE (CONCRETE)	
		4"	PAINT	6"	PAINT	6"	THERMO (ASPHALT) TYPE I TAPE (CONCRETE)	

\*OTHER DURABLE NON-WATERBORNE MARKINGS MAY BE USED WITH APPROVAL FROM THE DIVISION OF TRAFFIC OPERATIONS.

~ NOTES ~

1. INSTALL PAVEMENT STRIPING ON TWO LANE, TWO WAY ROADWAYS AS DETAILED IN THE ABOVE TABLE AND IN ACCORDANCE WITH THE PAVEMENT MARKINGS AND DELINEATION CHAPTER OF THE TRAFFIC OPERATIONS GUIDANCE MANUAL. CONTACT THE DIVISION OF TRAFFIC OPERATIONS FOR ADDITIONAL GUIDANCE IF NECESSARY.
2. THE TRAVELED WAY IS THE PORTION OF ROADWAY FOR THE MOVEMENT OF VEHICLES, EXCLUSIVE OF THE SHOULDERS.
3. ON TWO LANE, TWO WAY ROADWAYS THAT HAVE A TOTAL PAVEMENT WIDTH (W) THAT IS 20 FT OR GREATER, BUT LESS THAN 22 FT, EDGELINE RUMBLE STRIPS ARE AN OPTIONAL APPLICATION. THE DIVISION OF TRAFFIC OPERATIONS IS AVAILABLE TO ASSIST WITH THE DETERMINATION OF WHETHER OR NOT TO INSTALL EDGELINE RUMBLE STRIPS ON PAVEMENT WIDTHS LESS THAN 22 FT.
- ON TWO LANE, TWO WAY ROADWAYS THAT HAVE A TOTAL PAVEMENT WIDTH (W) THAT IS 22 FT OR GREATER, INSTALL PAVEMENT STRIPING AS DETAILED IN THE ABOVE TABLE AND IN CONJUNCTION WITH THE RUMBLE STRIPS AS DETAILED ON TPR-120 AND TPR-120N.
4. EDGELINES MAY BE OMITTED FROM ROADWAYS WITH A TRAVELED WAY WIDTH LESS THAN 16 FEET WITH THE APPROVAL OF THE DIVISION OF TRAFFIC OPERATIONS.
5. EDGELINES MAY BE OMITTED ON NON-STATE PRIMARY ROUTES WITH A TRAVELED WAY WIDTH GREATER THAN OR EQUAL TO 20 FEET AND AN ADT LESS THAN 1,000.
6. EDGELINES MAY BE OMITTED, BASED ON ENGINEERING JUDGMENT, IN AREAS WHERE THE PAVEMENT EDGE IS DELINEATED BY PHYSICAL OBJECTS SUCH AS CURBS, PARKING SPACES, OR OTHER MARKINGS. EDGELINES SHOULD BE INSTALLED ON ROADWAYS WITH CURB AND GUTTER IF THE POSTED SPEED LIMIT IS 45 MPH OR GREATER.

DRAWING NOT TO SCALE  
USE WITH CUR STD. DWGS.  
TPR-120 & TPR-120N

SUBMITTED:  09/28/2023  
WILLIAM B. RINGER



OpenRoads Designer v10.16.2.267

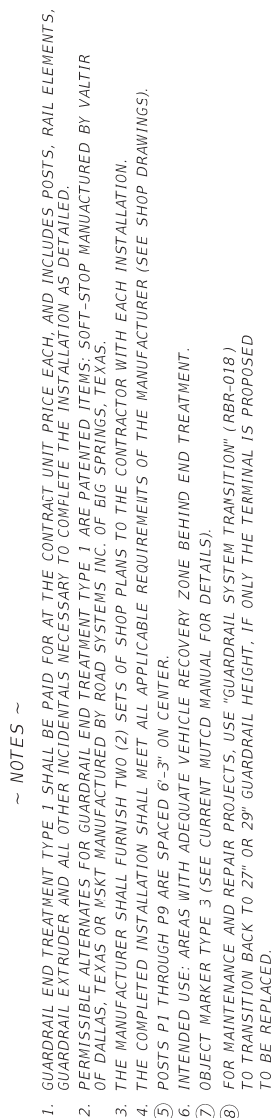
DATE PLOTTED: 8/1/2015 9:53:17 AM

FILE NAME: C:\PWORK\KVC\_C\WILLBINGER\020445\SEP0803.DGN

USER: willb-ringer

DRAWING TITLE: SEPIA 032 - PAVEMENT STRIPING DETAILS FOR TWO LANE TWO WAY ROADWAYS

ITEM NO.  
COUNTY OF  
SHEET NO.



