



**CALL NO. 404**

**CONTRACT ID. 213195**

**HANCOCK COUNTY**

**FED/STATE PROJECT NUMBER 046GR21R069-CB06**

**DESCRIPTION VARIOUS ROUTES IN HANCOCK COUNTY**

**WORK TYPE ASPHALT RESURFACING**

**PRIMARY COMPLETION DATE 6/30/2022**

**LETTING DATE: October 22,2021**

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 am EASTERN DAYLIGHT TIME October 22,2021. Bids will be publicly announced at 10:00 am EASTERN DAYLIGHT TIME.

**NO PLANS ASSOCIATED WITH THIS PROJECT.**

**REQUIRED BID PROPOSAL GUARANTY:** Not less than 5% of the total bid.

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**PART I**  
**SCOPE OF WORK**

**ADMINISTRATIVE DISTRICT - 02**

**CONTRACT ID - 213195**  
**046GR21R069-CB06**  
**COUNTY - HANCOCK**  
**PCN - 0204613892102**  
**CB06 046 1389 002-004**

KY-1389 (KY 1389) (MP 2.452) FROM LASLIE LOOP EXTENDING EAST TO KY 271 (MP 3.091), A DISTANCE OF 0.60 MILES.ASPHALT RESURFACING  
GEOGRAPHIC COORDINATES LATITUDE 37:52:10.45 LONGITUDE 86:49:51.89

**PCN - 0204618472101**  
**CB06 046 1847 000-003**

KY-1847 (KY 1847) (MP 0.000) FROM KY 271 EXTENDING EAST TO 2.136 (MP 2.136), A DISTANCE OF 02.14 MILES.ASPHALT RESURFACING  
GEOGRAPHIC COORDINATES LATITUDE 37:53:59.87 LONGITUDE 86:47:02.07

**PCN - 0204621812101**  
**CB06 046 2181 001-002**

KY-2181 (KY 2181) (MP 1.200) FROM 0.008 MILES NORTH OF DUTCH BILL LOOP EXTENDING NORTH TO 0.121 MILES NORTH OF DUTCH BILL LOOP (MP 1.700), A DISTANCE OF 0.50 MILES.ASPHALT RESURFACING  
GEOGRAPHIC COORDINATES LATITUDE 37:46:13.14 LONGITUDE 86:48:43.03

**COMPLETION DATE(S):**

COMPLETED BY 06/30/2022	APPLIES TO THE ENTIRE CONTRACT
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## **CONTRACT NOTES**

### **PROPOSAL ADDENDA**

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

### **BID SUBMITTAL**

Bidder must use the Department's electronic bidding software. The Bidder must download the bid file located on the Bid Express website ([www.bidx.com](http://www.bidx.com)) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

### **JOINT VENTURE BIDDING**

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

### **UNDERGROUND FACILITY DAMAGE PROTECTION**

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. When prescribed in said directives, the contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom shall be contacted through their individual Protection Notification Center. Non-compliance with these directives can result in the enforcement of penalties.

### **REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY**

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth ("certificate") from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

**For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity's solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.**

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

### **SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT**

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by fax (502) 564-7299 or email to [kytc.projectquestions@ky.gov](mailto:kytc.projectquestions@ky.gov). The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading "Questions & Answers" on the Construction Procurement website ([www.transportation.ky.gov/contract](http://www.transportation.ky.gov/contract)). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

### **HARDWOOD REMOVAL RESTRICTIONS**

The US Department of Agriculture has imposed a quarantine in Kentucky and several surrounding states, to prevent the spread of an invasive insect, the emerald ash borer. Hardwood cut in conjunction with the project may not be removed from the state. Chipping or burning on site is the preferred method of disposal.

### **INSTRUCTIONS FOR EXCESS MATERIAL SITES AND BORROW SITES**

Identification of excess material sites and borrow sites shall be the responsibility of the Contractor. The Contractor shall be responsible for compliance with all applicable state and federal laws and may wish to consult with the US Fish and Wildlife Service to seek protection under Section 10 of the Endangered Species Act for these activities.

### **ACCESS TO RECORDS**

The contractor, as defined in KRS 45A.030 (9) agrees that the contracting agency, the Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this contract for the purpose of financial audit or program review. Records and other prequalification information confidentially

disclosed as part of the bid process shall not be deemed as directly pertinent to the contract and shall be exempt from disclosure as provided in KRS 61.878(1)(c). The contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884.

In the event of a dispute between the contractor and the contracting agency, Attorney General, or the Auditor of Public Accounts over documents that are eligible for production and review, the Finance and Administration Cabinet shall review the dispute and issue a determination, in accordance with Secretary's Order 11-004.

April 30, 2018

## **SPECIAL NOTE FOR RECIPROCAL PREFERENCE**

### **RECIPROCAL PREFERENCE TO BE GIVEN BY PUBLIC AGENCIES TO RESIDENT BIDDERS**

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the electronic bidding software. Submittal of the Affidavit should be done along the bid in Bid Express.

April 30, 2018



### **ASPHALT MIXTURE**

Unless otherwise noted, the Department estimates the rate of application for all asphalt mixtures to be 110 lbs/sy per inch of depth.

### **DGA BASE**

Unless otherwise noted, the Department estimates the rate of application for DGA Base to be 115 lbs/sy per inch of depth.

### **INCIDENTAL SURFACING**

The Department has included in the quantities of asphalt mixtures established in the proposal estimated quantities required for resurfacing or surfacing mailbox turnouts, farm field entrances, residential and commercial entrances, curve widening, ramp gores and tapers, and road and street approaches, as applicable. Pave these areas to the limits as shown on Standard Drawing RPM-110-06 or as directed by the Engineer. In the event signal detectors are present in the intersecting streets or roads, pave the crossroads to the right of way limit or back of the signal detector, whichever is the farthest back of the mainline. Surface or resurface these areas as directed by the Engineer. The Department will not measure placing and compacting for separate payment but shall be incidental to the Contract unit price for the asphalt mixtures.

### **FUEL AND ASPHALT PAY ADJUSTMENT**

The Department has included the Contract items Asphalt Adjustment and Fuel Adjustment for possible future payments at an established Contract unit price of \$1.00. The Department will calculate actual adjustment quantities after work is completed. If existing Contract amount is insufficient to pay all items on the contract with the adjustments, the Department will establish additional monies with a change order.

### **OPTION B**

Be advised that the Department will control and accept compaction of asphalt mixtures furnished on this project under OPTION B in accordance with Sections 402 and 403.

## Special Notes for Contract

### Level and Wedging Bid Item:

Approximately 200 Tons of Leveling and Wedging Bid Item is intended to be used for superelevation corrections at MP 1.20 and MP 1.58. Leveling and Wedging depths will vary between 1 to 8 Inches. A combination of Asphalt Base and Asphalt Surface will to be used. Exact locations and depths to be determined by Engineer.

### Asphalt Base Bid Item:

Approximately 40 tons of Asphalt Base is intended to build a turnaround on Old KY 2181. Approximately 60 tons of Asphalt Base is intended to be used to widen the roadway on KY 2181 at MP 1.20 and MP 1.58. It is the intent to have the widening pavement structure shall match the existing pavement structure. Exact locations and depths to be determined by Engineer.

### Construct Turnaround on Old KY 2181:

Construct at turnaround on old KY 2181. It will be approximately 24 foot wide and 50 foot long. The pavement structure will be 6 IN of DGA, 4.5 IN of Asphalt Base and 1 IN of Asphalt Surface. It will also require a 50 foot Longitudinal Edge Key to tie the Asphalt Base into the existing roadway. Approximately 45 Linear Feet of 15 IN culvert pipe will also be installed in the roadside ditch, to accommodate this widening. The area needs to be graded to drain. All grade work will be incidental to the DGA.

### Widening Areas:

Widening areas will be excavated to receive 6 IN of DGA plus the existing pavement structure. Excavation for widening will be incidental to the DGA. Exact locations and depths to be determined by Engineer. Longitudinal Edge Key will be used at all widening locations.

### Remove Pavement:

The pavement removal area needs to be graded to drain. All grade work will be incidental to Remove Pavement. Backfill material needed to establish seed will be incidental to Remove Pavement. Remove Pavement areas will receive Permanent Seed and Protection.

## **SPECIAL PROVISION FOR WASTE AND BORROW SITES**

Obtain U.S. Army Corps of Engineer's approval before utilizing a waste or borrow site that involves "Waters of the United States". The Corps of Engineers defines "Waters of the United States" as perennial or intermittent streams, ponds or wetlands. The Corps of Engineers also considers ephemeral streams, typically dry except during rainfall but having a defined drainage channel, to be jurisdictional waters. Direct questions concerning any potential impacts to "Waters of the United States" to the attention of the appropriate District Office for the Corps of Engineers for a determination prior to disturbance. Be responsible for any fees associated with obtaining approval for waste and borrow sites from the U.S. Army Corps of Engineer or other appropriate regulatory agencies.

1-296 Waste & Borrow Sites  
01/02/2012

## SPECIAL NOTE FOR NON-TRACKING TACK COAT

1. DESCRIPTION AND USAGE. This specification covers the requirements and practices for applying a non-tracking tack asphalt coating. Place this material on the existing pavement course, prior to placement of a new asphalt pavement layer. Use when expedited paving is necessary or when asphalt tracking would negatively impact the surrounding area. This material is not suitable for other uses. Ensure material can “break” within 15 minutes under conditions listed in 3.2.
2. MATERIALS, EQUIPMENT, AND PERSONNEL.

2.1 Non-Tracking Tack. Provide material conforming to Subsection 2.1.1.

2.1.1 Provide a tack conforming to the following material requirements:

Property	Specification	Test Procedure
Viscosity, SFS, 77 ° F	20 – 100	AASHTO T 72
Sieve, %	0.3 max.	AASHTO T 59
Asphalt Residue <sup>1</sup> , %	50 min.	AASHTO T 59
Oil Distillate, %	1.0 max.	AASHTO T 59
Residue Penetration, 77 ° F	20 max.	AASHTO T 49
Original Dynamic Shear (G*/sin δ), 82 ° C	1.0 min.	AASHTO T 315
Softening Point, ° F	149 min.	AASHTO T 53
Solubility, %	97.5 min.	AASHTO T 44

<sup>1</sup> Bring sample to 212 °F over a 10-15 minute period. Maintain 212 °F for 15-20 minutes or until 30-40 mL of water has distilled. Continue distillation as specified in T59.

- 2.2. Equipment. Provide a distributor truck capable of heating, circulating, and spraying the tack between 170 °F and 180 °F. Do not exceed 180 °F. Circulate the material while heating. Provide the correct nozzles that is recommend by the producer to ensure proper coverage of tack is obtained. Ensure the bar can be raised to between 14” and 18” from the roadway.
- 2.3. Personnel. Ensure the tack supplier has provided training to the contractor on the installation procedures for this product. Make a technical representative from the supplier available at the request of the Engineer.

3. CONSTRUCTION.

3.1 Surface Preparation. Prior to the application of the non-tracking tack, ensure the pavement surface is thoroughly dry and free from dust or any other debris that would inhibit adhesion. Clean the surface by scraping, sweeping, and the use of compressed air. Ensure this preparation process occurs shortly before application to prevent the return of debris pavement. If rain is expected within one hour after application, do not apply material. Apply material only when the surface is dry, and no precipitation is expected.

