



**CALL NO. 351**

**CONTRACT ID. 254107**

**HARLAN COUNTY**

**FED/STATE PROJECT NUMBER FD04 048 0119 007-011**

**DESCRIPTION US HIGHWAY 119 (US 119)**

**WORK TYPE ASPHALT SURFACE WITH GRADE & DRAIN**

**PRIMARY COMPLETION DATE 11/15/2025**

**LETTING DATE: July 24,2025**

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME July 24,2025. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

**NO PLANS ASSOCIATED WITH THIS PROJECT.**

**REQUIRED BID PROPOSAL GUARANTY:** Not less than 5% of the total bid.

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# **PART I**

## **SCOPE OF WORK**

**ADMINISTRATIVE DISTRICT - 11**

**CONTRACT ID - 254107**  
**FD04 048 0119 007-011**  
**COUNTY - HARLAN**  
**PCN - 1104801192501**  
**FD04 048 0119 007-011**

US HIGHWAY 119 (US 119) (MP 7.596) FROM 0.03 MILES SOUTH OF TREMONT DRIVE TO 0.05 MILES NORTH OF CATE HOLLOW, MILL AND TRENCH SHOULDER WITH FULL DEPTH PAVEMENT, OVERLAY AND RESTRIPE ROADWAY WITH CENTERLINE BUFFER (MP 10.878), A DISTANCE OF 03.28 MILES.ASPHALT SURFACE WITH GRADE & DRAIN SYP NO. 11-09031.00.  
GEOGRAPHIC COORDINATES LATITUDE 36:50:32.00 LONGITUDE 83:22:42.00  
ADT 6,153

**COMPLETION DATE(S):**  
COMPLETED BY 11/15/2025                      APPLIES TO ENTIRE PROJECT



## **CONTRACT NOTES**

### **INSURANCE**

Refer to Kentucky Standard Specifications for Road and Bridge Construction, current edition.

### **PROPOSAL ADDENDA**

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

### **BID SUBMITTAL**

Bidder must use the Department's electronic bidding software. The Bidder must download the bid file located on the Bid Express website ([www.bidx.com](http://www.bidx.com)) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

### **JOINT VENTURE BIDDING**

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

### **UNDERGROUND FACILITY DAMAGE PROTECTION**

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. When prescribed in said directives, the contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom shall be contacted through their individual Protection Notification Center. Non-compliance with these directives can result in the enforcement of penalties.

### **REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY**

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth ("certificate") from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the

foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

**For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity's solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.**

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

### **SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT**

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by email to [kytc.projectquestions@ky.gov](mailto:kytc.projectquestions@ky.gov). The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading "Questions & Answers" on the Construction Procurement website ([www.transportation.ky.gov/construction-procurement](http://www.transportation.ky.gov/construction-procurement)). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

### **HARDWOOD REMOVAL RESTRICTIONS**

The US Department of Agriculture has imposed a quarantine in Kentucky and several surrounding states, to prevent the spread of an invasive insect, the emerald ash borer. Hardwood cut in conjunction with the project may not be removed from the state. Chipping or burning on site is the preferred method of disposal.

### **INSTRUCTIONS FOR EXCESS MATERIAL SITES AND BORROW SITES**

Identification of excess material sites and borrow sites shall be the responsibility of the Contractor. The Contractor shall be responsible for compliance with all applicable state and federal laws and may wish to consult with the US Fish and Wildlife Service to seek protection under Section 10 of the Endangered Species Act for these activities.

### **ACCESS TO RECORDS**

The state agency certifies that it is in compliance with the provisions of KRS 45A.150, "Access to contractor's books, documents, papers, records, or other evidence directly pertinent to the contract." The Contractor, as defined in KRS 45A.030, agrees that the contracting agency, the

Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this agreement for the purpose of financial audit or program review. The Contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884. Records and other prequalification information confidentially disclosed as part of the bid process shall not be deemed as directly pertinent to the agreement and shall be exempt from disclosure as provided in KRS 61.878(1)(c).

### **BOYCOTT PROVISIONS**

If applicable, the contractor represents that, pursuant to [KRS 45A.607](#), they are not currently engaged in, and will not for the duration of the contract engage in, the boycott of a person or an entity based in or doing business with a jurisdiction with which Kentucky can enjoy open trade. **Note:** The term Boycott does not include actions taken for bona fide business or economic reasons, or actions specifically required by federal or state law.

If applicable, the contractor verifies that, pursuant to KRS 41.480, they do not engage in, and will not for the duration of the contract engage in, in energy company boycotts as defined by KRS 41.472.

### **LOBBYING PROHIBITIONS**

The contractor represents that they, and any subcontractor performing work under the contract, have not violated the agency restrictions contained in [KRS 11A.236](#) during the previous ten (10) years, and pledges to abide by the restrictions set forth in such statute for the duration of the contract awarded.

The contractor further represents that, pursuant to [KRS 45A.328](#), they have not procured an original, subsequent, or similar contract while employing an executive agency lobbyist who was convicted of a crime related to the original, subsequent, or similar contract within five (5) years of the conviction of the lobbyist.

Revised: 1/1/2025

## 1.0 BUY AMERICA REQUIREMENT.

Follow the “Buy America” provisions as required by 23 U.S.C. § 313 and 23 C.F.R. § 635.410. Except as expressly provided herein all manufacturing processes of steel or iron materials including but not limited to structural steel, guardrail materials, corrugated steel, culvert pipe, structural plate, prestressing strands, and steel reinforcing bars shall occur in the United States of America, including the application of:

- Coating,
- Galvanizing,
- Painting, and
- Other coating that protects or enhances the value of steel or iron products.

The following are exempt, unless processed or refined to include substantial amounts of steel or iron material, and may be used regardless of source in the domestic manufacturing process for steel or iron material:

- Pig iron,
- Processed, pelletized, and reduced iron ore material, or
- Processed alloys.

The Contractor shall submit a certification stating that all manufacturing processes involved with the production of steel or iron materials occurred in the United States.

Produce, mill, fabricate, and manufacture in the United States of America all aluminum components of bridges, tunnels, and large sign support systems, for which either shop fabrication, shop inspection, or certified mill test reports are required as the basis of acceptance by the Department.

Use foreign materials only under the following conditions:

- 1) When the materials are not permanently incorporated into the project; or
- 2) When the delivered cost of such materials used does not exceed 0.1 percent of the total Contract amount or \$2,500.00, whichever is greater.

The Contractor shall submit to the Engineer the origin and value of any foreign material used.

## 2.0 – BUILD AMERICA, BUY AMERICA (BABA)

Contractor shall comply with the Federal Highway Administration (FHWA) Buy America Requirement in 23 C.F.R. § 635.410 and all relevant provisions of the Build America, Buy America Act (BABA), contained within the Infrastructure Investment and Jobs Act, Pub. L. No. 117-58, §§ 70901-52 enacted November 15, 2021. The BABA requires iron, steel, manufactured products, and construction materials used in infrastructure projects funded by federal financial assistance to be produced in the United States. Comply with 2 C.F.R § 184.

BABA permits FHWA participation in the Contract only if domestic steel and iron will be used on the Project. To be considered domestic, all steel and iron used, and all products manufactured from steel and iron must be produced in the United States and all manufacturing processes, including application of a coating, for these materials must occur in the United States. Coating includes all processes that protect or enhance the value of the material to which the coating is applied. This requirement does not preclude a minimal use of foreign steel and iron materials, provided the cost of such materials does not exceed 0.1% of the total contract amount under the Contract or \$2,500.00 whichever is greater.

BABA permits FHWA participation in the Contract only if all “construction materials” as defined in the Act are made in the United States. The Buy America preference applies to the following construction materials incorporated into infrastructure projects: non-ferrous metals; plastic and polymer-based products (including polyvinylchloride, composite building materials, and polymers used in fiber optic cables); glass (including optic glass); Fiber optic cable; optical fiber; lumber; engineered wood; and drywall. Contractor will be

required to use construction materials produced in the United States on this Project. The Contractor shall submit a certification stating that all construction materials are certified to be BABA compliant.

### **3.0 FINAL RULE – FHWA’S BUY AMERICA REGULATION TO TERMINATE GENERAL APPLICABILITY WAIVER FOR MANUFACTURED PRODUCTS**

- **March 17, 2025** (effective date): For all Federal-aid projects obligated on or after March 15, 2025, all iron or steel products, as defined in § 635.410(c)(1)(iii), must comply with FHWA’s Buy America requirements for steel and iron in § 635.410(b). In addition, for all Federal-aid projects obligated on or after March 15, 2025, per § 635.410(c)(2), articles, materials, and supplies should be classified as an iron or steel product, a manufactured product, or another product as specified by law or in 2 CFR part 184 (such other products specified by law or in 2 CFR part 184 include “excluded materials” and “construction materials”); an article, material, or supply must not be considered to fall into multiple categories.
- **October 1, 2025:** The final assembly requirement will become effective for Federal-aid projects obligated on or after October 1, 2025. This means that, for manufactured product to be Buy America compliant, for Federal-aid projects obligated on or after October 1, 2025, final assembly of the manufactured product must occur in the United States.
- **October 1, 2026:** The 55 percent requirement will become effective for Federal-aid projects obligated on or after October 1, 2026. This means that, for manufactured product to be Buy America-compliant, for Federal-aid projects obligated on or after October 1, 2026, all manufactured products permanently incorporated into the project must both be manufactured in the United States (satisfy the final assembly requirement) and have the cost of the components of the manufactured product that are mined, produced, or manufactured in the United States be greater than 55 percent of the total cost of all components of the manufactured product (satisfy the 55 percent requirement).

### **4.0 – ADDITIONAL REQUIREMENTS**

The Contractor has completed and submitted, or shall complete and submit, to the Cabinet a Buy America/Build America, Buy America Certificate prior to the Cabinet issuing the notice to proceed, in the format below. After submittal, the Contractor is bound by its original certification.

A false certification is a criminal act in violation of 18 U.S.C. § 1001. The Contractor has the burden of proof to establish that it’s in compliance.

At the Contractor’s request, the Cabinet may, but is not obligated to, seek a waiver of Buy America requirements if grounds for the waiver exist under 23 C.F.R. § 635.410(c) or will comply with the applicable Buy America requirements if a waiver of those requirements is not available or not pursued by the Cabinet.

Please refer to the Federal Highway Administration’s Buy America webpage for more information.

[Buy America - Construction Program Guide - Contract Administration - Construction - Federal Highway Administration \(dot.gov\)](#)

Effective - June 26, 2025, Letting

**BUY AMERICA / BUILD AMERICA, BUY AMERICA (ACT) MATERIALS CERTIFICATE OF COMPLIANCE**

The Contractor hereby certifies that it will comply with all relevant provisions of the Build America, Buy America Act, contained within the Infrastructure Investment and Jobs Act, Pub. L. NO. 117-58, §§ 70901-52, the requirements of 23 U.S.C. § 313, 23 C.F.R. § 635.410 and 2 C.F.R § 184.

Date Submitted:\_\_\_\_\_

Contractor:\_\_\_\_\_

Signature:\_\_\_\_\_

Printed Name:\_\_\_\_\_

Title:\_\_\_\_\_

**NOTE: THIS CERTIFICATION IS IN ADDITION TO ANY AND ALL REQUIREMENTS OUTLINED IN THE CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND/OR SPECIAL NOTES CONTAINED IN THE PROJECT PROPOSAL.**

## **SPECIAL NOTE FOR RECIPROCAL PREFERENCE**

### **RECIPROCAL PREFERENCE TO BE GIVEN BY PUBLIC AGENCIES TO RESIDENT BIDDERS**

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the electronic bidding software. Submittal of the Affidavit should be done along the bid in Bid Express.

April 30, 2018

**NATIONAL HIGHWAY**

Be advised this project is on the NATIONAL HIGHWAY SYSTEM.

**ASPHALT MIXTURE**

Unless otherwise noted, the Department estimates the rate of application for all asphalt mixtures to be 110 lbs/sy per inch of depth.

**FUEL AND ASPHALT PAY ADJUSTMENT**

The Department has included the Contract items Asphalt Adjustment and Fuel Adjustment for possible future payments at an established Contract unit price of \$1.00. The Department will calculate actual adjustment quantities after work is completed. If existing Contract amount is insufficient to pay all items on the contract with the adjustments, the Department will establish additional monies with a change order.

**OPTION B**

Be advised that the Department will control and accept compaction of asphalt mixtures furnished on this project under OPTION B in accordance with Sections 402 and 403.



## Special Notes Applicable to Project General Notes & Description of Work

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### **CAUTION**

The information in this proposal and the type of work listed herein are approximate only and are not to be taken as an exact evaluation of the materials and conditions to be encountered during construction; the bidder must draw his/her own conclusions when developing the Unit Bid Prices for each bid item. As such, if the conditions encountered are not in accordance with the information shown, the Department does not guarantee any changes to the Unit Bid Prices nor extension of the contract will be considered. The Department will pay for bid item quantity overruns, but only if pre-approved by the Engineer.

### **STATIONING**

The contractor is advised that the planned locations of work were established from a beginning station number, which is STA 401+07.79 which is 0.03 miles south of Tremont Drive and corresponds to Milepoint 7.596 along US 119. **NOTE:** The existing mile marker signs may not correspond to the proposed work locations.

### **RIGHT OF WAY LIMITS**

The Department has not established the exact limits of the Right-of-Way. Unless a consent and release form is obtained from the adjoining property owner, limit work activities to the obvious Right-of-Way and staging areas secured and environmentally cleared by the Contractor at no additional cost to the Department. In the event that private improvements (i.e., fences, buildings, etc.) encroach upon the Right-of-Way, the contractor shall notify the Engineer and limit work activities in order to NOT disturb the improvements. If they become necessary, the Department will secure consent and releases from property owners through the Engineer. Be responsible for all encroachments onto private lands.

### **CONTROL**

Perform all work under the absolute control of the Department of Highways. Obtain the Engineer's approval of all designs required to be furnished by the Contractor prior to incorporation into the work. The Department reserves the right to have other work performed by other contractors and its own forces and to permit public utility companies and others to do work during the construction within the limits of, or adjacent to, the project. Conduct operations and cooperate with such other parties so that interference with such other work will be reduced to a minimum. The Department will not honor any claims for money or time extension created by the operations of such other parties. Should a difference of opinion arise as to the rights of the Contractor and others working within the limits of, or adjacent to, the project, the Engineer will decide as to the respective rights of the various parties involved in order to assure the completion of the Department's work in general harmony and in a satisfactory manner, and his/her decision shall be final and binding upon the Contractor.

### **DESCRIPTION OF WORK**

Except as specified herein, perform all work in accordance with the Department's Standard Specifications, Supplemental Specifications, applicable Special Notes and Special Provisions, and applicable Standard and Sepia Drawings, current editions. Furnish all materials, labor, equipment, and incidentals for the following work:

**Pavement Resurfacing.** The existing roadway is to be resurfaced from Station 401+07.79 to Station 574+37.47. Paving limits to encompass entire roadway including existing/proposed paved shoulders. Other items that may be associated with the pavement resurfacing include: construction of edge keys, installation of rumble strips, and application of pavement markings. Refer to the rumble strip Standard Drawings for recommended placement of rumble strips and see Striping Detail Sheet in the plans.

General Notes & Description of Work  
Page 2 of 2

**Shoulder Mill & Trench Full Depth Pavement.** Construct a 6 ft trench along the shoulder on one side of the road as identified on the plan sheets. Replace with asphalt surface and asphalt base as shown on the typical section.

**Removal of Existing Signing Assemblies and Installation of Proposed Signing.** A quantity of "Remove Sign" has been included for removal of existing signs along the corridor, as identified on the Plans and the General Summary. An estimated quantity of new signing and sign post is included on the Signing Summary. The Contractor and Engineer will work with the District Traffic Section to determine the final signing layout and sign types prior to installation of the proposed signing. Refer to the Special Note for Signing and the Special Note for Signage for more details concerning the procedures for determining and staking the final layout and installation of the signing.

**Temporary Striping.** A quantity Pave Striping – Temp Paint – 6 in has been included in the contract. The Contractor and Engineer should work together to determine any locations throughout the project requiring temporary pavement striping. The Engineer will make the final determination as to the quantities and placement of temporary pavement striping.

**Thermoplastic 6" Striping.** A quantity of Pave Striping – Thermo – 6 in has been included in the contract to restripe the entire corridor with 6" thermoplastic paint. Use the Striping Detail sheet included in the plan set to layout striping.

### Special Note for Staking

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Perform Contractor Staking according to Section 201; except, in addition to the requirements of Section 201, perform the following:

1. Contrary to Section 201.03.01, perform items 1 & 2 usually performed by the Engineer.
2. Using stakes, paint marks on the pavement, mag nails, and/or any other means approved by the Engineer, the Contractor shall mark and/or stake the proposed sign locations in the field. NOTE: The proposed signs are listed in the proposal by approximate location and are NOT to be taken as the exact location for the signs. During staking operations the Contractor shall review the signing layout and existing field conditions and look for potential conflicts, including but not limited to utilities, driveways, visual obstructions, etc. When conflicts are found, adjust the staked location of signs to mitigate conflicts. Because the sign locations in the proposal are approximate and the location of some signs may need to be adjusted due to conflicts, during staking operations the Contractor shall refer to and utilize the information in the Manual on Uniform on Traffic Control Devices (MUTCD), current edition. The MUTCD cover items such as: appropriate sign location, advance placement distances, and spacing requirements for signing. The intent is for the proposed signs to be consistent with, and meet the requirements of, the MUTCD. Once the proposed sign locations have been staked, notify and coordinate with the District Traffic Engineer, and perform a review of the staked locations. Adjust the staked locations, as directed by the District Traffic Engineer and obtain approval of the final staked locations. This review will also be used to determine if there are any existing signs that require removal and/or relocation. Provide the District Traffic Engineer with 2 weeks of notice when a route will be ready for a review of the staked locations. NOTE: The District Traffic Engineer may determine that the proposed signing, including sign types and messages, needs to be adjusted and/or modified from what is shown in the proposal. Therefore, the Contractor shall not order any sign material for a route until the route has been staked and final sign location approval has been given by the District Traffic Engineer.
3. Using paint marks on the pavement, and/or any other means approved by the Engineer, the Contractor shall layout and pre-mark the proposed striping, pavement markings, etc. Adjust as necessary to accommodate the existing site conditions and to provide proper alignment of the proposed thru and turning lanes. Take extra care in laying out the proposed passing zones and obtain approval of the pre-marked passing zone layouts from the Engineer and/or District Traffic Engineer prior to installing the passing zone striping.
4. Prior to incorporating into the work, obtain the Engineers approval of all revisions determined by the Contractor.
5. Perform any and all other staking operations required to control and construct the work.

### **Special Note for Shoulder Milling/Trenching**

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Trench shoulders as shown on the Typical Section. The Engineer may eliminate locations along the route from shoulder trenching (e.g. road approaches, turn lanes, entrances, etc.). For entrances and road approaches, the Engineer will determine whether to omit the trenching or continue the trenching across the entrance or approach. DO NOT trench across entrances or road approaches without the Engineer's approval. If trenching is achieved by means other than milling, saw cut the pavement 7 inches deep to create a smooth edge prior to excavating the shoulder trench. Excavate the material from the shoulder and maintain the proposed cross-slope as shown on the Typical Sections. The intent is to mill, or excavate, the entire trench so that the proposed shoulder slope is retained at the end of the paving operation. Reshape and compact excavated material from the trench on the outside edge of the newly paved shoulder as shown on the Typical Section.

Retain possession of excess materials and/or materials the Engineer deems unsuitable for reuse and waste the materials off the right-of-way at sites obtained by the Contractor at no additional cost to the Department. See Special Provision for Waste and Borrow.

Accept payment at the contract unit price per square yard for SHOULDER MILLING/TRENCHING as full compensation for all labor, materials, equipment, and incidentals for excavating the shoulder trench and reuse and/or disposal of the excavated material.

### Special Note for Signage

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All sign sheeting shall be from the Cabinet's List of Approved Materials.

All permanent signs and sign components shall be fabricated using Type XI sheeting.

