



**CALL NO. 405**

**CONTRACT ID. 192317**

**CAMPBELL COUNTY**

**FED/STATE PROJECT NUMBER 019GR19062-FD05**

**DESCRIPTION KY 8 AND KY 547**

**WORK TYPE ASPHALT RESURFACING**

**PRIMARY COMPLETION DATE 7/31/2020**

**LETTING DATE: October 25,2019**

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME October 25,2019. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

**NO PLANS ASSOCIATED WITH THIS PROJECT.**

**REQUIRED BID PROPOSAL GUARANTY:** Not less than 5% of the total bid.

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**PART I**  
**SCOPE OF WORK**

## ADMINISTRATIVE DISTRICT - 06

**CONTRACT ID - 192317**

**019GR19062-FD05**

**COUNTY - CAMPBELL**

**PCN - MP01900081901**

**FD05 019 0008 004-007**

MARY INGLES HIGHWAY (KY 8) (MP 4.753) FROM MAPLE STREET IN SILVER GROVE EXTENDING EAST TO PAVEMENT JOINT 0.271 MILES WEST OF 8 MILE ROAD (MP 6.754), A DISTANCE OF 02.00 MILES.ASPHALT RESURFACING

GEOGRAPHIC COORDINATES LATITUDE 39:01:46.00 LONGITUDE 84:21:46.00

**PCN - MP01900081902**

**FD05 019 0008 009-014**

MARY INGLES HIGHWAY (KY 8) (MP 9.770) FROM PAVEMENT JOINT 80 FEET WEST FENDER ROAD EXTENDING EAST TO NORTH SIDE OF INTERSECTION WITH KY 2921 (MP 13.181), A DISTANCE OF 03.41 MILES.ASPHALT RESURFACING

GEOGRAPHIC COORDINATES LATITUDE 38:56:31.00 LONGITUDE 84:18:12.00

**PCN - MP01905471901**

**FD05 019 0547 000-007**

FOUR MILE ROAD (KY 547) (MP 0.000) FROM KY 10 IN ALEXANDRIA EXTENDING NORTH TO KY 8 IN SILVER GROVE (MP 6.517), A DISTANCE OF 06.52 MILES.ASPHALT RESURFACING

GEOGRAPHIC COORDINATES LATITUDE 38:59:48.00 LONGITUDE 84:21:47.00

### COMPLETION DATE(S):

COMPLETED BY 07/31/2020

SPECIFIED COMPLETION DATE -  
ALL ITEMS IN CONTRACT

## **CONTRACT NOTES**

### **PROPOSAL ADDENDA**

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

### **BID SUBMITTAL**

Bidder must use the Department's electronic bidding software. The Bidder must download the bid file located on the Bid Express website ([www.bidx.com](http://www.bidx.com)) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

### **JOINT VENTURE BIDDING**

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

### **UNDERGROUND FACILITY DAMAGE PROTECTION**

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. When prescribed in said directives, the contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom shall be contacted through their individual Protection Notification Center. Non-compliance with these directives can result in the enforcement of penalties.

### **REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY**

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth ("certificate") from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

**For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity's solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.**

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

### **SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT**

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by fax (502) 564-7299 or email to [kytc.projectquestions@ky.gov](mailto:kytc.projectquestions@ky.gov). The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading "Questions & Answers" on the Construction Procurement website ([www.transportation.ky.gov/contract](http://www.transportation.ky.gov/contract)). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

### **HARDWOOD REMOVAL RESTRICTIONS**

The US Department of Agriculture has imposed a quarantine in Kentucky and several surrounding states, to prevent the spread of an invasive insect, the emerald ash borer. Hardwood cut in conjunction with the project may not be removed from the state. Chipping or burning on site is the preferred method of disposal.

### **INSTRUCTIONS FOR EXCESS MATERIAL SITES AND BORROW SITES**

Identification of excess material sites and borrow sites shall be the responsibility of the Contractor. The Contractor shall be responsible for compliance with all applicable state and federal laws and may wish to consult with the US Fish and Wildlife Service to seek protection under Section 10 of the Endangered Species Act for these activities.

### **ACCESS TO RECORDS**

The contractor, as defined in KRS 45A.030 (9) agrees that the contracting agency, the Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this contract for the purpose of financial audit or program review. Records and other prequalification information confidentially

disclosed as part of the bid process shall not be deemed as directly pertinent to the contract and shall be exempt from disclosure as provided in KRS 61.878(1)(c). The contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884.

In the event of a dispute between the contractor and the contracting agency, Attorney General, or the Auditor of Public Accounts over documents that are eligible for production and review, the Finance and Administration Cabinet shall review the dispute and issue a determination, in accordance with Secretary's Order 11-004.

April 30, 2018

## **SPECIAL NOTE FOR RECIPROCAL PREFERENCE**

### **RECIPROCAL PREFERENCE TO BE GIVEN BY PUBLIC AGENCIES TO RESIDENT BIDDERS**

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the electronic bidding software. Submittal of the Affidavit should be done along the bid in Bid Express.

April 30, 2018



#### **SURFACING AREAS FD05 019 0008 004-007**

The Department estimates the mainline surfacing width to be 20 feet.

The Department estimates the total mainline area to be surfaced to be 24,650 square yards.

The Department estimates the shoulder width to be 1 foot on each side.

The Department estimates the total shoulder area to be surfaced to be 2,350 square yards.

#### **SURFACING AREAS FD05 019 0008 009-014**

The Department estimates the mainline surfacing width to be 20 feet.

The Department estimates the total mainline area to be surfaced to be 42,095 square yards.

The Department estimates the shoulder width to be 1 foot on each side.

The Department estimates the total shoulder area to be surfaced to be 4,000 square yards.

#### **SURFACING AREAS FD05 019 0547 000-007**

The Department estimates the mainline surfacing width to be 20 feet.

The Department estimates the total mainline area to be surfaced to be 85,120 square yards.

The Department estimates the shoulder width to be 1 foot on each side.

The Department estimates the total shoulder area to be surfaced to be 7,390 square yards. Portions of the project have curb and gutter with no shoulders.

#### **ASPHALT MIXTURE**

Unless otherwise noted, the Department estimates the rate of application for all asphalt mixtures to be 110 lbs/sy per inch of depth.

#### **INCIDENTAL SURFACING**

The Department has included in the quantities of asphalt mixtures established in the proposal estimated quantities required for resurfacing or surfacing mailbox turnouts, farm field entrances, residential and commercial entrances, curve widening, ramp gores and tapers, and road and street approaches, as applicable. Pave these areas to the limits as shown on Standard Drawing RPM-110-07 or as directed by the Engineer. In the event signal detectors are present in the intersecting streets or roads, pave the crossroads to the right of way limit or back of the signal detector, whichever is the farthest back of the mainline. Surface or resurface these areas as directed by the Engineer. The Department will not measure placing and compacting for separate payment but shall be incidental to the Contract unit price for the asphalt mixtures.

#### **FUEL AND ASPHALT PAY ADJUSTMENT**

The Department has included the Contract items Asphalt Adjustment and Fuel Adjustment for possible future payments at an established Contract unit price of \$1.00. The Department will calculate actual adjustment quantities after work is completed. If existing Contract amount is insufficient to pay all items on the contract with the adjustments, the Department will establish additional monies with a change order.

#### **OPTION B**

Be advised that the Department will control and accept compaction of asphalt mixtures furnished on these projects under OPTION B in accordance with Sections 402 and 403.

## SPECIAL NOTE FOR EXPERIMENTAL KYCT AND HAMBURG TESTING

FD05 019 0547 000-007

FD05 019 0008 004-007

FD05 019 0008 009-014

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### 1.0 General

**1.1 Description.** The KYCT (Kentucky Method for Cracking Test) and the Hamburg test results will help determine if the mixture is susceptible to cracking and rutting. During the experimental phase, data will be gathered and analyzed by the Department to determine the durability of the bituminous mixes. Additionally, the data will help the Department to create future performance based specifications which will include the KYCT and Hamburg test methods.

### 2.0 Equipment

**2.1 KYCT Testing Equipment.** The Department will require a Marshall Test Press with digital recordation capabilities. Other CT testing equipment may be used for testing with prior approval by the Department.

**2.2 Water Baths.** One or more water baths will be required that can maintain a temperature of 77• +/- 1.8° F with a digital thermometer showing the water bath temperature. Also, one water bath shall have the ability to suspend gyratory specimen fully submerged in water in accordance with AASHTO T-166, current edition.

**2.3 Hamburg Wheel Track Testing.** The department encourages the use of the PTI APA/Hamburg Jr. test equipment to perform the loaded wheel testing. The Department will allow different equipment for the Hamburg testing, but the testing device must be approved by the Department prior to testing.

**2.4 Gyratory Molds.** Gyratory molds will be required to assist in the production of gyratory specimens in accordance with AASHTO T-312, current edition.

**2.5 Ovens.** Adequate (minimum of two ovens) will be required to accommodate the additional molds and asphalt mixture necessary to perform the acceptance testing as outlined in Section 402 of the Kentucky Standard Specifications for Road and Bridge Construction, current edition.

**2.6 Department Equipment.** The Department will provide gyratory molds, PINE 850 Test Press with digital recordation, and CT testing equipment to assist during this experimental phase so data can be gathered. Hamburg test specimens will be submitted to the Division of Materials for testing on the PTI APA/Hamburg Jr if the asphalt contractor or district materials office does not have an approved Hamburg testing device.

### 3.0 Testing Requirements

**3.1 Acceptance Testing.** Perform all acceptance testing and aggregate gradation as according with Section 402 and Section 403 of the Kentucky Standard Specifications for Road and Bridge Construction, current edition.

**3.2 KYCT Testing.** Perform crack resistance analysis (KYCT) in accordance with the current Kentucky Method for KYCT Index Testing during the mix design phase and during the plant production of all surface mixtures. For mix design approvals, submit KYCT results on the Department MixPack. For Class 4 mixtures, submit ingredient materials to the Division of Materials for informational verification.

**3.2.1 KYCT Frequency.** Obtain an adequate sample of hot mix asphalt to insure the acceptance testing, gradation, and KYCT gyratory samples can be fabricated and is representative of the bituminous mixture. Acceptance specimens shall be fabricated first, then immediately after, fabricate the KYCT samples with the gyratory compactor in accordance with Section 2.4 of this Special Note. Analysis of the KYCT specimens and gradation will be required one per subplot produced from the same asphalt material and at the same time as the acceptance specimen is sampled and tested.

**3.2.2 Number of Specimens and Conditioning.** Fabricate specimens in accordance with the Kentucky Method for KYCT Index Testing. Contrary to the method, fabricate a minimum of 3 and up to 6 test specimens. The specimens shall be compacted at the temperature in accordance to KM 64-411. KYCT mix design specimens shall be short-term aged conditioned for four hours at compaction temperature in accordance to KM 64-411. Plant produced bituminous material will not be required for age conditioning and shall be fabricated immediately after the gyratory acceptance specimens have been fabricated. An acceptable transport container will be required to prevent the asphalt mixture from losing heat and to maintain the compaction temperature of the asphalt mixture until the KYCT gyratory samples can be fabricated. This will eliminate reheating of the asphalt mixture. To insure confidence and reliability of the test results provided by KYCT testing and Hamburg testing, reheating of the asphalt mixture is strongly discouraged. If reheating does occur, provide documentation on the Asphalt Mixtures Acceptance Workbook (AMAW).

**3.2.3 Record Times.** For each subplot, record the time required between drying aggregates in the plant to KYCT specimen fabrication. The production time may vary due to the time that the bituminous material is held in the silo. Record the preconditioning time when the time exceeds the one hour specimen cool down time as required in accordance to The Kentucky Method for KYCT Index Testing. The preconditioning time may exceed an hour if the technician is unable to complete the test on the same day or within the specified times as outlined in The Kentucky Method for KYCT Index Testing. The production time and the preconditioning time shall be recorded on the AMAW.

**3.2.4 File Name.** As according to section 7.12 of The Kentucky Method for KYCT Index Testing, save the filename with the following format; "CID\_Approved Mix Number\_Lot Number\_Sublot Number\_Date"

**3.3 Hamburg Testing.** Perform the rut resistance analysis (Hamburg) in accordance to AASTHO T-324, not to exceed 20,000 passes for all bituminous mixtures during the mix design phase and production. For mix design approvals, submit Hamburg results on the Department MixPack. For Class 4 mixtures, submit ingredient materials to the Division of Materials for informational verification.

**3.3.1 Hamburg Testing Frequency.** Perform testing and analysis per lot of material. The plant produced bituminous material sampled for the Hamburg test does not have to be obtained at the same time as the acceptance and KYCT sample. If the Hamburg test sample is not obtained at the same time as the KYCT sample, determine the Maximum Specific Gravity of the KYCT sample in accordance with AASHTO T-209 coinciding with the Hamburg specimens.

**3.3.2 Record Times.** Record the production time as according to section 3.2.3 in this special note. Also record the time that the specimens were fabricated and the time the Hamburg testing was started. All times shall be recorded on the AMAW.

**3.3.3 File Name.** Save the Excel spreadsheet with the following file name; “Hamburg\_CID\_Approved Mix Number\_Lot Number\_Sublot Number\_Date” and upload the file into the AMAW.

#### **4.0 Data**

Submit the AMAW and all test data that was obtained for acceptance, gradation, KYCT, and Hamburg testing within five working days once all testing has been completed for a lot to Central Materials Lab and the District Materials Engineer. Also, any data and or comments that the asphalt contractor or district personnel deem informational during this experimental phase, shall also be submitted to the Central Materials Lab and the District Materials Engineer. Any questions or comments regarding any item in this Special Note can be directed to the Central Office, Division of Materials, Asphalt Branch.

#### **5.0 KYCT Video Demonstration**

<https://youtu.be/84j0bM45-hg>

#### **6.0 Payment**

Any additional labor and testing equipment that is required to fabricate and test the KYCT and Hamburg specimens shall be considered to be incidental to the asphalt surface line item. The Department will perform the testing for the KYCT and Hamburg specimens if a producer does not possess the proper equipment.

June 3, 2019

**SPECIAL NOTE FOR POLISH-RESISTANT AGGREGATE  
FD05 019 0547 000-007**

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Contrary to the **Warrants for Selecting Asphalt Mixtures and Compaction Options**, use Type B polish-resistant aggregate on KY 547; however Compaction Option B will still apply.

## **SPECIAL PROVISION FOR WASTE AND BORROW SITES**

Obtain U.S. Army Corps of Engineer's approval before utilizing a waste or borrow site that involves "Waters of the United States". The Corps of Engineers defines "Waters of the United States" as perennial or intermittent streams, ponds or wetlands. The Corps of Engineers also considers ephemeral streams, typically dry except during rainfall but having a defined drainage channel, to be jurisdictional waters. Direct questions concerning any potential impacts to "Waters of the United States" to the attention of the appropriate District Office for the Corps of Engineers for a determination prior to disturbance. Be responsible for any fees associated with obtaining approval for waste and borrow sites from the U.S. Army Corps of Engineer or other appropriate regulatory agencies.