3.2 Non-tracking Tack Application. Ensure the roadway temperature is a minimum of 40 °F and rising during the application of the tack. This material is not suitable for use in colder temperatures. Prior to applying the tack, demonstrate competence in applying the tack according to this note to the satisfaction of the Engineer. Heat the tack in the distributor to between 170 – 180 °F. After initial heating to between 170 – 180 °F, the material may be sprayed between 165 °F and 180 °F. Do not apply outside this temperature range. Apply material at a minimum rate of 0.70 pounds (0.08 gallons) per square yard. Ensure full coverage of the material on the pavement surface. Full coverage of this material is critical. If full coverage is not achieved, material application rate may be increased to ensure full coverage. Do not heat material more than twice in one day.

3.3 Non-tracking Tack Certification. Furnish the tacks certification to the Engineer stating the material conforms to all requirements herein prior to use.

3.4 Sampling and Testing. The Department will require a sample of non-tracking tack be taken from the distributor at a rate of one sample per 15,000 tons of mix. Take two 1 gallon samples of the heated material and forward the sample to the Division of Materials for testing within 7 days. Ensure the product temperature is between 170 and 180 °F at the time of sampling.

4. MEASUREMENT. The Department will measure the quantity of non-tracking tack in tons. The Department will not measure for payment any extra materials, labor, methods, equipment, or construction techniques used to satisfy the requirements of this note. The Department will not measure for payment any trial applications of non-tracking tack, the cleaning of the pavement surface, or furnishing and placing the adhesive. The Department will consider all such items incidental to the non-tracking tack.

5. PAYMENT. The Department will pay for the non-tracking tack at the Contract unit bid price and apply an adjustment for each manufacturer's lot of material based on the degree of compliance as defined in the following schedule. When a sample fails on two or more tests, the Department may add the deductions, but the total deduction will not exceed 100 percent.

Non-Tracking Tack Price Adjustment Schedule						
Test	Specification	100% Pay	90% Pay	80% Pay	50% Pay	0% Pay
Viscosity, SFS, 77 ° F	20 – 100	19 - 102	17 - 18	15 - 16	14	≤13
			103 - 105	106 - 107	108 - 109	≥ 110
Sieve, %	0.30 max.	≤ 0.40	0.41 - 0.50	0.51 - 0.60	0.61 - 0.70	≥ 0.71
Asphalt Residue, %	50 min.	≥49.0	48.5 – 48.9	48.0 – 48.4	47.5-47.9	≤ 47.4
Oil Distillate, %	1.0 max.	≤1.0	1.1-1.5	1.6 - 1.7	1.8-1.9	>2.0
Residue Penetration, 77 ° F	20 max.	≤ 21	22 - 23	24 - 25	26 - 27	≥ 28
Original Dynamic Shear (G*/sin δ), 82 ° C	1.0 min.	≥0.95	0.92 – 0.94	0.90 – 0.91	0.85 - 0.89	≤ 0.84
Softening Point, ° F	149 min.	≥145	142 - 144	140 - 141	138 - 139	≤ 137
Solubility, %	97.5 min.	≥ 97.0	96.8 – 96.9	96.6 – 96.7	96.4 – 96.5	≤ 96.3

Code

24970EC

Pay Item

Asphalt Material for Tack Non-Tracking

Pay Unit

Ton

January 28, 2020

## **SPECIAL NOTE FOR BRIDGE DECK WATERPROOFING MEMBRANE AND ASPHALT OVERLAY ON BRIDGES**

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### **I. DESCRIPTION**

Except as specified herein, perform all work in accordance with the Department's Standard and Supplemental Specifications and Standard and Sepia Drawings, current editions. Article references are to the Standard Specifications. Furnish all materials, labor, equipment, and incidentals necessary to complete the One Step Bridge Deck Waterproofing Membrane.

### **II. MATERIALS**

**A. Maintain and Control Traffic.** See Traffic Control Plan

**B. One Step Membrane.** See Section 808.

**C. Asphalt Overlay.** Use the same class and type of asphalt surface mixture that is being placed on the adjacent roadway.

### **III. CONSTRUCTION METHODS**

**A. Maintain and Control Traffic.** See Traffic Control Plan

**B. Remove Asphalt Overlay.** When the bridge deck has an existing asphalt overlay, totally remove the overlay by milling and texturing, scarification, or other approved methods. Dispose of material off the right of way at sites obtained by the Contractor at no additional cost to the Department (See Special Note for Waste and Borrow).

**C. Surface Preparation.** After removal of the asphalt overlay, thoroughly clean the entire surface area of the bridge deck prior to paving by either air blasting or water blasting. If water blasting is used, thoroughly dry the area prior to placing the primer and membrane. Do not allow traffic on the cleaned bridge deck prior to applying the primer.

One Step Membrane  
Page 2 of 3

**D. Prime Coating.** Apply primer material to the bridge deck according to the Manufacturer's recommendations. Do not allow traffic on the primed bridge deck prior to placing the membrane.

**E. One Step Membrane.** Apply the membrane to the entire surface of the bridge deck. Overlap the membrane a minimum of 1 foot onto the adjacent roadway approaches. Extend the membrane up the face of the curbs a minimum of 1½ inches or the thickness of the asphalt overlay, whichever is greater. Overlap membrane as necessary to achieve 100% coverage of the bridge deck. Install as recommended by the manufacturer, straight and wrinkle free, with no curled or uplifted edges. Do not allow traffic on the membrane prior to placing the asphalt surface.

**F. Asphalt Surface.** Place the asphalt surface immediately after the membrane is in place. For crowned bridge decks, place the asphalt overlay so that it is 1" thicker than the nominal thickness specified in the Contract at the bridge centerline and taper to the specified nominal thickness at the gutter line. The intent is to increase the crown to accelerate water runoff from the bridge deck. For superelevated bridge decks, place the asphalt overlay at the nominal thickness specified in the Contract.

#### IV. METHOD OF MEASUREMENT

**A. Maintain and Control Traffic.** See Traffic Control Plan

**B. Remove Asphalt Overlay.** Regardless of the method of removal, the Department will measure removal of the existing asphalt overlay according to Section 408.04.01. If the Contract or the Engineer requires removal to be accomplished by milling and texturing, the Department will measure Mobilization for Asphalt Milling and Texturing according to Section 408.04.02.

**C. One Step Membrane.** The Department will measure the One Step Membrane in square yards. The Engineer will calculate the area of one step membrane (A) from the length of the bridge (L) plus 1 foot on each approach multiplied by the curb to curb width of the bridge (W):

$$A = \frac{(L+2) \times W}{9}$$

The Engineer will not measure surface preparation, primer, laps, cutoffs and waste for payment.



One Step Membrane  
Page 3 of 3

**V. METHOD OF PAYMENT**

**A. Maintain and Control Traffic.** See Traffic Control Plan

**B. Remove Asphalt Overlay.** Regardless of the method of removal, the Department will pay for removal of the existing asphalt overlay according to Section 408.05.01.

**C. One Step Membrane.** Payment at the contract unit price per square yard shall be full compensation for all labor, equipment, materials and incidentals for surface preparation and furnishing and placing the surface primer and the one step membrane according to the Manufacturer's recommendations and these notes.

1-3108 Waterproof Membrane by Contractor  
01/02/2012

### **SPECIAL NOTE FOR SHOULDER PREPARATION**

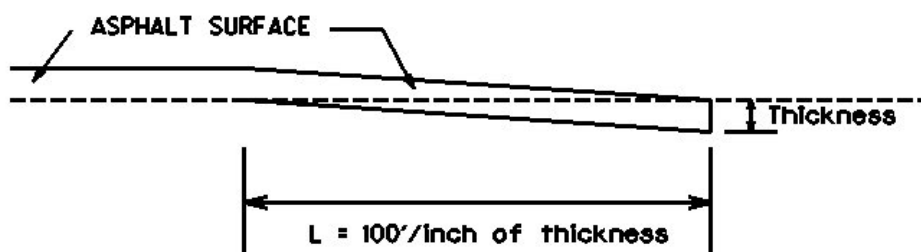
Grade, shape, and compact shoulder as directed by the Engineer to provide proper template and foundation to accommodate widening and superelevation corrections. The Department will not measure grading, shaping, and compacting shoulders for separate payment, but shall be incidental to the dense graded aggregate, asphalt base and/or surface placed on the shoulder.

1-3245 Shoulder Preparation Contractor  
01/02/2012

## SPECIAL NOTE FOR EDGE KEY

Construct Edge Keys at the beginning of project, end of project, at railroad crossings, and at ramps, as applicable. Unless specified in the Contract or directed by the Engineer, do not construct edge keys at intersecting streets, roads, alleys, or entrances. Cut out the existing asphalt surface to the required depth and width shown on the drawing and heel the new surface into the existing surface. The Department will make payment for this work at the Contract unit price per ton for Asphalt Pavement Milling and Texturing, which shall be full compensation for all labor, materials, equipment, and incidentals for removal and disposal of the existing asphalt surface required to construct the edge key.

### EDGE KEY



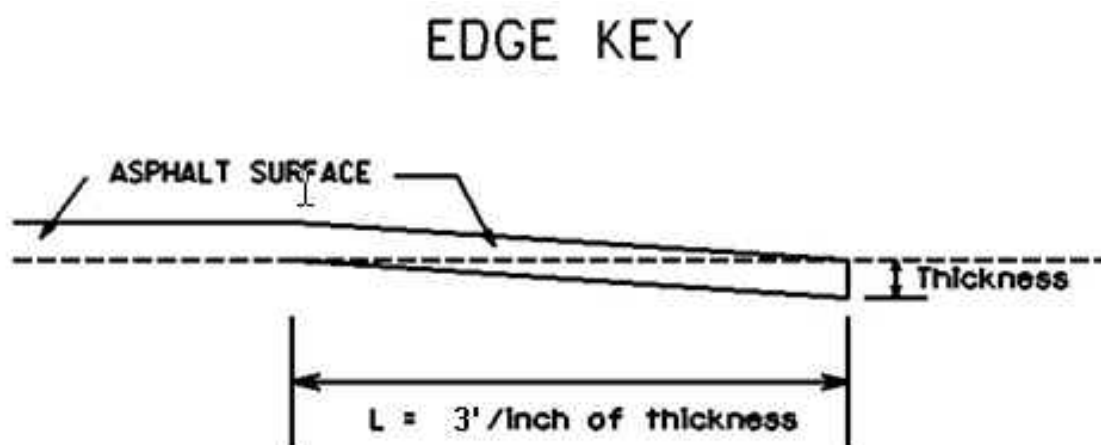
Thickness = 1 Inch

L = 100 LF

L = Length of Edge Key

## SPECIAL NOTE FOR EDGE KEY

Construct Edge Keys at the beginning of project, end of project, at railroad crossings, and at intersections with ramps, as applicable. Unless specified in the Contract or directed by the Engineer, do not construct edge keys at intersecting streets, roads, alleys, or entrances. Cut out the existing asphalt surface to the required depth and width shown on the drawing and heel the new surface into the existing surface. The Department will measure the Edge Key at the joint as the width of the pavement perpendicular to the centerline in linear feet. The Department will pay for this work at the Contract unit price per linear foot, which shall be full compensation for all labor, materials, equipment, and incidentals for removal and disposal of the existing asphalt surface required to construct the edge key.