The following signs and sign components shall be fabricated using Type XI fluorescent yellow sheeting:

- Horizontal Alignment Signs and Plaques, including signs shown in Figure 2C-1 of the MUTCD
- All Advisory Speed (W13-1P) plaques

The following signs shall be fabricated using Type XI fluorescent yellow-green sheeting:

- School and school bus warning signs, including the fluorescent yellow-green signs shown in Figures 7B-1 and 7B-6 of the MUTCD and other school-related warning signs that are not included in the MUTCD.
- Bicycle Warning (W11-1) signs and SHARE THE ROAD (W16-1P) plaques or diagonal downward pointing arrow (W16-7P) plaques that supplement Bicycle Warning signs.
- Pedestrian Warning signs and diagonal downward pointing arrow plaques that supplement Pedestrian Warning signs.
- In-Street Pedestrian Crossing (R1-6) signs and Overhead pedestrian Crossing (R1-9) signs
- Supplemental plaques to any of the previously listed signs

## Special Note for Signing

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### I. DESCRIPTION

Except as provided herein, this work shall be performed in accordance with the current edition of the Manual on Uniform Traffic Control Devices (MUTCD), the Department's current Standard Specifications and Interim Supplemental Specifications, applicable Standard and Sepia Drawings, and applicable Special Provisions. Article references are to the Standard Specifications. This project shall consist of furnishing all labor, equipment, materials, and incidentals for the following:

(1) Maintaining and Controlling Traffic; (2) Furnish, Fabricate, and Erect Signs; and (3) All other work specified in the Contract.

### II. MATERIALS

All materials shall be sampled and tested in accordance with the Department's Sampling Manual and the materials shall be available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing unless otherwise specified in these Notes.

**A. Maintain and Control Traffic.** See Traffic Control Plan.

### III. CONSTRUCTION METHODS

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Site Preparation.** Be responsible for all site preparation including, but not limited to: clearing and grubbing, staking, excavation, backfill, and removal of obstructions or any other material not covered by other items. Perform all site preparation only as approved or directed by the Engineer.

**C. Staking.** See Special Note for Staking.

**D. Signs and Posts.** Before beginning installation, the Contractor shall furnish to the Engineer drawings, descriptions, manufacturer's cuts, etc. describing and/or detailing all material to be used. Mill test reports for beams, steel panels, and each different gauge of aluminum or steel sheeting used must be submitted to the Division of Construction and approved prior to erection.

Fabricate sheet signs from .080 or .125 gauge aluminum alloy 5052-H38 or 6061-T6, in accordance with ASTM B-209, and to the size and shape specified. Prepare the side of the aluminum sheet to receive the retroreflective background material according to the recommendations of the sheeting and retroreflective material manufacturer(s). Sheeting used as background material for sign faces is to be the color specified and visually in accordance with the standard requirements of ASTM D-4956 and meet the requirements of Section 830 of the Standard Specifications. Contrary to Section 830.02.06, only the types and colors of sheeting as specified in the proposal will be accepted. All retroreflective material shall be fabricated and assembled in accordance with the specifications and/or recommendations of the manufacturer(s).

Signing  
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All hardware for the erection of sheeting signs shall be rust resistant: stainless steel, zinc coated, aluminum, or an Engineer approved material. All beams and sign posts shall be of sufficient lengths so that a single, continuous length of sign post extends from the top of the sign to the required embedment in the anchor. Splicing of the sign post shall NOT be allowed. For installations in soil, Type I steel posts shall be mounted on either a standard anchor, with soil stabilizer plate, or on a Type D breakaway sign support. Refer to Sheeting Sign Detail Sheet 1 of 2 for installation details for a standard anchor with soil stabilizer plate. When installing a standard anchor with soil stabilizer plate, if solid rock is encountered, the Contractor shall drill a hole to the required depth into the rock, install the anchor into the hole, and backfill the anchor post with concrete, or other method approved by the Engineer. The cost shall be incidental to Type I steel post, and a soil stabilizer plate will not be required. Refer to Standard Drawing RGX-065, current edition, for installation details of Type D breakaway sign supports. Approved manufacturers for Type D breakaway sign supports have been placed on the list of approved materials. For installations to be installed on sufficiently cured existing concrete, such as a sidewalk, concrete median, etc., Type I steel posts shall be mounted on a Type D Surface Mount. For Type D Surface Mounts use only Redi-Torque Model 280 Surface Mount Slip Base Assembly (part number SMSB) by Xcessories Squared of Auburn, IL. Prior to installation, the Contractor shall submit to the Engineer shop drawings of the Type D Surface Mount. Install the Type D Surface Mount according to all the applicable requirements of the manufacturer (see shop drawings). If a Type D breakaway sign support is specified for a location that has an asphalt surface, the Contractor shall install the Type D breakaway sign support detailed on Standard Drawing RGX-065, current edition. All steel post shall meet the requirements of Section 832. All hardware including, but not limited to, sign post anchors, soil stabilizer plates, nuts, bolts, washers, fasteners, fittings, and bracing, or any other incidentals necessary to erect the signs shall be furnished by the Contractor and will be incidental to the work.

New concrete bases, posts, support anchors, signs, etc. are to be installed prior to dismantling any existing sign(s). The removal of existing signs, posts, and support anchors is to be performed concurrently with the installation of new signs, posts, and support anchors, under the same lane closure during the same work shift. Completely remove existing sign support anchors or remove them to a minimum depth of six (6) inches below existing ground line and backfill the disturbed area to the existing ground line.

When listed on the plans and/or summaries, fabricate Reflective Sign Post Panels from .080 gauge aluminum alloy 5052-H38 or 6061-T6, in accordance with ASTM B-209 and to the size(s) specified. Prepare the side of the aluminum sheet to receive the retroreflective background material according to the recommendations of the sheeting and retroreflective material manufacturer(s). Sheeting for the Reflective Sign Post Panels shall be the same Type and color as the sign installed on the post. Examples include:

- Red, fluorescent yellow, and fluorescent yellow-green (Type XI Sheeting)
- White and yellow (Type XI Sheeting)

Reflective Sign Post Panels shall be 2 inches wide and will typically have a height of 60 inches for rural installations and typically have a height of 84 inches for urban installations. There will be certain instances where a proposed Reflective Sign Post Panel will have a height dimension less than 60 inches; typically, this will be when the bottom of the bottom-most sign is mounted lower than the standard 5 ft minimum mounting height (e.g. 3 ft or 4 ft mount heights). In those cases, the height

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of the Reflective Sign Post Panel is expected to closely match (within 1-2 inches) the distance between the top of the anchor or support to the bottom edge of the bottom-most sign. Reflective Sign Post Panels shall have three 3/8" holes (one hole in the top 3", one hole near the center, and one hole in the bottom 3") that align with the holes on the Type I steel post.

All manufactured sheeting signs shall be free of visual defects including, but not limited to: cracks, tears, ridges, humps, discoloration, etc., and defective signs shall be replaced at no additional cost to the Department.

All sign blanks shall be hole punched by the manufacturer for either horizontal or vertical installation. Attach all aluminum sheeting signs to square post with 3/8" all steel rivets and nylon washers. Use bracing as indicated on the plans, summaries, and/or standard signing detail sheets, and/or when directed by the Engineer and/or District Traffic Engineer.

All sign posts shall be attached to anchors with 5/16" corner bolts and 5/16" flanged nuts, and all post and anchor cuts shall be treated with a Cold Galvanizing Compound spray.

Sign posts shall be erected vertically by using a bubble level. The tolerance shall be a two (2) degree angle in any direction. For locations where more than one sign is mounted beside each other, the posts shall be spaced to provide approximately six inches (6") of spacing between signs.

- E. Remove & Relocate Sheet Signs.** When listed on the plans and/or summaries, and/or as directed by the Engineer and/or District Traffic Engineer, remove the specified existing sheet sign(s) from the existing post(s) and reinstall on a new sign post. Once the specified existing sheet sign(s) have been removed and relocated, and if the existing sign post(s) are no longer needed to support other existing signs, removal of the existing sign post(s) will be paid under the bid item REMOVE SIGN. If any of the existing hardware components (bracing, brackets, bolts, rivets, etc.) are found to have pre-existing damage or are damaged during the Contractor's removal and reinstallation efforts, the Contractor shall provide the necessary replacement hardware for proper re-installation of the sheet sign. These components shall be incidental to the bid item REMOVE AND RELOCATE SHEET SIGNS.

Prior to removing and reinstalling a sheet sign, the Contractor shall first review the existing sheet sign for damage. It is the Contractor's responsibility to notify the Engineer of any existing sheet sign damage prior to removal and relocation of the sheet sign, so that it can be documented that the existing sheet sign had pre-existing damage. If the Contractor does not make the Engineer aware of pre-existing damage prior to detaching the sheet sign from its existing post, the Department will assume the damage was the result of the Contractor's removal and reinstallation efforts. The Contractor shall replace any sheet signs that are damaged during the removal and reinstallation efforts. Replacement of sheet signs damaged by the Contractor shall be incidental to the bid item REMOVE AND RELOCATE SHEET SIGNS.

If the existing sheet sign is found to have pre-existing damage, the Department will provide the Contractor with a new sheet sign to replace the sheet sign with pre-existing damage. Detaching the existing, damaged sheet sign from the existing post and attaching the new, Department-provided sheet sign to the new sign post shall be incidental the bid item REMOVE AND RELOCATE SHEET SIGNS.



- F. Remove & Relocate Sign Assemblies.** When listed on the plans and/or summaries, and/or as directed by the Engineer and/or District Traffic Engineer, remove the specified existing sign assemblies from the existing location and reinstall in a new location. The Department will consider all signs attached to one or more connected posts as a single sign assembly, no matter how many signs are attached to the existing sign assembly. If any of the existing hardware components (bracing, brackets, bolts, rivets, etc.) are found to have pre-existing damage or are damaged during the Contractor's removal and reinstallation efforts, the Contractor shall provide the necessary replacement hardware for proper re-installation of the sign assembly. These components shall be incidental to the bid item REMOVE AND RELOCATE SIGN ASSEMBLY.

Prior to removing and relocating a sign assembly, the Contractor shall review the existing sign(s) and sign post(s) for damage. It is the Contractor's responsibility to notify the Engineer of any sign or sign post damage prior to removal and relocation of the sign assembly, so that it can be documented that the existing sign and/or sign post had pre-existing damage. If the Contractor does not make the Department aware of pre-existing damage prior to removing a sign assembly from its existing location, the Department will assume the damage was the result of the Contractor's removal and reinstallation efforts. The Contractor shall replace any components of a sign assembly that are damaged during removal and relocation. Replacement of any components damaged by the Contractor shall be incidental to the bid item REMOVE AND RELOCATE SIGN ASSEMBLY.

If an existing sign that is part of a sign assembly to be removed and relocated is found to have pre-existing damage, the Department will provide the Contractor with a new sign to replace the sign with pre-existing damage. Detaching the existing, damaged sign from the existing post and attaching the new, Department-provided sign to the relocated existing post shall be incidental to the bid item REMOVE AND RELOCATE SIGN ASSEMBLY.

If an existing sign assembly that is to be removed and relocated is found to not have an existing soil stabilizer plate, or if the soil stabilizer plate and/or anchor is damaged during removal, then a new soil stabilizer plate and/or anchor shall be provided by the Contractor and shall be incidental to the bid item REMOVE AND RELOCATE SIGN ASSEMBLY.

If an existing sign assembly that is being relocated is not currently mounted on a Type D breakaway sign support, but the plans and/or summaries indicate, or wind load standards dictate, a Type D breakaway sign support or a Type D Surface Mount is required, provide and install the specified Type D support as part of the removal and reinstallation efforts. Type D breakaway sign supports shall be paid under the bid item GMSS TYPE D and Type D Surface Mount supports shall be paid under the bid item GMSS TYPE D (SURFACE MOUNT).

If an existing sign that is being relocated is found to have pre-existing damage to one or more of the sign post, the Department will NOT utilize the bid item REMOVE AND RELOCATE SIGN ASSEMBLY for removing and relocating such a sign assembly. Instead, the Department will require the Contractor to install a new sign post(s) at the new location, and pay for the new post(s) under the bid item STEEL POST TYPE I. Detaching the existing sign(s) from the existing, damaged post(s) and attaching the existing sign(s) to the new sign post(s) shall be incidental to the bid item STEEL POST TYPE I. Any hardware that is needed to complete the installation shall also be incidental to the bid item STEEL

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POST TYPE I. Removal of the existing damaged post(s) and any other sign components not needed will be paid under the bid item REMOVE SIGN.

- G. Property Damage.** The Contractor shall be responsible for all damage to public and/or private property resulting from the Contractor's activities. Repair or replace damaged roadway features in like kind materials and design as directed by the Engineer at no additional cost to the Department. Repair or replace damaged private property in like kind materials and design to the satisfaction of the owner and the Engineer at no additional cost to the Department.
- H. Coordination with Utility Companies.** Locate all underground, above ground, and overhead utilities prior to beginning construction. Be responsible for contacting and maintaining liaison with all utility companies that have utilities located within the project limits. Do not disturb existing overhead or underground utilities. It is not anticipated that any utility facilities will need to be relocated and/or adjusted; however, in the event that it is discovered that the work does require that utilities be relocated and/or adjusted, the utility companies will work concurrently with the Contractor while relocating their facilities. Be responsible for repairing all utility damage that occurs due to the Contractor's operations at no additional cost to the Department. NOTIFY THE ENGINEER AND THE UTILITY OWNER(S) IMMEDIATELY WHEN IT IS DISCOVERED OR ANTICIPATED THAT ANY UTILITY CONFLICT COULD DELAY THE CONTRACTOR'S OPERATIONS. If the total delay exceeds ten working days, an extension of the specified completion date will be negotiated with the Contractor for delay to the Contractor's work; however, no extension will be granted for any delay caused by the Contractor's failure to notify the Engineer and/or the utility company as specified above when a conflict is discovered or anticipated as specified.
- I. Caution.** The information in this proposal and the type of work listed herein are approximate only and are not to be taken as an exact evaluation of the materials and conditions to be encountered during construction; the bidder must draw his/her own conclusions when developing the Unit Bid Prices for each bid item. As such, if the conditions encountered are not in accordance with the information shown, the Department does not guarantee any changes to the Unit Bid Prices nor extension of the contract will be considered. The Department will pay for bid item quantity overruns, but only if pre-approved by the Engineer.
- J. Control.** Perform all work under the absolute control of the Department. Obtain the Engineer's approval of all designs required to be furnished by the Contractor prior to incorporation into the work. The Department reserves the right to have other work performed by other contractors and its own forces, and to permit public utility companies and others to do work during the construction within the limits of, or adjacent to, the project. Conduct operations and cooperate with such other parties so that interference with such other work will be reduced to a minimum. The Department will not honor any claims for money or time extension created by the operations of such other parties.

Should a difference of opinion arise as to the rights of the Contractor and others working within the limits of, or adjacent to, the project, the Engineer will decide as to the respective rights of the various parties involved in order to assure the completion of the Department's work in general harmony and in a satisfactory manner, and the Engineer's decision shall be final and binding upon the Contractor.

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- K. Clean Up, Disposal of Waste.** Clean up the project area as work progresses. Dispose of all removed concrete, debris, and other waste as per Section 204.03.08. The Department will incur no cost to obtain the disposal sites. The Department will NOT make direct payment for disposal of waste and debris from the project. Existing anchors, signs, posts, and any other hardware or material removed from the site are to become the property of the Contractor. See Special Provision for Waste and Borrow Sites.
- L. Final Dressing, Seeding and Protection.** Grade all disturbed areas to blend with the adjacent roadways features and to provide a suitable seed bed. Apply Class A Final Dressing to all disturbed areas, both on and off the Right-of-Way. Sow all disturbed earthen areas with the applicable seed mixture(s) according to Section 212.03.03.

#### IV. METHOD OF MEASUREMENT

- A. Maintain and Control Traffic.** See Traffic Control Plan.
- B. Site Preparation.** Other than the bid items listed, the Department will NOT measure Site Preparation for payment, but shall be incidental to the project bid items.
- C. Signs and Reflective Sign Post Panels.** The Department will measure the finished in-place area of signs and Reflective Sign Post Panels in Square Feet.
- D. Sign Posts.** The Department will measure the finished in-place length of sign posts in Linear Feet, from the top of the anchor, or top of the sign support, to the top of the sign post. Laps, cutoffs, excess, and waste will NOT be measured for payment.
- E. Type D Breakaway Sign Supports.** The Department will measure Type D breakaway sign supports as Each support installed.
- F. Type D Surface Mounts.** The Department will measure Type D Surface Mounts as Each surface mount installed.
- G. Class A Concrete for Signs.** The Department will measure the Class A Concrete used in conjunction with Type D breakaway sign support installations in Cubic Yards. Any concrete that is required as backfill due to hitting rock during a standard installation shall be incidental to the bid item STEEL POST TYPE I, and soil stabilizers will not be required.
- H. Remove Sign.** The Department will consider all signs attached to one or more connected posts as a single sign. The Department will measure as Each sign assembly removed and NOT each individual sign removed.
- I. Remove & Relocate Sheet Signs.** The Department will measure sheet signs removed from an existing sign post and reinstalled on a new sign post as Each sheet sign removed and reinstalled. as indicated in the contract documents, or as directed by the Engineer. The new sign post shall be measured as indicated in paragraph D. of this section.

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- J. Remove & Relocate Sign Assemblies.** The Department will consider all signs attached to one or more connected posts as a single sign assembly. When the contract documents indicate that an existing sign assembly is to be removed from its existing location and reinstalled in a new location, the Department will measure and pay for "Remove and Relocate Sign Assembly" as each sign assembly removed and relocated; NOT each individual sign removed and relocated.
- K. Items Provided by KYTC.** The Department will NOT measure for payment the installation of signs and/or surface mounts provided by KYTC. These activities shall be incidental to the bid item STEEL POST TYPE I.
- L. Clean Up, Disposal of Waste, Final Dressing, Seeding and Protection.** The Department will NOT measure for payment the following activities: Clean Up, Disposal of Waste, Final Dressing and Seeding and Protection.

#### **V. BASIS OF PAYMENT**

- A. Maintain and Control Traffic.** See Traffic Control Plan.
- B. Signs and Reflective Sign Post Panels.** The Department will make payment for the completed and accepted quantities under the bid item SBM ALUM SHEET SIGNS .125 IN or .080 IN. The Department will consider payment full compensation for all work and incidentals necessary to install the signs and any Reflective Sign Post Panels, as required by these notes and the details found elsewhere in the plans/proposal, at the locations indicated on the summary sheets, plans, and/or as directed by the Engineer.
- C. Sign Posts.** The Department will make payment for the completed and accepted quantities under the bid item STEEL POST TYPE I. The Department will consider payment full compensation for all work and incidentals necessary to install the sign posts as required by these notes and the details found elsewhere in the plans/proposal.
- D. Type D Breakaway Sign Supports.** The Department will make payment for the completed and accepted quantities under the bid item GMSS TYPE D. The Department will consider payment full compensation for all work and incidentals necessary to install the Type D breakaway sign supports as required by Standard Drawing RGX-065, current edition.
- E. Type D Surface Mounts.** The Department will make payment for the completed and accepted quantities under the bid item GMSS TYPE D (SURFACE MOUNT). The Department will consider payment full compensation for all work and incidentals necessary to install the Type D surface mounts according to all applicable manufacturer requirements.  
NOTE: The permissible Type D Surface Mount alternative is: Redi-Torque Model 280 Surface Mount Slip Base Assembly (part number SMSB) by Xcessories Squared of Auburn, IL
- F. Class A Concrete for Signs.** The Department will make payment for the completed and accepted quantities, used in conjunction with Type D breakaway sign support installations, under the bid item

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CLASS A CONCRETE FOR SIGNS. The Department will consider payment full compensation for all work and incidentals necessary to install the concrete as required by Standard Drawing RGX-065, current edition.

- G. Remove Sign.** The Department will make payment for the completed and accepted quantities under the bid item REMOVE SIGN. The Department will consider payment full compensation for all work and incidentals necessary to remove the existing signs, posts, anchors, and any other sign material or hardware, from the locations indicated on the summary sheets, plans, and/or as directed by the Engineer.
- H. Remove & Relocate Sheet Signs.** The Department will make payment for the completed and accepted quantities under the bid item REMOVE AND RELOCATE SHEET SIGNS. Any hardware that is needed to complete the removal and reinstallation shall be incidental. The Department will consider payment full compensation for all work and incidentals necessary to remove and reinstall the existing sheet signs as indicated on the plans, summaries, and/or as directed by the Engineer.
- I. Remove & Relocate Sign Assemblies.** The Department will make payment for the completed and accepted quantities under the bid item REMOVE AND RELOCATE SIGN ASSEMBLY. Any hardware that is needed to complete the removal and reinstallation shall be incidental. The Department will consider payment full compensation for all work and incidentals necessary to remove and reinstall the existing sign assembly as indicated on the plans, summaries, and/or as directed by the Engineer

## Special Note for Completion Date & Liquidated Damages

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### I. COMPLETION DATE

The ultimate fixed completion date for this project will be **November 15, 2025**. Liquidated Damages for failure to complete the project on time will be assessed following Section 108.09.

### II. LIQUIDATED DAMAGES

In addition to the requirements of Section 108.09, the Department will assess Liquidated Damages in the amount of **\$1,000** per hour for each hour, or fraction of an hour, for any and all lane closures that are in place beyond the time frame(s) noted in the Traffic Control Plan and approved by the Engineer.