1-296 Waste & Borrow Sites  
01/02/2012

## **COORDINATION OF WORK WITH OTHER CONTRACTS**

**FD05 019 0008 004-007**

**FD05 019 0008 009-014**

**FD05 019 0547 000-007**

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Be advised, there may be active project(s) adjacent to or within these projects. These may be KYTC administered contracts, work being performed as part of a KYTC issued encroachment permit, or work being performed by Department forces. The Engineer will coordinate the work of the Contractor and others within the limits of this project. See Sections 105.06, 107.06 and 107.14.

**SPECIAL NOTE FOR  
ASPHALT MILLING AND TEXTURING**

Begin paving operations within **48 hours** of commencement of the milling operation. Continue paving operations continuously until completed. If paving operations are not begun within this time period, the Department will assess liquidated damages at the rate prescribed by Section 108.09 until such time as paving operations are begun.

Take possession of the millings and recycle the millings or dispose of the millings off the Right-of-Way at sites obtained by the Contractor at no additional cost to the Department.

1-3520 48 hours Contractor keeps millings  
01/2/2012



## **SPECIAL NOTES FOR BASE FAILURE REPAIR**

**FD05 019 0008 0004-007**

**FD05 019 0008 009-014**

**FD05 019 0547 000-007**

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Consider the listed locations and dimensions of base failure repair to be approximate only. The Engineer will determine exact locations at the time of construction. Prior to resurfacing saw cut the existing pavement, asphalt surface, base, DGA, and PCC pavement (if present). Excavate approximately nine (9) inches below the existing pavement level. Remove and dispose of all materials. Use all possible care to avoid damaging existing culvert pipes and any existing underground utilities. Repair or restore any damaged items at no additional cost to the Department. Waste all removed materials off the Right-of-Way at sites obtained by the Contractor.

Backfill the excavated area with nine (9) inches of Class 2 Asphalt Base 1.0D PG64-22 in 4½ inch maximum courses up to the existing pavement surface. Seal the Asphalt Base with Leveling and Wedging. Compact each course of asphalt base to the proper compaction as required by the Section 403. Perform all base failure repairs in such a manner that removal and replacement are completed on the same day. Do this work as one of the Contractor's first operations in order to allow further compaction by traffic. Do not place new asphalt surface over repaired base failure areas until a minimum of seven (7) days has elapsed after placement of the final course of asphalt base. Prior to milling or constructing the new asphalt surface, level and wedge any settlement of the repair areas.

The bidder must draw his own conclusions as to the conditions encountered. The Department does not give any guarantee as to the accuracy of the data and no claim will be considered for additional compensation of the materials encountered that are not in accord with the classification shown.

Accept payment at the Contract unit prices per Square Yard for Base Failure Repair and per Ton of Leveling and Wedging as full compensation for all labor, materials, equipment, and incidentals for saw cutting pavement, excavating and disposing of all materials, backfilling trench up to the pavement boundary, furnishing, placing the asphalt base, leveling and wedging, and all other items necessary to complete the work to the satisfaction of the Engineer.

### **SPECIAL NOTE FOR TYPICAL SECTION DIMENSIONS**

Consider the dimensions shown on the typical sections for pavement and shoulder widths and thickness' to be nominal or typical dimensions. The Engineer may direct or approve varying the actual dimensions to be constructed to fit existing conditions. Do not widen existing pavement or shoulders unless specified elsewhere in this proposal or directed by the engineer.

1-3725 Typical Section Dimensions  
01/02/2012

**TRAFFIC CONTROL PLAN**  
**FD05 019 0008 004-007**  
**FD05 019 0008 009-014**  
**FD05 019 0547 000-007**

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**TRAFFIC CONTROL GENERAL**

Except as provided herein, maintain and control traffic in accordance with the 2019 Standard and Supplemental Specifications and the Standard and Sepia Drawings, current editions. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic".

Contrary to Section 106.01, furnish new, or used in like new condition, traffic control devices at the beginning of the work and maintain in like new condition until completion of the work.

**PROJECT PHASING & CONSTRUCTION PROCEDURES**

The Engineer may specify days and hours when lane closures will not be allowed.

Maintain alternating one way traffic during construction. Provide a minimum clear lane width of 9 feet; however, provide for passage of vehicles of up to 16 feet in width. If traffic should be stopped due to construction operations, and a school bus on an official run arrives on the scene, make provisions for the passage of the bus as quickly as possible.

**LANE CLOSURES**

Do not leave lane closures in place during non-working hours.

**SIGNS**

The Engineer may require additional signing and/or traffic control devices in addition to the items shown on the Standard Drawings. Sign posts and splices shall be compliant with NCHRP 350 or MASH. Manufacturer's documentation validating this compliance shall be provided to the Engineer prior to installation. Signs, including any splices, shall be installed according to manufacturer's specifications and installation recommendations. Contrary to section 112.04.02, the Department will measure only long term signs (signs intended to be continuously in place for more than 3 days) for payment. The Department will not measure; short term signs (signs intended to be left in place for 3 days or less) for payment, but shall be incidental to Maintain and Control Traffic. Contrary to Section 112.04.02, the Department will measure individual signs only once for payment, regardless of how many times they are erected or relocated. The Department will not measure replacements for damaged signs directed by the Engineer to be replaced due to poor condition or reflectivity.

Traffic Control Plan  
FD05 019 0008 004-007  
FD05 019 0008 009-014  
FD05 019 0547 000-007  
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## **CHANGEABLE MESSAGE SIGNS**

If deemed necessary by the Engineer, the Department will furnish, operate, and maintain Changeable Message Signs.

## **BARRICADES**

The Department will not measure barricades used in lieu of barrels and cones for channelization or delineation, but shall be incidental to Maintain and Control Traffic according to Section 112.04.01.

The Department will measure barricades used to protect pavement removal areas in individual units Each. The Department will measure for payment the maximum number of barricades in concurrent use at the same time on a single day on all sections of the contract. The Department will measure individual barricades only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. The Department will not measure replacements for damaged barricades the Engineer directs to be replaced due to poor condition or reflectivity. Retain possession of the Barricades upon completion of construction.

## **PAVEMENT STRIPING**

If there is to be a deviation from the existing striping plan, the Engineer will furnish the Contractor a striping plan prior to placement of the final surface course.

Install Temporary Striping according to Section 112 with the following exceptions:

1. Include edgelines in temporary striping.
2. Include bridges in permanent striping.
3. If the Contractor's operations or phasing requires temporary markings that must subsequently be removed from the final surface course, use an approved removable lane tape; however, the Department will not measure removable lane tape for separate payment, but will measure and pay for removable lane tape as temporary striping.

## **THERMOPLASTIC PAVEMENT MARKINGS**

Consider the locations listed on the summary as approximate only. Prior to milling and/or resurfacing, locate and document the locations of the existing markings. After resurfacing, replace the markings at their approximate existing locations or as directed by Engineer. Place markings not existing prior to resurfacing as directed by the Engineer.

Traffic Control Plan  
FD05 019 0008 004-007  
FD05 019 0008 009-014  
FD05 019 0547 000-007  
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## **PAVEMENT EDGE DROP-OFFS**

Do not allow a pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation with an elevation difference greater than 1½". Place Warning signs (MUTCD W8-11 or W8-9A) in advance of and at 1500' intervals throughout the drop-off area. Dual post the signs on both sides of the traveled way. Wedge all transverse transitions between resurfaced and unresurfaced areas which traffic may cross with asphalt mixture for leveling and wedging. Remove the wedges prior to placement of the final surface course.

Protect pavement edges that traffic is not expected to cross, except accidentally, as follows:

Less than 2" - No protection required.

2" to 4" - Place plastic drums, vertical panels, or barricades every 50 feet. During daylight working hours only, the Engineer will allow the Contractor to use cones in lieu of plastic drums, panels, and barricades. Wedge the drop-off with DGA or asphalt mixture for leveling and wedging with a 1:1 or flatter slope in daylight hours, or 3:1 or flatter slope during nighttime hours, when work is not active in the drop-off area.

Greater than 4' - Protect drop-offs greater than 4 inches within 10 feet of traffic by placing drums, vertical panels, or barricades every 25 feet. The Engineer will not allow the use of cones in lieu of drums, vertical panels, or barricades for drop-offs greater than 4". Place Type III Barricades directly in front of the drop-off facing on coming traffic in both directions of travel. Provide warning signs as shown on the Standard Drawings or as directed by the Engineer

Pedestrians & Bicycles - Protect pedestrian and bicycle traffic as directed by the engineer.

## SPECIAL NOTE FOR EDGELINE AND SHOULDER RUMBLE STRIPS

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Construct edgeline rumble strips and/or shoulder rumble strips at the locations listed on the Pavement Markings Summary, Typical Sections, and/or according to the following drawings, as applicable:

- Shoulder & Edgeline Rumble Strip Details
- Edgeline Rumble Strip Details Two Lane Roadways
- Shoulder Rumble Strip Details Two Lane Roadways
- Rumble Strip Details Multi-Lane Roadways

The locations listed for edgeline rumble strips and/or shoulder rumble strips, and the dimensions on the drawings are approximate only and may vary from the existing marking's locations. The Engineer shall determine the exact locations for edgeline rumble strips and/or shoulder rumble strips at the time of construction. If the Typical Section shows a Lane Width (Y) and/or Shoulder Width (Z) that differs from the widths listed on the drawings the Engineer shall determine the appropriate Lane Width (Y) and/or Shoulder Width (Z) at the time of construction.

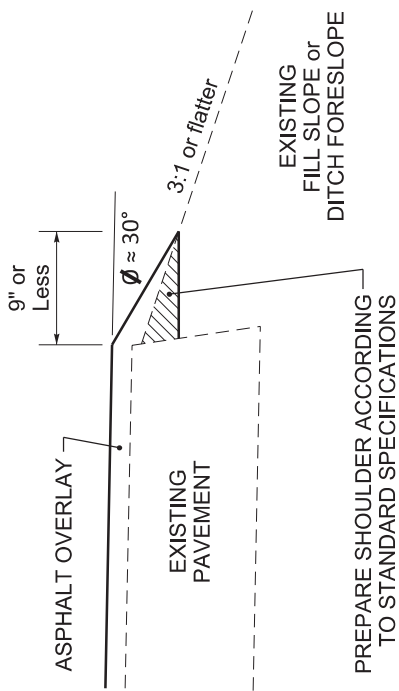
(Note to the Engineer: In order to achieve consistent installations statewide, rumble strips should be installed according to the dimensions specified in the drawings, unless there is an Engineering basis that supports a change in dimension(s) – e.g. the existing shoulder width is wider than the drawing specifies and/or the existing lane width is narrower than the drawing specifies AND the existing shoulder pavement depth is not suitable to be used as a driving lane.)

Unless directed otherwise by the Engineer, DO NOT install edgeline rumble strips or shoulder rumble strips where the posted speed limit is 45 MPH or less. Before sawing edgeline rumble strips, pre-mark the pavement surface and obtain the Engineer's approval of the proposed location, alignment, and control guides. After sawing edgeline rumble strips, construct a rumble stripe by applying permanent edgeline striping according to Section 713 on the sawed edgeline rumble strips at the locations approved by the Engineer. Before sawing shoulder rumble strips, obtain the Engineer's approval of the proposed layout, location, and alignment. Notify the Engineer if questions arise regarding changes in striping and/or rumble patterns. If necessary, the Engineer will obtain guidance from the District Traffic Engineer and/or the Division of Traffic Operations.

The Department will measure edgeline rumble strips and shoulder rumble strips according to Section 403. Unless required by the Traffic Control Plan or directed by the Engineer, the Department will not measure for payment temporary edgeline striping used for pre-marking edgeline rumble strips. The Department will measure permanent edgeline striping according to Section 713. The Department will not measure the removal of existing markings, pre-marking and layout, surface preparation, corrective work, labor, equipment, and any incidentals necessary to construct edgeline rumble strips and/or shoulder rumble strips, and will consider these items incidental to the installation of the sawed rumble strips.

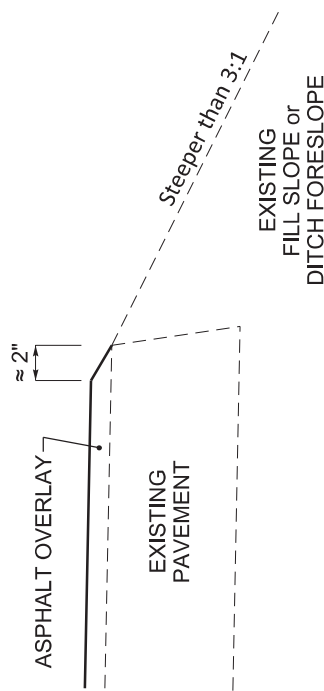
DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to fill slope or ditch foreslope that is 3:1 or less)



DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to fill slope or ditch foreslope that is steeper than 3:1)



NOTES

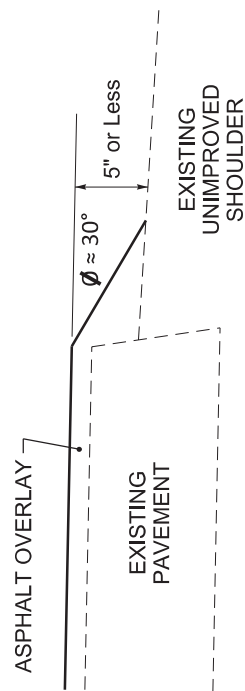
1. DETAILS DO NOT APPLY TO OVERLAYS LESS THAN 1 INCH THICK.
2. THE DURABLE PAVEMENT EDGE DEVICE MAY BE DISENGAGED AT DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT, AS APPROVED BY THE ENGINEER.

DURABLE PAVEMENT EDGE  
DETAILS

DRAWING NOT TO SCALE

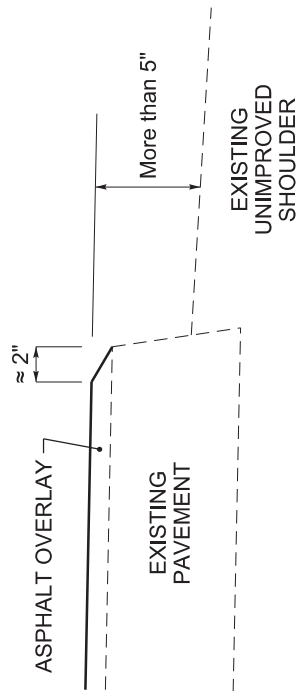
DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to low shoulder with dropoff of 5 inches or less)



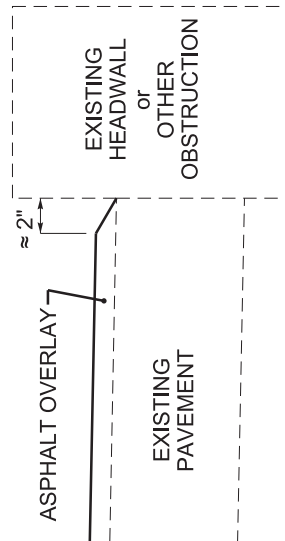
DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to low shoulder with dropoff of more than 5 inches)



DURABLE PAVEMENT EDGE DETAIL

(Resurfacing adjacent to an obstruction, such as an existing headwall)



## SPECIAL NOTE FOR TRAFFIC SIGNAL LOOP DETECTORS

**1.0 DESCRIPTION.** Be advised that there are existing traffic signal loop detectors within the construction limits of this project. Except as specified herein, perform traffic signal loop replacement in accordance with the Department's Standard/Supplemental Specifications, Special Provisions, Special Notes, and Standard/Sepia Drawings, current editions and as directed by the Engineer. Article references are to the Standard Specifications. Furnish all materials, labor, equipment, and incidentals for replacement of traffic signal loop installation(s) and all other work specified as part of this contract.