**Thickness = 1 Inch**

**L = 3 LF**

**L = Length of Edge Key**

1-3310 Edge Key by LF  
01/02/2012

## SPECIAL NOTES FOR GUARDRAIL

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### I. DESCRIPTION

Except as specified herein, perform all work in accordance with the Department's Standard and Supplemental Specifications and Standard and Sepia Drawings, current editions. Article references are to the Standard Specifications.

Furnish all equipment, labor, materials, and incidentals for the following work items:

(1) Site preparation; (2) Remove and replace existing guardrail systems; (3) Construct Bridge guardrail systems; (4) Delineators for guardrail; (5) Maintain and control traffic; and (6) all other work specified as part of this contract.

### II. MATERIALS

Except as specified herein, provide for all materials to be sampled and tested in accordance with the Department's Sampling Manual and make the materials available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing.

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Guardrail.** Furnish guardrail system components according to section 814 and the Standard and Sepia Drawings; except use steel posts only, no alternates.

**C. Delineators for Guardrail. Delineators for Guardrail.** Furnish Delineators for Guardrail according to the Sepia Drawings.

### III. CONSTRUCTION METHODS

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Site Preparation.** Remove existing guardrail system including the guardrail end treatments, Bridge End connectors and all other elements of the existing guardrail system according to Section 719, except that the Contractor will take possession of all concrete posts and all concrete associated with existing bridge and/or guardrail end treatments. Locate all disposal areas off the Right of Way at sites obtained by the Contractor at no additional cost to the Department. Be responsible for all site preparation, including but not limited to, clearing and grubbing, excavation, embankment, and removal of all obstructions or any other items; regrading, reshaping, adding and compacting of suitable materials on the

## Guardrail

### Page 2 of 3

existing shoulders to provide proper template or foundation for the guardrail; filling voids left as the result of removing existing guardrail and guard posts with dry sand; temporary pollution and erosion control; disposal, of excess and waste materials and debris; and final dressing, cleanup, and seeding and protection. Perform all site preparation as approved or directed by the Engineer.

**C. Guardrail.** Except as specified herein, construct guardrail system according to Section 719 and the Standard and Sepia Drawings. Locations listed on the summary and/or shown on the drawings are approximate only. The Engineer will determine the exact termini for individual guardrail installations at the time of construction. Unless directed otherwise by the Engineer, provide a minimum two (2) foot shoulder width. Construct radii at entrances and road intersections as directed by the Engineer.

Erect guardrail to the lines and grades shown on current Standard and Sepia Drawings or as directed by the Engineer by any method approved by the Engineer which allows construction of the guardrail to the true grade without apparent sags. Support cantilevered terminal sections with an additional post.

When removing and installing existing guardrail, do not leave the blunt end exposed where it would be hazardous to the public. When it is not practical to complete the construction of the guardrail and the permanent end treatments and terminal sections first, provide a temporary end by connecting at least 25 feet of rail to the last post, and by slightly flaring, and burying the end of the rail completely into the existing shoulder. If left overnight, place a drum with bridge panel in advance of the guardrail end and maintain during use.

**D. Delineators for Guardrail.** Install delineators for guardrail according to the Standard and Sepia Drawings.

**E. Property Damage.** Be responsible for all damage to public and/or private property resulting from the work. Restore damaged roadway features and private property at no additional cost to the Department.

**F. Coordination with Utility Companies.** Locate all underground, above ground and overhead utilities prior to beginning construction. Be responsible for contacting and maintaining liaison with all utility companies that have utilities located within the project limits. Do not disturb existing overhead or underground utilities. It is not anticipated that any utility facilities will need to be relocated and/or adjusted; however, in the event that it is discovered that the work does require that utilities be relocated and/or adjusted, the utility companies will work concurrently with the Contractor while relocating their facilities. Be responsible for repairing all utility damage that occurs as a result of guardrail operations at no additional cost to the Department.

**G. Right of Way Limits.** The Department has not established exact limits of the Right-of-Way. Limit work activities to obvious Right-of-Way, permanent or temporary easements,

Guardrail  
Page 3 of 3

and work areas secured by the Department through consent and release of the adjacent property owners. Be responsible for all encroachments onto private lands.

**H. Disposal of Waste.** Dispose of all removed concrete, debris, and other waste and debris off the Right-of-Way at sites obtained by the Contractor at no additional cost to the Department. See Special; Note for Waste and Borrow.

**I. Final Dressing, Clean Up, and Seeding and Protection.** Apply Class A Final Dressing to all disturbed areas, both on and off the Right-of-Way. Sow all disturbed earthen areas according to the Special Notes for Erosion Control.

#### **IV. METHOD OF MEASUREMENT**

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Site preparation.** Other than the bid items listed, the Department will not measure Site Preparation for separate payment but shall be incidental to Guardrail, End Treatments, Bridge End Connectors, and Terminal Sections as applicable.

**C. Guardrail.** See Section 719.04.

**D. Delineators for Guardrail.** See the Sepia Drawing.  
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#### **V. BASIS OF PAYMENT**

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Guardrail.** See Section 719.05.

**C. Delineators for Guardrail.** See the Sepia Drawing

**SPECIAL NOTE FOR  
ASPHALT MILLING AND TEXTURING**

Begin paving operations within 2 weeks of commencement of the milling operation. Continue paving operations continuously until completed. If paving operations are not begun within this time period, the Department will assess liquidated damages at the rate prescribed by Section 108.09 until such time as paving operations are begun.

Contrary to Section 408, the Department will retain possession of the material obtained from the milling operations. Deliver this material to the State Maintenance facility in Hancock County.



## **SPECIAL NOTE FOR BASE FAILURE REPAIR**

Repair locations listed on the summary are approximate only. The Engineer will determine actual repair locations and dimensions at the time of construction. Prior to overall milling and/or leveling and wedging, excavate the designated base failure areas to a depth 8 inches below the existing asphalt pavement surface level. Dispose of the excavated materials at waste sites off the Right-of-Way obtained by the Contractor at no additional cost to the Department. See Special Note for Waste and Borrow.

Backfill the excavated areas with Class 2 Asphalt Base 1.00D PG64-22. Compact the asphalt base to the compaction required in Section 403.03.10. Seal the asphalt base with leveling and wedging. Perform all base failure repairs in such a manner that removal and replacement are completed on the same day. Do this work as one of the Contractor's first operations in order to allow further compaction by traffic. Do not mill or place new asphalt surface over repaired base failure areas until a minimum of 7 calendar days have elapsed after placement of the asphalt base. After a minimum of 7 calendar days and when the Engineer determines the base failure repair areas have sufficiently stabilized, begin milling and/or resurfacing operations. Prior to milling and/or constructing the new asphalt surface, level and wedge any settlement of the repair areas.

The bidder must draw conclusions as to the conditions encountered. The Department does not give any guarantee as to the accuracy of the data and will not consider any claim for additional compensation if the materials encountered that are not in accord with the classification shown.

Accept payment at the Contract unit prices per square yard for Base Failure repair and per ton for Leveling and Wedging as full compensation for all labor, materials, equipment, and incidentals for removing pavement and disposing of the materials, furnishing and placing asphalt base, leveling and wedging, and all other items necessary to complete the work according to these notes to the satisfaction of the Engineer.

1-3606basefailurerepairmillinlaypaybysy  
01/02/2012

### **SPECIAL NOTE FOR TYPICAL SECTION DIMENSIONS**

Consider the dimensions shown on the typical sections for pavement and shoulder widths and thickness' to be nominal or typical dimensions. The Engineer may direct or approve varying the actual dimensions to be constructed to fit existing conditions. Do not widen existing pavement or shoulders unless specified elsewhere in this proposal or directed by the engineer.

1-3725 Typical Section Dimensions  
01/02/2012

## **TRAFFIC CONTROL PLAN**

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### **TRAFFIC CONTROL GENERAL**

Except as provided herein, maintain and control traffic in accordance with the Standard and Supplemental Specifications and the Standard and Sepia Drawings, current editions. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic".

Contrary to Section 106.01, furnish new, or used in like new condition, traffic control devices at the beginning of the work and maintain in like new condition until completion of the work.

### **PROJECT PHASING & CONSTRUCTION PROCEDURES**

The Engineer may specify days and hours when lane closures will not be allowed.

Maintain alternating one way traffic during construction. Provide a minimum clear lane width of 8 feet; however, provide for passage of vehicles of up to 16 feet in width. If traffic should be stopped due to construction operations, and a school bus on an official run arrives on the scene, make provisions for the passage of the bus as quickly as possible.

### **LANE CLOSURES**

Do not leave lane closures in place during non-working hours.

### **SIGNS**

Sign posts and splices shall be compliant with NCHRP 350 or MASH. Manufacturer's documentation validating this compliance shall be provided to the Engineer prior to installation. Signs, including any splices, shall be installed according to manufacturer's specifications and installation recommendations. Contrary to section 112.04.02, only long-term signs (signs intended to be continuously in place for more than 3 days) will be measured for payment. Short-term signs (signs intended to be left in place for 3 days or less) will not be measured for payment but will be incidental to Maintain and Control Traffic.

### **CHANGEABLE MESSAGE SIGNS**

If deemed necessary by the Engineer, the Department will furnish, operate, and maintain Changeable Message Signs.

### **TEMPORARY ENTRANCES**

## Traffic Control Plan

### Page 2 of 3

The Engineer will not require the Contractor to provide continuous access to farms, single family, duplex, or triplex residential properties during working hours; however, provide reasonable egress and ingress to each such property when actual operations are not in progress at that location. Limit the time during which a farm or residential entrance is blocked to the minimum length of time required for actual operations, not extended for the Contractor's convenience, and in no case exceeding six (6) hours. Notify all residents twenty-four hours in advance of any driveway or entrance closings and make any accommodations necessary to meet the access needs of disabled residents.

Except as allowed by the Phasing as specified above, maintain direct access to all side streets and roads, schools, churches, commercial properties and apartments or apartment complexes of four or more units at all times.

The Department will measure asphalt materials required to construct and maintain any temporary entrances which may be necessary to provide temporary access; however, the Department will not measure aggregates, excavation, and/or embankment, but shall be incidental to Maintain and Control Traffic. The Engineer will determine the type of surfacing material, asphalt or aggregate, to be used at each entrance.

## **THERMOPLASTIC INTERSECTION MARKINGS**

Consider the locations listed on the summary as approximate only. Prior to milling and/or resurfacing, locate and document the locations of the existing markings. After resurfacing, replace the markings at their approximate existing locations or as directed by Engineer. Place markings not existing prior to resurfacing as directed by the Engineer.

## **BARRICADES**

The Department will not measure barricades used in lieu of barrels and cones for channelization or delineation, but shall be incidental to Maintain and Control Traffic according to Section 112.04.01.

The Department will measure barricades used to protect pavement removal areas in individual units. Each. The Department will measure for payment the maximum number of barricades in concurrent use at the same time on a single day on all sections of the contract. The Department will measure individual barricades only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. The Department will not measure replacements for damaged barricades the Engineer directs to be replaced due to poor condition or reflectivity. Retain possession of the Barricades upon completion of construction.