Contrary to Section 108.09, Liquidated Damages will be assessed regardless of whether seasonal limitations prohibit the Contractor from performing work on the controlling operation.

All liquidated damages will be applied accumulatively.

All other applicable portions of Section 108 apply.

## **SPECIAL NOTE FOR ELECTRONIC DELIVERY MANAGEMENT SYSTEM (e-Ticketing) ASPHALT**

This Special Note will apply when indicated on the plans or in the proposal. Section references herein are to the Department's Standard Specifications for Road and Bridge Construction current edition.

**1.0 DESCRIPTION.** Incorporate an e-Ticketing Delivery Software for weighed asphalt material delivered to the project to report loads and provide daily running totals of weighed asphalt material for pay items and incidental work during the construction processes from the point of measurement and loading to the point of incorporation to the project.

**2.0 MATERIALS AND EQUIPMENT.** Contractor shall supply material data in JavaScript Object Notation (JSON) documents to the KYTC e-Ticketing Delivery Software (KYTC e-Ticketing Portal) via Application Programming Interface (API) or direct connection. Test and verify that ticket data can be shared from the original source no fewer than 30 days prior to material placement activities. An e-Ticketing Delivery Software supplier can provide a qualified representative for on-site technical assistance during the initial setup, pre-construction verifications, and data management and processing as needed during the Project to maintain material data delivery capabilities. Virtual meetings may be hosted in lieu of on-site meetings when deemed appropriate by the Engineer.

Provide e-Ticketing Delivery Software that will meet the following:

1. The e-Ticketing Delivery Software shall be fully integrated with the Contractor's Load Read-Out scale system at the material source location.
2. The e-Ticketing Delivery Software shall provide real-time delivery to KYTC e-Ticketing Portal.
3. Transmit any updates to the ticket data within 5 minutes of a change.

**3.0 CONSTRUCTION.** Provide the Engineer with the manufacturer's specifications and all required documentation for data access at the pre-construction conference.

### **A. Construction Requirements**

1. Install and operate software in accordance with the manufacturer's specifications.
2. Verify that all pertinent information is provided by the software within the requirements of this Special Note.

### **B. Data Deliverables**

Provide to the Engineer a means in which to gather report summaries by way of iOS apps, web pages, or any other method at the disposal of the Engineer. The Engineer may request data at any time during the project.

#### **1. Asphalt Material**

##### **a. Real-time Continuous Data Items**

Provide the Engineer access to JSON documents capable of being transmitted through the KYTC's e-Ticketing Portal that displays the following information in real-time with a web-based system compatible with iOS and Windows environments.

- Each Truck
  - Supplier Name
  - Supplier Address
  - Supplier Phone
  - Plant location
  - Date
  - Time at source
  - Project Location

- Contract ID#
- Carrier Name
- Unique Truck ID
- Description of Material
- Mix Design Number
- Gross, Tare and Net Weight
- Weighmaster

**4.0 MEASUREMENT.** The Department will not measure the electronic delivery management system.

**5.0 PAYMENT.** The Department will not measure this work for payment and will consider all items contained in this note to be incidental to the asphalt mixtures on the project, as applicable.

May 5, 2025



## SPECIAL NOTE FOR EXPERIMENTAL KYCT AND FIELD RUT TESTING

### June 2025 Update

#### 1.0 General

**1.1 Description.** The KYCT (Kentucky Method for Cracking Test) and the IDEAL-RT/IDT-HT test results will help determine if the mixture is susceptible to cracking and rutting. During the experimental phase, data will be gathered and analyzed by the Department to determine the durability and stability of the bituminous mixes. Additionally, the data will help the Department to create future performance-based specifications which will include the KYCT and field rutting test methods.

#### 2.0 Equipment

**2.1 KYCT Testing Equipment.** The Department will require a Marshall Test Press with digital recording capabilities. Other CT testing equipment may be used for testing with prior approval by the Department.

**2.2 Water Baths.** One or more water baths will be required that can maintain a temperature of 77° +/- 1.8° F with a digital thermometer showing the water bath temperature. Also, one water bath shall have the ability to suspend gyratory specimen fully submerged in water in accordance with AASHTO T-166, current edition.

**2.3 Field Rutting Tests.** If the contractor elects to perform the IDEAL-RT test, in conformance with ASTM D8360-22, the acquisition of the "Option A" or "Option B" test fixture is required. If the IDT-HT is desired, the test press utilized for the KYTC is sufficient. The Department shall approve all test configurations at their discretion.

**2.4 Gyratory Molds.** Gyratory molds will be required to assist in the production of gyratory specimens in accordance with AASHTO T-312, current edition.

**2.5 Ovens.** Adequate (minimum of two ovens) will be required to accommodate the additional molds and asphalt mixture necessary to perform the acceptance testing as outlined in Section 402 of the Kentucky Standard Specifications for Road and Bridge Construction, current edition.

**2.6 Department Equipment.** The Department will provide gyratory molds, PINE 850 Test Press with digital recordation, and CT testing equipment to assist during this experimental phase so data can be gathered.

#### 3.0 Testing Requirements

**3.1 Acceptance Testing.** Perform all acceptance testing and aggregate gradation as according with Section 402 and Section 403 of the Kentucky Standard Specifications for Road and Bridge Construction, current edition.

**3.2 KYCT Testing.** Perform crack resistance analysis (KYCT) in accordance with the current Kentucky Method for KYCT Index Testing during the plant production of all surface mixtures. Conform to KYTC Specifications for Mix Design approvals. All production testing is currently informational.

**3.2.1 KYCT Frequency.** Obtain an adequate sample of hot mix asphalt to ensure the acceptance testing, gradation, and KYCT gyratory samples can be fabricated and is representative of the bituminous mixture. Acceptance specimens shall be fabricated first, then after the specified amount of oven conditioning, fabricate the KYCT samples with the gyratory compactor in accordance with Section 2.4 of this Special Note. Analysis of the KYCT specimens will be required one per subplot produced from the same asphalt material and at the same time as the acceptance specimen is sampled and tested.

**3.2.2 Number of Specimens and Conditioning.** Fabricate specimens in accordance with the Kentucky Method for KYCT Index Testing. Contrary to the method, for field specimens, fabricate three replicates for cracking resistance analyses and three replicates for rutting resistance analyses. The specimens shall be compacted at the temperature in accordance with KM 64-411.

Contrary to the Kentucky Method, plant produced bituminous material shall be short-term conditioned immediately after sampling for two hours uncovered in the oven at compaction temperature in accordance with KM 64-411.

While the fabricated specimens are allowed to cool in air (fan is permissible) for 30 minutes +/- 5 minutes, find the bulk specific gravity of each specimen according to AASHTO T166. Next, condition the replicates in a 77 °F water bath for 30 minutes +/- 5 minutes. To ensure confidence and reliability of the test results provided by KYCT testing and Field Rut testing, reheating of the asphalt mixture is prohibited.

**3.2.3 Long Term Aging CT's.** For long-term aging and cracking resistance considerations in mix design, mix and condition 3 specimens uncovered for 20 hours at compaction temperature in accordance with KM 64-411. Perform KYCT testing in accordance with KM 64-450 and record the results on the Long-Term KYCT tab of the latest version of the MixPack.

**3.2.4 Record Times.** For each subplot, record the time required between drying aggregates in the plant to KYCT specimen fabrication. The production time may vary due to the time that the bituminous material is held in the silo. Record the preconditioning time when the time exceeds the one-hour specimen cool down time as required in accordance with The Kentucky Method for KYCT Index Testing. The preconditioning time may exceed an hour if the technician is unable to complete the test on the same day or within the specified times as outlined in The Kentucky Method for KYCT Index Testing. The production time and the preconditioning time shall be recorded on the AMAW.

**3.2.5 File Name.** As according to section 7.12 of The Kentucky Method for KYCT Index Testing, save the filename with the following format: "CID\_Aproved Mix Number\_Lot Number\_Sublot Number\_Date"

**3.3 Field Rut Testing.** Perform the rut resistance analysis (IDEAL-RT or IDT-HT) in accordance with ASTM D8360-22 or ALDOT458, respectively. Contrary to ASTM D8360 & ALDOT458, precondition the test specimens in a water bath or forced draft oven at 50 °C +/- 1 °C for 60 +/- 5 min before completing the test.

**3.3.1 Field Rut Testing Frequency.** Perform one test per lot of mixture produced. The plant produced bituminous material sampled for the field rut test does not have to be obtained at the same time as the acceptance and KYCT sample. If the field rut test sample is not obtained at the same time as the KYCT sample, determine the Maximum Specific Gravity of the KYCT sample in accordance with AASHTO T-209 coinciding with the test specimens.

**3.3.2 Number of Specimens and Conditioning.** Fabricate in accordance with the Kentucky Method for KYCT Index Testing. Contrary to the method, for field specimens, fabricate three

replicates for rutting resistance analyses. The specimens shall be compacted at the temperature in accordance with KM 64-411. Contrary to the Kentucky Method, plant produced bituminous material shall be short-term conditioned immediately after sampling for two hours uncovered in the oven at compaction temperature in accordance with KM 64-411.

**3.3.3 Record Times.** Record the production time as according to section 3.2.3 in this special note. Also record the time that the specimens were fabricated. All times shall be recorded on the AMAW.

**3.3.4 File Name.** Record all field rut data in the latest version of the AMAW.

#### **4.0 Data**

Submit the AMAW and all test data that was obtained for acceptance, gradation, KYCT, and field rut testing within five working days once all testing has been completed for a lot to Central Materials Lab and the District Materials Engineer. Also, any data and or comments that the asphalt contractor or district personnel deem informational during this experimental phase, shall also be submitted to the Central Materials Lab and the District Materials Engineer. Any questions or comments regarding any item in this Special Note can be directed to the Central Office, Division of Materials, Asphalt Branch.

#### **5.0 Payment**

Any additional labor and testing equipment that is required to fabricate and test the KYCT and field rut specimens shall be considered incidental to the asphalt surface line item. The Department will perform the testing for the KYCT and field rut specimens if a producer does not possess the proper equipment.

June 12<sup>th</sup>, 2025

## **COORDINATION OF WORK WITH OTHER CONTRACTS**

Be advised, there may be an active project(s) adjacent to or within this project. The Engineer will coordinate the work of the Contractors. See Section 105.06.

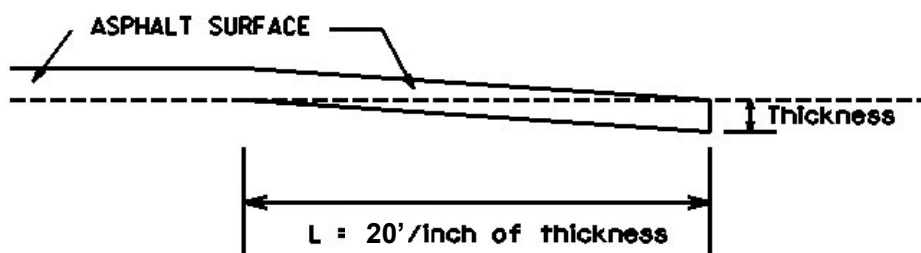
1-3193 Coordination Contracts  
01/02/2012

### SPECIAL NOTE FOR EDGE KEY

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Construct Edge Keys at the beginning of project, end of project, at railroad crossings, and at intersections with ramps, as applicable. Unless specified in the Contract or directed by the Engineer, do not construct edge keys at intersecting streets, roads, alleys, or entrances. Cut out the existing asphalt surface to the required depth and width shown on the drawing and heel the new surface into the existing surface. The Department will measure the Edge Key at the joint as the width of the pavement perpendicular to the centerline in linear feet. The Department will pay for this work at the Contract unit price per linear foot, which shall be full compensation for all labor, materials, equipment, and incidentals for removal and disposal of the existing asphalt surface required to construct the edge key.

### EDGE KEY



Thickness = 1.25 Inches

L = 25 LF

L = Length of Edge Key

### **SPECIAL NOTE FOR TYPICAL SECTION DIMENSIONS**

Consider the dimensions shown on the typical sections for pavement and shoulder widths and thickness' to be nominal or typical dimensions. The Engineer may direct or approve varying the actual dimensions to be constructed to fit existing conditions. Do not widen existing pavement or shoulders unless specified elsewhere in this proposal or directed by the engineer.

1-3725 Typical Section Dimensions  
01/02/2012

## TRAFFIC CONTROL PLAN

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### TRAFFIC CONTROL GENERAL

Except as provided herein, traffic shall be maintained in accordance with the current editions of the Manual on Uniform Traffic Control Devices (MUTCD), Standard Specifications, Supplemental Specifications, and the Standard and Sepia Drawings. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic shall be paid at the lump sum bid price to "Maintain and Control Traffic".

Contrary to Section 106.01, traffic control devices used on this project may be new, or used in like new condition, at the beginning of the work and maintained in like new condition until completion of the work. Any temporary traffic control items, devices, materials, and incidentals shall remain the property of the contractor unless otherwise addressed, when no longer needed.

### PROJECT PHASING & CONSTRUCTION PROCEDURES

At locations with three or more lanes, unless otherwise approved by the engineer, maintain one lane of traffic in each direction at all times during construction. At locations with two lanes, utilizing appropriate traffic control devices and flaggers, maintain alternating one-way traffic during construction and provide a minimum clear lane width of 10 feet. NOTE: During any lane closure, make provisions for the passage of vehicles of up to 16 feet in width. If traffic should be stopped due to construction operations, and a school bus or emergency vehicle on an official run arrives on the scene, make provisions for the passage of the school bus or emergency vehicle as quickly as possible.

Take these restrictions into account in submitting bid. The Department will not consider any claims for money or grant contract time extensions for any delays to the Contractor as a result of these restrictions.

Unless otherwise approved by the Engineer, no lane closures will be allowed during the following times:

Labor Day Weekend	3 pm Friday, August 29, 2025 – 8 pm Monday, September 1, 2025
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At the discretion of the Engineer, additional days and hours may be specified when lane closures will not be allowed.

The Contractor shall submit proposed lane closure days and times to the Engineer at least 7 calendar days in advance for approval. Liquidated Damages will be assessed for each hour or fraction of an hour that a lane closure is in place outside of an approved time period. See the Special Notes for Completion Dates & Liquidated Damages for details on the Liquidated Damages amount.

Traffic Control Plan  
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## **LANE CLOSURES**

Long term lane closures shall not be allowed; therefore, lane closures will not be measured for payment. Do not leave lane closures in place during non-working hours and prohibited periods

## **TEMPORARY SIGNS**

Temporary signposts and splices shall be compliant with NCHRP 350 or MASH. Manufacturer's documentation validating this compliance shall be provided to the Engineer prior to installation. Temporary signs, including any splices, shall be installed according to manufacturer's specifications and installation recommendations. Contrary to section 112.04.02, only long-term temporary signs (temporary signs intended to be continuously in place for more than 3 days) will be measured for payment. Short-term temporary signs (temporary signs intended to be left in place for 3 days or less) will not be measured for payment but will be incidental to Maintain and Control Traffic.

## **CHANGEABLE MESSAGE SIGNS**

Provide changeable message signs in advance of and within the project at locations determined by the Engineer. If work is in progress concurrently in both directions or if more than one lane closure is in place in the same direction of travel, provide additional changeable message signs as directed by the Engineer. Place changeable message signs approximately one mile in advance of the anticipated queue at each lane closure. As the actual queue lengthens and/or shortens, relocate or provide additional changeable message signs so that traffic has warning of slowed or stopped traffic at least one mile but not more than two miles before reaching the end of the actual queue. The Engineer may vary the designated locations as the work progresses. The Engineer will determine the messages to be displayed. In the event of damage or mechanical/electrical failure, repair or replace the Changeable Message Sign. If the damage or mechanical/electrical failure is identified during active work operations, repair or replace the Changeable Message Sign within 6 hours. If the damage or mechanical/electrical failure is identified when there are no active work operations on the project, repair or replace the Changeable Message Sign within 12 hours. The Department will measure for payment the maximum number of Changeable Message Signs in concurrent use at the same time on a single day on all sections of the contract. The Department will measure individual Changeable Message Signs only once for payment, regardless of how many times they are set, reset, removed, and/or relocated during the duration of the project. The Department will not measure for payment any replacements for damaged Changeable Message Signs or any changeable message signs the Engineer directs to be replaced due to poor condition or readability. Retain possession of the Changeable Message Signs upon completion of the work.

## **BARRICADES**

The Department will not measure barricades used in lieu of barrels and cones for channelization or delineation but shall be incidental to Maintain and Control Traffic according to Section 112.04.01.



Traffic Control Plan  
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### **TEMPORARY ENTRANCES**

The Engineer will not require the Contractor to provide continuous access to farms, single family, duplex, or triplex residential properties during working hours; however, provide reasonable egress and ingress to each such property when actual operations are not in progress at that location. Limit the time during which a farm or residential entrance is blocked to the minimum length of time required for actual operations, not extended for the Contractor's convenience, and in no case exceeding six (6) hours. Notify all residents twenty-four hours in advance of any driveway or entrance closings and make any accommodations necessary to meet the access needs of disabled residents.

Except as allowed by the Phasing as specified above, maintain direct access to all side streets and roads, schools, churches, commercial properties, and apartments or apartment complexes of four or more units at all times. Access to fire hydrants must also be maintained at all times

### **THERMOPLASTIC PAVEMENT MARKINGS**

Consider the locations listed on the summary and/or shown on the plan sheets as approximate only. Prior to milling and/or resurfacing, locate and document the locations of the existing markings. After final surfacing operations, replace the markings at their approximate existing locations, as shown on the plan sheets, or as directed by the Engineer. Place markings not existing prior to resurfacing as shown on the plan sheets or as directed by the Engineer.

### **PAVEMENT MARKINGS**

If there is to be a deviation from the existing striping plan, the Engineer will furnish the Contractor a striping plan prior to placement of the final surface course. Install Temporary Striping according to Section 112 with the following exception:

If the Contractor's operations or phasing requires temporary markings that must subsequently be removed from the final surface course, use an approved removable lane tape; however, the Department will not measure removable lane tape for separate payment, but will measure and pay for removable lane tape as temporary striping.

### **PAVEMENT EDGE DROP-OFFS**

Do not allow a pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation with an elevation difference greater than 1½". Place Warning signs (MUTCD W8-11 or W8-9A) in advance of and at 1500' intervals throughout the drop-off area. Dual post the signs on both sides of the traveled way. Wedge all transverse transitions between resurfaced and un-resurfaced areas which traffic may cross with asphalt mixture for leveling and wedging. Remove the wedges prior to placement of the final surface course.

Traffic Control Plan  
Page 4 of 9

Protect pavement edges that traffic is not expected to cross, except accidentally, as follows:

Less than 2" - No protection required.

2" to 4" - Place plastic drums, vertical panels, or barricades every 50 feet. During daylight working hours only, the Engineer will allow the Contractor to use cones in lieu of plastic drums, panels, and barricades. Wedge the drop-off with DGA or asphalt mixture for leveling and wedging with a 1:1 or flatter slope in daylight hours, or 3:1 or flatter slope during nighttime hours, when work is not active in the drop-off area.

Greater than 4" - Protect drop-offs greater than 4 inches within 10 feet of traffic by placing drums, vertical panels, or barricades every 25 feet. The Engineer will not allow the use of cones in lieu of drums, vertical panels, or barricades for drop-offs greater than 4". Place Type III Barricades directly in front of the drop-off facing oncoming traffic in both directions of travel. Provide warning signs as shown on the Standard Drawings or as directed by the Engineer

Pedestrians & Bicycles - Protect pedestrian and bicycle traffic as directed by the Engineer.

## **USE AND PLACEMENT OF CHANGEABLE MESSAGE SIGNS**

The following policy is based upon current Changeable Message Signs (CMS) standards and practice from many sources, including the Federal Highway Administration (FHWA), other State Departments of Transportation, and Traffic Safety Associations. It is understood that each CMS installation or use requires individual consideration due to the specific location or purpose. However, there will be elements that are constant in nearly all applications. Accordingly, these recommended guidelines bring a level of uniformity, while still being open to regional experience and engineering judgment.