**1.1 Pre-bid Requirements.** Each Contractor submitting a bid for this work shall make a thorough inspection of the site prior to submitting his bid and shall thoroughly familiarize himself with existing conditions so that the work can be expeditiously performed after a Contract is awarded. Information provided in the Plans regarding types and quantities of work is not to be taken as an accurate or complete evaluation of the materials and conditions to be encountered during construction. The bidder must make his own determinations as to the conditions encountered.

**2.0 MATERIALS.** Except as specified herein, furnish materials in accordance with Subsection 732.02 and Section 835. Provide for materials to be sampled and tested in accordance with the Department's Sampling Manual. Make materials available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing, unless otherwise specified in this Special Note.

**2.1 Maintain and Control Traffic.** See Traffic Control Plan.

**2.2 Sand.** Furnish natural sand meeting the requirements of Subsection 804.04.01.

**2.3 Seeding.** Furnish Seed Mix Type I.

**2.4 Loop Saw Slot and Fill.** Furnish loop sealant, backer rod, and non-shrink grout according to the Saw Slot Detail.

**2.5 Junction Boxes.** Furnish junction box type B, #57 aggregate, and geotextile filter type IV according to junction box detail.

**2.6 Cable No. 14/1 Pair (Lead-in).** Furnish cable that is specified in Section 835. Cable shall be ran splice free. This shall include splice kits to connect to the loop wire.

**2.7 Conduit.** Furnish and install appropriate conduit from transitions to the roadway, junction boxes and poles. See details below.

**3.0 CONSTRUCTION.** Except as specified herein, install and test Traffic Signal Loop Detectors in accordance with Section 723 and the drawings.

**3.1 Testing.** The Contractor shall test all loops and cable no. 14/1 pair (lead-in) according to Subsection 723.03.17 before and after milling the roadway. The Contractor may have to separate the loop from the lead-in to perform this test. If the loop/lead-in meets the requirement in Section 723 at the controller cabinet, the loop/lead-in shall not be replaced. If existing loops do not meet the requirements in Subsection 723.03.17, the loops shall be replaced. Replacement loops may be installed either before or after the milling process.



## Traffic Signal Loop Detectors

### Page 2 of 8

The Contractor shall verify that loops (both existing and replacement loops) meet the requirements per Subsection 723.03.17 before the final surface is laid. If loops do not meet conditions of Subsection 723.03.17, the Contractor shall replace them before the resurfacing activities begin. If replacement loops have to be reinstalled, the costs of reinstallation shall be incidental to the milling bid item. The Contractor shall re-splice loops to the lead-in with the proper splice as noted in the spec book.

**3.2 Coordination.** Notify the Engineer in writing, two (2) weeks prior to beginning any work. The Engineer will contact the District Traffic Engineer to coordinate the Department's operations with the Contractor's work.

**3.3 Connection.** The Contractor shall schedule all signal loop installation to ensure the new loops are connected to the lead-in and operational within 5 calendar days of the old loops being damaged and/or disconnected. This requirement includes damage caused by any work activity associated with the project. If the new signal loops are not functioning as intended following 5 calendar days, the Department may assess Liquidated Damages at a rate of \$500 per calendar day per signal location until the loops are operating at pre-construction conditions. All liquidated damages will be applied cumulatively.

**3.4 Maintain and Control Traffic.** See Traffic Control Plan.

**3.5 Milling.** On projects involving milling and texturing of the existing pavement, install loops in the existing pavement before or after performing the milling and texturing. After milling, the remnant contents of the existing saw slot (grout, loop wires, backer rod, and/or loop sealant) may not be flush with the top of the milled portion of the surface. In such cases, clear the saw slot of loose remnant contents and refill the saw slot with natural sand. Obtain the Engineer's approval of the stabilized saw slot prior to resurfacing. The Department will not measure for separate payment clearing and stabilizing the saw slot and shall consider this work incidental to milling.

**3.6 Loop Saw Slot and Fill.** The following is a typical step by step procedure for the installation of a loop.

- 1) Carefully mark the slot to be cut, perpendicular to the flow of traffic and centered in the lane.
- 2) Make each saw-cut 3/8-inch wide and at a depth such that the top of the backer rod is a minimum of 4 inches below the surface of asphalt pavement.
- 3) Drill a 1½ inch core hole at each corner and use a chisel to smooth corners to prevent sharp bends in the wire.
- 4) Clean all foreign and loose matter out of the slots and drilled cores and within 1 foot on all sides of the slots using a high pressure washer.
- 5) Completely dry the slots and drilled cores and within 1 foot on all sides of the slots.
- 6) Measure 9-12 inches from the edge of the paved surface (shoulder break or face of curb) and drill a 1½ inch hole on a 45° angle to the conduit adjacent to the roadway.
- 7) Closely inspect all cuts, cores, and slots for jagged edges or protrusions prior to the placement of the wire. All jagged edges and protrusions shall be ground or re-cut and cleaned again.
- 8) Place the loop wire splice-free from the termination point (cabinet or junction box) to the loop, continue around the loop for two turns (6'x30' loop) or three turns (6'x6'

Traffic Signal Loop Detectors  
Page 3 of 8

- loop), and return to the termination point. Twist unshielded loop wires (IMSA 51-7) with 3 to 5 turns per foot from the start of the homerun to the junction box, cabinet, or pole. Slot can be widened to 1/2" to 5/8" to help with the installation of the twisted wire.
- 9) Push the wire into the saw slot with a blunt object such as a wooden stick. Make sure that the loop wire is pushed fully to the bottom of the saw slot. Screwdrivers shall not be used.
  - 10) Install duct sealant to a minimum of 1 inch deep into the cored 1½ inch hole.
  - 11) Apply loop sealant from the bottom up and fully encapsulate the loop wires in the saw slot. The wire should not be able to move when the sealant has set.
  - 12) Cover the encapsulated loop wire with a continuous layer of backer rod along the entire loop and home run saw slots such that no voids are present between the loop sealant and backer rod.
  - 13) Finish filling the saw cut with non-shrinkable grout per manufacturer's instructions. Alleviate all air pockets and refill low spaces. There shall be no concave portion to the grout in the saw slot. Any excess grout shall be cleaned from the roadway to alleviate tracking.
  - 14) Clean up the site and dispose of all waste off the project.
  - 15) Ensure that the grout has completely cured prior to subjecting the loop to traffic. Curing time varies with temperature and humidity.

**3.7 Final Dressing, Clean Up, and Seeding.** After all work is completed, clean work sites and all disturbed areas. Dispose of all waste and debris off the right of way at sites obtained by the Contractor at no additional cost to the Department. Sow all disturbed earthen areas with Seed Mix Type I.

**3.8 Removal.** The Contractor shall remove all existing junction boxes, wire from spans/poles/junction boxes/conduits, and conduits. The removal will be incidental to the project.

**3.9 Property/Roadway Damage.** The Contractor shall be responsible for all damage to public and/or private property resulting from the work. Upon completion of the work, restore all disturbed highway features and private property in like kind design and materials at no additional cost to the Department.

**3.10 Right-of-Way Limits.** The Department has not established exact limits of Right-of-Way. Limit work activities to obvious Right-of-Way and work areas secured by the Department through Consent and Release of the adjacent property owners. Contractor is responsible for all encroachments onto private lands.

**3.11 Utility Clearance.** Work around and do not disturb existing utilities. The Department does not anticipate any utility impacts for loop installation. If utilities are impacted, work with associated utility companies to resolve issues.

**3.12 Control.** Obtain the Engineer's approval of all designs required to be furnished by the Contractor prior to incorporation into the work. The Department reserves the right to permit other contractors, state forces, public utility companies, and others to do work during the construction within the limits of, or adjacent to, the project. Conduct operations and cooperate with such other parties so that interference with each other's work will be reduced to a minimum. The Contractor agrees to make no claims against the Department for additional compensation due to delays or other conditions created by the operations of such other parties. Should a difference of opinion arise as to the rights of the Contractor

## Traffic Signal Loop Detectors

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and others working within the limits of, or adjacent to, the project, the Engineer will decide as to the respective rights of the various parties involved in order to ensure the completion of the work in general harmony and in a satisfactory manner, and the Engineer's decision shall be final and binding upon the Contractor.

**3.13 Bore and Jack.** Except for situations outlined in 3.15, bore and jack will be used if conduit is under pavement of any kind. The conduit shall be 2" rigid steel conduit under all pavement areas except for the area where the loop transitions from the saw slot. The installation of conduit should follow the below detail.

**3.14 Open Cut Roadway.** With permission of the Engineer, roadway may be open cut if the conduit is under pavement. The conduit shall be 2" rigid steel conduit under all pavement areas except for the area that the loop transition from the saw slot. The installation of conduit should follow requirements per Section 723.

**4.0 MEASUREMENT.** See Subsection 723.04 for bid item notes. Additional bid items include the following:

**4.1 Loop Test.** The Department will measure the quantity as each individual unit loop tested. The Department will not measure disconnection, reconnection, traffic control, re-splicing per specifications, before and after testing per note above, and any associated hardware for payment and will consider them incidental to this item of work.

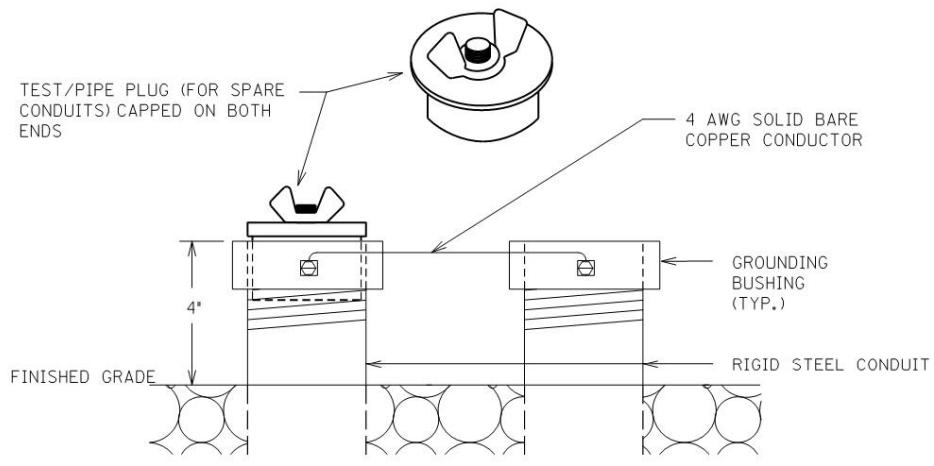
**5.0 PAYMENT.** The Department will make payment for the completed and accepted quantities of listed items according to Subsection 723.05 in addition to the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
Conduit 1"	4792	Linear Foot
PVC Conduit – 1 ¼ inch – sch 80	24900EC	Linear Foot
PVC Conduit – 2 inch – sch 80	24901EC	Linear Foot
Conduit 2"	4795	Linear Foot
Electrical Junction Box type B	4811	Each
Loop Test	24963ED	Each
Trenching and Backfilling	4820	Linear Foot
Loop Wire	4830	Linear Foot
Cable-No. 14/1 Pair	4850	Linear Foot <sup>1</sup>
Loop Saw Slot and Fill	4895	Linear Foot <sup>1</sup>
Bore and Jack Conduit	21543EN	Linear Foot <sup>3</sup>
Open Cut Roadway	4821	Linear Foot <sup>3</sup>

The Department will consider payment as full compensation for all work required under these notes and the Standard Specifications.

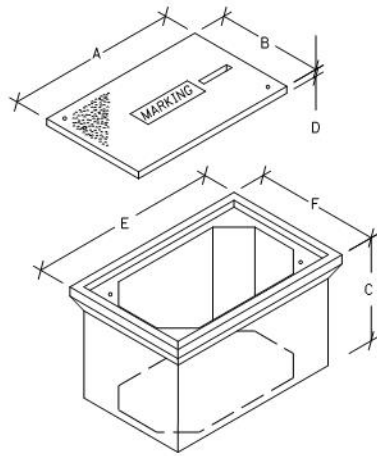
# Traffic Signal Loop Detectors

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TEST/PIPE PLUG(FOR SPARE CONDUITS) AND GROUNDING DETAIL

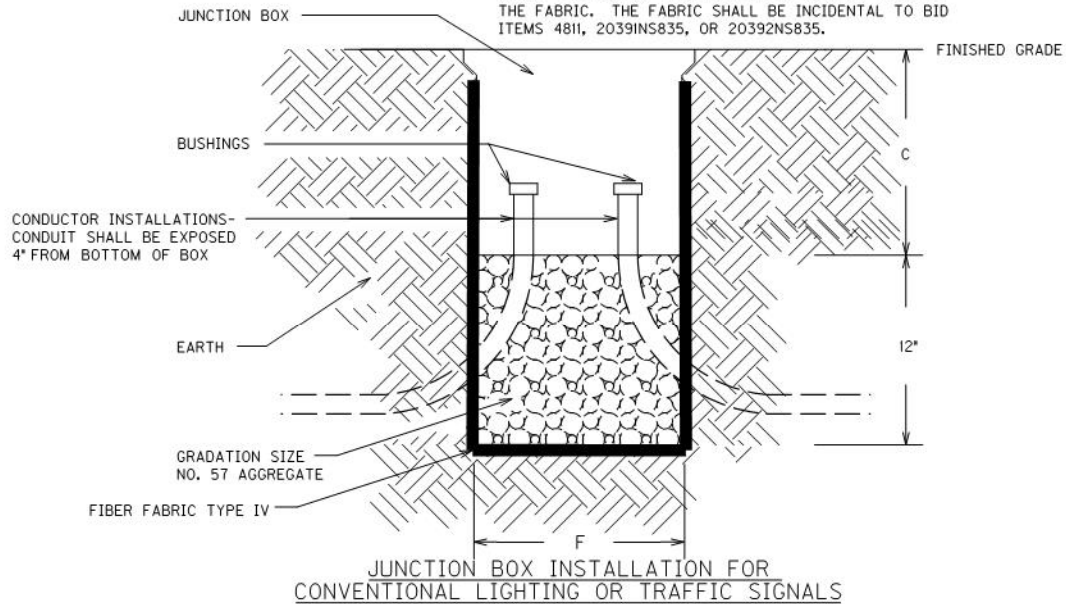
Traffic Signal Loop Detectors  
Page 6 of 8



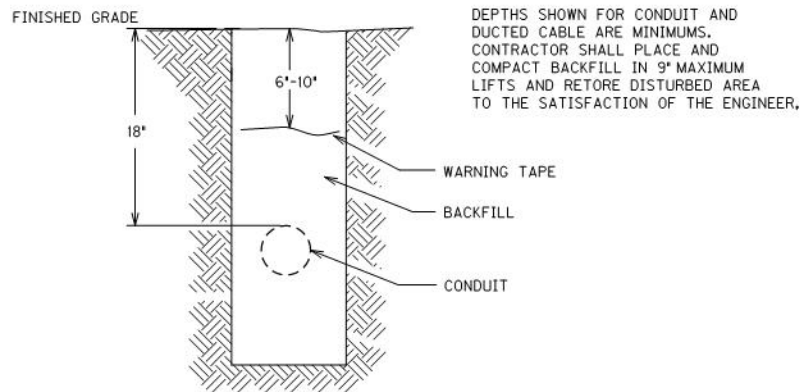
JUNCTION BOX DIMENSIONS (NOMINAL)						
	A	B	C	D	E	F
TYPE A	23"	14"	27"	2"	25"	15"
TYPE B	18"	11"	12"	1 3/4"	20"	13"
TYPE C	36"	24"	30"	3"	38"	26"

\* MINIMUM  
NOTE: STACKABLE BOXES ARE PERMITTED

BEFORE THE INSTALLATION OF THE #57 AGGREGATE AND JUNCTION BOX, THE CONTRACTOR SHALL INSTALL GEOTEXTILE FILTER FABRIC TYPE IV IN THE HOLE. THE FABRIC SHALL EXTEND TO JUST BELOW THE LIP OF THE JUNCTION BOX AND SHALL BE CONTINUOUSLY ADHERED TO THE EXTERIOR OF THE BOX WITH ADHESIVE. ANY LOCATIONS WHERE CONDUITS ENTER THE BOX, THE FABRIC SHALL BE 'X CUT' ONLY AS MUCH AS NECESSARY TO ALLOW PASSAGE OF EACH INDIVIDUAL CONDUIT THROUGH THE FABRIC. THE FABRIC SHALL BE INCIDENTAL TO BID ITEMS 4811, 20391NS835, OR 20392NS835.