## **PAVEMENT MARKINGS**

If there is to be a deviation from the existing striping plan, the Engineer will furnish the Contractor a striping plan prior to placement of the final surface course.

## Traffic Control Plan

### Page 3 of 3

Install Temporary Striping according to Section 112 with the following exception:

If the Contractor's operations or phasing requires temporary markings that must subsequently be removed from the final surface course, use an approved removable lane tape; however, the Department will not measure removable lane tape for separate payment, but will measure and pay for removable lane tape as temporary striping.

## PAVEMENT EDGE DROP-OFFS

Do not allow a pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation with an elevation difference greater than 1½". Place Warning signs (MUTCD W8-11 or W8-9A) in advance of and at 1500' intervals throughout the drop-off area. Dual post the signs on both sides of the traveled way. Wedge all transverse transitions between resurfaced and unresurfaced areas which traffic may cross with asphalt mixture for leveling and wedging. Remove the wedges prior to placement of the final surface course.

Protect pavement edges that traffic is not expected to cross, except accidentally, as follows:

Less than 2" - No protection required.

2" to 4" - Place plastic drums, vertical panels, or barricades every 50 feet. During daylight working hours only, the Engineer will allow the Contractor to use cones in lieu of plastic drums, panels, and barricades. Wedge the drop-off with DGA or asphalt mixture for leveling and wedging with a 1:1 or flatter slope in daylight hours, or 3:1 or flatter slope during nighttime hours, when work is not active in the drop-off area.

Greater than 4" - Protect drop-offs greater than 4 inches within 10 feet of traffic by placing drums, vertical panels, or barricades every 25 feet. The Engineer will not allow the use of cones in lieu of drums, vertical panels, or barricades for drop-offs greater than 4". Place Type III Barricades directly in front of the drop-off facing on coming traffic in both directions of travel. Provide warning signs as shown on the Standard Drawings or as directed by the Engineer

Pedestrians & Bicycles - Protect pedestrian and bicycle traffic as directed by the engineer.

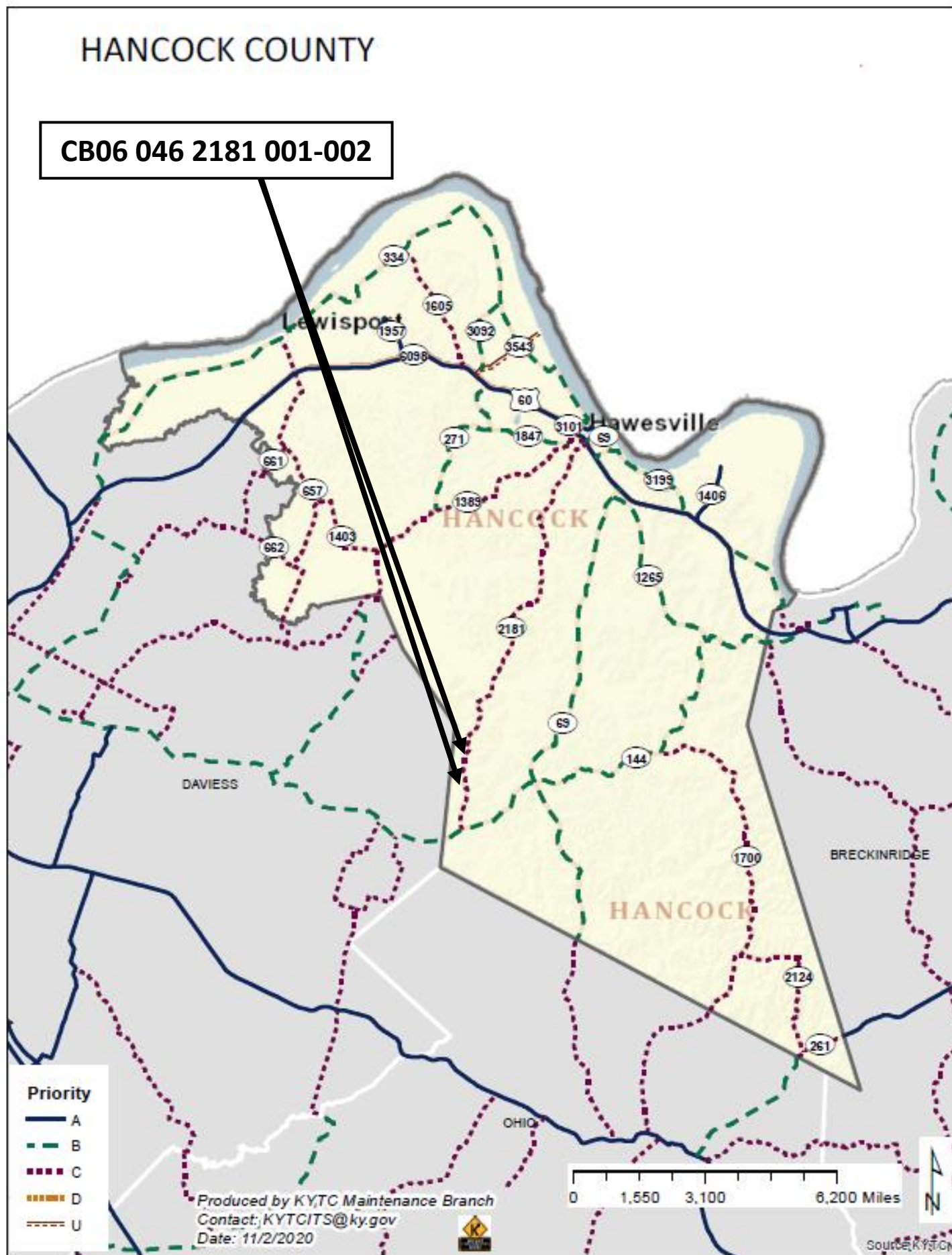


EXISTING  
FILL SLOPE or  
DITCH FORESLOPE

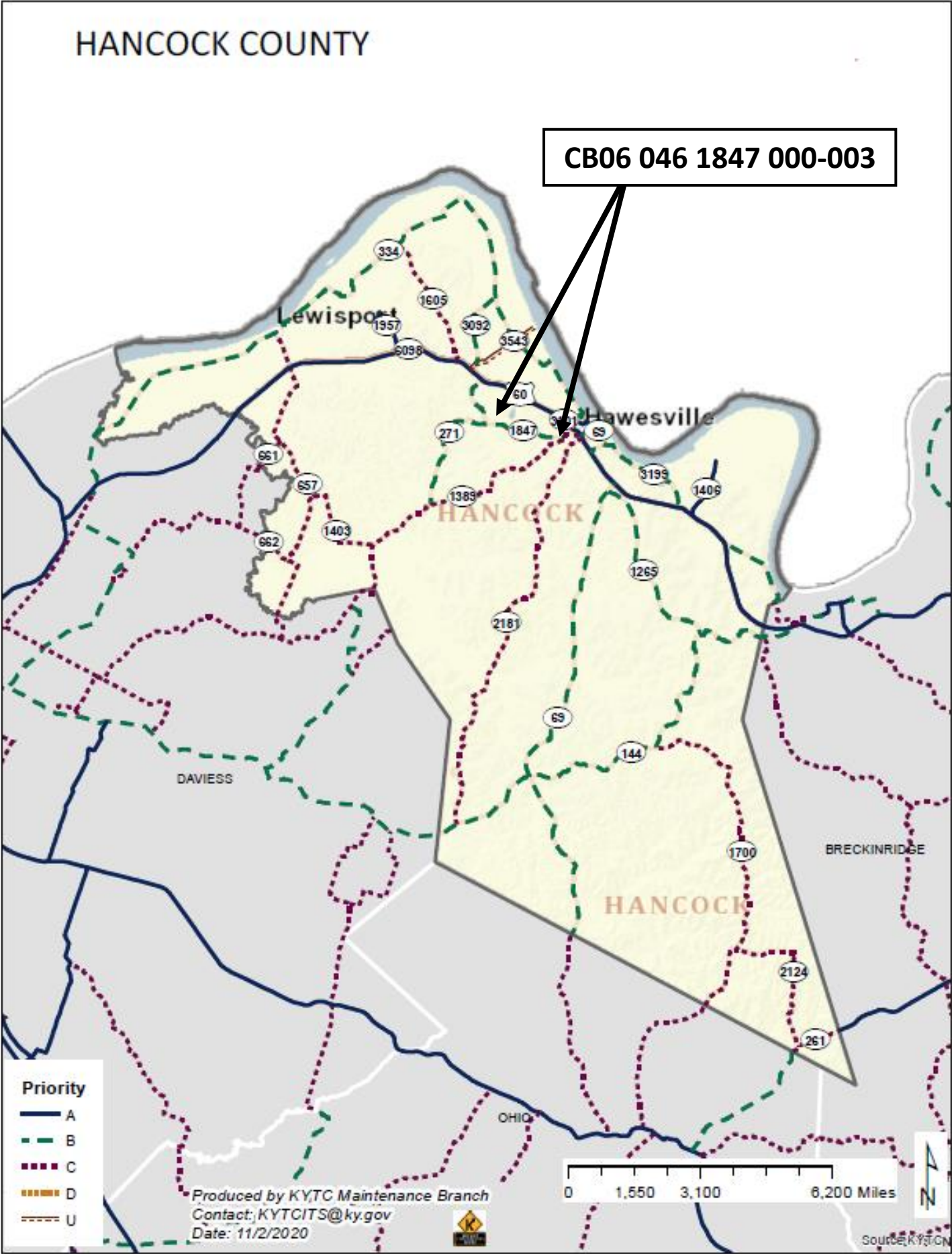
EXISTING  
FILL SLOPE or  
DITCH FORESLOPE

1. DETAILS DO NOT APPLY TO OVERLAYS LESS THAN 1 INCH THICK.
2. THE DURABLE PAVEMENT EDGE DEVICE MAY BE DISENGAGED AT DRIVEWAYS, SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT, AS APPROVED BY THE ENGINEER.