### **Application**

The primary purpose of CMS is to advise the driver of unexpected traffic and routing situations. Examples of applications where CMS can be effective include:

- Closures (road, lane, bridge, ramp, shoulder, interstate)
- Changes in alignment or surface conditions
- Significant delays, congestion
- Construction/maintenance activities (delays, future activities)
- Detours/alternative routes
- Special events with traffic and safety implications
- Crash/incidents
- Vehicle restrictions (width, height, weight, flammable)
- Advance notice of new traffic control devices
- Real-time traffic conditions (must be kept up to date)
- Weather /driving conditions, environmental conditions, Roadway Weather Information Systems
- Emergency Situations
- Referral to Highway Advisory Radio (if available)
- Messages as approved by the County Engineer's Office

### **CMS should not be used for:**

- Replacement of static signs (e.g. ROAD WORK AHEAD), regulatory signage (e.g. speed limits), pavement markings, standard traffic control devices, conventional warning or guide signs.
- Replacement of lighted arrow board
- Advertising (Don't advertise the event unless clarifying "action" to be taken by driver – e.g. Speedway traffic next exit)
- Generic messages
- Test messages (portable signs only)
- Describe recurrent congestion (e.g. rush hour)
- Public service announcements (not traffic related)

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## **Messages**

Basic principles that are important to providing proper messages and ensuring the proper operation of a CMS are:

- Visible for at least ½ mile under ideal daytime and nighttime conditions
- Legible from all lanes a minimum of 650 feet
- Entire message readable twice while traveling at the posted speed
- No more than two message panels should be used (three panels may be used on roadways where vehicles are traveling less than 45 mph). A panel is the message that fits on the face of the sign without flipping or scrolling.
- Each panel should convey a single thought; short and concise
- Do not use two unrelated panels on a sign
- Do not use the sign for two unrelated messages
- Should not scroll text horizontally or vertically
- Should not contain both the words left and right
- Use standardized abbreviations and messages
- Should be accurate and timely
- Avoid filler/unnecessary words and periods (hazardous, a, an, the)
- Avoid use of speed limits
- Use words (not numbers) for dates

## **Placement**

Placement of the CMS is important to ensure that the sign is visible to the driver and provides ample time to take any necessary action. Some of the following principles may only be applicable to controlled access roadways. The basic principles of placement for a CMS are:

- When 2 signs are needed, place on same side of roadway and at least 1,000 feet apart
- Place behind semi-rigid/rigid protection (guardrail, barrier) or outside of the clear zone
- Place 1,000 feet in advance of work zone; at least one mile ahead of decision point
- Normally place on right side of roadway; but should be placed closest to the affected lane so that either side is acceptable
- Signs should not be dual mounted (one on each side of roadway facing same direction)
- Point trailer hitch downstream
- Secure to immovable object to prevent theft (if necessary)
- Do not place in sags or just beyond crest
- Check for reflection of sun to prevent the blinding of motorist
- Should be turned ~3 degrees outward from perpendicular to the edge of pavement
- Bottom of sign should be 7 feet above the elevation of edge of roadway
- Should be removed when not in use

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# **Standard Abbreviations**

The following is a list of standard abbreviations to be used on CMS:

<b><u>Word</u></b>	<b><u>Abbrev</u></b>	<b><u>Example</u></b>
Access	ACCS	CRASH AHEAD/ USE ACCS RD NEXT RIGHT
Alternate	ALT	CRASH AHEAD/ USE ALT RTE NEXT RIGHT
Avenue	AVE	FIFTH AVE CLOSED/ DETOUR NEXT LEFT
Blocked	BLKD	FIFTH AVE BLKD/ MERGE LEFT
Boulevard	BLVD	MAIN BLVD CLOSED/ USE ALT RTE
Bridge	BRDG	SMITH BRDG CLOSED/ USE ALT RTE
Cardinal Directions	N, S, E, W	N I75 CLOSED/ DETOUR EXIT 30
Center	CNTR	CNTR LANE CLOSED/ MERGE LEFT
Commercial	COMM	OVRSZ COMM VEH/ USE I275
Condition	COND	ICY COND POSSIBLE
Congested	CONG	HVY CONG NEXT 3 MI
Construction	CONST	CONST WORK AHEAD/ EXPECT DELAYS
Downtown	DWNTN	DWNTN TRAF USE EX 40
Eastbound	E-BND	E-BND I64 CLOSED/ DETOUR EXIT 20
Emergency	EMER	EMER VEH AHEAD/ PREPARE TO STOP
Entrance, Enter	EX, EXT	DWNTN TRAF USE EX 40
Expressway	EXPWY	WTRSN EXPWY CLOSED/ DETOUR EXIT 10
Freeway	FRWY, FWY	GN SYNDR FWY CLOSED/ DETOUR EXIT 15
Hazardous Materials	HAZMAT	HAZMAT IN ROADWAY/ ALL TRAF EXIT 25
Highway	HWY	CRASH ON AA HWY/ EXPECT DELAYS
Hour	HR	CRASH ON AA HWY/ 2 HR DELAY
Information	INFO	TRAF INFO TUNE TO 1240 AM
Interstate	I	E-BND I64 CLOSED/ DETOUR EXIT 20
Lane	LN	LN CLOSED MERGE LEFT
Left	LFT	LANE CLOSED MERGE LFT
Local	LOC	LOC TRAF USE ALT RTE
Maintenance	MAINT	MAINT WRK ON BRDG/ SLOW
Major	MAJ	MAJ DELAYS I75/ USE ALT RTE
Mile	MI	CRASH 3 MI AHEAD/ USE ALT RTE
Minor	MNR	CRASH 3 MI MNR DELAY
Minutes	MIN	CRASH 3 MI/ 30 MIN DELAY
Northbound	N-BND	N-BND I75 CLOSED/ DETOUR EXIT 50
Oversized	OVRSZ	OVRSZ COMM VEH/ USE I275 NEXT RIGHT
Parking	PKING	EVENT PKING NEXT RGT
Parkway	PKWY	CUM PKWAY TRAF/ DETOUR EXIT 60
Prepare	PREP	CRASH 3 MI/ PREP TO STOP
Right	RGT	EVENT PKING NEXT RGT
Road	RD	HAZMAT IN RD/ ALL TRAF EXIT 25
Roadwork	RDWK	RDWK NEXT 4 MI/ POSSIBLE DELAYS
Route	RTE	MAJ DELAYS I75/ USE ALT RTE
Shoulder	SHLDR	SHLDR CLOSED NEXT 5 MI
Slippery	SLIP	SLIP COND POSSIBLE/ SLOW SPD
Southbound	S-BND	S-BND I75 CLOSED/ DETOUR EXIT 50
Speed	SPD	SLIP COND POSSIBLE/ SLOW SPD

Traffic Control Plan  
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**Standard Abbreviations** (cont.)

<b><u>Word</u></b>	<b><u>Abbrev</u></b>	<b><u>Example</u></b>
Street	ST	MAIN ST CLOSED/ USE ALT RTE
Traffic	TRAF	CUM PKWAY TRAF/ DETOUR EXIT 60
Vehicle	VEH	OVRSZ COMM VEH/ USE I275 NEXT RIGHT
Westbound	W-BND	W-BND I64 CLOSED/ DETOUR EXIT 50
Work	WRK	CONST WRK 2MI/ POSSIBLE DELAYS

Certain abbreviations are prone to inviting confusion because another word is abbreviated or could be abbreviated in the same way. DO NOT USE THESE ABBREVIATIONS:

<b><u>Abbrev</u></b>	<b><u>Intended Word</u></b>	<b><u>Word Erroneously Given</u></b>
ACC	Accident	Access (Road)
CLRS	Clears	Colors
DLY	Delay	Daily
FDR	Feeder	Federal
L	Left	Lane (merge)
LOC	Local	Location
LT	Light (traffic)	Left
PARK	Parking	Park
POLL	Pollution (index)	Poll
RED	Reduce	Red
STAD	Stadium	Standard
TEMP	Temporary	Temperature
WRNG	Warning	Wrong

**Typical Messages**

The following is a list of typical messages used on CMS. The list consists of the reason or problem that you want the driver to be aware of and the action that you want the driver to take.

<b><u>Reason/Problem</u></b>	<b><u>Action</u></b>
CRASH AHEAD	ALL TRAFFIC EXIT RT
CRASH/XX MILES	AVOID DELAY USE XX
XX ROAD CLOSED	CONSIDER ALT ROUTE
XX EXIT CLOSED	DETOUR
BRIDGE CLOSED	DETOUR XX MILES
BRIDGE/(SLIPPERY, ICE, ETC.)	DO NOT PASS
CENTER/LANE/CLOSED	EXPECT DELAYS
DELAY(S), MAJOR/DELAYS	FOLLOW ALT ROUTE
DEBRIS AHEAD	KEEP LEFT
DENSE FOG	KEEP RIGHT
DISABLED/VEHICLE	MERGE XX MILES
EMER/VEHICLES/ONLY	MERGE LEFT
EVENT PARKING	MERGE RIGHT
EXIT XX CLOSED	ONE-WAY TRAFFIC
FLAGGER XX MILES	PASS TO LEFT

Typical Messages (cont.)

Reason/Problem	Action
FOG XX MILES	PASS TO RIGHT
FREEWAY CLOSED	PREPARE TO STOP
FRESH OIL	REDUCE SPEED
HAZMAT SPILL	SLOW
ICE	SLOW DOWN
INCIDENT AHEAD	STAY IN LANE
LANES (NARROW, SHIFT, MERGE, ETC.)	STOP AHEAD
LEFT LANE CLOSED	STOP XX MILES
LEFT LANE NARROWS	TUNE RADIO 1610 AM
LEFT 2 LANES CLOSED	USE NN ROAD
LEFT SHOULDER CLOSED	USE CENTER LANE
LOOSE GRAVEL	USE DETOUR ROUTE
MEDIAN WORK XX MILES	USE LEFT TURN LANE
MOVING WORK ZONE, WORKERS IN ROADWAY	USE NEXT EXIT
NEXT EXIT CLOSED	USE RIGHT LANE
NO OVERSIZED LOADS	WATCH FOR FLAGGER
NO PASSING	
NO SHOULDER	
ONE LANE BRIDGE	
PEOPLE CROSSING	
RAMP CLOSED	
RAMP (SLIPPERY, ICE, ETC.)	
RIGHT LANE CLOSED	
RIGHT LANE NARROWS	
RIGHT SHOULDER CLOSED	
ROAD CLOSED	
ROAD CLOSED XX MILES	
ROAD (SLIPPERY, ICE, ETC.)	
ROAD WORK	
ROAD WORK (OR CONSTRUCTION) (TONIGHT, TODAY, TOMORROW, DATE)	
ROAD WORK XX MILES	
SHOULDER (SLIPPERY, ICE, SOFT, BLOCKED, ETC.)	
NEW SIGNAL XX MILES	
SLOW 1 (OR 2) - WAY TRAFFIC	
SOFT SHOULDER	
STALLED VEHICLES AHEAD	
TRAFFIC BACKUP	
TRAFFIC SLOWS	
TRUCK CROSSING	
TRUCKS ENTERING	
TOW TRUCK AHEAD	
UNEVEN LANES	
WATER ON ROAD	
WET PAINT	
WORK ZONE XX MILES	
WORKERS AHEAD	

## UTILITIES AND RAIL CERTIFICATION NOTE

Harlan County  
US 119 Centerline Buffer Project  
Item No. 11-9031.00

### GENERAL PROJECT NOTES ON UTILITIES

For all projects over 2000 linear feet, which are defined as a "Large Project" in KRS 367.4903(18), the awarded contractor shall initially mark the first 2000 linear feet minimally of proposed excavation or construction boundaries of the project to be worked using the procedure set forth in KRS 367.4909(9)(k). This temporary field locating of the project excavation boundary shall take place prior to submitting an excavation location request to the underground utility protection Kentucky Contact Center. For large projects, the awarded contractor shall work with the impacted utilities to determine when additional white lining of the remainder of the project site will take place. This provision shall not alter or relieve the awarded contractor from complying with requirements of KRS 367.4905 to 367.4917 in their entirety.

***NOTE: The information presented in this Utility Note is informational in nature and the information contained herein is not guaranteed.***

The contractor will be responsible for contacting all utility facility owners on the subject project to coordinate his/her activities. The contractor will coordinate his/her activities to minimize and, where possible, avoid conflicts with utility facilities. Due to the nature of the work proposed, it is unlikely to conflict with the existing utilities beyond minor facility adjustments. Where conflicts with utility facilities are unavoidable, the contractor will coordinate any necessary relocation work with the facility owner and Resident Engineer. The Kentucky Transportation Cabinet maintains the right to remove or alter portions of this contract if a utility conflict occurs. The utility facilities as noted in the previous section(s) have been determined using data garnered by varied means and with varying degrees of accuracy: from the facility owners, a result of S.U.E., field inspections, and/or reviews of record drawings. The facilities defined may not be inclusive of all utilities in the project scope and are not Level A quality, unless specified as such. It is the contractor's responsibility to verify all utilities and their respective locations before excavating.

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. The contractor is instructed to contact KY 811 for the location of existing underground utilities. Contact shall be made a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY 811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom are to be contacted through their individual Protection Notification Center. It may be necessary for the contractor to contact the County Court Clerk to determine what utility companies have facilities in the area. Non-compliance with these directives can result in the enforcement of penalties.



**UTILITIES AND RAIL CERTIFICATION NOTE**

Harlan County  
US 119 Centerline Buffer Project  
Item No. 11-9031.00

**NOTE: DO NOT DISTURB THE FOLLOWING UTILITIES LOCATED WITHIN THE PROJECT DISTURB LIMITS**

There are no known utility relocations within the project limits. If an unknown utility is encountered, the contractor will be responsible for arranging an on-site meeting with the utility owner/representative(s) and the Engineer to discuss potential impacts and solutions to either avoid the utility or relocate the utility. Depending on the solution selected, the Engineer will determine whether or not additional contract time is appropriate.

**\*The Contractor is fully responsible for protection of all utilities encountered\***

**THE FOLLOWING COMPANIES ARE RELOCATING/ADJUSTING THEIR UTILITIES WITHIN THE PROJECT LIMITS  
AND WILL BE COMPLETE PRIOR TO CONSTRUCTION**

None

**THE FOLLOWING COMPANIES HAVE FACILITIES TO BE RELOCATED/ADJUSTED BY THE COMPANY OR THE  
COMPANY’S SUBCONTRACTOR AND IS TO BE COORDINATED WITH THE ROAD CONTRACT**

None

**THE FOLLOWING COMPANIES HAVE FACILITIES TO BE RELOCATED/ADJUSTED BY THE ROAD  
CONTRACTOR AS INCLUDED IN THIS CONTRACT**

None

**RAIL COMPANIES HAVE FACILITIES IN CONJUNCTION WITH THIS PROJECT AS NOTED**

☒ No Rail Involvement    ☐ Rail Involved    ☐ Rail Adjacent





CENTERLINE BUFFER PROJECT  
HARLAN COUNTY  
US 119



**GENERAL SUMMARY**

**CENTERLINE BUFFER**

Item No.	Item	Unit	Quantity
<b>PAVEMENT</b>			
212	CL2 ASPH BASE 1.00D PG64-22	TON	3,852
307	CL2 ASPH SURF 0.38B PG64-22	TON	4,030
339	CL2 ASPH SURF 0.38D PG64-22	TON	2,181
356	ASPHALT MATERIAL FOR TACK	TON	42
<b>ROADWAY</b>			
2562	TEMPORARY SIGNS	SQFT	200
2585	EDGE KEY	LF	71
2650	MAINTAIN & CONTROL TRAFFIC	LS	1
2671	PORTABLE CHANGEABLE MESSAGE SIGN	EACH	2
2697	EDGE LINE RUMBLE STRIPS	LF	34,680
2726	STAKING	LS	1
6511	PAVE STRIPING-TEMP PAINT-6 IN	LF	106,979
6542	PAVE STRIPING-THERMO-6 IN W	LF	36,117
6543	PAVE STRIPING-THERMO-6 IN Y	LF	70,861
6569	PAVE MARKING-THERMO CROSS HATCH Y	SQFT	2,698
6574	PAVE MARKING-THERMO CURV ARROW	EACH	14
6578	PAVE MARKING-THERMO MERGE ARROW	EACH	6
21289ED	LONGITUDINAL EDGE KEY	LF	14,005
23608EC	YELLOW PAINT FOR MEDIAN SAFETY NOSE	SQFT	251
20748ED	SHOULDER MILLING/TRENCHING	SQYD	9,337
10020NS	FUEL ADJUSTMENT	DOLL	15,664
10030NS	ASPHALT ADJUSTMENT	DOLL	39,342
20458ES403	CENTERLINE RUMBLE STRIPS	LF	34,680
<b>SIGNING</b>			
6407	SBM ALUM SHEET SIGNS .125 IN	SQFT	22
6410	STEEL POST TYPE 1	LF	44
24631EC	BARCODE SIGN INVENTORY	EACH	4
21373ND	REMOVE SIGN	EACH	1
<b>MISC</b>			
2569	DEMOBILIZATION	LS	1

		CENTERLINE BUFFER PROJECT HARLAN COUNTY US 119					
SHOULDER MILL & TRENCH SUMMARY							
Side of Road	Begin		End		Length (LF)	Trench Width (LF)	Trench Area (SY)
	MilePoint	Station	MilePoint	Station			
LT	7.596	401+09	8.676	458+09	5,700	6	3801
RT	8.654	456+94	9.133	482+23	2,529	6	1686
LT	9.333	492+76	9.916	523+55	3,079	6	2053
LT	10.305	544+13	10.816	571+10	2,697	6	1799
				TOTAL	14,005	TOTAL	9,339
PAVING SUMMARY							
Item Code	Item Description	Depth (IN)	Travel Lanes Area (SY)	Shoulders Area (SY)	Full Depth Trench (SY)	Total (TON)	
307	CL2 SURF 0.38B PG64-22	1.25	58,615			4,030	
301	CL2 SURF 0.38D PG64-22	1.25		31,722		2,181	
212	CL2 BASE 1.00D P64-22	3.00			10,895	1,798	
212	CL2 BASE 1.00D P64-22	4.00			9,337	2,054	

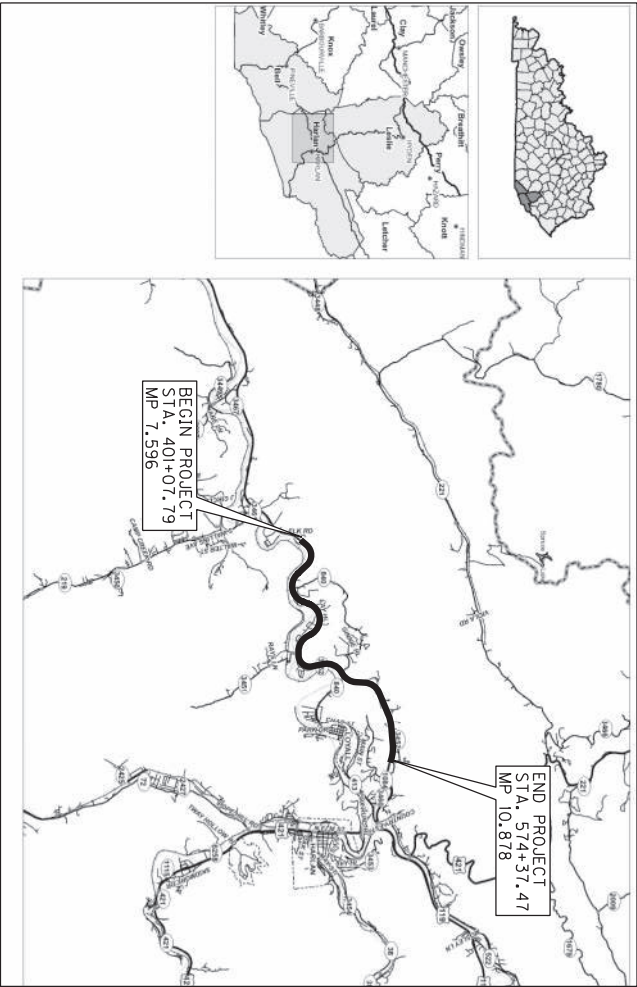
[illegible]

[illegible]

<sup>1</sup> REMOVE ALL EXISTING NO PASSING ZONE SIGNS AT THE SAME TIME AS INSTALLATION AS NEW NO PASSING ZONE SIGNS. SEE PLAN SHEETS FOR LOCATIONS.