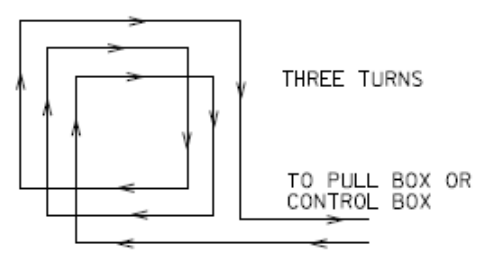
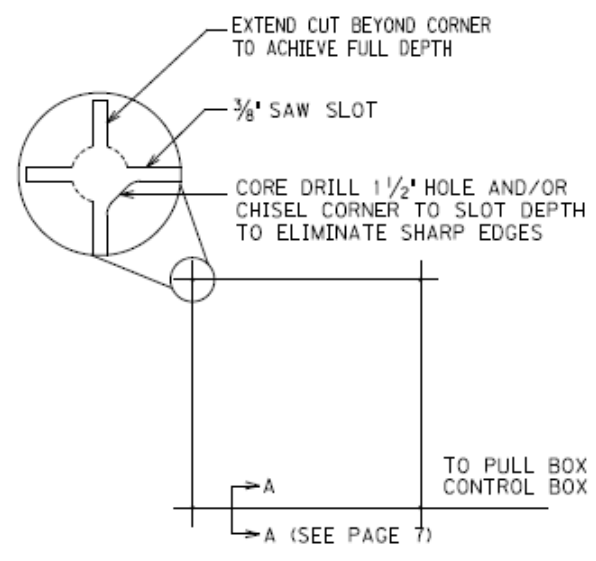
JUNCTION BOX INSTALLATION FOR CONVENTIONAL LIGHTING OR TRAFFIC SIGNALS



CONDUIT AND WARNING TAPE TRENCH

# Traffic Signal Loop Detectors

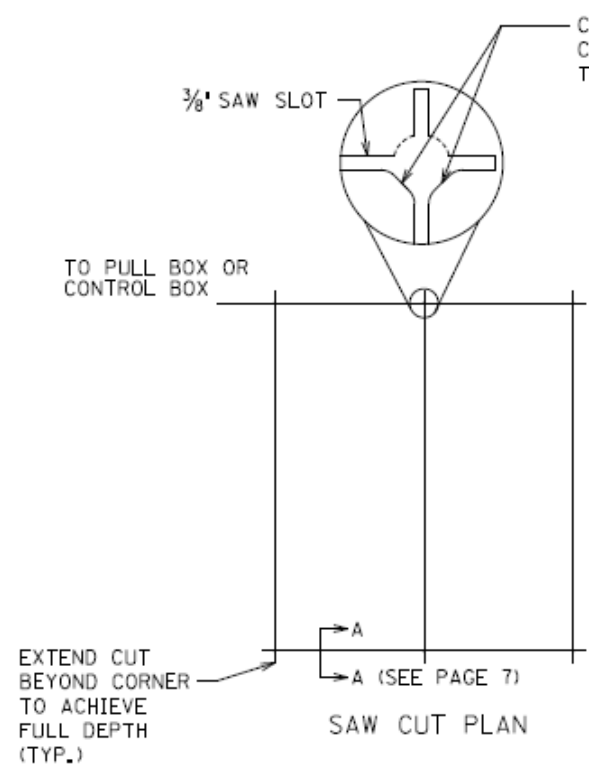
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SAW CUT PLAN

WIRING PLAN

6'X6' LOOP



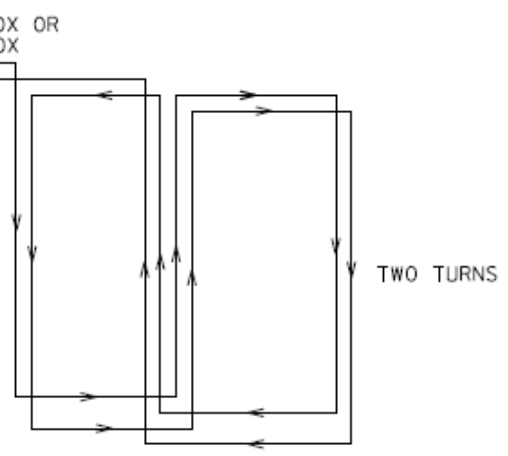
CORE DRILL 1 1/2" HOLE AND/OR  
CHISEL CORNERS TO SLOT DEPTH  
TO ELIMINATE SHARP EDGES

TO PULL BOX OR  
CONTROL BOX

EXTEND CUT  
BEYOND CORNER  
TO ACHIEVE  
FULL DEPTH  
(TYP.)

A  
A (SEE PAGE 7)

SAW CUT PLAN

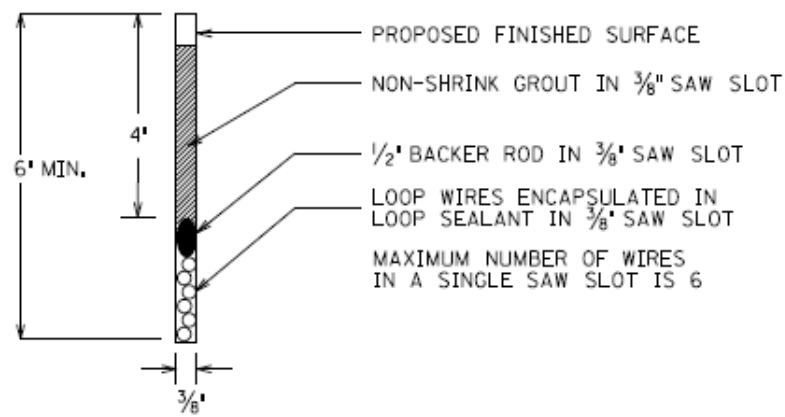


WIRING PLAN

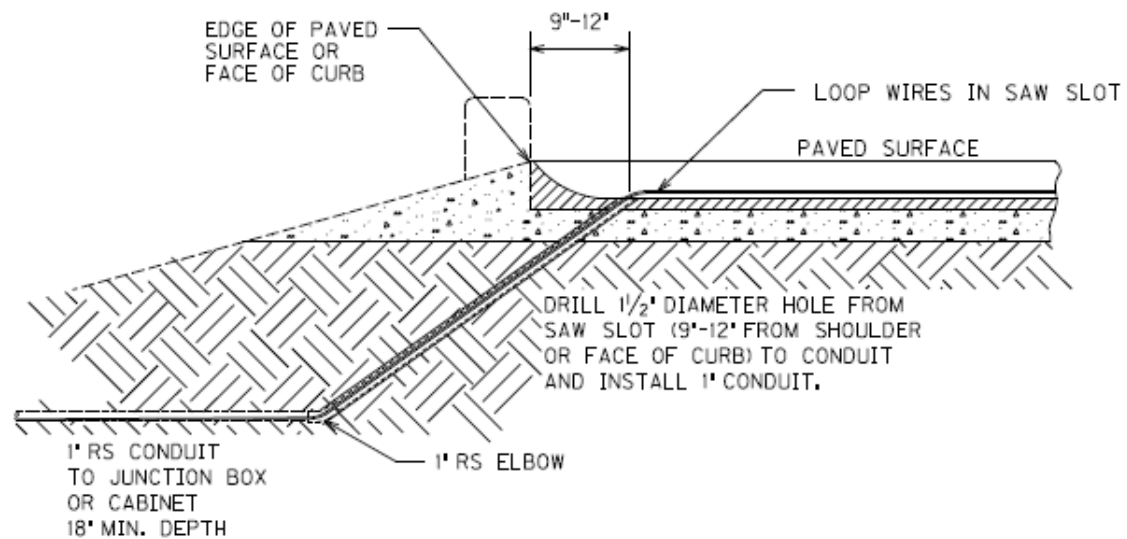
6'X30' QUADRAPOLE LOOP

# Traffic Signal Loop Detectors

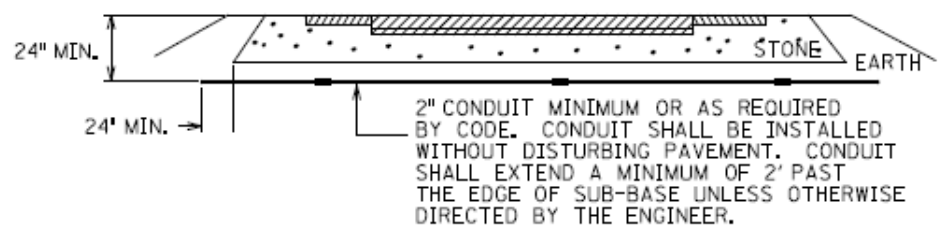
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SECTION A-A (SAW SLOT DETAIL)