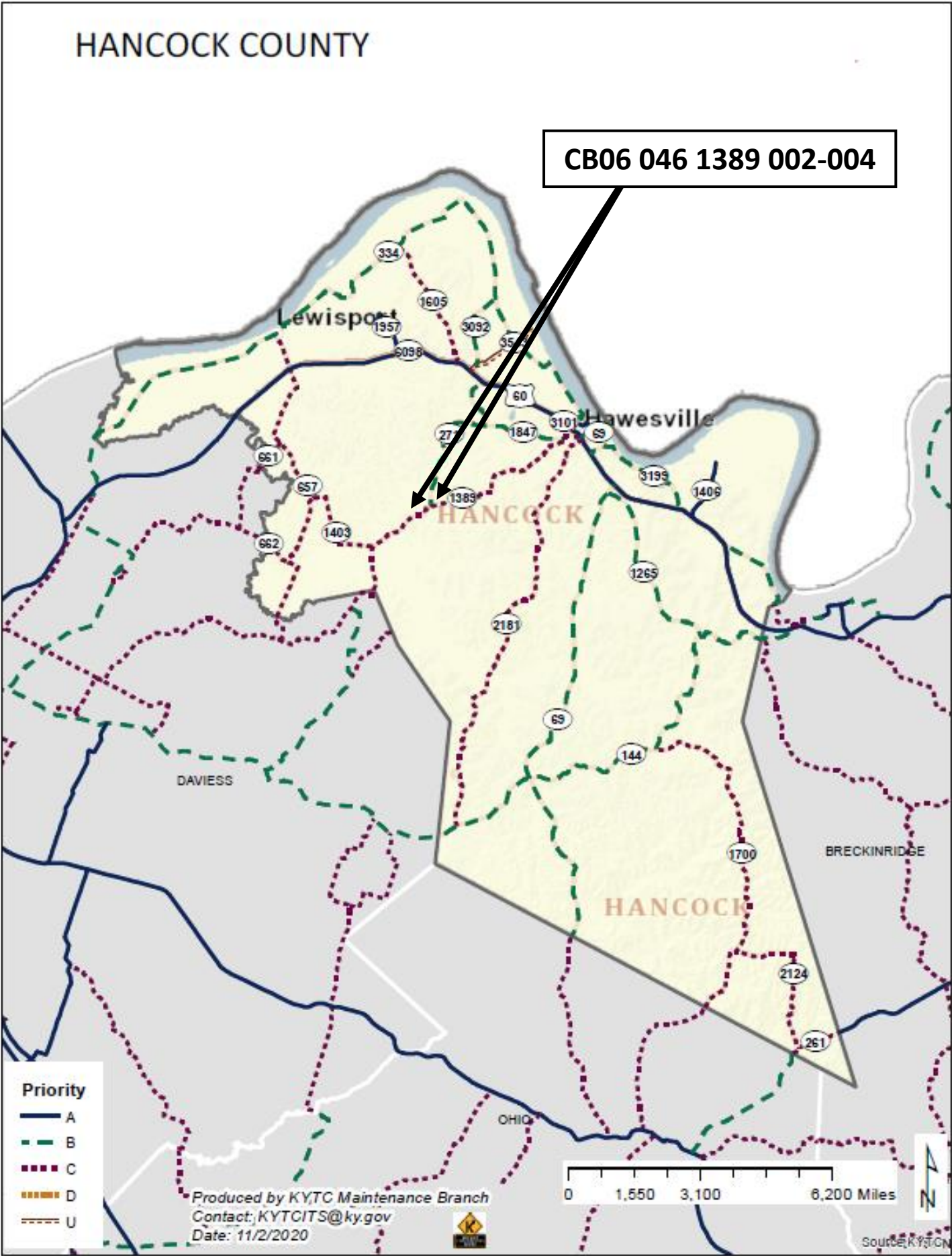
## DURABLE PAVEMENT EDGE DETAILS











MATERIAL SUMMARY

CONTRACT ID: 213195

046GR21R069-CB06

0204613892102

KY-1389 (KY 1389) FROM LASLIE LOOP EXTENDING EAST TO KY 271 ASPHALT RESURFACING, A DISTANCE OF .6 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0080	00001	DGA BASE	45.00	TON
0085	00190	LEVELING & WEDGING PG64-22	40.00	TON
0090	00301	CL2 ASPH SURF 0.38D PG64-22	410.00	TON
0095	02562	TEMPORARY SIGNS	160.00	SQFT
0100	02650	MAINTAIN & CONTROL TRAFFIC - (KY 1389)	1.00	LS
0105	02676	MOBILIZATION FOR MILL & TEXT - (KY 1389)	1.00	LS
0110	02677	ASPHALT PAVE MILLING & TEXTURING	15.00	TON
0115	06510	PAVE STRIPING-TEMP PAINT-4 IN	3,400.00	LF
0120	06514	PAVE STRIPING-PERM PAINT-4 IN	6,000.00	LF
0125	10020NS	FUEL ADJUSTMENT	836.00	DOLL
0130	10030NS	ASPHALT ADJUSTMENT	2,099.00	DOLL
0135	24970EC	ASPHALT MATERIAL FOR TACK NON-TRACKING	2.00	TON
0140	02569	DEMOBILIZATION	1.00	LS

CONTRACT ID: 213195

046GR21R069-CB06

0204618472101

KY-1847 (KY 1847) FROM KY 271 EXTENDING EAST TO 2.136 ASPHALT RESURFACING, A DISTANCE OF 2.14 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0005	00001	DGA BASE	150.00	TON
0010	00190	LEVELING & WEDGING PG64-22	140.00	TON
0015	00301	CL2 ASPH SURF 0.38D PG64-22	1,550.00	TON
0020	02562	TEMPORARY SIGNS	160.00	SQFT
0025	02650	MAINTAIN & CONTROL TRAFFIC - (KY 1847)	1.00	LS
0030	02676	MOBILIZATION FOR MILL & TEXT - (KY 1847)	1.00	LS
0035	02677	ASPHALT PAVE MILLING & TEXTURING	55.00	TON
0040	03240	BASE FAILURE REPAIR	28.00	SQYD
0045	06510	PAVE STRIPING-TEMP PAINT-4 IN	11,000.00	LF
0050	06514	PAVE STRIPING-PERM PAINT-4 IN	19,500.00	LF
0055	06569	PAVE MARKING-THERMO CROSS-HATCH	770.00	SQFT
0060	10020NS	FUEL ADJUSTMENT	2,641.00	DOLL
0065	10030NS	ASPHALT ADJUSTMENT	6,635.00	DOLL
0070	24970EC	ASPHALT MATERIAL FOR TACK NON-TRACKING	7.00	TON
0075	02569	DEMOBILIZATION	1.00	LS

MATERIAL SUMMARY

CONTRACT ID: 213195

046GR21R069-CB06

0204621812101

KY-2181 (KY 2181) FROM 0.008 MILES NORTH OF DUTCH BILL LOOP EXTENDING NORTH TO 0.121 MILES NORTH OF DUTCH BILL LOOP ASPHALT RESURFACING, A DISTANCE OF .5 MILES.

Project Line No	Bid Code	DESCRIPTION	Quantity	Unit
0145	00001	DGA BASE	170.00	TON
0150	00190	LEVELING & WEDGING PG64-22	380.00	TON
0155	00212	CL2 ASPH BASE 1.00D PG64-22	100.00	TON
0160	00301	CL2 ASPH SURF 0.38D PG64-22	480.00	TON
0165	00440	ENTRANCE PIPE-15 IN	45.00	LF
0170	02091	REMOVE PAVEMENT	250.00	SQYD
0175	02383	REMOVE & RESET GUARDRAIL	200.00	LF
0180	02562	TEMPORARY SIGNS	160.00	SQFT
0185	02650	MAINTAIN & CONTROL TRAFFIC - (KY 2181)	1.00	LS
0190	02676	MOBILIZATION FOR MILL & TEXT - (KY 2181)	1.00	LS
0195	02677	ASPHALT PAVE MILLING & TEXTURING	20.00	TON
0200	03250	WATERPROOFING MEMBRANE	50.00	SQYD
0205	05985	SEEDING AND PROTECTION	200.00	SQYD
0210	06510	PAVE STRIPING-TEMP PAINT-4 IN	3,700.00	LF
0215	06514	PAVE STRIPING-PERM PAINT-4 IN	6,500.00	LF
0220	08808	GUARDRAIL-BRIDGE CASE II	40.00	LF
0225	10020NS	FUEL ADJUSTMENT	1,499.00	DOLL
0230	10030NS	ASPHALT ADJUSTMENT	3,765.00	DOLL
0235	20430ED	SAW CUT	150.00	LF
0240	21289ED	LONGITUDINAL EDGE KEY	370.00	LF
0245	24096EC	REMOVE AND RESET END TREATMENT	3.00	EACH
0250	24970EC	ASPHALT MATERIAL FOR TACK NON-TRACKING	3.00	TON
0255	02569	DEMOBILIZATION	1.00	LS

Base Failure Summary Sheet

Project: CB06 046  
1847 000 003

County: Hancock

District: D2 - Madisonville

Direction	Milepoint	Length (ft)	Width (ft)	Assumed Depth (in)	Total Sq. Yards	NoBid Item
South Bound	0.600	50	5	8	28	0
				Total:	28	0

Project: CB06 046  
1847 000 003

County: Hancock

District: D2 - Madisonville

Bridge No	Milepoint	W (FT)	T (IN)	L (FT)	L1 (FT)	L2 (FT)	PR (IN)	TR (IN)	Milling and Texturing (in tons)
046B0002 8N	0.247	18	4	132	100	100	1	0	15

Project: CB06 046  
1847 000 003

County: Hancock

District: D2 - Madisonville

Mile Point	Notes	Length	Width	Avg Depth	Tons
0.000		100	24	0.5	7.3
2.140		100	100	0.5	30.6
	Bridge Milling				15
				Total:	52.9

Thermo Summary Sheet

Project: CB06 046  
1847 000 003

County: Hancock

District: D2 - Madisonville

Milepoint	Location	Notes	PAVE MARKIN G- THERMO CROSS- HATCH
2.140	KY 271	Cross Hatch at KY 271	770
Total:			770

Project: CB06 046  
1389 002 004

County: Hancock

District: D2 - Madisonville

Mile Point	Notes	Length	Width	Avg Depth	Tons
2.450	Edge Key	100	18	0.5	5.5
3.550	Edge Key	100	18	0.5	5.5
				Total:	11



Guardrail Summary Sheet		
Project: CB06 046 2181 001 002	County: Hancock	District: D2 - Madisonville

Road Direction	Start MP	END MP	Comments	REMOVE & RESET GUARDRAIL
North Bound	1.311	1.329		100
South Bound	1.632	1.650		100
			Total:	200