SUBMITTED W. J. Lysen 05-03-2024  
DIVISION DIRECTOR /RE



COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS

TEAM KENTUCKY  
TRANSPORTATION

DRAWING TITLE: SEPIA 034 - GUARDRAIL END TREATMENT TYPE 1

FILE NAME: C:\PW\WORK\CORRINE SCHIRMAN\022419221\SEPIA034.DGN

LISER: corinne schürmann

ITEM NO.	COUNTY OF
SHEET NO.	

## **PART III**

### **EMPLOYMENT, WAGE AND RECORD REQUIREMENTS**

**TRANSPORTATION CABINET  
DEPARTMENT OF HIGHWAYS**

**LABOR AND WAGE REQUIREMENTS  
APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS**

- I. Application
- II. Nondiscrimination of Employees (KRS 344)

**I. APPLICATION**

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual because of his race, color, religion, national origin, sex, disability or age forty (40) and over, in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

Revised: January 25, 2017

**II. NONDISCRIMINATION OF EMPLOYEES**

**AN ACT OF THE KENTUCKY  
GENERAL ASSEMBLY TO PREVENT  
DISCRIMINATION IN EMPLOYMENT  
KRS CHAPTER 344  
EFFECTIVE JUNE 16, 1972**

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (forty and above); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age forty (40) and over. The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, except that such a notice or advertisement may indicate a preference, limitation, or specification based on religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, when religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, is a bona fide occupational qualification for employment.

## EXECUTIVE BRANCH CODE OF ETHICS

The Executive Branch Code of Ethics created by Kentucky Revised Statutes (KRS) Chapter 11A, effective July 14, 1992, establishes the ethical standards that govern the conduct of all executive branch employees. The Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (7) provides:

A present or former public servant listed in KRS 11A.010(9)(a) to (g) shall not, within one (1) year following termination of his or her office or employment, accept employment, compensation, or other economic benefit from any person or business that contracts or does business with, or is regulated by, the state in matters in which he was directly involved during the last thirty-six (36) months of his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, or for which he received, prior to his state employment, a professional degree or license, provided that, for a period of one (1) year, he or she personally refrains from working on any matter in which he was directly involved during the last thirty-six (36) months of his or her tenure in state government. This subsection shall not prohibit the performance of ministerial functions, including but not limited to filing tax returns, filing applications for permits or licenses, or filing incorporation papers, nor shall it prohibit the former officer or public servant from receiving public funds disbursed through entitlement programs.

KRS 11A.040 (9) states:

A former public servant shall not represent a person or business before a state agency in a matter in which the former public servant was directly involved during the last thirty-six (36) months of his tenure, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not to obtain private benefits.

If you have worked for the executive branch of state government within the past year, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, 1025 Capital Center Drive, Suite 105, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Revised: March 11, 2025



### **Kentucky Equal Employment Opportunity Act of 1978**

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:  
**<https://www.eProcurement.ky.gov>**.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **[finance.contractcompliance@ky.gov](mailto:finance.contractcompliance@ky.gov)** or by phone at 502-564-2874.

# EMPLOYEE RIGHTS UNDER THE FAIR LABOR STANDARDS ACT

THE UNITED STATES DEPARTMENT OF LABOR WAGE AND HOUR DIVISION

## FEDERAL MINIMUM WAGE

**\$7.25** PER HOUR

BEGINNING JULY 24, 2009

### OVERTIME PAY

At least 1½ times your regular rate of pay for all hours worked over 40 in a workweek.

### CHILD LABOR

An employee must be at least **16** years old to work in most non-farm jobs and at least **18** to work in non-farm jobs declared hazardous by the Secretary of Labor.

Youths **14** and **15** years old may work outside school hours in various non-manufacturing, non-mining, non-hazardous jobs under the following conditions:

**No more than**

- **3** hours on a school day or **18** hours in a school week;
- **8** hours on a non-school day or **40** hours in a non-school week.

Also, work may not begin before **7 a.m.** or end after **7 p.m.**, except from June 1 through Labor Day, when evening hours are extended to **9 p.m.** Different rules apply in agricultural employment.

### TIP CREDIT

Employers of “tipped employees” must pay a cash wage of at least \$2.13 per hour if they claim a tip credit against their minimum wage obligation. If an employee’s tips combined with the employer’s cash wage of at least \$2.13 per hour do not equal the minimum hourly wage, the employer must make up the difference. Certain other conditions must also be met.

### ENFORCEMENT

The Department of Labor may recover back wages either administratively or through court action, for the employees that have been underpaid in violation of the law. Violations may result in civil or criminal action.