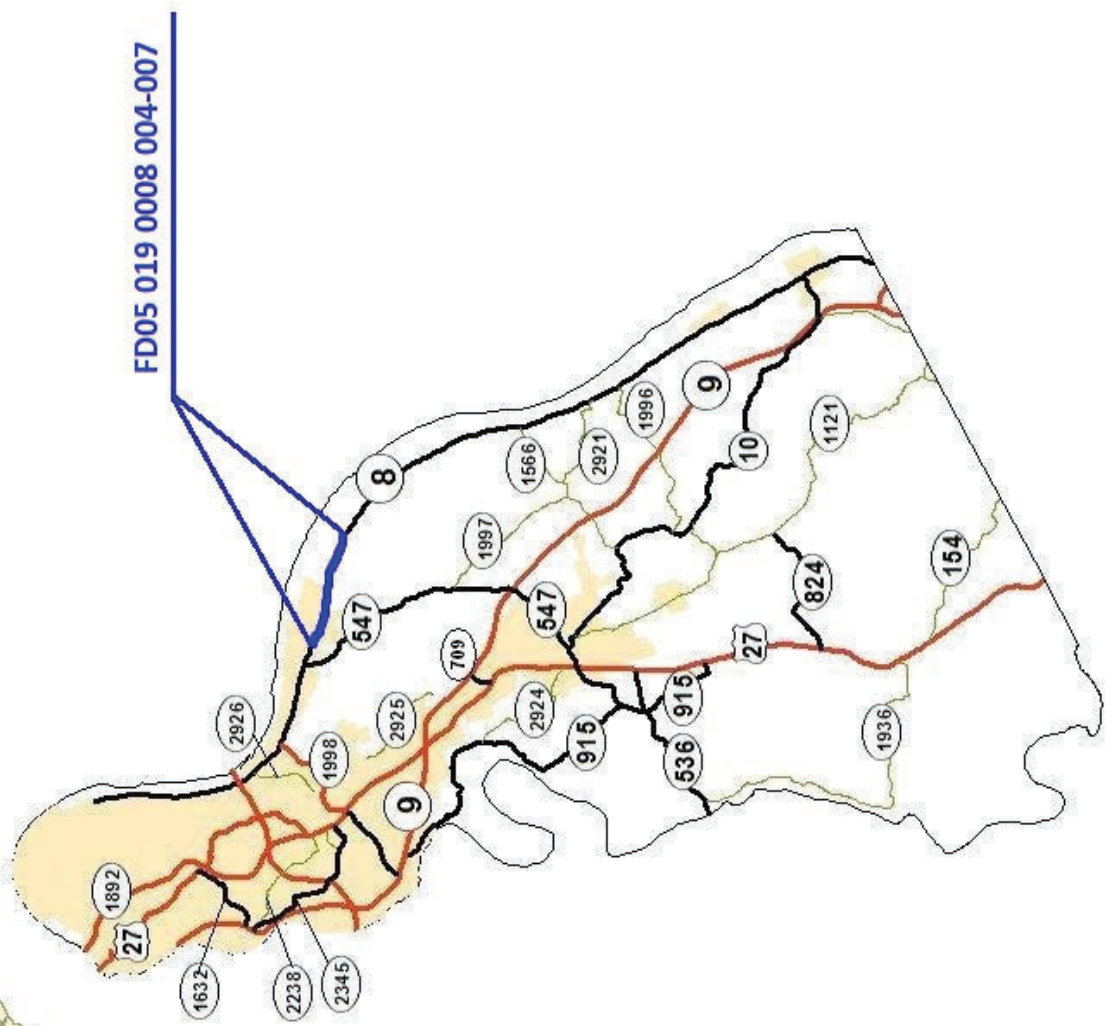


SAW SLOT EDGE OF PAVEMENT TRANSITION



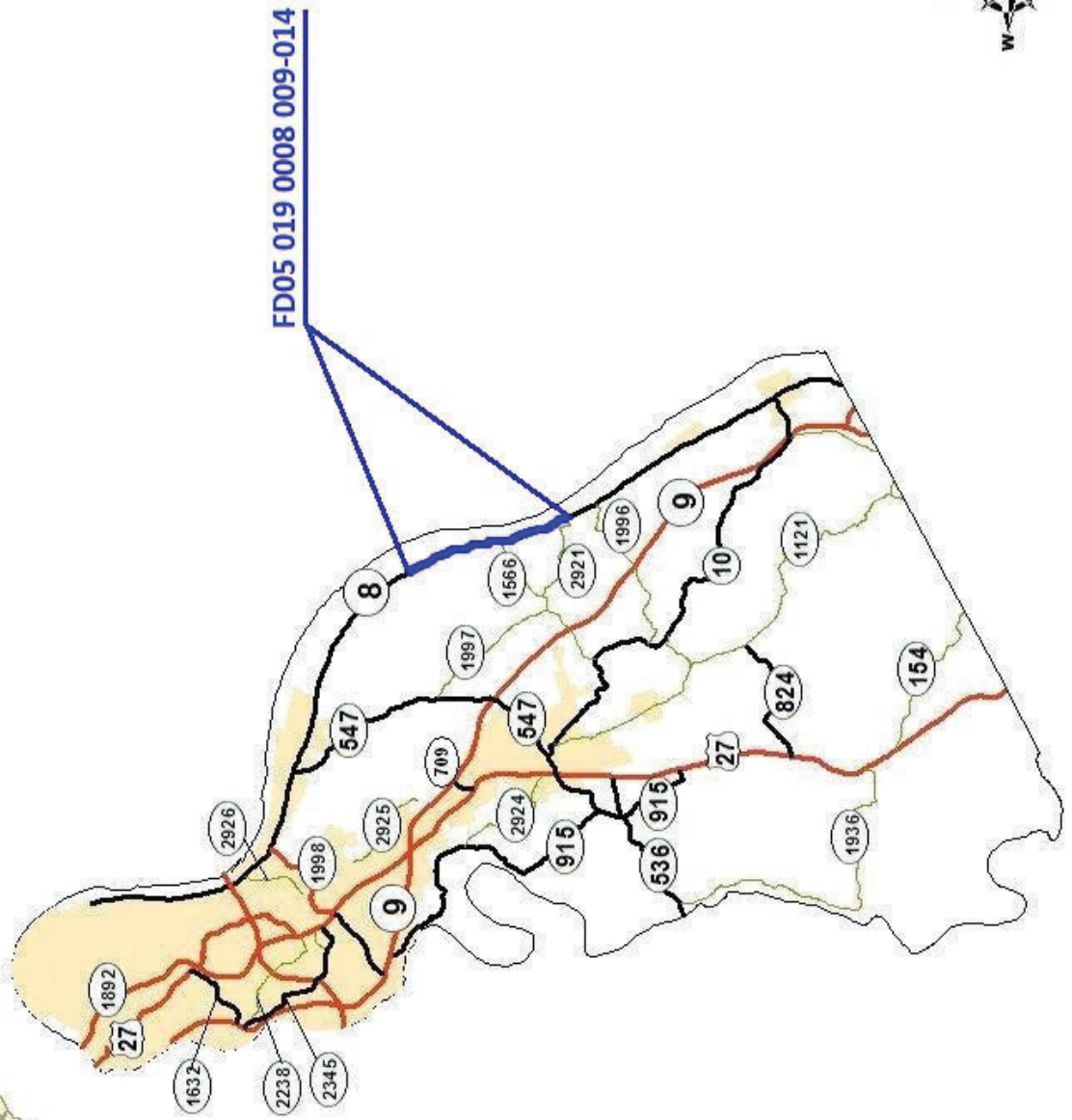
CONDUIT UNDER EXISTING PAVEMENT DETAIL

DEPARTMENT OF HIGHWAYS  
MAP OF  
CAMPBELL COUNTY

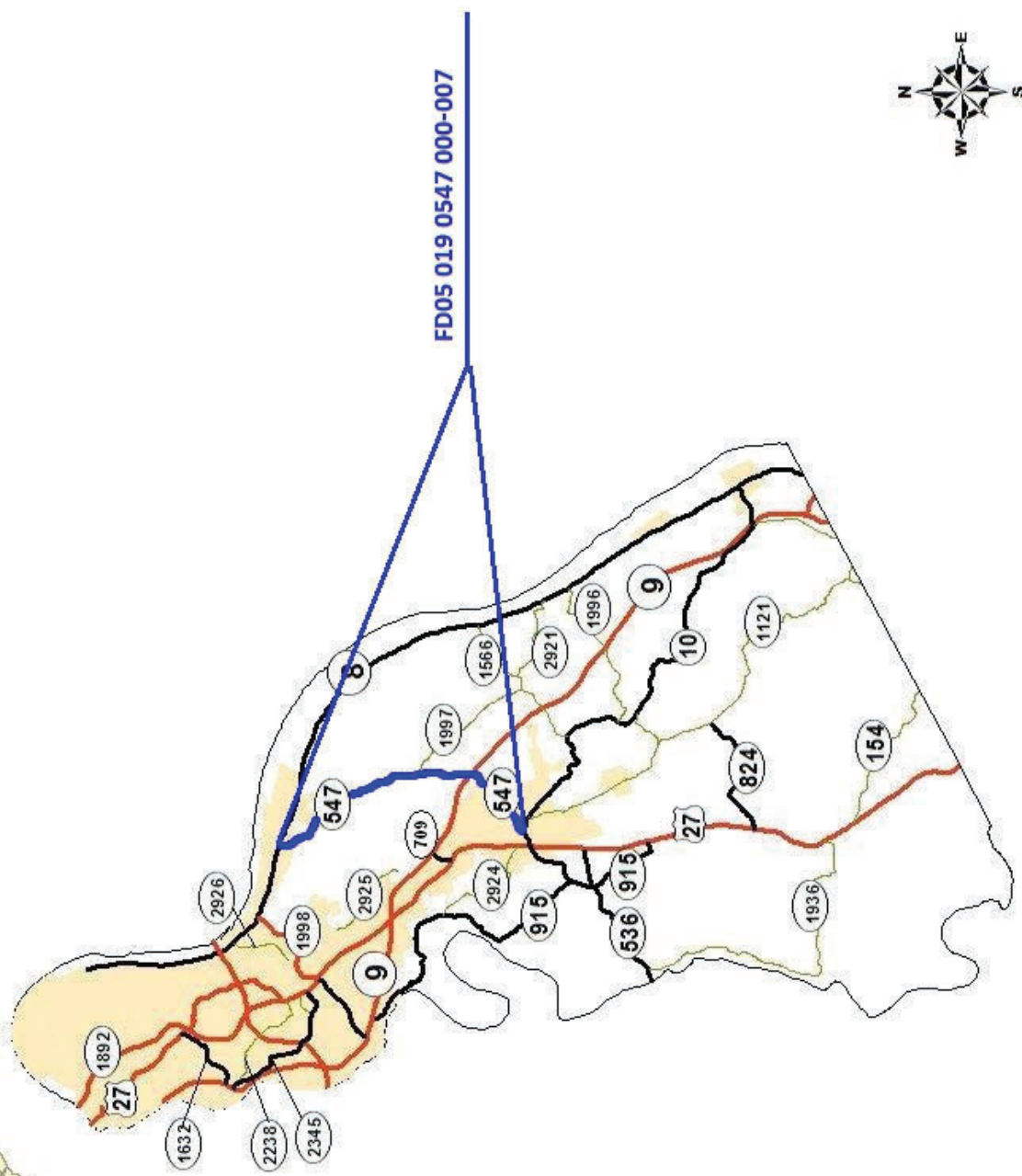




DEPARTMENT OF HIGHWAYS  
MAP OF  
CAMPBELL COUNTY



DEPARTMENT OF HIGHWAYS  
MAP OF  
CAMPBELL COUNTY



# MATERIAL SUMMARY

**CONTRACT ID: 192317**

**019GR19062-FD05**

**MP01900081901**

MARY INGLES HIGHWAY (KY 8) FROM MAPLE STREET IN SILVER GROVE EXTENDING EAST TO PAVEMENT JOINT 0.271 MILES WEST OF 8 MILE ROAD ASPHALT RESURFACING, A DISTANCE OF 2 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0130	02014	BARRICADE-TYPE III	6.00	EACH
0135	02562	TEMPORARY SIGNS	430.00	SQFT
0140	02650	MAINTAIN & CONTROL TRAFFIC - (KY 8 MILEPOINT 004-007)	1.00	LS
0145	00190	LEVELING & WEDGING PG64-22	50.00	TON
0150	00301	CL2 ASPH SURF 0.38D PG64-22	1,500.00	TON
0155	02676	MOBILIZATION FOR MILL & TEXT - (KY 8 MILEPOINT 004-007)	1.00	LS
0160	02677	ASPHALT PAVE MILLING & TEXTURING	1,500.00	TON
0165	03240	BASE FAILURE REPAIR	212.00	SQYD
0170	06510	PAVE STRIPING-TEMP PAINT-4 IN	39,500.00	LF
0175	06514	PAVE STRIPING-PERM PAINT-4 IN	35,750.00	LF
0180	06566	PAVE MARKING-THERMO X-WALK-12 IN	50.00	LF
0185	10020NS	FUEL ADJUSTMENT	2,415.00	DOLL
0190	10030NS	ASPHALT ADJUSTMENT	6,060.00	DOLL
0195	02569	DEMOBILIZATION	1.00	LS
0200	00356	ASPHALT MATERIAL FOR TACK	11.30	TON

# MATERIAL SUMMARY

**CONTRACT ID: 192317**

**019GR19062-FD05**

**MP01900081902**

MARY INGLES HIGHWAY (KY 8) FROM PAVEMENT JOINT 80 FEET WEST FENDER ROAD EXTENDING EAST TO NORTH SIDE OF INTERSECTION WITH KY 2921 ASPHALT RESURFACING, A DISTANCE OF 3.41 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0205	02014	BARRICADE-TYPE III	4.00	EACH
0210	02562	TEMPORARY SIGNS	200.00	SQFT
0215	02650	MAINTAIN & CONTROL TRAFFIC - (KY 8 MILEPOINT 009-014)	1.00	LS
0220	00190	LEVELING & WEDGING PG64-22	80.00	TON
0225	00301	CL2 ASPH SURF 0.38D PG64-22	2,535.00	TON
0230	02676	MOBILIZATION FOR MILL & TEXT - (KY 8 MILEPOINT 009-014)	1.00	LS
0235	02677	ASPHALT PAVE MILLING & TEXTURING	2,535.00	TON
0240	03240	BASE FAILURE REPAIR	215.00	SQYD
0245	06510	PAVE STRIPING-TEMP PAINT-4 IN	66,325.00	LF
0250	06514	PAVE STRIPING-PERM PAINT-4 IN	60,300.00	LF
0255	10020NS	FUEL ADJUSTMENT	4,070.00	DOLL
0260	10030NS	ASPHALT ADJUSTMENT	10,225.00	DOLL
0265	02569	DEMOBILIZATION	1.00	LS
0270	02697	EDGE LINE RUMBLE STRIPS	36,000.00	LF
0275	00356	ASPHALT MATERIAL FOR TACK	19.30	TON

# MATERIAL SUMMARY

**CONTRACT ID: 192317**

**019GR19062-FD05**

**MP01905471901**

FOUR MILE ROAD (KY 547) FROM KY 10 IN ALEXANDRIA EXTENDING NORTH TO KY 8 IN SILVER GROVE ASPHALT RESURFACING, A DISTANCE OF 6.52 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0005	02562	TEMPORARY SIGNS	510.00	SQFT
0010	02650	MAINTAIN & CONTROL TRAFFIC - (KY 547)	1.00	LS
0015	02014	BARRICADE-TYPE III	4.00	EACH
0020	00307	CL2 ASPH SURF 0.38B PG64-22	5,100.00	TON
0025	00190	LEVELING & WEDGING PG64-22	160.00	TON
0030	02676	MOBILIZATION FOR MILL & TEXT - (KY 547)	1.00	LS
0035	02677	ASPHALT PAVE MILLING & TEXTURING	5,100.00	TON
0040	03240	BASE FAILURE REPAIR	96.00	SQYD
0045	06514	PAVE STRIPING-PERM PAINT-4 IN	135,110.00	LF
0050	06510	PAVE STRIPING-TEMP PAINT-4 IN	148,625.00	LF
0055	06566	PAVE MARKING-THERMO X-WALK-12 IN	350.00	LF
0060	06568	PAVE MARKING-THERMO STOP BAR-24IN	32.00	LF
0065	06574	PAVE MARKING-THERMO CURV ARROW	6.00	EACH
0070	10030NS	ASPHALT ADJUSTMENT	20,565.00	DOLL
0075	10020NS	FUEL ADJUSTMENT	8,190.00	DOLL
0080	04895	LOOP SAW SLOT AND FILL	130.00	LF
0085	04830	LOOP WIRE	344.00	LF
0090	24900EC	PVC CONDUIT-1 1/4 IN-SCHEDULE 80	100.00	LF
0095	04850	CABLE-NO. 14/1 PAIR	150.00	LF
0100	04811	ELECTRICAL JUNCTION BOX TYPE B	1.00	EACH
0105	02569	DEMOBILIZATION	1.00	LS
0110	04792	CONDUIT-1 IN	10.00	LF
0115	24963ED	LOOP TEST	1.00	EACH
0120	04820	TRENCHING AND BACKFILLING	100.00	LF
0125	00356	ASPHALT MATERIAL FOR TACK	36.00	TON

### Base Failure Repair Summary FD05 019 0008 004-007

Milepoint	Lane	Length	Width	Depth	SQYD
4.94	East	3	40	9	13
6.33	East	8	8	9	7
6.35	East	10	30	9	33
6.54	East	4	20	9	9
6.57	East	11	40	9	49
6.64	West	11	20	9	24
6.63	West	3	30	9	10
6.06	West	4	60	9	27
6.08	West	4	20	9	9
TO BE DETERMINED BY THE ENGINEER					30

### Base Failure Repair Summary FD05 019 0008 009-014

Milepoint	Lane	Length	Width	SQYD
10.71	North	60	6	40
10.87	South	60	6	40
11.01	South	70	11	86
11.08	South	10	6	7
13.03	on CL	50	4	22
TO BE DETERMINED BY THE ENGINEER				20

### Base Failure Repair Summary FD05 019 0547 000-007

Milepoint	Lane	Length	Width	Depth	SQYD
6.40	South	40	6	9	27
1.20	North	30	6	9	20
2.60	North	20	4	9	9
4.69	North	15	10	9	17
5.74	North	20	4	9	9
TO BE DETERMINED BY THE ENGINEER					15



**TRAFFIC LOOP SUMMARY**  
**FD05 019 0547 000-007**

INTERSECTION	SAW, SLOT AND FILL LF	LOOP WIRE LF	CONDUIT 1 INCH LF	PVC CONDUIT 1 1/4 INCH LF	CABLE NO. 14/1 LF	JUNCTION TYPE B EA	TRENCHING & BACKFILLING LF	LOOP TEST EA	NOTES
Jct KY 8 MP 6.517	130	344	10	100	150	1	100	1	1-6x30 Loop Main Line @ Stop Bar
<b>TOTALS</b>	130	344	10	100	150	1	100	1	

**NOTES for all intersections:**

- Quantities are for estimating purposes only. The Contractor shall field measure and inspect items to verify quantities.
- 2 - 1 1/4 inch conduits may be used in place of 2" conduit. Field verify conduit to match existing facilities/tie-in to poles/cabinets. Only replace existing conduit if damaged.