COUNTY OF	ITEM NO.	SHEET NO.
HARLAN	-	RI

Commonwealth of Kentucky  
DEPARTMENT OF HIGHWAYS  
US 119 CENTERLINE BUFFER PROJECT  
HARLAN COUNTY  
M.P. 7.596 TO M.P. 10.878



LAYOUT MAP

DESIGN CRITERIA

CLASS OF HIGHWAY	PRINCIPLE ARTERIAL
TYPE OF TERRAIN	ROLLING
DESIGN SPEED	55 MPH
ADT PRESENT ( 2024 )	6153
GEOGRAPHIC COORDINATES	
LATITUDE	36 DEGREES 52 MINUTES 32 SECONDS NORTH
LONGITUDE	83 DEGREES 22 MINUTES 42 SECONDS WEST

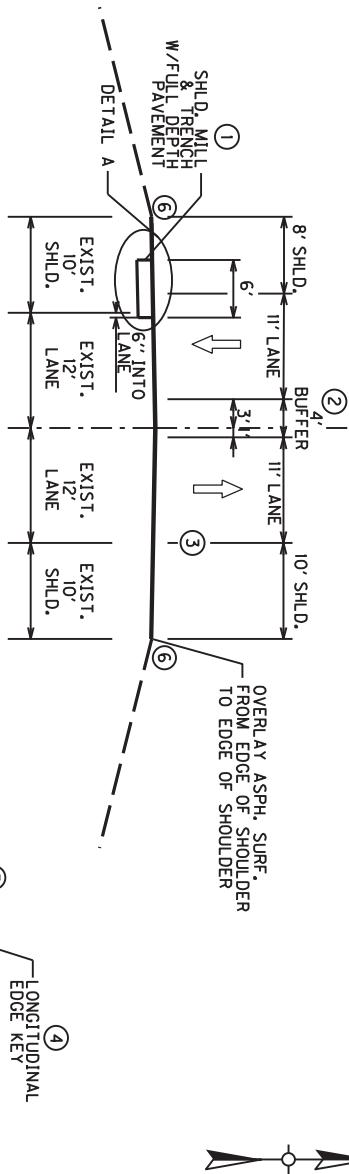
BEFORE YOU DIG



The contractor is instructed to call 1-800-752-6007 to reach KY 811, the one-call system for information on the location of existing underground utilities. The call is to be placed a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor should be aware that owners of underground facilities are not required to be members of the KY 811 one-call Before-You-Dig (BYD) service. The contractor must coordinate excavation with the utility owners, including those whom do not subscribe to KY 811. It may be necessary for the contractor to contact the County Court Clerk to determine what utility companies have facilities in the area.

# TYPICAL SECTION

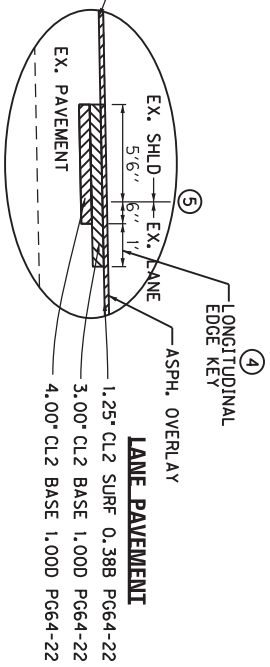
CL US 119



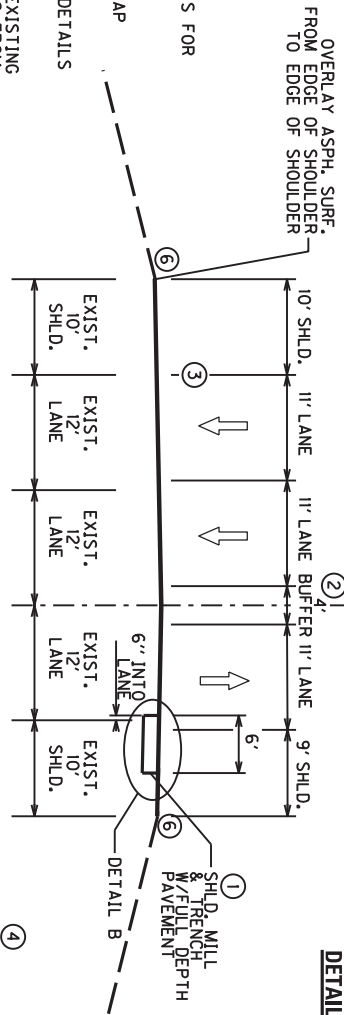
CL US 119

## SHOULDER PAVEMENT

1.25' CL2 SURF 0.380 PG64-22

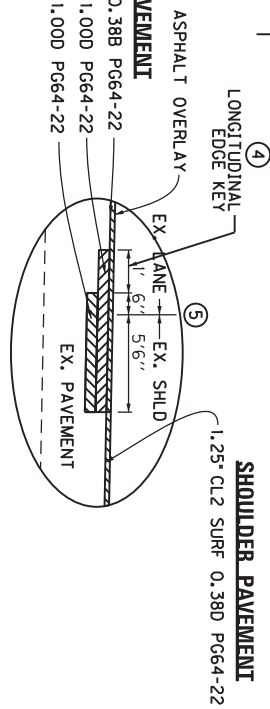


## DETAIL A



## LANE PAVEMENT

1.25' CL2 SURF 0.380 PG64-22  
3.00' CL2 BASE 1,000 PG64-22  
4.00' CL2 BASE 1,000 PG64-22



## DETAIL B

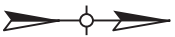
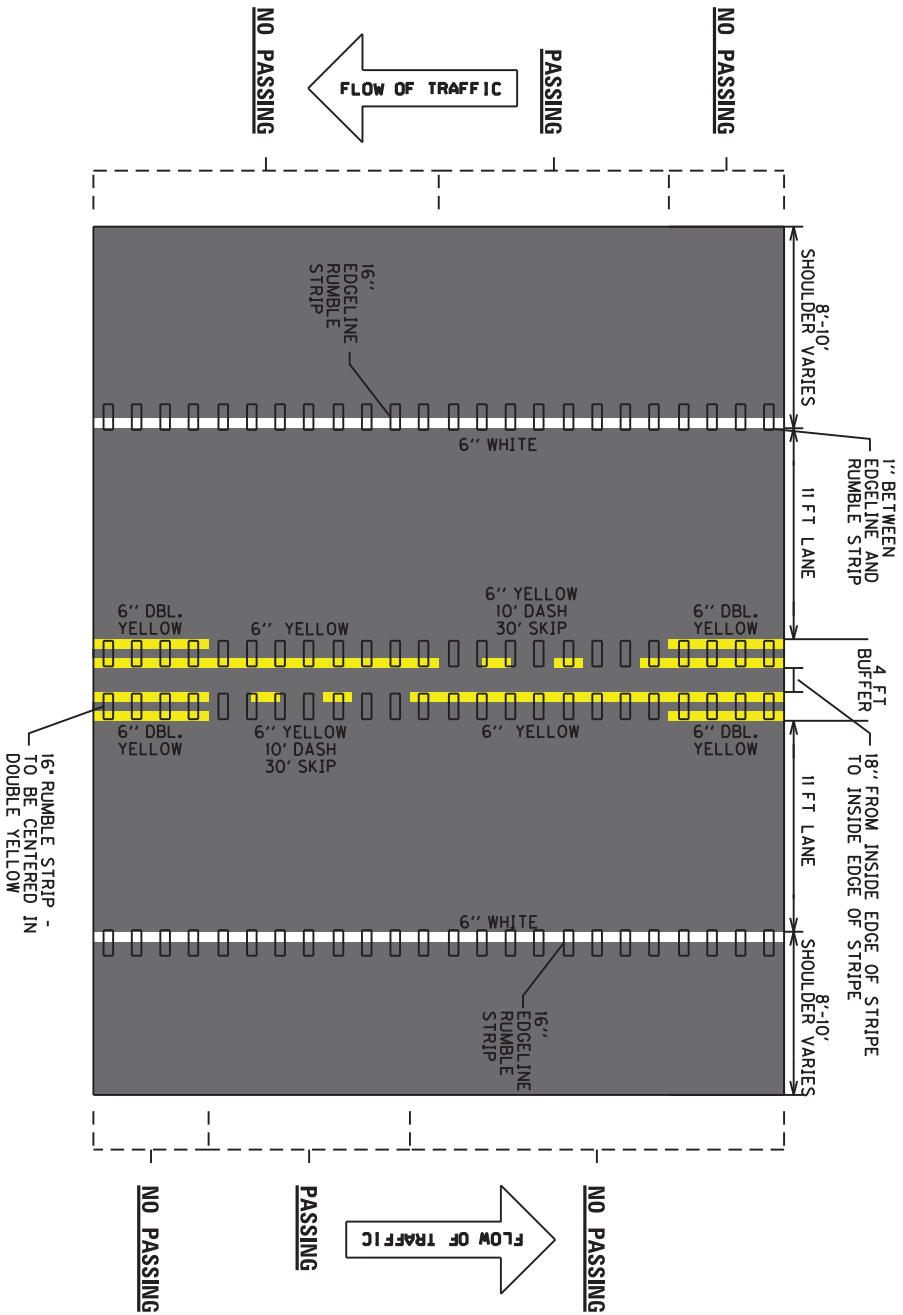
- SEE PLAN SHEET AND SUMMARY SHEETS FOR FURTHER DETAILS ON SHOULDER MILL & TRENCHING LOCATIONS. TYPICAL MAY SWITCH TO ALTERNATE SIDES OF THE ROADWAY. TRENCHING WILL OVERLAP SIDES OF THE ROAD WHEN SWITCHING.
- SEE STRIPING DETAIL SHEET R3 FOR DETAILS ON STRIPING AND RUMBLE STRIPS.
- THIS PROPOSED EDGE LINE TO MATCH EXISTING EDGE LINE. DIMENSION REMAINING LANES FROM THIS EDGE LINE TOWARDS OPPOSITE SHOULDER.
- LONGITUDINAL EDGE KEY TO BE USED WHERE FULL DEPTH IS ADJACENT TO OVERLAY AND SHALL BE PAID BY LF.
- INSIDE EDGE OF TRENCH TO BE 6" MEASURED FROM INSIDE EDGE, 6" ONTO THE SHOULDER.
- ASPHALT SURFACE THICKNESS AT THE OUTSIDE EDGE OF THE PAVED SHOULDER IS TO BE 1.0" THICK, WHERE EXISTING SITE CONDITIONS PERMIT.

COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS

DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT  
TYPICAL SECTIONS

ITEM NO.	COUNTY OF
SHEET NO.	HARLAN
R2	

# STRIPING & RUMBLE STRIP PLACEMENT DETAIL



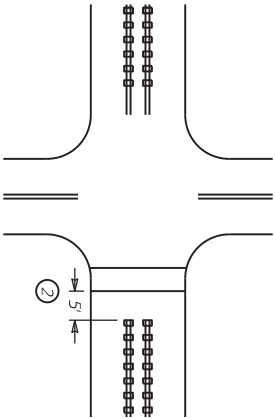
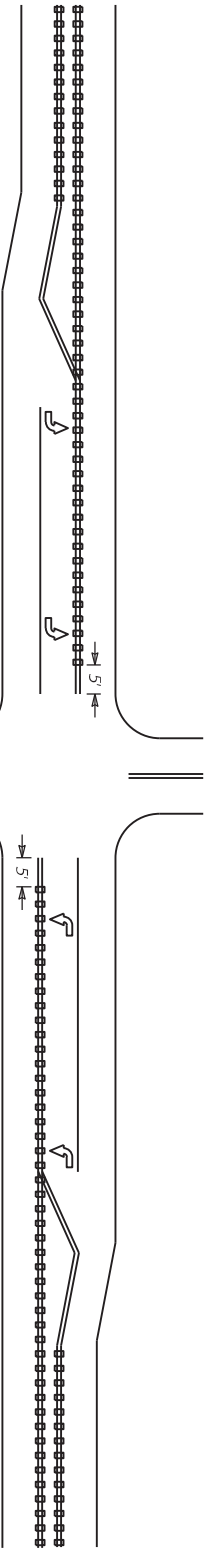
COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



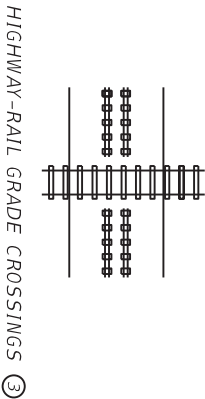
DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT  
STRIPING DETAIL

ITEM NO.	COUNTY OF
-	HARLAN
SHEET NO.	
R3	



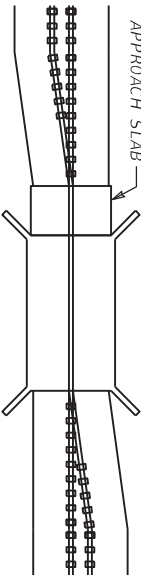


INTERSECTIONS WITH LEFT-TURN LANES ①

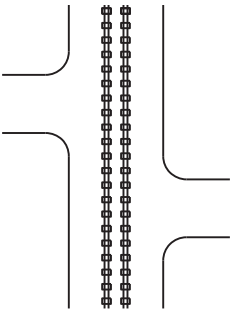


HIGHWAY-RAIL GRADE CROSSINGS ③

INTERSECTIONS WITHOUT LEFT-TURN LANES ①



BRIDGE DECK/APPROACH SLAB ④



DRIVEWAYS/MINOR COMMERCIAL ENTRANCES ⑤

~ NOTES ~

- ① CENTERLINE RUMBLE STRIPS SHALL BE OMITTED THROUGH MAJOR INTERSECTIONS WITH, OR WITHOUT, LEFT-TURN LANES. OMIT THE CENTERLINE RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF THE AREA WHERE THE CENTERLINE PAVEMENT MARKINGS HAVE BEEN OMITTED (NORMALLY WHERE SIDE STREET RADIUS INTERSECTS MAINLINE).
- ② CENTERLINE RUMBLE STRIPS SHALL NOT BE INSTALLED THROUGH MARKED CROSSWALKS. OMIT THE CENTERLINE RUMBLE STRIPS APPROXIMATELY 5' IN ADVANCE OF MARKED CROSSWALKS.
- ③ CENTERLINE RUMBLE STRIPS SHALL NOT BE INSTALLED ACROSS HIGHWAY-RAIL GRADE CROSSINGS.
- ④ CENTERLINE RUMBLE STRIPS SHALL NOT BE INSTALLED ON BRIDGE DECKS OR APPROACH SLABS.
- ⑤ CENTERLINE RUMBLE STRIPS SHALL BE INSTALLED THROUGH DRIVEWAYS & MINOR COMMERCIAL ENTRANCES.

DRAWING NOT TO SCALE

USE WITH CUR. STD. DWGS.  
TPR-105, TPR-110, TPR-120,  
AND TPR-125

BID ITEM AND UNIT TO BID  
CENTERLINE RUMBLE STRIPS

LF

COORDINATE SYSTEM

Coordinates are based on State Plane Coordinate System Single Zone and in U.S. Survey Feet.

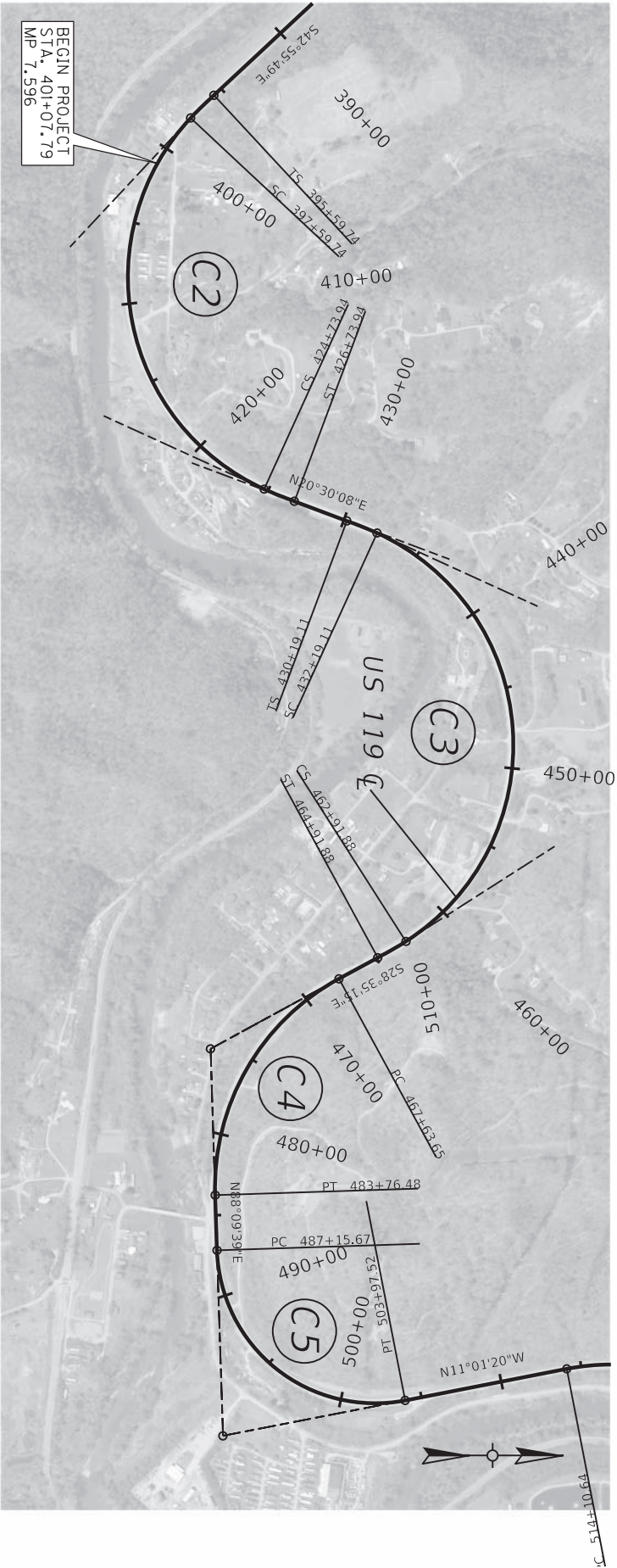
STATION  
BEGIN CONSTRUCTION: 401+07.79  
END CONSTRUCTION: 574+37.47  
MILEPOINT  
7.596  
10.878

GEOMETRIC CONTROL POINTS - US 119

Point Type	Station	Northing	Easting
START	359+35.72	3475150.856	5605848.594
PC	366+23.65	3475667.845	5606302.426
HPI	379+00.49	3476627.417	5607144.773
PT	385+52.80	3475692.534	5608014.439
TS	395+59.74	3474955.271	5608700.272
SC	397+59.74	3474812.075	5608839.834
HPI	417+51.95	3473451.619	5610295.187
CS	424+73.94	3475264.421	5611121.41
TS	430+19.11	3475773.34	5611316.675
HPI	458+52.35	3478355.056	5612483.123
CS	462+91.88	3476136.368	5613901.351
PC	467+63.65	3475724.426	5614131.131

GEOMETRIC CONTROL POINTS - US 119

Point Type	Station	Northing	Easting
HPI	476+63.35	3474934.413	5614561.657
PT	483+76.48	3474963.29	5615460.872
PC	487+15.67	3474974.177	5615799.89
HPI	498+56.91	3475010.807	5616940.544
PT	503+97.52	3476130.997	5616722.353
PC	514+10.64	3477125.423	5616528.659



BEGIN PROJECT  
STA. 401+07.79  
MP 7.596

COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS

DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT  
COORDINATE CONTROL 1 OF 2



ITEM NO. COUNTY OF  
SHEET NO. HARLAN  
R4

COORDINATE SYSTEM

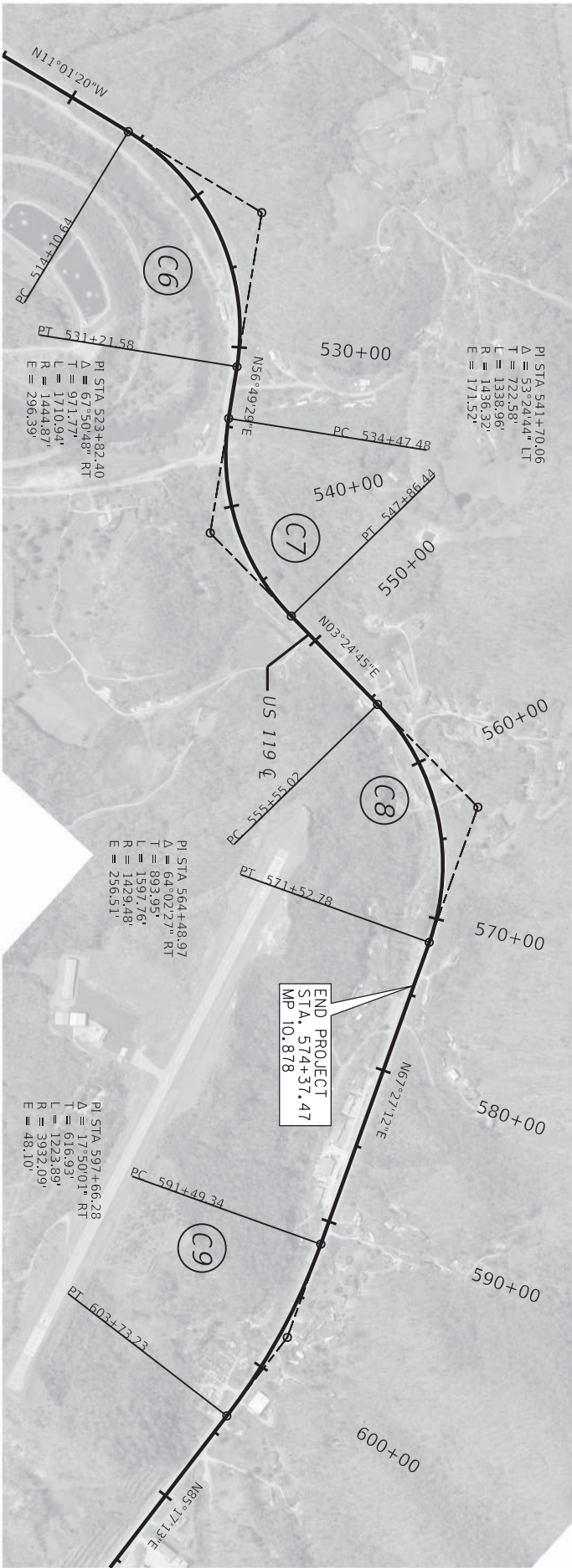
Coordinates are based on State Plane Coordinate System Single Zone and in U.S. Survey Feet.