Employers may be assessed civil money penalties of up to \$1,100 for each willful or repeated violation of the minimum wage or overtime pay provisions of the law and up to \$11,000 for each employee who is the subject of a violation of the Act’s child labor provisions. In addition, a civil money penalty of up to \$50,000 may be assessed for each child labor violation that causes the death or serious injury of any minor employee, and such assessments may be doubled, up to \$100,000, when the violations are determined to be willful or repeated. The law also prohibits discriminating against or discharging workers who file a complaint or participate in any proceeding under the Act.

### ADDITIONAL INFORMATION

- Certain occupations and establishments are exempt from the minimum wage and/or overtime pay provisions.
- Special provisions apply to workers in American Samoa and the Commonwealth of the Northern Mariana Islands.
- Some state laws provide greater employee protections; employers must comply with both.
- The law requires employers to display this poster where employees can readily see it.
- Employees under 20 years of age may be paid \$4.25 per hour during their first 90 consecutive calendar days of employment with an employer.
- Certain full-time students, student learners, apprentices, and workers with disabilities may be paid less than the minimum wage under special certificates issued by the Department of Labor.

For additional information:



**1-866-4-USWAGE**

(1-866-487-9243)

TTY: 1-877-889-5627



**WWW.WAGEHOUR.DOL.GOV**

**PART IV**

**BID ITEMS**

252326

Section: 0001 - ROADWAY

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0010	00001		DGA BASE	400.00	TON		\$	
0020	00078		CRUSHED AGGREGATE SIZE NO 2	4,514.00	TON		\$	
0030	00190		LEVELING & WEDGING PG64-22	205.00	TON		\$	
0040	00212		CL2 ASPH BASE 1.00D PG64-22	415.00	TON		\$	
0050	00301		CL2 ASPH SURF 0.38D PG64-22	400.00	TON		\$	
0070	01987		DELINEATOR FOR GUARDRAIL BI DIRECTIONAL WHITE	65.00	EACH		\$	
0080	02200		ROADWAY EXCAVATION	3,009.00	CUYD		\$	
0090	02351		GUARDRAIL-STEEL W BEAM-S FACE	2,975.00	LF		\$	
0100	02367		GUARDRAIL END TREATMENT TYPE 1	2.00	EACH		\$	
0110	02371		GUARDRAIL END TREATMENT TYPE 7	10.00	EACH		\$	
0120	02562		TEMPORARY SIGNS	400.00	SQFT		\$	
0130	02602		FABRIC-GEOTEXTILE CLASS 1	1,633.00	SQYD		\$	
0140	02650		MAINTAIN & CONTROL TRAFFIC (KY 57 LEWIS COUNTY 010-011)	1.00	LS		\$	
0150	02650		MAINTAIN & CONTROL TRAFFIC (KY 57 LEWIS COUNTY 013-015)	1.00	LS		\$	
0160	02650		MAINTAIN & CONTROL TRAFFIC (KY 8 LEWIS COUNTY)	1.00	LS		\$	
0170	02650		MAINTAIN & CONTROL TRAFFIC (US 60 CARTER COUNTY)	1.00	LS		\$	
0180	02676		MOBILIZATION FOR MILL & TEXT (KY 57 LEWIS COUNTY 010-011)	1.00	LS		\$	
0190	02676		MOBILIZATION FOR MILL & TEXT (KY 57 LEWIS COUNTY 013-015)	1.00	LS		\$	
0200	02676		MOBILIZATION FOR MILL & TEXT (KY 8 LEWIS COUNTY)	1.00	LS		\$	
0210	02676		MOBILIZATION FOR MILL & TEXT (US 60 CARTER COUNTY)	1.00	LS		\$	
0220	02677		ASPHALT PAVE MILLING & TEXTURING	450.00	TON		\$	
0230	03234		RAILROAD RAILS-DRILLED	12,560.00	LF		\$	
0240	03236		CRIBBING	14,090.00	SQFT		\$	
0250	06514		PAVE STRIPING-PERM PAINT-4 IN	6,900.00	LF		\$	
0260	26248EC		ELECTRONIC DELIVERY MGMT SYSTEM - AGG (KY 57 MP 010-011)	1.00	LS		\$	
0270	26248EC		ELECTRONIC DELIVERY MGMT SYSTEM - AGG (KY 57 MP 013-015)	1.00	LS		\$	
0280	26248EC		ELECTRONIC DELIVERY MGMT SYSTEM - AGG (KY 8)	1.00	LS		\$	
0290	26248EC		ELECTRONIC DELIVERY MGMT SYSTEM - AGG (US 60)	1.00	LS		\$	

Section: 0002 - DEMOBILIZATION

Report Date 7/22/25

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0300	02569		DEMOBILIZATION	1.00	LS		\$	