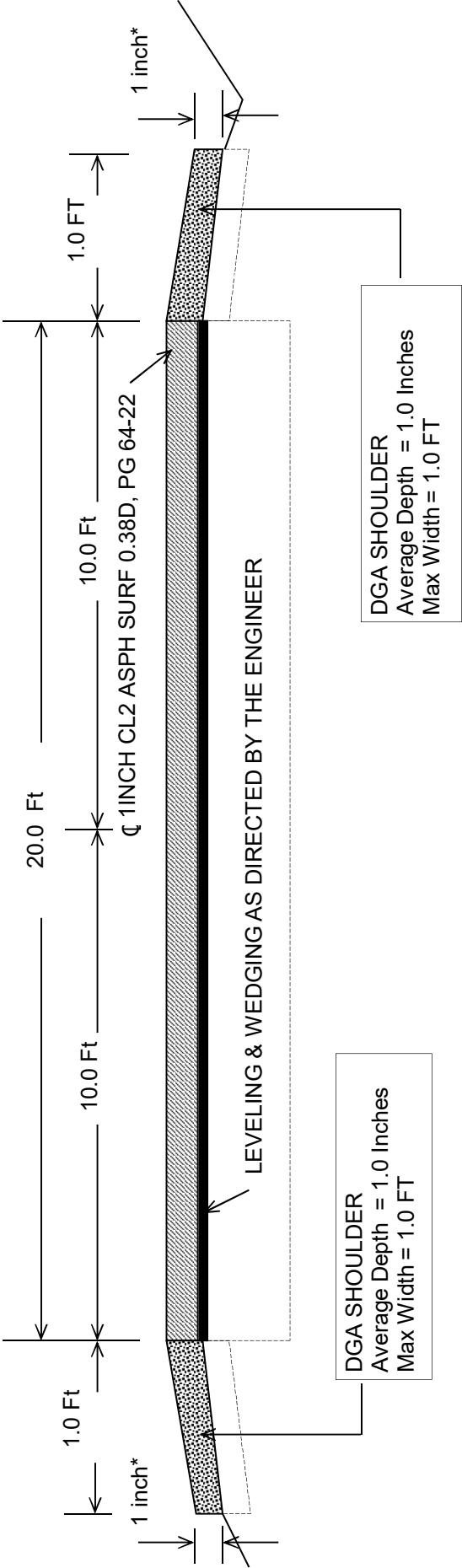
Project: CB06 046  
2181 001 002

County: Hancock

District: D2 - Madisonville

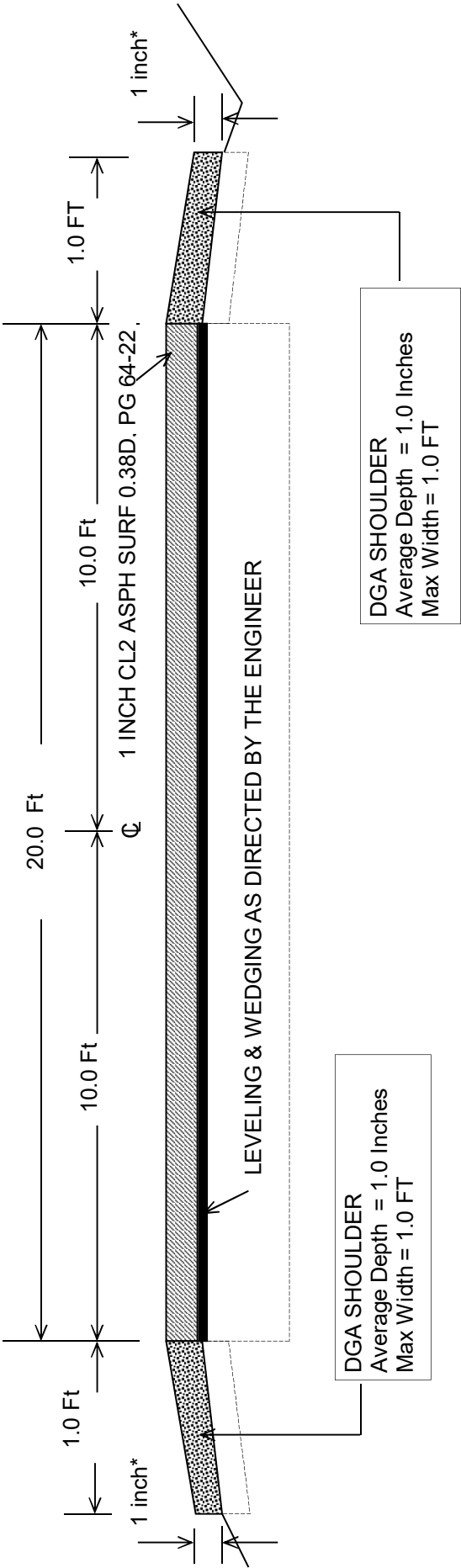
Mile Point	Notes	Length	Width	Avg Depth	Tons
1.100	Edge Key	100	20	0.5	6.1
1.500	Edge Key on Old KY 2181	100	20	0.5	6.1
1.700	Edge Key	100	20	0.5	6.1
				Total:	18.3

**TYPICAL SECTION**  
**HANCOCK COUNTY**  
**CB06 046-2181-001-002**



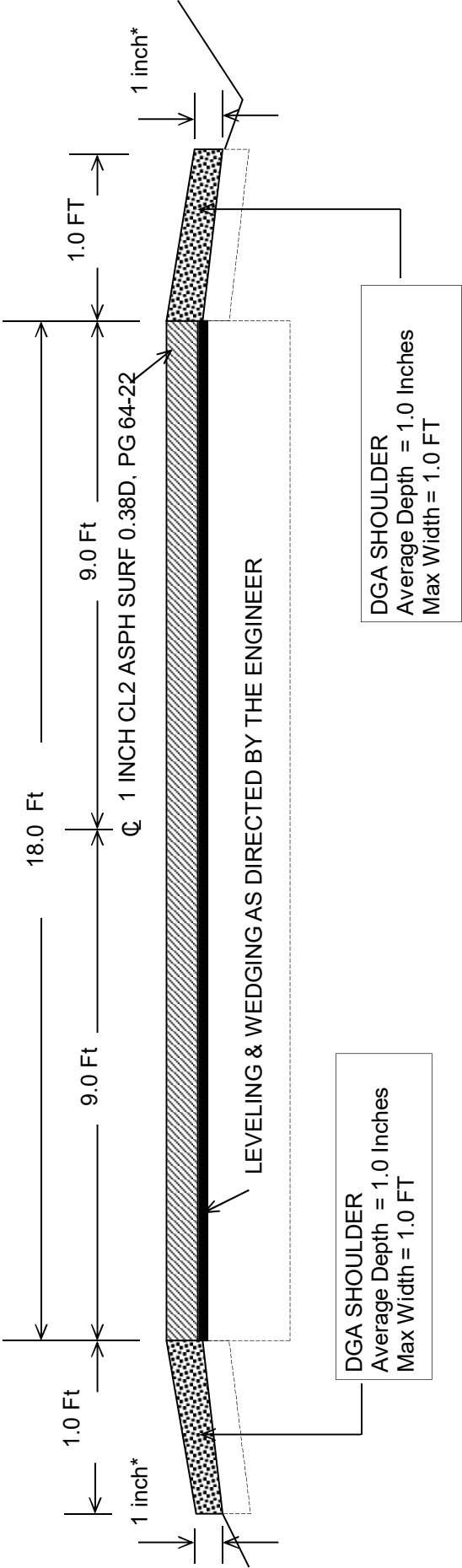
**\* Where Existing Site Conditions Permit**

**TYPICAL SECTION**  
**HANCOCK COUNTY**  
**CB06 046-1847-000-003**

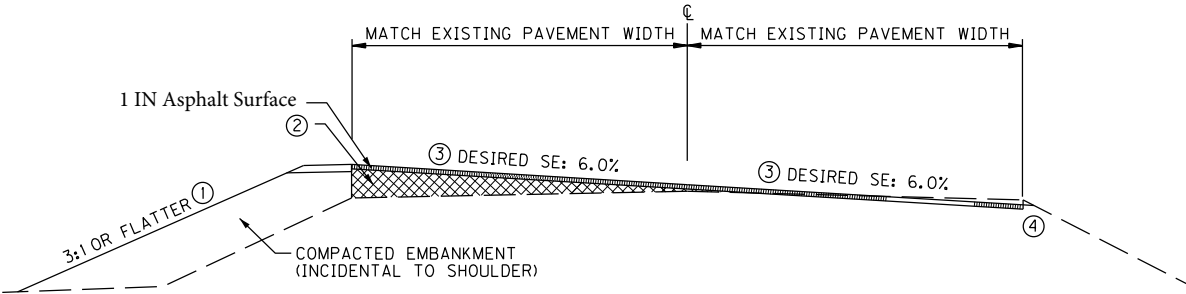


**\* Where Existing Site Conditions Permit**

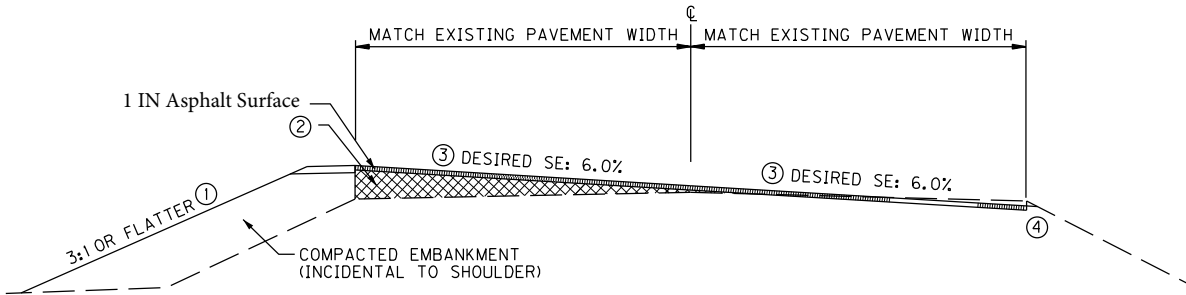
**TYPICAL SECTION**  
**HANCOCK COUNTY**  
**CB06 046-1389-002-004**



**\* Where Existing Site Conditions Permit**



SUPERELEVATION CORRECTION  
CURVE AT MP 1.2

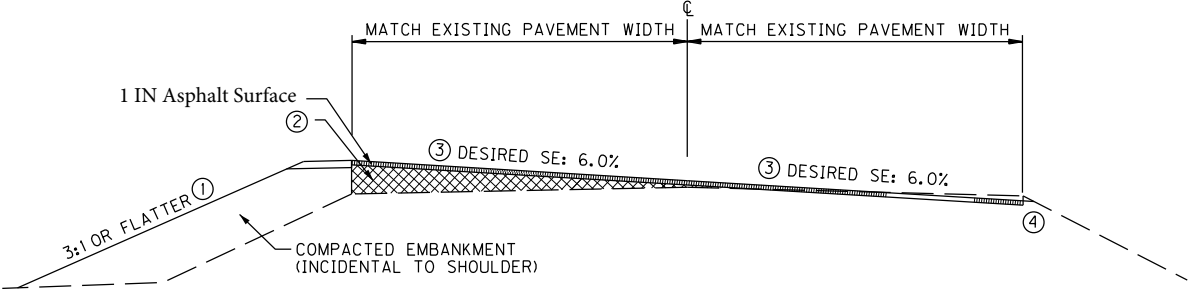


SUPERELEVATION CORRECTION  
CURVE AT MP 1.5

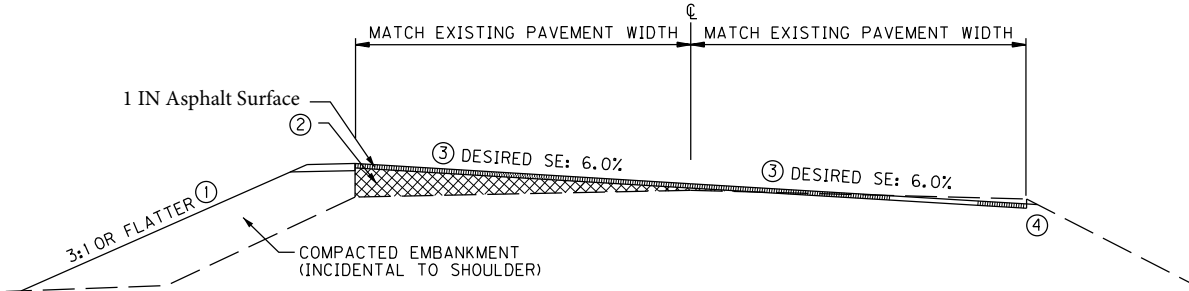
- ① 3:1 OR FLATTER IS DESIRABLE. LOCATIONS THAT ARE LIMITED DUE TO RIGHT-OF-WAY, UTILITY POLES, TREES, FENCES, OR OTHER SENSITIVE OBSTRUCTIONS MAY REQUIRE EMBANKMENT BUT ONLY OUT TO THE EDGE OF THE RIGHT-OF-WAY OR SENSITIVE OBSTRUCTION(S).
- ② IF 1-2" INCHES OF ADDITIONAL PAVEMENT DEPTH IS REQUIRED, LEVELING & WEDGING PG64-22 WILL BE USED. IF 3+" OF ADDITIONAL PAVEMENT IS REQUIRED, CL 2 ASPH BASE 1.000 PG64-22 WILL BE REQUIRED.
- ③ FINAL SUPERELEVATION RATE TO BE DETERMINED BY THE ENGINEER IN THE FIELD. DESIRED SUPERELEVATION RATE MAY NEED TO BE MODIFIED IN ORDER TO REMAIN WITHIN RIGHT-OF-WAY OR AVOID A SENSITIVE OBSTRUCTION.
- ④ MAXIMUM DEPTH OF MILLING TO ACHIEVE DESIRED SUPERELEVATION RATE SHALL BE 2".