GEOMETRIC CONTROL POINTS - US 119

Point Type	Station	Northing	Easting
HPI	523+82.40	3478079.265	5616342.869
PT	531+21.58	3478611.019	5617156.238
PC	534+47.48	3478789.354	5617429.02
HPI	541+70.06	3479184.754	5618033.822
PT	547+86.44	3479906.057	5618076.832
PC	555+55.02	3480673.271	5618122.58
HPI	564+48.97	3481565.633	5618175.791
PT	571+52.78	3481908.407	5619001.41
PC	591+49.34	3482673.963	5620845.367
HPI	597+66.28	3482910.519	5621415.144
PT	603+73.23	3482961.21	5622029.99
PC	617+45.30	3483073.949	5623397.424
HPI	624+36.46	3483130.74	5624086.248
PT	631+12.80	3482942.317	5624751.229
END	633+67.81	3482872.797	5624996.578

CURVE DATA

NO.	RADIUS	DELTA	LENGTH	TANGENT	ALIGNMENT
C6	1444.869	67°50'48.268"	1710.941	971.768	US 119 CL
C7	1436.316	53°24'44.036"	1338.961	722.584	US 119 CL
C8	1429.482	64°02'26.801"	1597.764	893.947	US 119 CL
C9	3932.088	17°50'01.192"	1223.887	616.932	US 119 CL



COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT  
COORDINATE CONTROL SHEET 2 OF 2



ITEM NO. COUNTY OF  
SHEET NO. HARLAN  
R4A



COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT  
COORDINATE CONTROL SHEET 2 OF 2

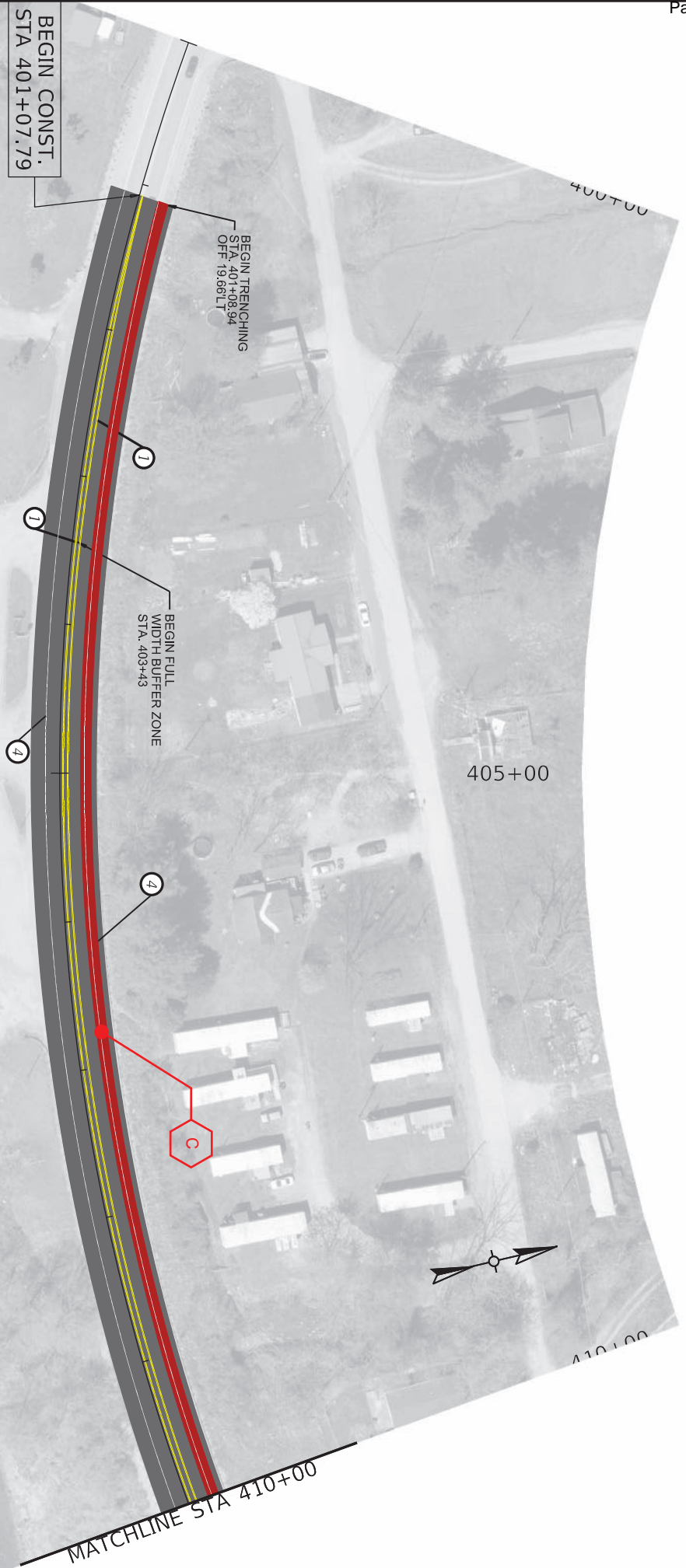


ITEM NO. COUNTY OF  
SHEET NO. HARLAN  
R4A



- PAVEMENT MARKING LEGEND**
- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
  - ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
  - ③ PAVE STRIPING - THERMO 6 INCH YELLOW
  - ④ PAVE STRIPING - THERMO 6 INCH WHITE
  - ⑤ PAVE MARKING - THERMO CURV ARROW
  - ⑥ PAVE MARKING - THERMO MERGE ARROW
  - ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE

PAVEMENT DIMENSIONS		
<b>A</b>	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	9 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH
<b>B</b>	11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>C</b>	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	
ASPHALT LEGEND		
ASPHALT OVERLAY		
SHOULDER MILLING/TRENCHING		





COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



TEAM  
KENTUCKY  
TRANSPORTATION  
CABINET

DRAWING TITLE: US 119

## CENTERLINE BUFFER PROJECT

STA. 410+00.00 TO STA. 420+00.00



HORIZONTAL SCALE: 1"=100

ITEM NO.	COUNTY OF
-	HARLAN
SHEET NO.	
R6	

COUNTY OF HARLAN

### PAVEMENT MARKING LEGEND

- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
- ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
- ③ PAVE STRIPING - THERMO 6 INCH YELLOW
- ④ PAVE STRIPING - THERMO 6 INCH WHITE
- ⑤ PAVE MARKING - THERMO CURV ARROW
- ⑥ PAVE MARKING - THERMO MERGE ARROW
- ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE

**PAVEMENT DIMENSIONS**

**Diagram A:**

- 8 FT SH
- 11 FT LANE
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE
- 9 FT SH

**Diagram B:**

- 9 FT SH
- 11 FT LANE
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE
- 8 FT SH

**Diagram C:**

- 8 FT SH
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE
- 9 FT SH


**Diagram D:**


- 9 FT SH
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE
- 8 FT SH


**Diagram E:**

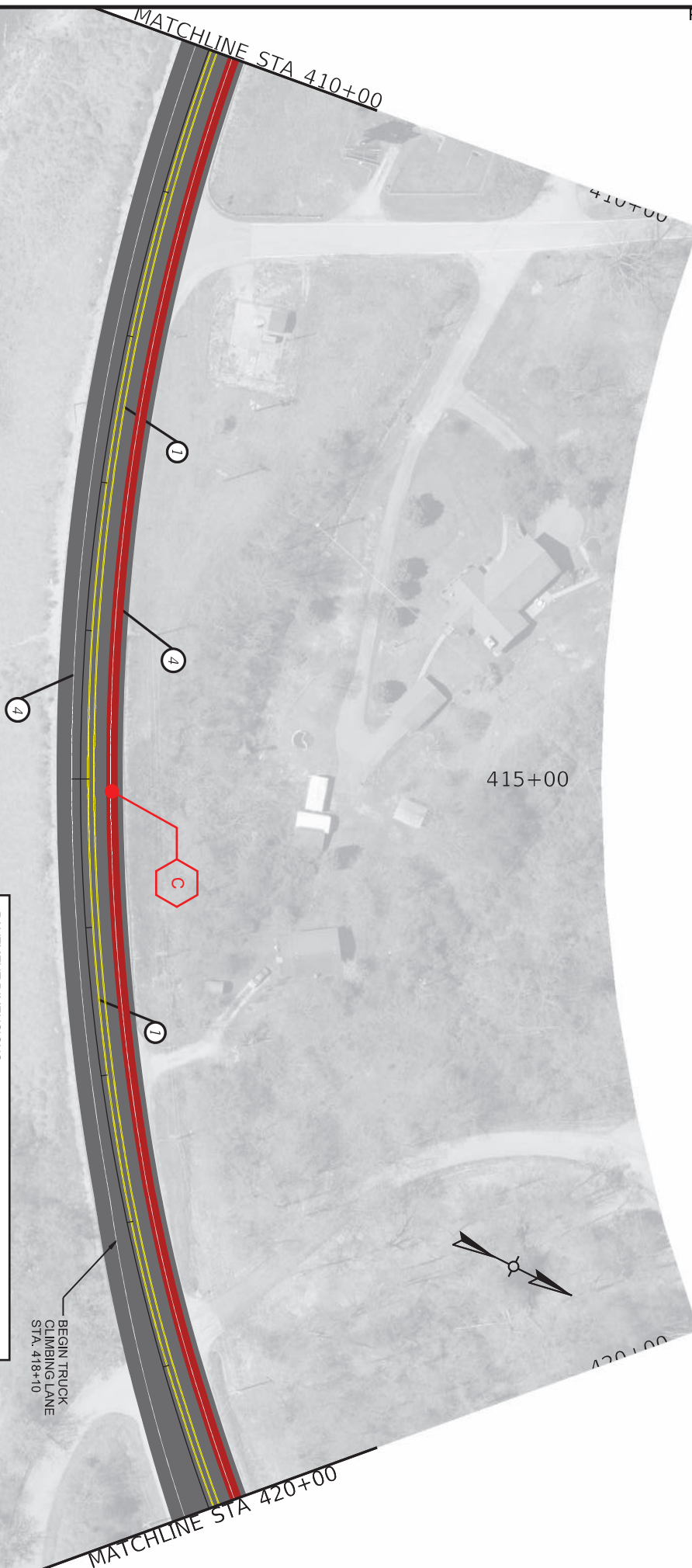
- 8 FT SH
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE
- 11 FT LANE
- 9 FT SH

**ASPHALT LEGEND**

ASPHALT OVERLAY 

SHOULDER DITCH/TRENCHING 

SHOULDER 







COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



TEAM  
KENTUCKY  
TRANSFORMATION  
COUNCIL

DRAWING TITLE: US 119

85+45  
CENTERLINE BUFFER PROJECT  
STA. 420+00.00 TO STA. 430+00.00



ITEM NO.	COUNTY OF
-	HARLAN
SHEET NO.	
R7	

**PAVEMENT MARKING - LEGEND**

- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
- ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
- ③ PAVE STRIPING - THERMO 6 INCH YELLOW
- ④ PAVE STRIPING - THERMO 6 INCH WHITE
- ⑤ PAVE MARKING - THERMO CURV ARROW
- ⑥ PAVE MARKING - THERMO MERGE ARROW
- ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE
- ⑧ PAVE MARKING - THERMO 6 INCH WHITE - 10 FT LENGTH, 30 FT GAPS

**PAVEMENT DIMENSIONS**

**Cross-section A:**

- 9 FT SH
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE
- 9 FT SH

**Cross-section B:**

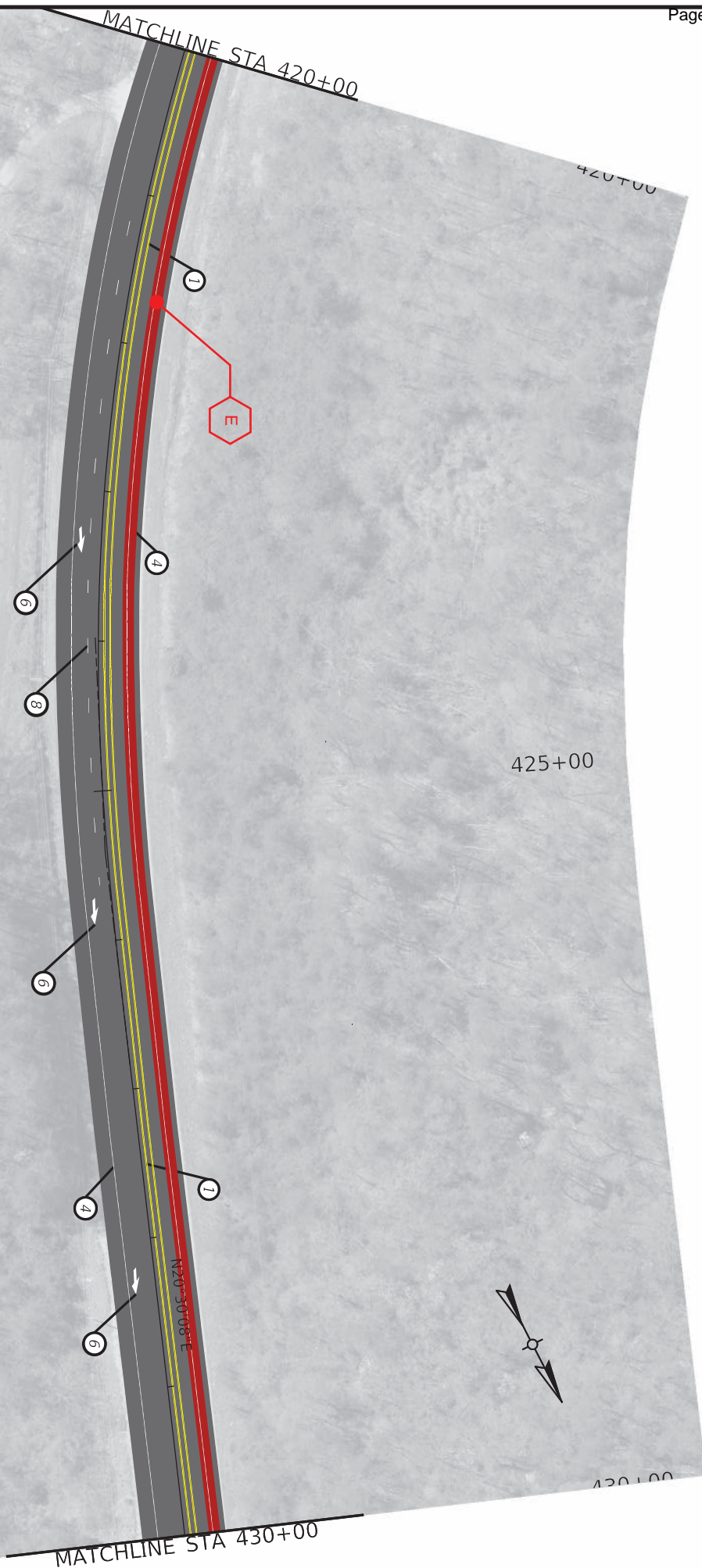
- 9 FT SH
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE
- 8 FT SH

**Cross-section C:**

- 9 FT SH
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE
- 9 FT SH

**ASPHALT LEGEND**

- ASPHALT OVERLAY
- SHOULDER MILLING/TRENCHING





COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT

STA. 430+00.00 TO STA. 440+00.00



ITEM NO.	COUNTY OF
SHEET NO.	HARLAN
R8	

PAVEMENT DIMENSIONS		
<b>A</b>	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	9 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH
<b>B</b>	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>C</b>	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>D</b>	9 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>E</b>	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
ASPHALT LEGEND		
ASPHALT OVERLAY		
SHOULDER MILLING/TRENCHING		

PAVEMENT MARKING LEGEND	
①	PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
②	PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
③	PAVE STRIPING - THERMO 6 INCH YELLOW
④	PAVE STRIPING - THERMO 6 INCH WHITE
⑤	PAVE MARKING - THERMO CURV ARROW
⑥	PAVE MARKING - THERMO MERGE ARROW
⑦	PAVE MARKING - THERMO YELLOW NOSE CONE

MATCHLINE STA 430+00

END TRUCK  
CLIMBING LANE  
STA. 433+18

435+00

MATCHLINE STA 440+00







COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



TEAM  
KENTUCKY  
TRANSPORTATION  
COUNCIL

DRAWING TITLE: US 119  
CENTR

88+45  
CENTERLINE BUFFER PROJECT  
STA. 440+00.00 TO STA. 450+00.00



HORIZONTAL SCALE: 1"=100'

ITEM NO.	COUNTY OF
-	HARLAN

SHEET NO. R9

COUNTY OF  
HARLAN

MATCHLINE STA 440+00

445+00

MATCHLINE STA 450+00



### PAVEMENT MARKING LEGEND

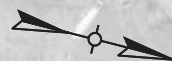
- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
- ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
- ③ PAVE STRIPING - THERMO 6 INCH YELLOW
- ④ PAVE STRIPING - THERMO 6 INCH WHITE
- ⑤ PAVE MARKING - THERMO CURV ARROW
- ⑥ PAVE MARKING - THERMO MERGE ARROW
- ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE

## PAVEMENT DIMENSIONS

- 
- Diagram illustrating the cross-section of a three-lane highway with a median and two side paths. The diagram shows the following lane widths and shoulder widths:
- Left Side (Lane A):** 8 FT SH, 11 FT LANE, 11 FT LANE, 4 FT BUFFER, 11 FT LANE, 9 FT SH.
  - Middle (Lane B):** 9 FT SH, 11 FT LANE, 11 FT LANE, 4 FT BUFFER, 11 FT LANE, 8 FT SH.
  - Right Side (Lane C):** 8 FT SH, 11 FT LANE, 4 FT BUFFER, 11 FT LANE, 9 FT SH.