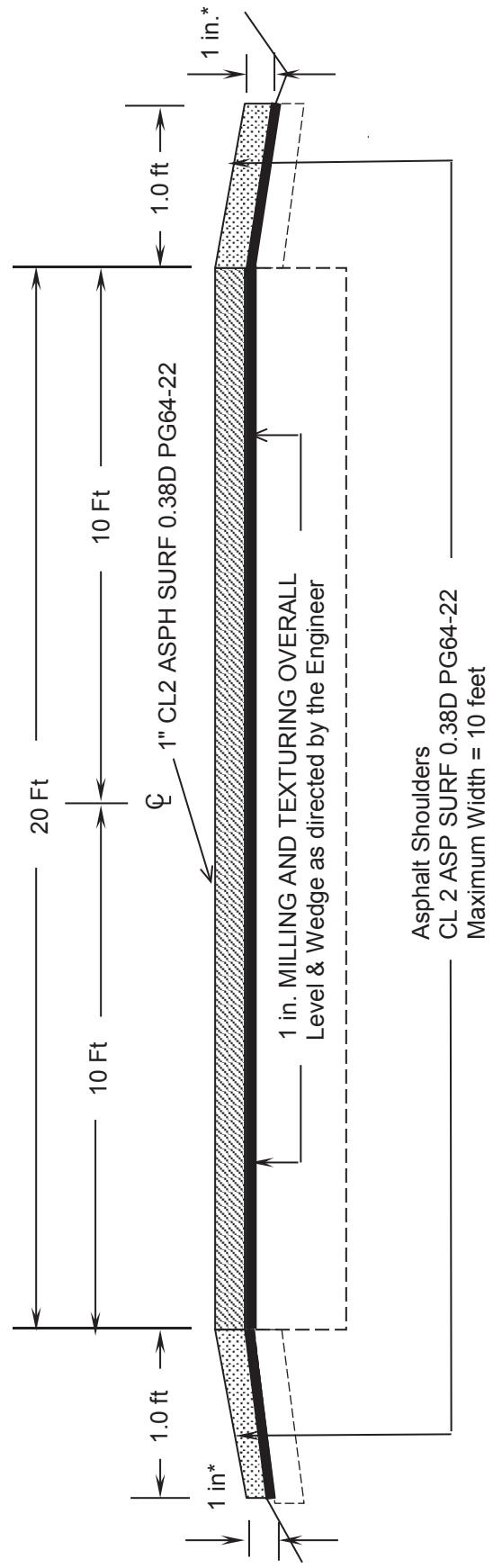
**THERMOPLASTIC INTERSECTION PAVEMENT MARKINGS SUMMARY**  
**FD05 019 0008 004-007**

MILEPOINT	LOCATION	X-WALKS 12 INCH LF	STOP BARS 24 INCH LF	ARROWS			CATRAXX 6 INCH LF	NOTES
				CURVE EA	STR EA	COMB EA		
6.203	St. Philip Parish	50	0	0	0	0	0	
<b>TOTALS</b>		50	0	0	0	0	0	

**THERMOPLASTIC INTERSECTION PAVEMENT MARKINGS SUMMARY**  
**FD05 019 0547 000-007**

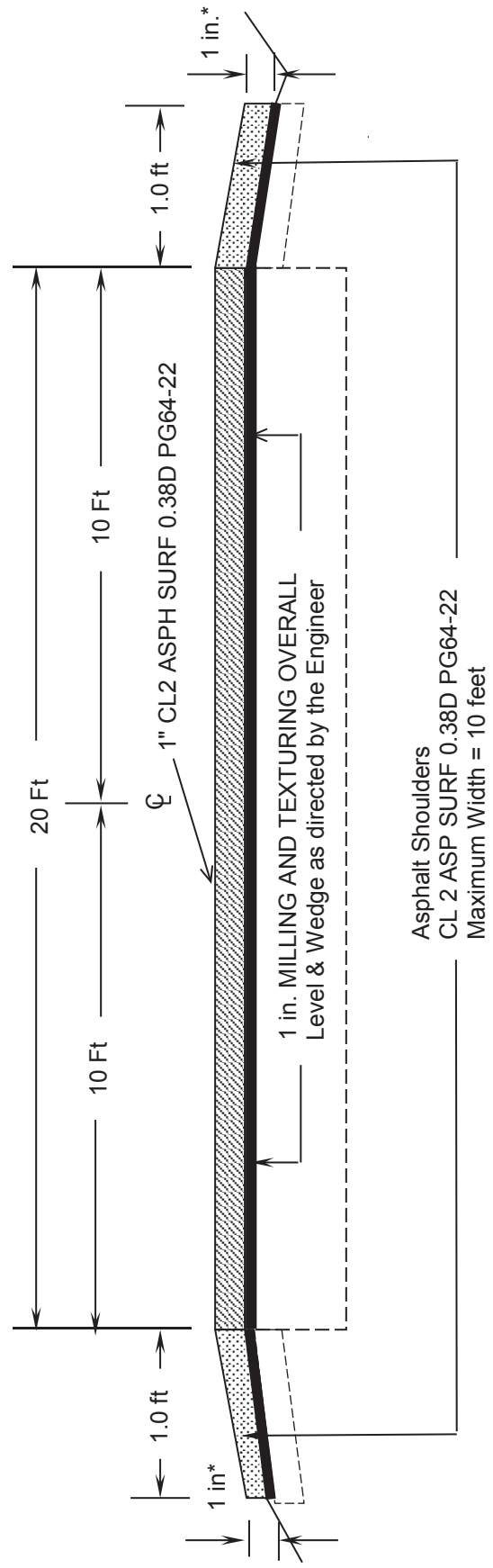
MILEPOINT	INTERSECTION	X-WALKS 12 INCH LF	STOP BARS 24 INCH LF	ARROWS			CATRAXX 6 INCH LF	NOTES
				CURVE EA	STR EA	COMB EA		
0.000	KY 10		13					
1.757	AA HIGHWAY			6				
6.298	THIRD STREET	152						
6.375	SECOND STREET	68						
6.446	FIRST STREET	48						
6.517	KY 8	82	19					
<b>TOTAL</b>		<b>350</b>	<b>32</b>	<b>6</b>	<b>0</b>	<b>0</b>	<b>0</b>	

**FD05 019 0008 004-007  
TYPICAL SECTION**



**\* 1 Inch Maximum Drop-Off Where Existing Site Conditions Permit  
Asphalt shoulders may be placed monolithically**

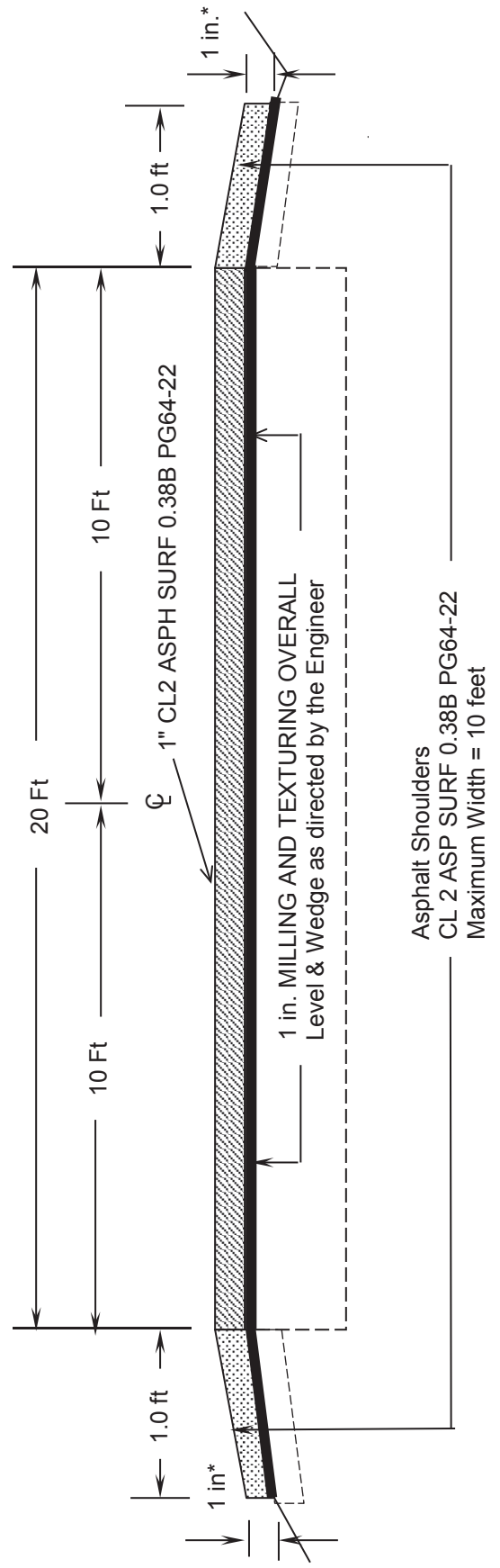
**FD05 019 0008 009-014  
TYPICAL SECTION**



**\* 1 Inch Maximum Drop-Off Where Existing Site Conditions Permit  
Asphalt shoulders may be placed monolithically**

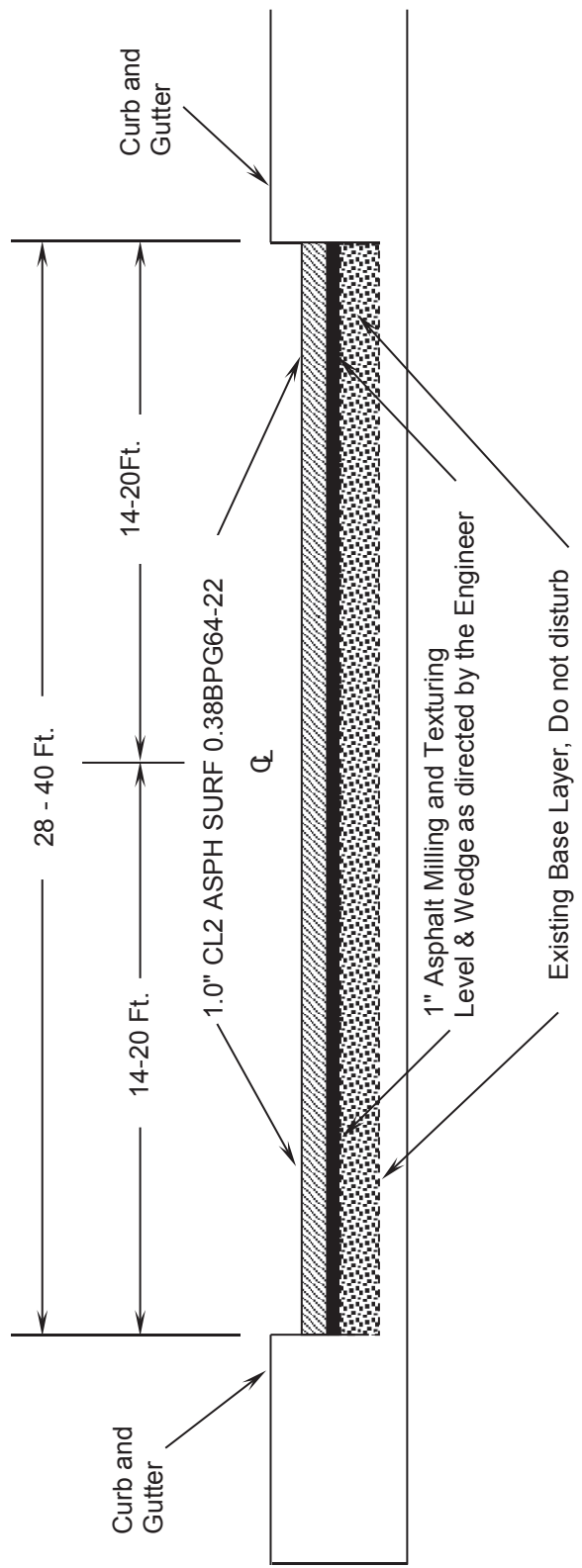
**Construct Edgeline Rumble Strips Milepoint 9.770-13.181**

**FD05 019 0547 000-007  
TYPICAL SECTION  
MILEPOINTS 0.000 - 6.298**



**\* 1 Inch Maximum Drop-Off Where Existing Site Conditions Permit  
Asphalt shoulders may be placed monolithically**

**FD05 019 0547 000-007**  
**TYPICAL SECTION**  
**MILEPOINT 6.298 - 6.517**



**PART II**  
**SPECIFICATIONS AND STANDARD DRAWINGS**



### **SPECIFICATIONS REFERENCE**

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2019* and *Standard Drawings, Edition of 2016*.

## **SUPPLEMENTAL SPECIFICATIONS**

The contractor shall use the Supplemental Specifications that are effective at the time of letting.  
The Supplemental Specifications can be found at the following link:

<http://transportation.ky.gov/Construction/Pages/Kentucky-Standard-Specifications.aspx>

## 2016 KENTUCKY STANDARD DRAWINGS

**FD05 019 0008 004-007**

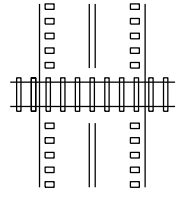
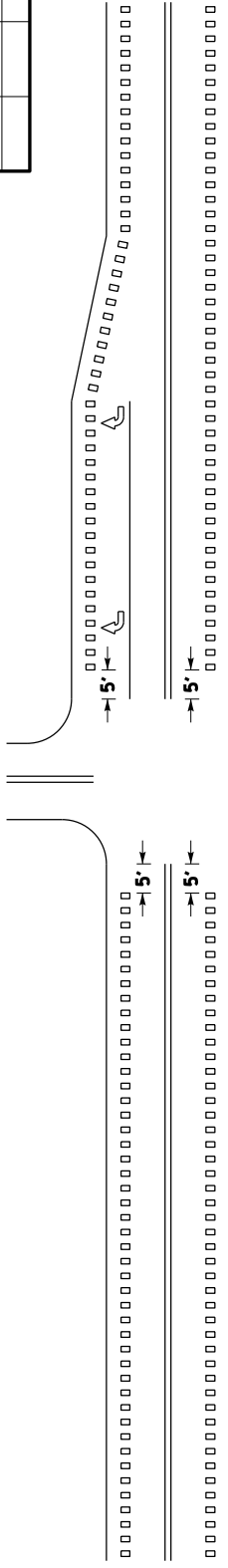
**FD05 019 0008 009-014**

**FD05 019 0547 000-007**

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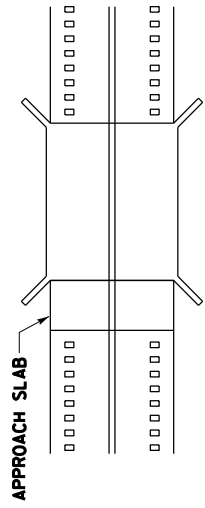
CURVE WIDENING AND SUPERELEVATION TRANSITIONS .....	RGS-001-07
MISCELLANEOUS STANDARDS .....	RGX-001-06
APPROACHES, ENTRANCES, AND MAIL BOX TURNOUT .....	RPM-110-07
LANE CLOSURE TWO-LANE HIGHWAY .....	TTC-100-04
SHOULDER CLOSURE.....	TTC-135-02
PAVEMENT CONDITION WARNING SIGNS .....	TTD-125-02
MOBILE OPERATION FOR PAINT STRIPING CASE I .....	TTS-100-02
MOBILE OPERATION FOR PAINT STRIPING CASE II.....	TTS-105-02

COUNTY OF	TOWNSHIP	SHEET OF



**INTERSECTIONS WITH OR WITHOUT  
RIGHT TURN LANES ①**

**MARKED CROSSWALK ②**



**DRIVEWAYS/MINOR  
COMMERCIAL ENTRANCES ⑤**

**MAILBOX TURNOUTS ⑥**

**NOTES ~**

- ① RUMBLE STRIPS SHALL BE OMITTED THROUGH MAJOR INTERSECTIONS WITH, OR WITHOUT, RIGHT-TURN LANES. OMIT RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF THE AREA WHERE EDGELINE PAVEMENT MARKINGS HAVE BEEN OMITTED (NORMALLY WHERE SIDE STREET RADIUS INTERSECTS MAINLINE).
- ② RUMBLE STRIPS SHALL NOT BE INSTALLED THROUGH MARKED CROSSWALKS. OMIT RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF MARKED CROSSWALKS.
- ③ RUMBLE STRIPS SHALL NOT BE INSTALLED ACROSS HIGHWAY-RAIL GRADE CROSSINGS.
- ④ RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS OR APPROACH SLABS.
- ⑤ RUMBLE STRIPS SHALL BE INSTALLED THROUGH DRIVEWAYS & MINOR COMMERCIAL ENTRANCES.
- ⑥ RUMBLE STRIPS SHALL BE INSTALLED THROUGH MAILBOX TURNOUTS.
7. RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS.