NOT TO SCALE

KY 2181  
SUPERELEVATION CORRECTION  
TYPICAL SECTIONS



SUPERELEVATION CORRECTION  
CURVE AT MP 1.2

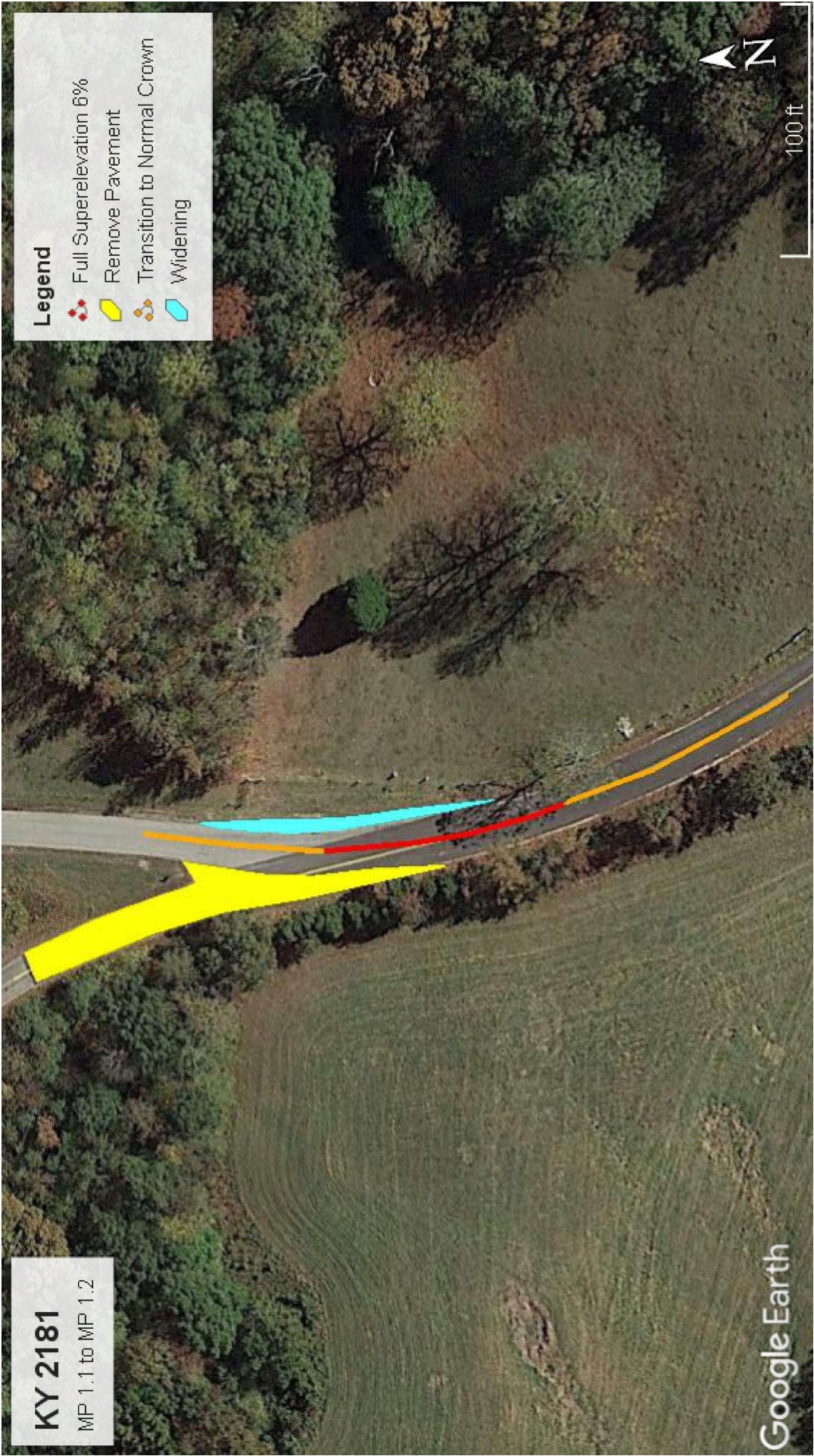


SUPERELEVATION CORRECTION  
CURVE AT MP 1.5

- ① 3:1 OR FLATTER IS DESIRABLE. LOCATIONS THAT ARE LIMITED DUE TO RIGHT-OF-WAY, UTILITY POLES, TREES, FENCES, OR OTHER SENSITIVE OBSTRUCTIONS MAY REQUIRE EMBANKMENT BUT ONLY OUT TO THE EDGE OF THE RIGHT-OF-WAY OR SENSITIVE OBSTRUCTION(S).
- ② IF 1-2" INCHES OF ADDITIONAL PAVEMENT DEPTH IS REQUIRED, LEVELING & WEDGING PG64-22 WILL BE USED. IF 3+" OF ADDITIONAL PAVEMENT IS REQUIRED, CL 2 ASPH BASE 1.000 PG64-22 WILL BE REQUIRED.
- ③ FINAL SUPERELEVATION RATE TO BE DETERMINED BY THE ENGINEER IN THE FIELD. DESIRED SUPERELEVATION RATE MAY NEED TO BE MODIFIED IN ORDER TO REMAIN WITHIN RIGHT-OF-WAY OR AVOID A SENSITIVE OBSTRUCTION.
- ④ MAXIMUM DEPTH OF MILLING TO ACHIEVE DESIRED SUPERELEVATION RATE SHALL BE 2".

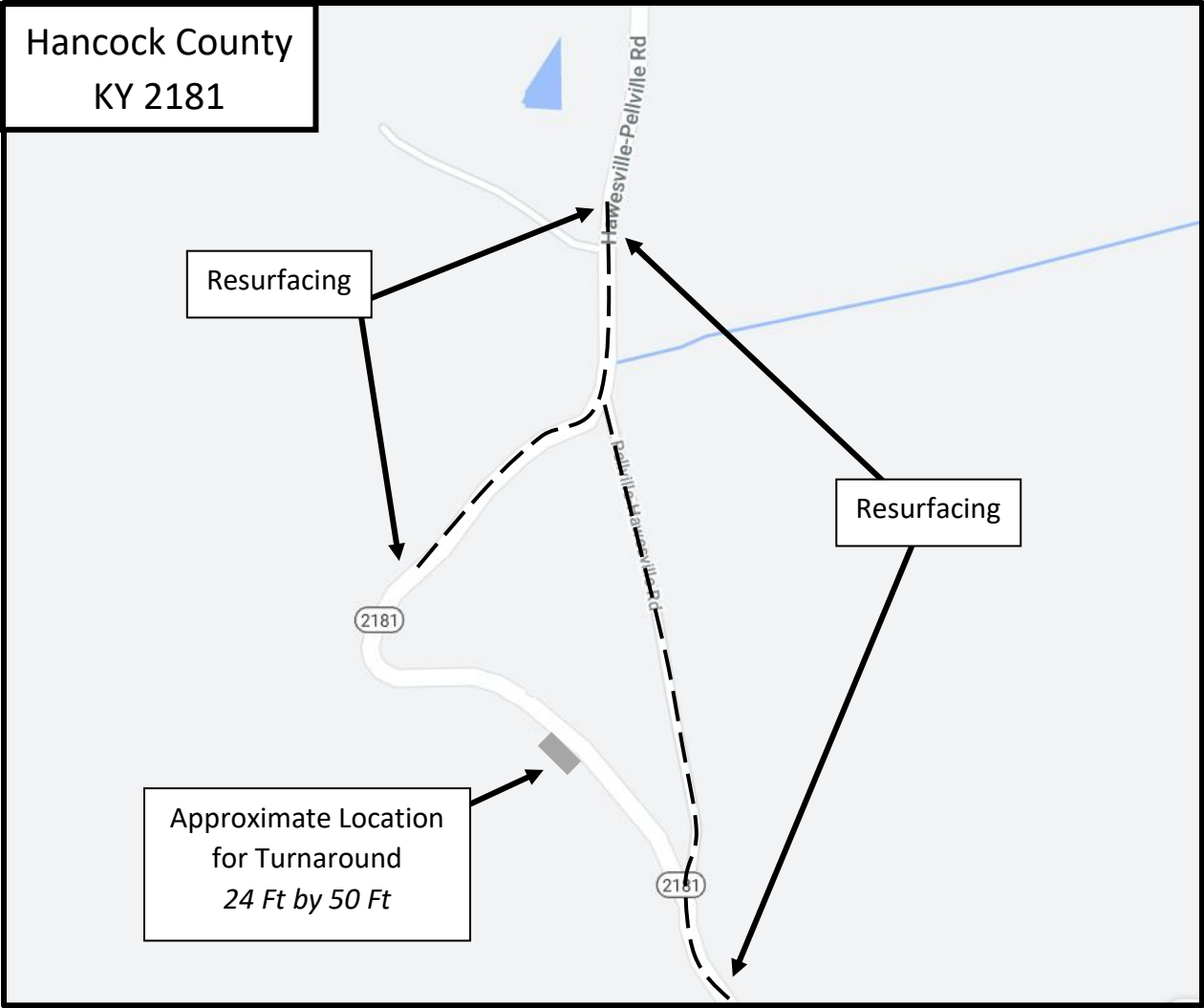
NOT TO SCALE

KY 2181  
SUPERELEVATION CORRECTION  
TYPICAL SECTIONS



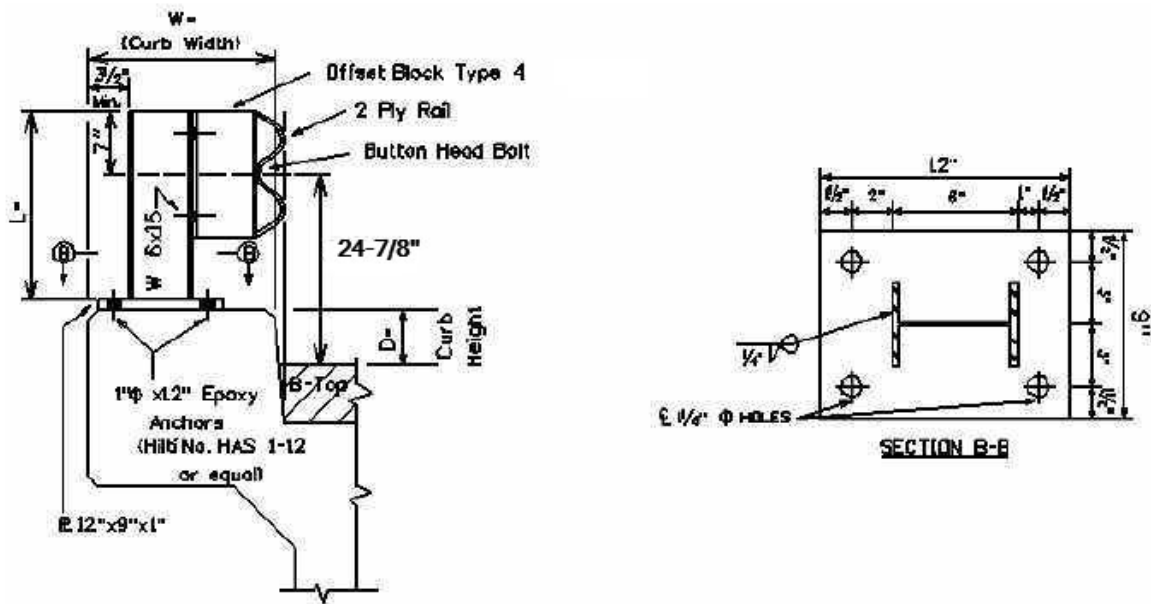






CB06 046 2181 001 002

GUARDRAIL ON BRIDGE, CASE II  
GUARDRAIL ON TOP OF BRIDGE CURB



Bridge MP	D =	W =	L =	No. Posts	LF of 2 PLY Rail
1.6	0	0	Varies	8	40

L = Length of Guardrail Post      D = Curb Height      W = Width of Bridge Curb

**Warrants:** When guardrail can be bolted to the top of the bridge curb and curb width is 18 inches or greater, remove existing concrete and/or guardrail bridge rail and use Case II Bridge Guardrail..

- NOTES:**
- 1.If the face of the guardrail for Case II Guardrail extends beyond the face of the curb or if the clear distance between the faces of the guardrail for the Case II guardrail is less than 22 feet, delete the offset block.
  - 2. When Case II Guardrail is used without the offset block, use a Precast Concrete Bridge Block at the ends of the bridge which face opposing traffic. See detail drawing for Precast Concrete Bridge Block.

**PART II**

**SPECIFICATIONS AND STANDARD DRAWINGS**

### **SPECIFICATIONS REFERENCE**

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2019* and *Standard Drawings, Edition of 2020*.

## **SUPPLEMENTAL SPECIFICATIONS**

The contractor shall use the Supplemental Specifications that are effective at the time of letting.  
The Supplemental Specifications can be found at the following link:

<http://transportation.ky.gov/Construction/Pages/Kentucky-Standard-Specifications.aspx>

2020 STANDARD DRAWINGS THAT APPLY

ROADWAY

~GENERAL~

MISCELLANEOUS STANDARDS

MISCELLANEOUS STANDARDS PART 1.....RGX-001-06

~PAVEMENT~

MEDIANS, CURBS, APPROACHES, ENTRANCES, ETC.

APPROACHES, ENTRANCES, AND MAIL BOX TURNOUT.....RPM-110-07

TRAFFIC

~TEMPORARY~

DEVICES

LANE CLOUSRE TWO-LANE HIGHWAY .....TTC-100-04

## **PART III**

### **EMPLOYMENT, WAGE AND RECORD REQUIREMENTS**



**TRANSPORTATION CABINET  
DEPARTMENT OF HIGHWAYS**

**LABOR AND WAGE REQUIREMENTS  
APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS**

- I. Application
- II. Nondiscrimination of Employees (KRS 344)

**I. APPLICATION**

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual because of his race, color, religion, national origin, sex, disability or age forty (40) and over, in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

Revised: January 25, 2017

**II. NONDISCRIMINATION OF EMPLOYEES**

**AN ACT OF THE KENTUCKY  
GENERAL ASSEMBLY TO PREVENT  
DISCRIMINATION IN EMPLOYMENT  
KRS CHAPTER 344  
EFFECTIVE JUNE 16, 1972**

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (forty and above); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age forty (40) and over. The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, except that such a notice or advertisement may indicate a preference, limitation, or specification based on religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, when religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, is a bona fide occupational qualification for employment.

## EXECUTIVE BRANCH CODE OF ETHICS

In the 1992 regular legislative session, the General Assembly passed and Governor Brereton Jones signed Senate Bill 63 (codified as KRS 11A), the Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (7) provides:

No present or former public servant shall, within six (6) months following termination of his office or employment, accept employment, compensation, or other economic benefit from any person or business that contracts or does business with, or is regulated by, the state in matters in which he was directly involved during the last thirty-six (36) months of his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, or for which he received, prior to his state employment, a professional degree or license, provided that, for a period of six (6) months, he personally refrains from working on any matter in which he was directly involved during the last thirty-six (36) months of his tenure in state government. This subsection shall not prohibit the performance of ministerial functions, including but not limited to filing tax returns, filing applications for permits or licenses, or filing incorporation papers, nor shall it prohibit the former officer or public servant from receiving public funds disbursed through entitlement programs.

KRS 11A.040 (9) states:

A former public servant shall not represent a person or business before a state agency in a matter in which the former public servant was directly involved during the last thirty-six (36) months of his tenure, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not as a way to obtain private benefits.

If you have worked for the executive branch of state government within the past six months, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, 3 Fountain Place, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Revised: January 27, 2017

### **Kentucky Equal Employment Opportunity Act of 1978**

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:  
**<https://www.eProcurement.ky.gov>**.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **[finance.contractcompliance@ky.gov](mailto:finance.contractcompliance@ky.gov)** or by phone at 502-564-2874.

# EMPLOYEE RIGHTS UNDER THE FAIR LABOR STANDARDS ACT

THE UNITED STATES DEPARTMENT OF LABOR WAGE AND HOUR DIVISION

## FEDERAL MINIMUM WAGE

# \$7.25

 PER HOUR

BEGINNING JULY 24, 2009

### OVERTIME PAY

At least  $1\frac{1}{2}$  times your regular rate of pay for all hours worked over 40 in a workweek.

### CHILD LABOR

An employee must be at least **16** years old to work in most non-farm jobs and at least **18** to work in non-farm jobs declared hazardous by the Secretary of Labor.

Youths **14** and **15** years old may work outside school hours in various non-manufacturing, non-mining, non-hazardous jobs under the following conditions:

#### *No more than*

- **3** hours on a school day or **18** hours in a school week;
- **8** hours on a non-school day or **40** hours in a non-school week.

Also, work may not begin before **7 a.m.** or end after **7 p.m.**, except from June 1 through Labor Day, when evening hours are extended to **9 p.m.** Different rules apply in agricultural employment.

### TIP CREDIT

Employers of "tipped employees" must pay a cash wage of at least \$2.13 per hour if they claim a tip credit against their minimum wage obligation. If an employee's tips combined with the employer's cash wage of at least \$2.13 per hour do not equal the minimum hourly wage, the employer must make up the difference. Certain other conditions must also be met.

### ENFORCEMENT

The Department of Labor may recover back wages either administratively or through court action, for the employees that have been underpaid in violation of the law. Violations may result in civil or criminal action.

Employers may be assessed civil money penalties of up to \$1,100 for each willful or repeated violation of the minimum wage or overtime pay provisions of the law and up to \$11,000 for each employee who is the subject of a violation of the Act's child labor provisions. In addition, a civil money penalty of up to \$50,000 may be assessed for each child labor violation that causes the death or serious injury of any minor employee, and such assessments may be doubled, up to \$100,000, when the violations are determined to be willful or repeated. The law also prohibits discriminating against or discharging workers who file a complaint or participate in any proceeding under the Act.

### ADDITIONAL INFORMATION

- Certain occupations and establishments are exempt from the minimum wage and/or overtime pay provisions.
- Special provisions apply to workers in American Samoa and the Commonwealth of the Northern Mariana Islands.
- Some state laws provide greater employee protections; employers must comply with both.
- The law requires employers to display this poster where employees can readily see it.
- Employees under 20 years of age may be paid \$4.25 per hour during their first 90 consecutive calendar days of employment with an employer.
- Certain full-time students, student learners, apprentices, and workers with disabilities may be paid less than the minimum wage under special certificates issued by the Department of Labor.

For additional information:



# 1-866-4-USWAGE

(1-866-487-9243)

TTY: 1-877-889-5627



# WWW.WAGEHOUR.DOL.GOV

## **PART IV**

## **INSURANCE**

Refer to  
*Kentucky Standard Specifications for Road and Bridge Construction,*  
current edition

**PART V**

**BID ITEMS**

Report Date 9/22/21

Section: 0001 - PAVING

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0010	00001		DGA BASE	365.00	TON		\$	
0020	00190		LEVELING & WEDGING PG64-22	560.00	TON		\$	
0030	00212		CL2 ASPH BASE 1.00D PG64-22	100.00	TON		\$	
0040	00301		CL2 ASPH SURF 0.38D PG64-22	2,440.00	TON		\$	
0050	00440		ENTRANCE PIPE-15 IN	45.00	LF		\$	
0060	02091		REMOVE PAVEMENT	250.00	SQYD		\$	
0070	02383		REMOVE & RESET GUARDRAIL	200.00	LF		\$	
0080	02562		TEMPORARY SIGNS	480.00	SQFT		\$	
0090	02650		MAINTAIN & CONTROL TRAFFIC (KY 1389)	1.00	LS		\$	
0100	02650		MAINTAIN & CONTROL TRAFFIC (KY 1847)	1.00	LS		\$	
0110	02650		MAINTAIN & CONTROL TRAFFIC (KY 2181)	1.00	LS		\$	
0120	02676		MOBILIZATION FOR MILL & TEXT (KY 1389)	1.00	LS		\$	
0130	02676		MOBILIZATION FOR MILL & TEXT (KY 1847)	1.00	LS		\$	
0140	02676		MOBILIZATION FOR MILL & TEXT (KY 2181)	1.00	LS		\$	
0150	02677		ASPHALT PAVE MILLING & TEXTURING	90.00	TON		\$	
0160	03240		BASE FAILURE REPAIR	28.00	SQYD		\$	
0170	03250		WATERPROOFING MEMBRANE	50.00	SQYD		\$	
0180	05985		SEEDING AND PROTECTION	200.00	SQYD		\$	
0190	06510		PAVE STRIPING-TEMP PAINT-4 IN	18,100.00	LF		\$	
0200	06514		PAVE STRIPING-PERM PAINT-4 IN	32,000.00	LF		\$	
0210	06569		PAVE MARKING-THERMO CROSS-HATCH	770.00	SQFT		\$	
0220	08808		GUARDRAIL-BRIDGE CASE II	40.00	LF		\$	
0230	10020NS		FUEL ADJUSTMENT	4,976.00	DOLL	\$1.00	\$	\$4,976.00
0240	10030NS		ASPHALT ADJUSTMENT	12,499.00	DOLL	\$1.00	\$	\$12,499.00
0250	20430ED		SAW CUT	150.00	LF		\$	
0260	21289ED		LONGITUDINAL EDGE KEY	370.00	LF		\$	
0270	24096EC		REMOVE AND RESET END TREATMENT	3.00	EACH		\$	
0280	24970EC		ASPHALT MATERIAL FOR TACK NON- TRACKING	12.00	TON		\$	

Section: 0002 - DEMOBILIZATION

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0290	02569		DEMOBILIZATION	1.00	LS		\$	