**DRAWING NOT TO SCALE**

**USE WITH SEPIA 006, 007,  
AND 008**

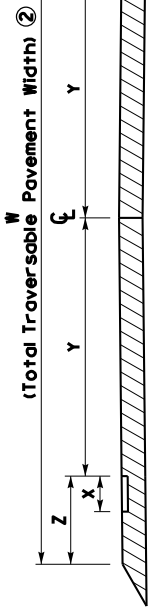
KENTUCKY  
DEPARTMENT OF HIGHWAYS  
**SHOULDER & EDGELINE  
RUMBLE STRIP DETAILS**

BID ITEMS AND UNIT TO BID  
EDGELINE RUMBLE STRIPS  
SHOULDER RUMBLE STRIPS

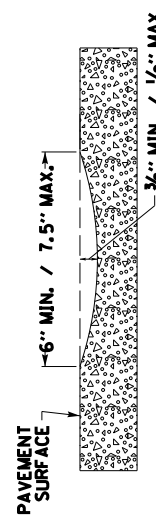
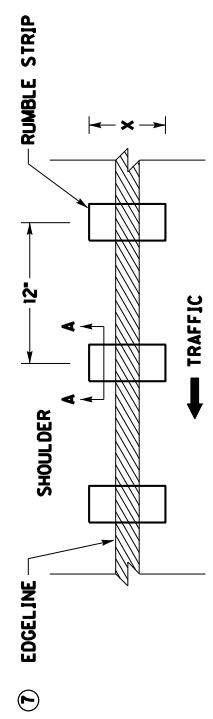
LF  
LF

SUBMITTED: *B. [Signature]* JL-23-16 DATE  
005

COUNT OF	TITLE	SHEET OF



**PAVEMENT CROSS-SECTION**



PAVEMENT WIDTH (W) ②	RUMBLE LENGTH (X) ⑤	ELRS ONLY		CLRS & ELRS	
		LANE WIDTH (Y) ③	SHOULDER WIDTH (Z) ④	LANE WIDTH (Y) ③	SHOULDER WIDTH (Z) ④
20'	8"	9'	1'	N/A	N/A
21'	8"	9.5'	1'	N/A	N/A
22'	8"	10'	1'	N/A	N/A
23'	8"	10'	1.5'	N/A	N/A
24'	8"	10.5'	1.5'	N/A	N/A
25'	8"	N/A	N/A	11'	1.5'
26'	8"	N/A	N/A	11'	2'
27'	8"	N/A	N/A	11.5'	2'
28'	8"	N/A	N/A	12'	2'
29'	8"	N/A	N/A	12'	2.5'
30'	8"	N/A	N/A	12'	3'
31'	8"	N/A	N/A	12'	3.5'
32'	8"	N/A	N/A	12'	4'
33'	8"	N/A	N/A	12'	4.5'

**NOTES**

1. EDGELINE RUMBLE STRIPS SHOULD BE INSTALLED ACCORDING TO THE DIMENSIONS PROPOSED ABOVE UNLESS THERE IS AN ENGINEERING BASIS THAT SUPPORTS A CHANGE IN DIMENSION. FOR EXAMPLE, IF THE EXISTING LANE WIDTH IS NARROWER THAN THE LANE WIDTH PROPOSED IN THIS DRAWING AND THE EXISTING SHOULDER PAVEMENT DEPTH IS NOT SUITABLE TO BE CONVERTED INTO A PORTION OF THE PROPOSED LANE WIDTH, THEN THE EXISTING LANE WIDTH SHOULD BE USED INSTEAD OF THE WIDTH PROPOSED IN THIS DRAWING.
2. PAVEMENT WIDTH (W) IS THE TOTAL WIDTH OF TRAVERSABLE PAVEMENT. DO NOT INCLUDE THE WIDTH OF ANY NON-TRAVERSABLE PAVEMENT, SUCH AS PAVEMENT WEDGES, WHEN MEASURING THE PAVEMENT WIDTH (W).
3. LANE WIDTH (Y) TO BE MEASURED FROM CENTER OF ROAD TO LANE SIDE EDGE OF RUMBLE STRIP.
4. PAVED SHOULDER WIDTH (Z) TO BE MEASURED FROM LANE SIDE EDGE OF RUMBLE STRIP TO OUTSIDE EDGE OF TRAVERSABLE PAVEMENT.
5. DISTANCES SHOWN ARE APPROXIMATE. MAINTAIN RUMBLE STRIP DIMENSIONS AND SPACING AS MUCH AS POSSIBLE. IF THE TYPICAL SECTION SHOWS A LANE WIDTH (Y) AND/OR SHOULDER WIDTH (Z) THAT DIFFERS FROM THE WIDTHS LISTED IN THIS DRAWING, THE ENGINEER SHALL DETERMINE THE LANE WIDTH (Y) AND/OR SHOULDER WIDTH (Z) AT THE TIME OF CONSTRUCTION.  
NOTE: CENTERLINE RUMBLE STRIPS SHOULD BE OMITTED WHEN THE LANE WIDTH (Y) IS LESS THAN 11 FT.
6. RUMBLE LENGTH (X) MAY BE MODIFIED AS THE ENGINEER DIRECTS. IF THE SHOULDER WIDTH (Z) IS EQUAL TO OR LESS THAN THE PROPOSED RUMBLE LENGTH (X).
7. PLACE THE EDGELINE MARKING IN THE CENTER OF THE RUMBLE STRIP.
8. EDGELINE RUMBLE STRIPS SHOULD BE OMITTED WHERE THE POSTED SPEED LIMIT IS 45 MPH OR LESS.

BID ITEM AND UNIT TO BID  
EDGELINE RUMBLE STRIPS

LF

DRAWING NOT TO SCALE

USE WITH SEPIA 005

KENTUCKY  
DEPARTMENT OF HIGHWAYS  
EDGELINE RUMBLE STRIP  
DETAILS  
TWO LANE ROADWAYS

SUBMITTED: *B. [Signature]*  
DATE: 11-23-16  
006

COUNTY OF	SHEET NO.
ITEM NO.	

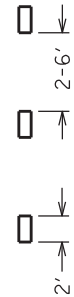
**STRIPING NOTES:**

- ARROWS SHALL BE USED IN ANY EXCLUSIVE TURN LANES.
- IN A SINGLE TURN LANE, DOTTED WHITE LINE EXTENSIONS MAY BE USED THROUGH THE TAPER OF THE TURN LANE.
- IF USED, DOTTED WHITE LINE EXTENSIONS SHALL BE NORMAL WIDTH, AND SHOULD BE 2' LONG, WITH A GAP OF 2-6' BETWEEN EACH LINE.
- IN DUAL TURN LANES, DOTTED WHITE LINE EXTENSIONS SHOULD BE USED THROUGH THE TAPER OF THE TURN LANE. BOTH SOLID LINES FORMING THE TURN LANES SHALL BEGIN AT THE DOWNSTREAM END OF THE TAPER.

**ARROW SPACING NOTES:**

- IN SINGLE-DIRECTION TURN LANES, ARROWS SHOULD BE SPACED AS FOLLOWS:
- AT LEAST TWO ARROWS SHOULD BE USED IN EACH TURN LANE. HOWEVER, IF A TURN LANE IS LESS THAN 80' IN LENGTH, THE DOWNSTREAM ARROW MAY BE ELIMINATED.
- THE FIRST UPSTREAM ARROW SHALL BE PLACED AT THE BEGINNING OF THE SOLID LINE FOR THE TURN LANE.
- THE LAST DOWNSTREAM ARROW SHOULD BE PLACED 40' FROM THE STOP BAR.
- ANY ADDITIONAL ARROWS SHOULD BE EVENLY SPACED. SPACING SHOULD NOT EXCEED 80'.
- ARROW SPACING AND NUMBER OF ARROWS MAY VARY BASED ON SITE CONDITIONS.

**DOTTED EXTENSION DIMENSIONS:**



Dotted extensions shall be normal width.

**KENTUCKY**  
**DEPARTMENT OF HIGHWAYS**

**TYPICAL MARKINGS FOR TURN LANES**

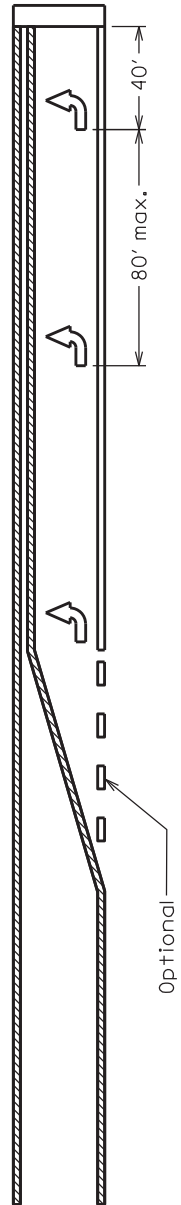
SUBMITTED: *R. [Signature]* DATE: 11-30-18 **042**

**DRAWING NOT TO SCALE**

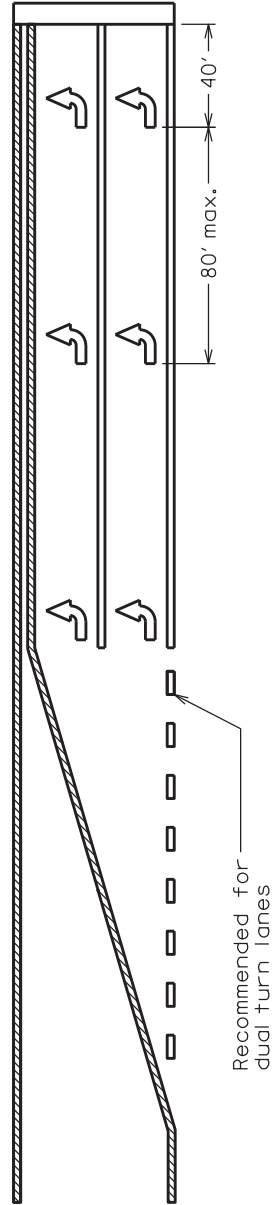
**LEGEND**

MARKINGS	WHITE
	YELLOW

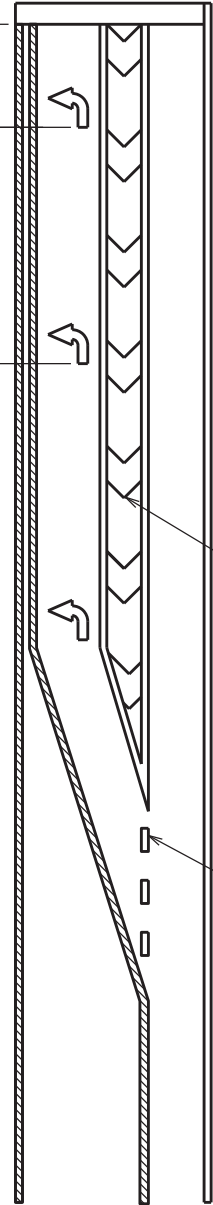
Single turn lane



Dual turn lane



Offset turn lane



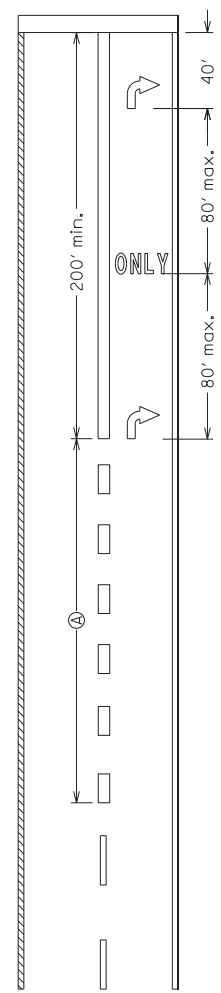
Chevron markings shall be used for offsets greater than 6'. Follow crosshatching guidelines shown in Sepia 046 for dimensions and spacing.

SHEET NO.	
COUNTY OF	
ITEM NO.	

**LANE\_DROP\_MARKINGS\_NOTES:**

- IN SITUATIONS WHERE A THROUGH LANE BECOMES A MANDATORY TURN LANE, THE FOLLOWING GUIDELINES APPLY:
- A WIDE SOLID LINE SHOULD EXTEND BACK A MINIMUM OF 200' FROM THE STOP BAR.
- A WIDE, DOTTED LINE SHALL EXTEND FROM THE END OF THE SOLID LINE BACK A MINIMUM OF THE DISTANCE SHOWN IN THE CHART (A). THESE LINES SHALL BE 3' LONG, WITH A SPACE OF 9' BETWEEN LINES.
- ALTERNATING ARROWS AND "ONLY" WORD MESSAGES SHALL BE USED, WITH THE FIRST AND LAST MARKING BEING AN ARROW.
- ALTERNATING ARROWS AND "ONLY" WORD MESSAGES SHOULD BE SPACED EVENLY, FOLLOWING GUIDELINES FOR ARROW SPACING. THESE SYMBOLS SHALL EXTEND BACK AT LEAST TO THE END OF THE SOLID STRIPE, BUT MAY BE EXTENDED BACK FARTHER IF ADDITIONAL GUIDANCE IS NEEDED.

Lane drop scenario



Speed Limit	(A)
25	125'
35	245'
45	540'
55	660'
65	780'

**WIDE DOTTED LANE LINE DIMENSIONS:**

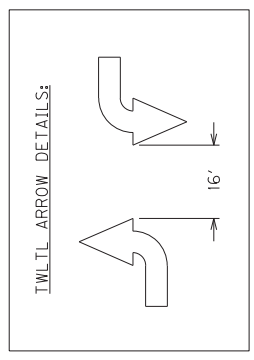
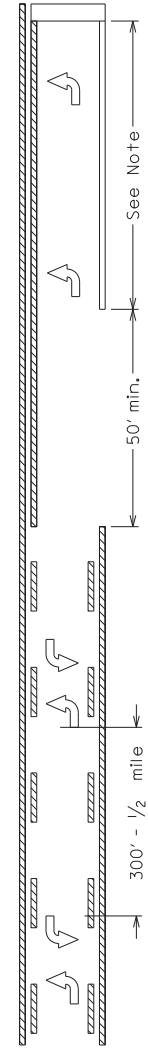
3' → [Symbol] → 9' → [Symbol] → 3'

Dotted lane lines shall be twice the normal width in lane drop scenarios.

**TWO-WAY LEFT-TURN LANE NOTES:**

- IN A TWO-WAY LEFT-TURN LANE, THE FOLLOWING GUIDELINES APPLY:
- ONE SET OF ARROWS SHOULD BE PLACED AT OR NEAR THE BEGINNING OF THE TWO-WAY LEFT-TURN LANE.
- ADDITIONAL SETS OF ARROWS SHOULD BE PLACED THROUGHOUT THE TWO-WAY LEFT-TURN LANE IF LEFT TURN MOVEMENTS ARE EXPECTED. THEY SHOULD BE SPACED NO LESS THAN 300' AND NO MORE THAN 1/2 MILE.
- THE SPACING BETWEEN EACH ARROW IN A SINGLE ARROW SET SHOULD BE 16 FEET.
- TWO-WAY LEFT-TURN LANES SHALL TERMINATE IN A DEDICATED LEFT-TURN LANE AT A SIGNALIZED INTERSECTION. THEY MAY TERMINATE IN A DEDICATED LEFT-TURN LANE AT OTHER LOCATIONS IF DEEMED NECESSARY.
- CONTACT TRAFFIC ENGINEER FOR RECOMMENDED DISTANCE FOR LEFT TURN STORAGE AT INTERSECTIONS.
- REFER TO THE TRAFFIC OPERATIONS GUIDANCE MANUAL SECTION TO-504 FOR MORE GUIDANCE ON TWO-WAY LEFT-TURN LANES.

Two-way left-turn lane



DRAWING NOT TO SCALE

**LEGEND**

MARKINGS	WHITE
	YELLOW

**KENTUCKY DEPARTMENT OF HIGHWAYS**

**TYPICAL MARKINGS FOR TURN LANES**

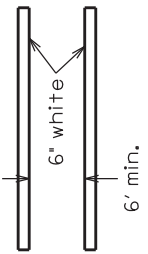
SUBMITTED: *R. Offenberg* DATE: 4-22-19

043

SHEET NO.	ITEM NO.	COUNTY OF
-----------	----------	-----------

CROSSWALK DETAIL

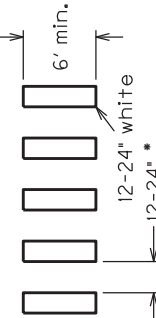
"TRANSVERSE"



TRANSVERSE LINES SHOULD EXTEND ACROSS THE FULL WIDTH OF THE APPROACH PAVEMENT.

TRANSVERSE LINES MAY BE COMBINED WITH LONGITUDINAL LINES TO FORM A "LADDER-STYLE" CROSSWALK.

"LONGITUDINAL"

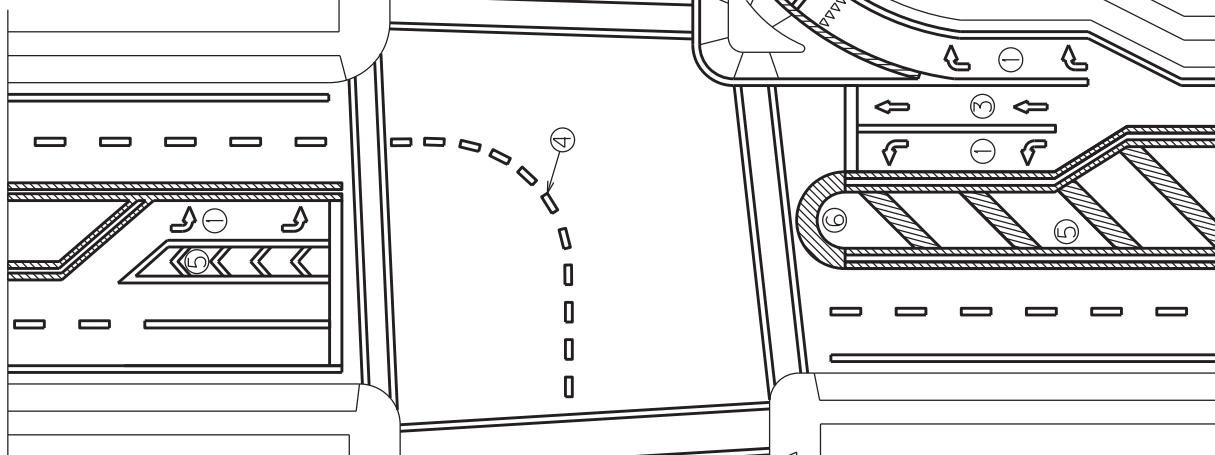


CROSSWALK BARS SHALL BE INSTALLED PARALLEL WITH ONCOMING TRAFFIC.

\*SPACING OF BARS SHOULD BE MODIFIED SO AS TO AVOID TIRE PATHS OF APPROACHING VEHICLES.

STOP LINE NOTES

1. STOP LINES SHALL BE 24" WHITE AND SHALL EXTEND ACROSS ALL APPROACH LANES.
2. STOP LINES SHOULD BE PLACED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF A MARKED CROSSWALK. IN THE ABSENCE OF A MARKED CROSSWALK, STOP LINES SHOULD BE PLACED NO MORE THAN 30' OR NO LESS THAN 4' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
3. STOP LINES IN LEFT TURN LANES MAY BE PULLED BACK SO AS NOT TO INTERFERE WITH THE WHEEL PATH OF TURNING VEHICLES.



SIDEWALK

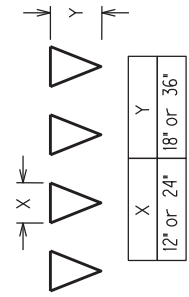
SIDEWALK

SIDEWALK

SIDEWALK

- ① ARROWS SHALL BE USED IN ALL DEDICATED TURN LANES AT SIGNALIZED INTERSECTIONS. REFER TO SEPIA 042 FOR GUIDANCE ON SPACING.
- ② ARROWS, ALONG WITH THE WORD "ONLY", SHALL ONLY BE USED IN A LANE DROP SCENARIO. REFER TO SEPIA 043 FOR GUIDANCE ON SPACING.
- ③ ARROWS ARE OPTIONAL IN THROUGH LANES.
- ④ DOTTED LANE LINE EXTENSIONS SHALL BE USED WITH DUAL TURN LANES. THEY SHALL BE THE SAME COLOR OF THE LINES WHICH THEY EXTEND AND SHOULD BE 6" IN WIDTH, 2-IN LENGTH, WITH A GAP OF 2-6' BETWEEN LINES.
- ⑤ REFER TO SEPIA 046 FOR GUIDANCE ON, AND DIMENSIONS OF, CROSSHATCH AND CHEVRON MARKINGS.
- ⑥ REFER TO SEPIA 046 FOR GUIDANCE ON MEDIAN NOSES.
- ⑦ SOLID LINE SEPARATING THROUGH LANES SHOULD EXTEND BACK A MINIMUM OF 50' FROM THE STOP BAR.

YIELD LINE DETAIL



NOTE: SPACING BETWEEN TRIANGLES SHOULD BE 3-12"

DRAWING NOT TO SCALE

LEGEND	
MARKINGS	WHITE
	YELLOW

KENTUCKY  
DEPARTMENT OF HIGHWAYS

TYPICAL MARKINGS  
AT SIGNALIZED  
INTERSECTIONS

SUBMITTED *R. [Signature]*  
DATE 11-30-18  
046

\*NOTE: OTHER THAN LONGITUDINAL STRIPING, ALL MARKINGS SHOWN ON THIS DRAWING SHOULD BE THERMOPLASTIC.



## **PART III**

### **EMPLOYMENT, WAGE AND RECORD REQUIREMENTS**

**TRANSPORTATION CABINET  
DEPARTMENT OF HIGHWAYS**

**LABOR AND WAGE REQUIREMENTS  
APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS**

- I. Application
- II. Nondiscrimination of Employees (KRS 344)

**I. APPLICATION**

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual because of his race, color, religion, national origin, sex, disability or age forty (40) and over, in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administrating agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

Revised: January 25, 2017

**II. NONDISCRIMINATION OF EMPLOYEES**

**AN ACT OF THE KENTUCKY  
GENERAL ASSEMBLY TO PREVENT  
DISCRIMINATION IN EMPLOYMENT  
KRS CHAPTER 344  
EFFECTIVE JUNE 16, 1972**

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (forty and above); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age forty (40) and over. The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, except that such a notice or advertisement may indicate a preference, limitation, or specification based on religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, when religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, is a bona fide occupational qualification for employment.

## EXECUTIVE BRANCH CODE OF ETHICS

In the 1992 regular legislative session, the General Assembly passed and Governor Brereton Jones signed Senate Bill 63 (codified as KRS 11A), the Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (7) provides:

No present or former public servant shall, within six (6) months following termination of his office or employment, accept employment, compensation, or other economic benefit from any person or business that contracts or does business with, or is regulated by, the state in matters in which he was directly involved during the last thirty-six (36) months of his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, or for which he received, prior to his state employment, a professional degree or license, provided that, for a period of six (6) months, he personally refrains from working on any matter in which he was directly involved during the last thirty-six (36) months of his tenure in state government. This subsection shall not prohibit the performance of ministerial functions, including but not limited to filing tax returns, filing applications for permits or licenses, or filing incorporation papers, nor shall it prohibit the former officer or public servant from receiving public funds disbursed through entitlement programs.

KRS 11A.040 (9) states:

A former public servant shall not represent a person or business before a state agency in a matter in which the former public servant was directly involved during the last thirty-six (36) months of his tenure, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not as a way to obtain private benefits.

If you have worked for the executive branch of state government within the past six months, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, 3 Fountain Place, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Revised: January 27, 2017

### **Kentucky Equal Employment Opportunity Act of 1978**

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:  
**<https://www.eProcurement.ky.gov>**.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **[finance.contractcompliance@ky.gov](mailto:finance.contractcompliance@ky.gov)** or by phone at 502-564-2874.

# EMPLOYEE RIGHTS UNDER THE FAIR LABOR STANDARDS ACT

THE UNITED STATES DEPARTMENT OF LABOR WAGE AND HOUR DIVISION

## FEDERAL MINIMUM WAGE

# \$7.25 PER HOUR

BEGINNING JULY 24, 2009

**OVERTIME PAY** At least 1½ times your regular rate of pay for all hours worked over 40 in a workweek.

**CHILD LABOR** An employee must be at least **16** years old to work in most non-farm jobs and at least **18** to work in non-farm jobs declared hazardous by the Secretary of Labor.

Youths **14** and **15** years old may work outside school hours in various non-manufacturing, non-mining, non-hazardous jobs under the following conditions:

**No more than**

- **3** hours on a school day or **18** hours in a school week;
- **8** hours on a non-school day or **40** hours in a non-school week.

Also, work may not begin before **7 a.m.** or end after **7 p.m.**, except from June 1 through Labor Day, when evening hours are extended to **9 p.m.** Different rules apply in agricultural employment.

**TIP CREDIT** Employers of “tipped employees” must pay a cash wage of at least \$2.13 per hour if they claim a tip credit against their minimum wage obligation. If an employee’s tips combined with the employer’s cash wage of at least \$2.13 per hour do not equal the minimum hourly wage, the employer must make up the difference. Certain other conditions must also be met.

**ENFORCEMENT** The Department of Labor may recover back wages either administratively or through court action, for the employees that have been underpaid in violation of the law. Violations may result in civil or criminal action.

Employers may be assessed civil money penalties of up to \$1,100 for each willful or repeated violation of the minimum wage or overtime pay provisions of the law and up to \$11,000 for each employee who is the subject of a violation of the Act’s child labor provisions. In addition, a civil money penalty of up to \$50,000 may be assessed for each child labor violation that causes the death or serious injury of any minor employee, and such assessments may be doubled, up to \$100,000, when the violations are determined to be willful or repeated. The law also prohibits discriminating against or discharging workers who file a complaint or participate in any proceeding under the Act.

### ADDITIONAL INFORMATION

- Certain occupations and establishments are exempt from the minimum wage and/or overtime pay provisions.
- Special provisions apply to workers in American Samoa and the Commonwealth of the Northern Mariana Islands.
- Some state laws provide greater employee protections; employers must comply with both.
- The law requires employers to display this poster where employees can readily see it.
- Employees under 20 years of age may be paid \$4.25 per hour during their first 90 consecutive calendar days of employment with an employer.
- Certain full-time students, student learners, apprentices, and workers with disabilities may be paid less than the minimum wage under special certificates issued by the Department of Labor.

For additional information:



# 1-866-4-USWAGE

(1-866-487-9243) TTY: 1-877-889-5627



# WWW.WAGEHOUR.DOL.GOV

**PART IV**  
**INSURANCE**

Refer to  
*Kentucky Standard Specifications for Road and Bridge Construction,*  
current edition

**PART V**  
**BID ITEMS**

### PROPOSAL BID ITEMS

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#### Section: 0001 - ASPHALT PAVING

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0010	00190		LEVELING & WEDGING PG64-22	290.00	TON		\$	
0020	00301		CL2 ASPH SURF 0.38D PG64-22	4,035.00	TON		\$	
0030	00307		CL2 ASPH SURF 0.38B PG64-22	5,100.00	TON		\$	
0040	00356		ASPHALT MATERIAL FOR TACK	66.60	TON		\$	
0050	02676		MOBILIZATION FOR MILL & TEXT (KY 547)	1.00	LS		\$	
0060	02676		MOBILIZATION FOR MILL & TEXT (KY 8 MILEPOINT 004-007)	1.00	LS		\$	
0070	02676		MOBILIZATION FOR MILL & TEXT (KY 8 MILEPOINT 009-014)	1.00	LS		\$	
0080	02677		ASPHALT PAVE MILLING & TEXTURING	9,135.00	TON		\$	
0090	02697		EDGE LINE RUMBLE STRIPS	36,000.00	LF		\$	
0100	03240		BASE FAILURE REPAIR	523.00	SQYD		\$	
0110	06510		PAVE STRIPING-TEMP PAINT-4 IN	254,450.00	LF		\$	
0120	06514		PAVE STRIPING-PERM PAINT-4 IN	231,160.00	LF		\$	
0130	06566		PAVE MARKING-THERMO X-WALK-12 IN	400.00	LF		\$	
0140	06568		PAVE MARKING-THERMO STOP BAR-24IN	32.00	LF		\$	
0150	06574		PAVE MARKING-THERMO CURV ARROW	6.00	EACH		\$	
0160	10020NS		FUEL ADJUSTMENT	14,675.00	DOLL	\$1.00	\$	\$14,675.00
0170	10030NS		ASPHALT ADJUSTMENT	36,850.00	DOLL	\$1.00	\$	\$36,850.00

#### Section: 0002 - TRAFFIC SIGNAL ITEMS

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0180	04792		CONDUIT-1 IN	10.00	LF		\$	
0190	04811		ELECTRICAL JUNCTION BOX TYPE B	1.00	EACH		\$	
0200	04820		TRENCHING AND BACKFILLING	100.00	LF		\$	
0210	04830		LOOP WIRE	344.00	LF		\$	
0220	04850		CABLE-NO. 14/1 PAIR	150.00	LF		\$	
0230	04895		LOOP SAW SLOT AND FILL	130.00	LF		\$	
0240	24900EC		PVC CONDUIT-1 1/4 IN-SCHEDULE 80	100.00	LF		\$	
0250	24963ED		LOOP TEST	1.00	EACH		\$	

#### Section: 0003 - MAINTAIN & CONTROL TRAFFIC

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0260	02014		BARRICADE-TYPE III	14.00	EACH		\$	
0270	02562		TEMPORARY SIGNS	1,140.00	SQFT		\$	
0280	02650		MAINTAIN & CONTROL TRAFFIC (KY 547)	1.00	LS		\$	
0290	02650		MAINTAIN & CONTROL TRAFFIC (KY 8 MILEPOINT 004-007)	1.00	LS		\$	
0300	02650		MAINTAIN & CONTROL TRAFFIC (KY 8 MILEPOINT 009-014)	1.00	LS		\$	



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### PROPOSAL BID ITEMS

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#### Section: 0004 - DEMOBILIZATION

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0310	02569		DEMOBILIZATION	1.00	LS		\$	