## ASPHALT LEGEND

- ASPHALT LEGEND**
- |   |                            |
|---|----------------------------|
|  | ASPHALT OVERLAY            |
|  | SHOULDER MILLING/TRENCHING |





MATCHLINE STA 450+00

455+00

MATCHLINE STA 460+00

- PAVEMENT MARKING LEGEND**
- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
  - ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
  - ③ PAVE STRIPING - THERMO 6 INCH YELLOW
  - ④ PAVE STRIPING - THERMO 6 INCH WHITE
  - ⑤ PAVE MARKING - THERMO CURV ARROW
  - ⑥ PAVE MARKING - THERMO MERGE ARROW
  - ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE

PAVEMENT DIMENSIONS		
<div>A</div>	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	9 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH
<div>B</div>	11 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<div>C</div>	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<div>D</div>	9 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<div>E</div>	11 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH

**ASPHALT LEGEND**

ASPHALT OVERLAY

SHOULDER MILLING/TRENCHING

BEGIN TRENCHING RT  
STA. 458+34.48  
OFF 24.16 RT

END TRENCHING  
STA. 458+09.00  
OFF 11.70 LT

END TRUCK  
CLIMBING LANE  
STA. 458+09



048 0119 007-011



COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS

DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT  
STA. 450+00.00 TO STA. 460+00.00

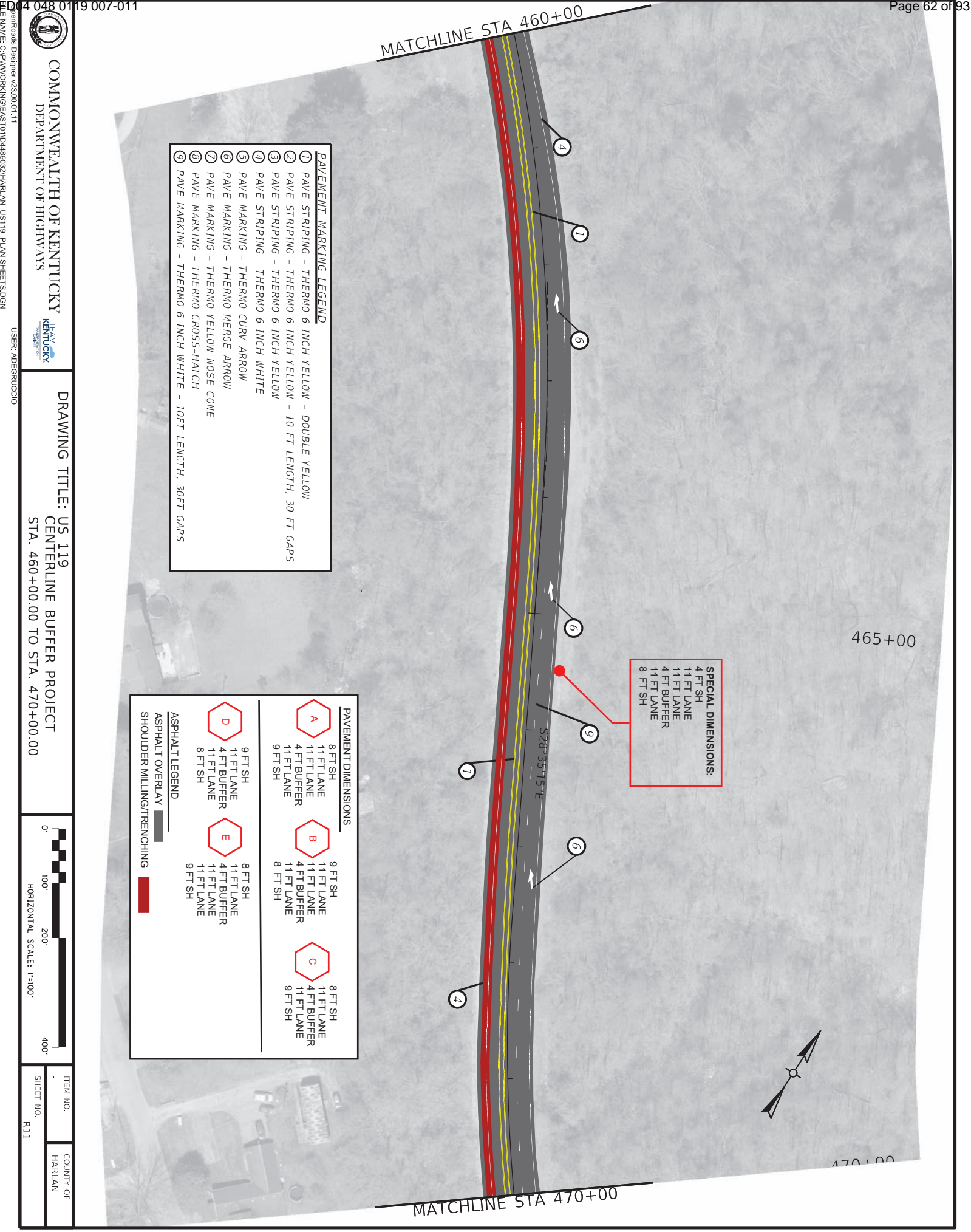


ITEM NO.	COUNTY OF
SHEET NO.	HARLAN
R10	

- PAVEMENT MARKING LEGEND**
- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
  - ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
  - ③ PAVE STRIPING - THERMO 6 INCH YELLOW
  - ④ PAVE STRIPING - THERMO 6 INCH WHITE
  - ⑤ PAVE MARKING - THERMO CURV ARROW
  - ⑥ PAVE MARKING - THERMO MERGE ARROW
  - ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE
  - ⑧ PAVE MARKING - THERMO CROSS-HATCH
  - ⑨ PAVE MARKING - THERMO 6 INCH WHITE - 10FT LENGTH, 30FT GAPS

- PAVEMENT DIMENSIONS**
- |          |   |          |   |          |   |
|----------|---|----------|---|----------|---|
| <b>A</b> | 8 FT SH<br>11 FT LANE<br>11 FT LANE<br>4 FT BUFFER<br>11 FT LANE<br>9 FT SH | <b>B</b> | 9 FT SH<br>11 FT LANE<br>11 FT LANE<br>4 FT BUFFER<br>11 FT LANE<br>8 FT SH | <b>C</b> | 8 FT SH<br>11 FT LANE<br>4 FT BUFFER<br>11 FT LANE<br>9 FT SH |
| <b>D</b> | 9 FT SH<br>11 FT LANE<br>4 FT BUFFER<br>11 FT LANE<br>8 FT SH               | <b>E</b> | 8 FT SH<br>11 FT LANE<br>4 FT BUFFER<br>11 FT LANE<br>11 FT LANE<br>9 FT SH |          |   |
- ASPHALT LEGEND**
- ASPHALT OVERLAY
- SHOULDER MILLING/TRENCHING

- SPECIAL DIMENSIONS:**
- 4 FT SH
  - 11 FT LANE
  - 11 FT LANE
  - 4 FT BUFFER
  - 11 FT LANE
  - 8 FT SH







COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



TEAM  
KENTUCKY.  
TRANSPORTATION  
CABINET

USER: ADEGRUCCIO

DRAWING TITLE: US 119

83+00  
CENTERLINE BUFFER PROJECT  
STA. 470+00.00 TO STA. 480+00.00



MATCHLINE STA 470+00

475+00

MATCHLINE STA 480+00

- PAVEMENT MARKING LEGEND**
- ① PAVE STRIPING – THERMO 6 INCH YELLOW – DOUBLE YELLOW
  - ② PAVE STRIPING – THERMO 6 INCH YELLOW – 10 FT LENGTH, 30 FT GAPS
  - ③ PAVE STRIPING – THERMO 6 INCH YELLOW
  - ④ PAVE STRIPING – THERMO 6 INCH WHITE
  - ⑤ PAVE MARKING – THERMO CURV ARROW
  - ⑥ PAVE MARKING – THERMO MERGE ARROW
  - ⑦ PAVE MARKING – THERMO YELLOW NOSE CONE
  - ⑧ PAVE MARKING – THERMO CROSS-HATCH
  - ⑨ PAVE MARKING – THERMO 6 INCH WHITE – 10FT LENGTH, 30FT GAPS





## SPECIAL DIMENSIONS:

- 4 FT SH  
11 FT LANE  
11 FT LANE  
4 FT BUFFER  
11 FT LANE  
8 FT SH

## PAVEMENT DIMENSIONS

- 
- The diagram shows a three-lane highway with three interchanges labeled A, B, and C. The lanes are labeled as follows:
- Interchange A (Left Side):**
    - Approaching from the left: 8 FT SH, 11 FT LANE, 11 FT LANE, 4 FT BUFFER, 11 FT LANE, 9 FT SH
  - Interchange B (Middle):**
    - Approaching from the left: 9 FT SH, 11 FT LANE, 11 FT LANE, 4 FT BUFFER, 11 FT LANE, 8 FT SH
  - Interchange C (Right Side):**
    - Approaching from the right: 8 FT SH, 11 FT LANE, 4 FT BUFFER, 11 FT LANE, 9 FT SH

### ASPHALT LEGEND

- ASPHALT LEGEND**
- |   |   |
|---|---|
|  | 9 FT SH<br>11 FT LANE<br>4 FT BUFFER<br>11 FT LANE<br>8 FT SH               |
|  | 8 FT SH<br>11 FT LANE<br>4 FT BUFFER<br>11 FT LANE<br>11 FT LANE<br>9 FT SH |
- ASPHALT OVERLAY** 
- SHOULDER MILLING/TRENCHING** 

## SHOULDER MILLING/TRENCHING



HORIZONTAL SCALE: 1"=100'

ITEM NO.

COUNTY OF  
HARLAN

SHEET NO.

R12



COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS

DEPARTMENT OF HIGHWAYS

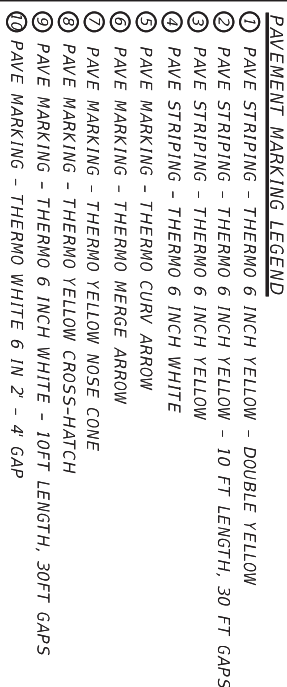


TEAM  
KENTUCKY

DRAWING TITLE: US 119

## CENTERLINE BUFFER PROJECT

STA. 480+00.00 TO STA. 490+00.00



**PAVEMENT DIMENSIONS**

**SECTION A**

- 9 FT SH
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE

**SECTION B**

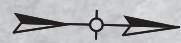
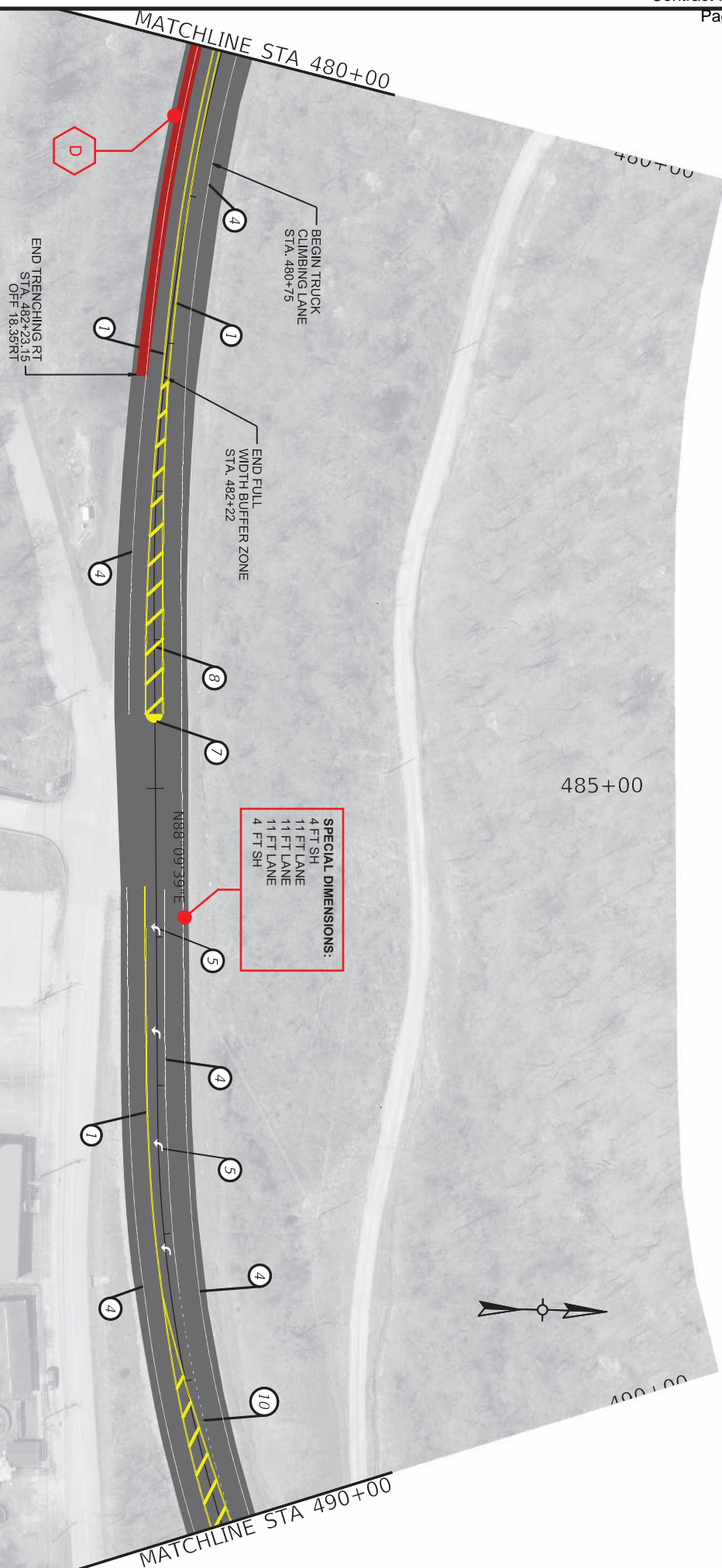
- 9 FT SH
- 11 FT LANE
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE

**SECTION C**

- 9 FT SH
- 11 FT LANE
- 4 FT BUFFER
- 11 FT LANE

**ASPHALT LEGEND**

- ASPHALT OVERLAY
- SHOULDER MILLING/TRENCHING



HORIZONTAL SCALE: 1"=100'

ITEM NO.

COUNTY OF  
HARLAN

SHEET NO.

R13

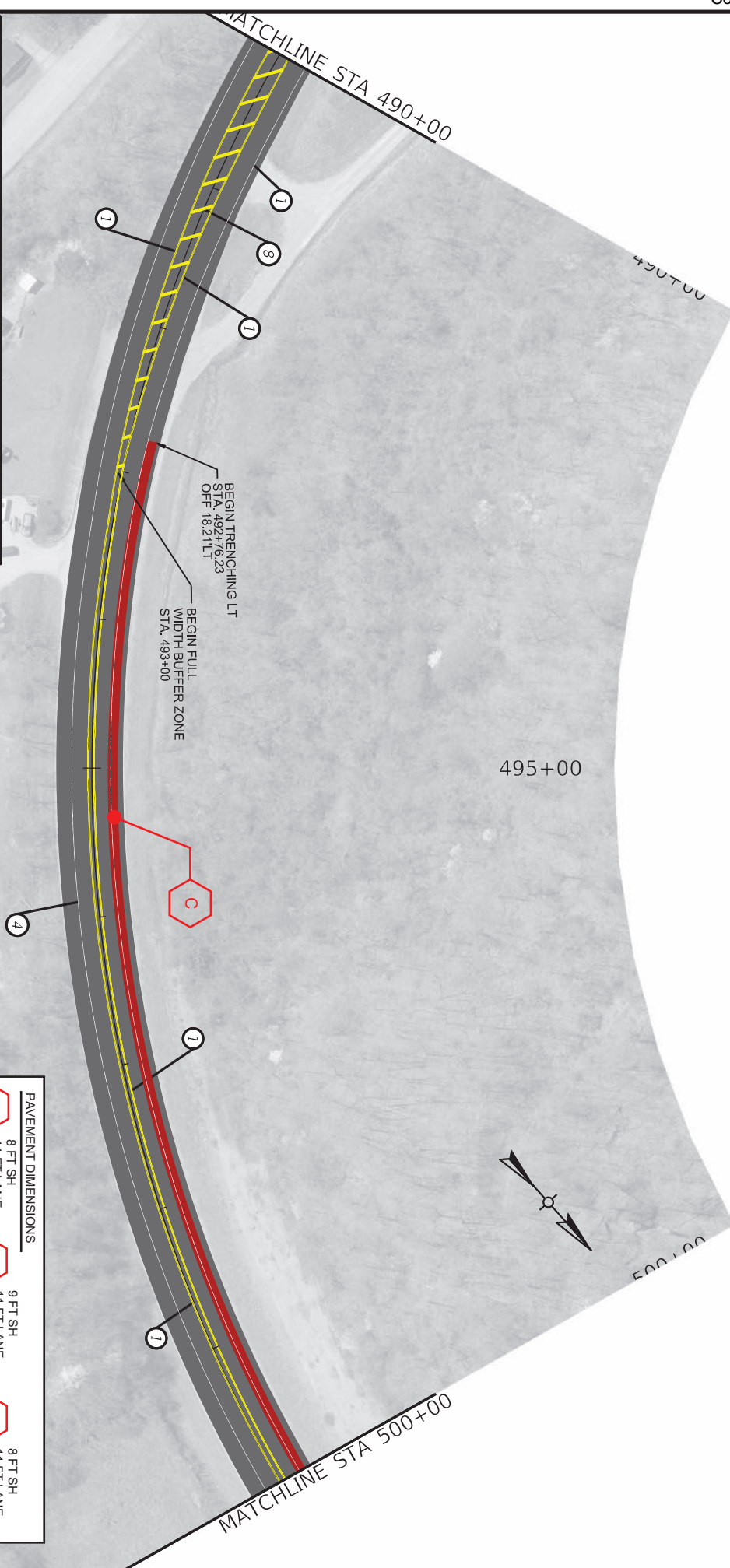




TEAM  
KENTUCKY  
TRANSPORTATION  
CABINET

DRAWING TITLE: US 119

CENTERLINE BUFFER PROJECT  
STA. 490+00.00 TO STA. 500+00.01



**PAVEMENT DIMENSIONS**

<p><b>A</b></p> <p>8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 9 FT SH</p>	<p><b>B</b></p> <p>9 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 8 FT SH</p>	<p><b>C</b></p> <p>8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH</p>	<p><b>D</b></p> <p>9 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH</p>
--	--	--	--

**ASPHALT LEGEND**

ASPHALT OVERLAY

SHOULDER MILLING/TRENCHING



ITEM NO.	COUNTY OF
-	HARLAN
SHEET NO.	
R14	



COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



TEAM  
KENTUCKY  
TRANSPORTATION  
COUNCIL

USER: ADEGRUCCIO

DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT  
STA. 500+00.01 TO STA. 510+00.01



ITEM NO.	COUNTY OF
-	HARLAN
SHEET NO.	
R15	

### PAVEMENT MARKING LEGEND

- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
- ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
- ③ PAVE STRIPING - THERMO 6 INCH YELLOW
- ④ PAVE STRIPING - THERMO 6 INCH WHITE
- ⑤ PAVE MARKING - THERMO CURV ARROW
- ⑥ PAVE MARKING - THERMO MERGE ARROW
- ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE
- ⑧ PAVE MARKING - THERMO YELLOW CROSS-HATCH
- ⑨ PAVE MARKING - THERMO 6 INCH WHITE - 10FT LENGTH, 30FT GAPS
- ⑩ PAVE MARKING - THERMO WHITE 6 IN 2' - 4' GAP

## PAVEMENT DIMENSIONS

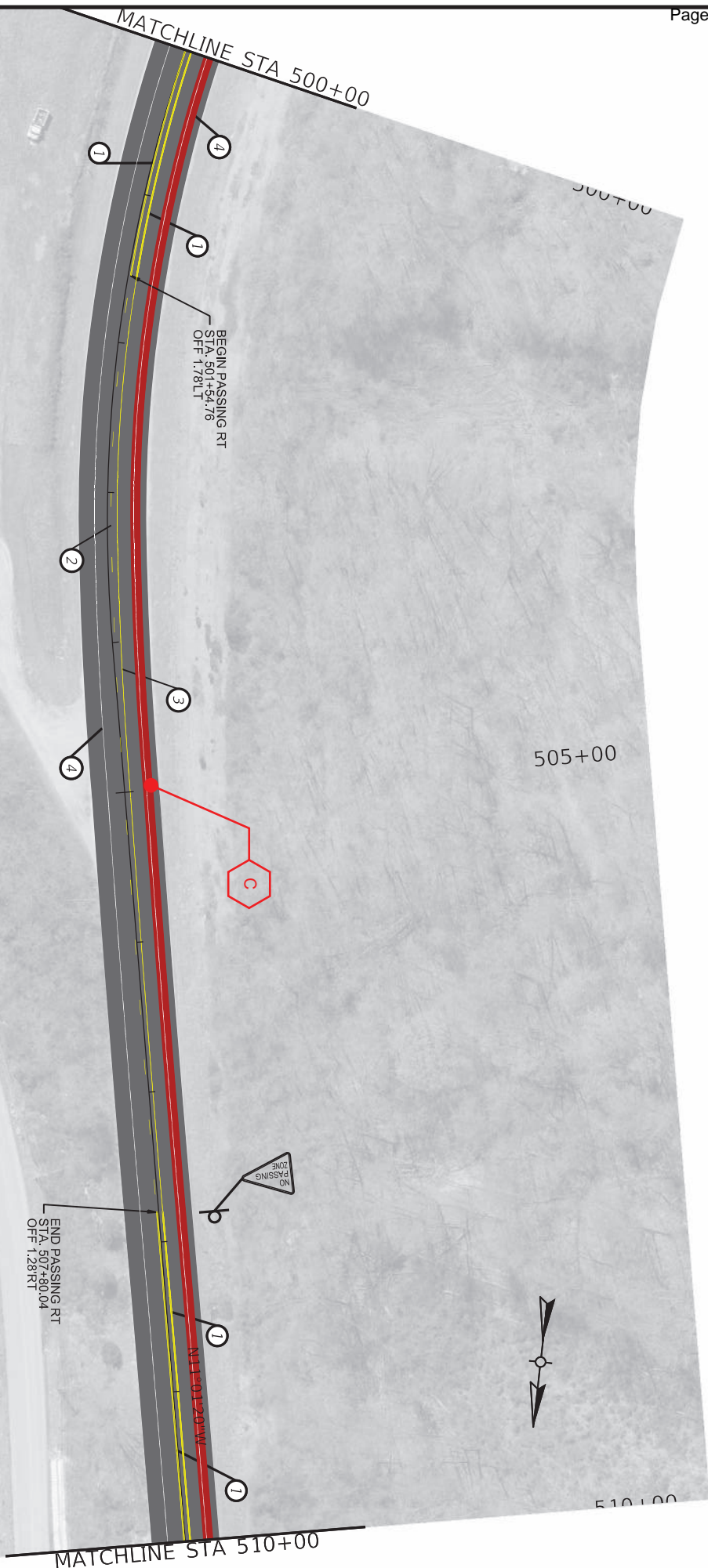
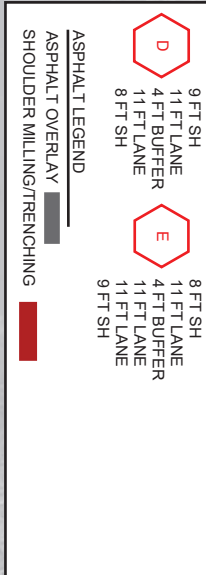
Diagram illustrating the lane widths and shoulder widths for a three-lane road with a center turn lane:

- Lane A (Left):** 9 FT SH, 11 FT LANE, 4 FT BUFFER, 11 FT LANE, 9 FT SH
- Lane B (Center):** 9 FT SH, 11 FT LANE, 4 FT BUFFER, 4 FT LANE, 8 FT SH
- Lane C (Right):** 8 FT SH, 11 FT LANE, 4 FT BUFFER, 11 FT LANE, 9 FT SH

### ASPHALT LEGEND

## ASPHALT OVERLAY

## SHOULDER MILLING/TRENCHING







COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



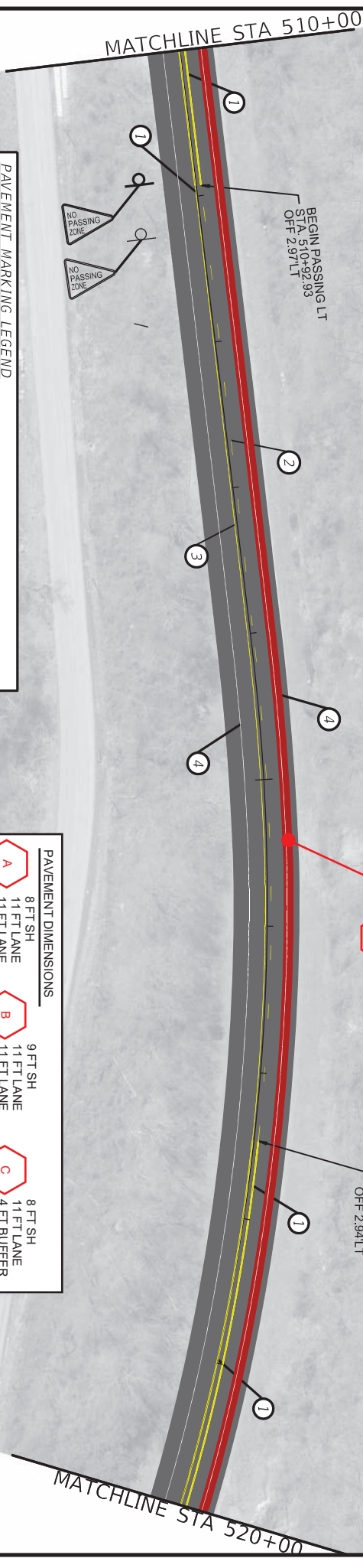
DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT  
STA. 510+00.01 TO STA. 520+00.01



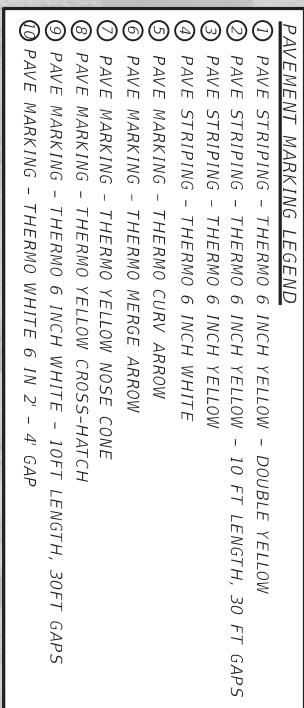
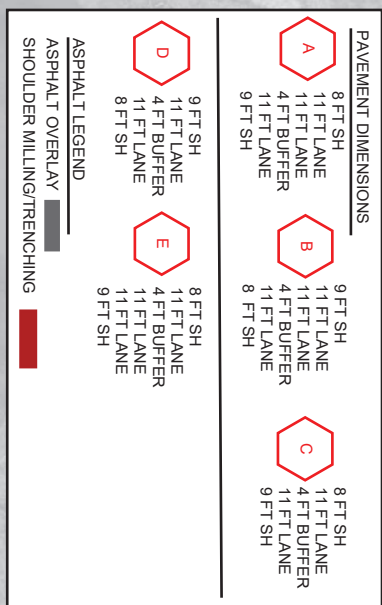
ITEM NO.	COUNTY OF
SHEET NO.	HARLAN
R16	

- PAVEMENT MARKING LEGEND**
- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
  - ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
  - ③ PAVE STRIPING - THERMO 6 INCH YELLOW
  - ④ PAVE STRIPING - THERMO 6 INCH WHITE
  - ⑤ PAVE MARKING - THERMO CURV ARROW
  - ⑥ PAVE MARKING - THERMO MERGE ARROW
  - ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE
  - ⑧ PAVE MARKING - THERMO YELLOW CROSS-HATCH
  - ⑨ PAVE MARKING - THERMO 6 INCH WHITE - 10FT LENGTH, 30FT GAPS
  - ⑩ PAVE MARKING - THERMO WHITE 6 IN 2' - 4' GAP

PAVEMENT DIMENSIONS		
<b>ASPHALT LEGEND</b>		
ASPHALT OVERLAY		
SHOULDER MILLING/TRENCHING		



MATCHLINE STA 530+00



END TRENCHING LT  
STA. 523+55.08  
OFF 20.61' LT

**SPECIAL  
DIMENSIONS:**  
4 FT SH  
11 FT LANE  
11 FT LANE  
11 FT LANE  
4 FT SH

COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS

DRAWING TITLE: US 119

CENTERLINE BUFFER PROJECT  
STA. 520+00.01 TO STA. 530+00.01

HORIZONTAL SCALE: 1"=100'

ITEM NO.	COUNTY OF
-	HARLAN
SHEET NO.	
R17	

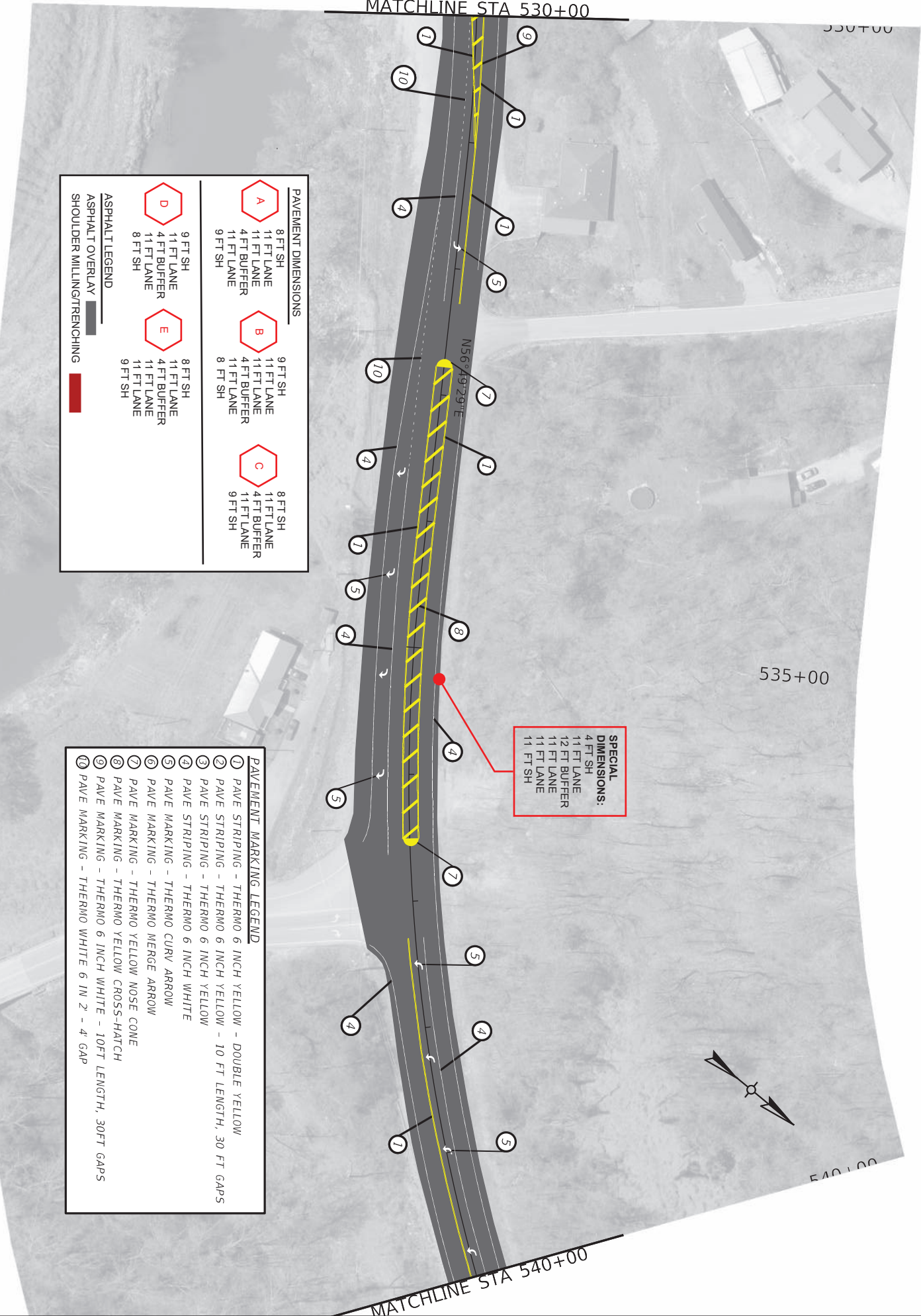
R17



PAVEMENT DIMENSIONS		
<b>A</b>	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	9 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH
<b>B</b>	11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>C</b>	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>D</b>	9 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>E</b>	11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH

PAVEMENT MARKING LEGEND	
①	PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
②	PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
③	PAVE STRIPING - THERMO 6 INCH YELLOW
④	PAVE STRIPING - THERMO 6 INCH WHITE
⑤	PAVE MARKING - THERMO CURV ARROW
⑥	PAVE MARKING - THERMO MERGE ARROW
⑦	PAVE MARKING - THERMO YELLOW NOSE CONE
⑧	PAVE MARKING - THERMO YELLOW CROSS-HATCH
⑨	PAVE MARKING - THERMO 6 INCH WHITE - 10FT LENGTH, 30FT GAPS
⑩	PAVE MARKING - THERMO WHITE 6 IN 2 - 4' GAP

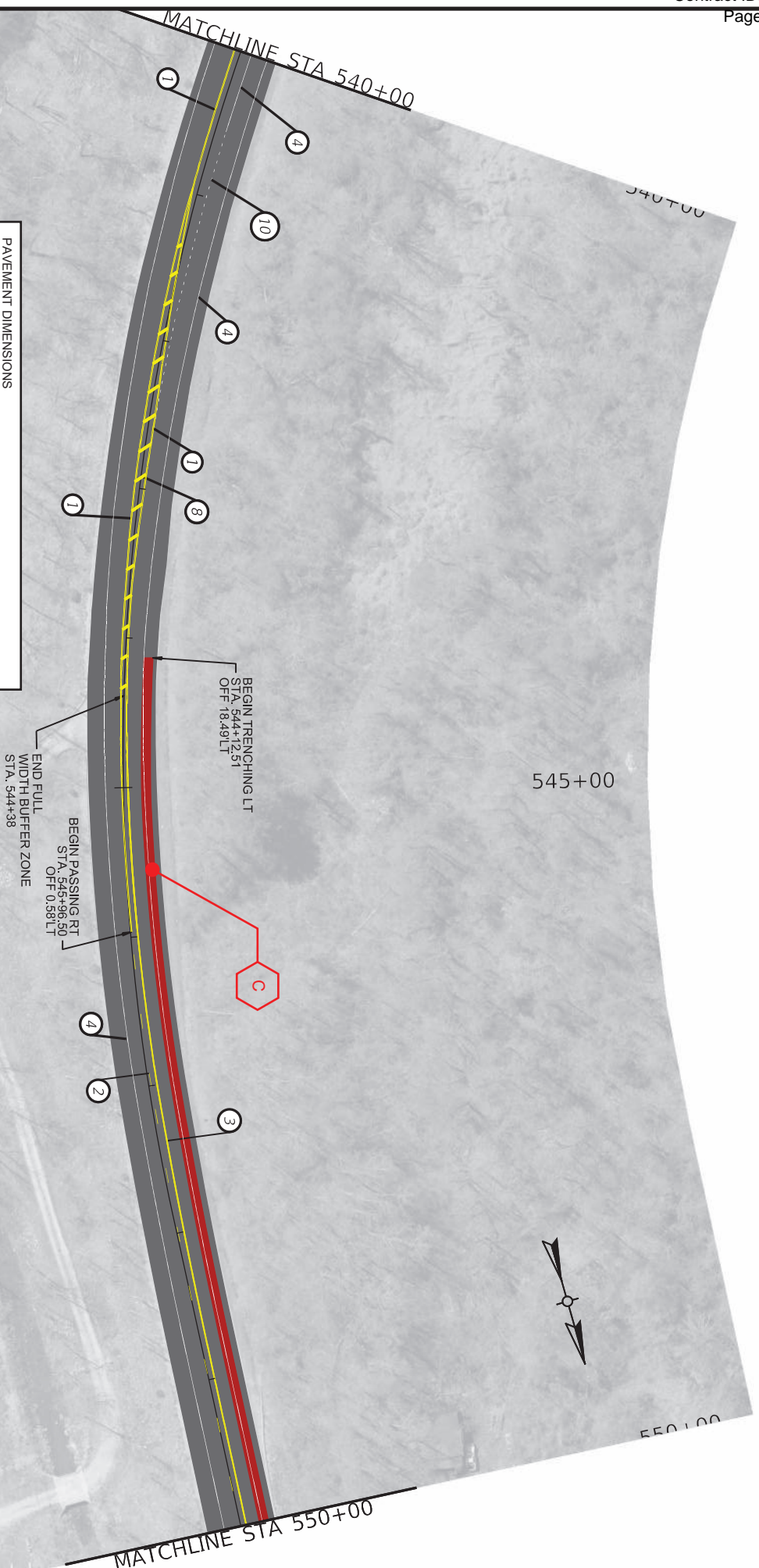
SPECIAL DIMENSIONS:	
4 FT SH	11 FT LANE
12 FT BUFFER	11 FT LANE
11 FT LANE	11 FT LANE
11 FT SH	11 FT SH



PAVEMENT DIMENSIONS		
<div>A</div>	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	9 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH
<div>B</div>	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<div>C</div>	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<div>D</div>	9 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<div>E</div>	9 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH

ASPHALT OVERLAY  
SHOULDER MILLING/TRENCHING

- PAVEMENT MARKING LEGEND
- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
  - ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
  - ③ PAVE STRIPING - THERMO 6 INCH YELLOW
  - ④ PAVE STRIPING - THERMO 6 INCH WHITE
  - ⑤ PAVE MARKING - THERMO CURV ARROW
  - ⑥ PAVE MARKING - THERMO MERGE ARROW
  - ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE
  - ⑧ PAVE MARKING - THERMO YELLOW CROSS-HATCH
  - ⑨ PAVE MARKING - THERMO 6 INCH WHITE - 10FT LENGTH, 30FT GAPS
  - ⑩ PAVE MARKING - THERMO WHITE 6 IN 2 - 4 GAP







COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS



DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT

STA. 550+00.01 TO STA. 560+00.01

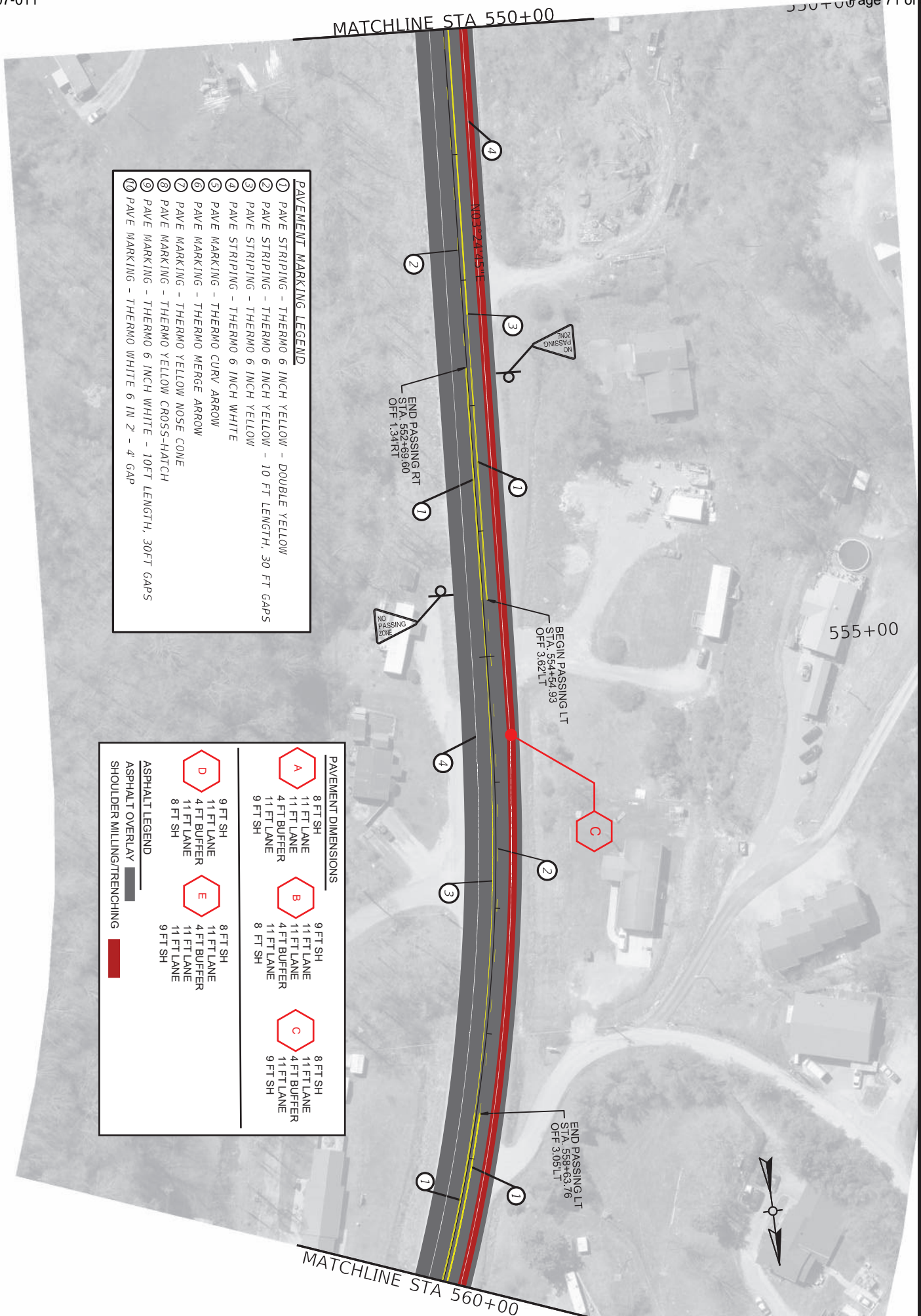


HORIZONTAL SCALE: 1"=100'

ITEM NO.	COUNTY OF
SHEET NO.	HARLAN
R20	

- PAVEMENT MARKING LEGEND**
- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
  - ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
  - ③ PAVE STRIPING - THERMO 6 INCH YELLOW
  - ④ PAVE STRIPING - THERMO 6 INCH WHITE
  - ⑤ PAVE MARKING - THERMO CURV ARROW
  - ⑥ PAVE MARKING - THERMO MERGE ARROW
  - ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE
  - ⑧ PAVE MARKING - THERMO YELLOW CROSS-HATCH
  - ⑨ PAVE MARKING - THERMO 6 INCH WHITE - 10FT LENGTH, 30FT GAPS
  - ⑩ PAVE MARKING - THERMO WHITE 6 IN 2 - 4' GAP

PAVEMENT DIMENSIONS		
<b>ASPHALT LEGEND</b>		
ASPHALT OVERLAY		
SHOULDER MILLING/TRENCHING		





COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS










DRAWING TITLE: US 119











CENTERLINE BUFFER PROJECT

STA. 560+00.01 TO STA. 570+00.01



ITEM NO.	COUNTY OF
SHEET NO.	HARLAN
R21	

PAVEMENT DIMENSIONS		
	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	9 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH
	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	
	9 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH	
	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 11 FT LANE 9 FT SH	
ASPHALT LEGEND		
ASPHALT OVERLAY 		
SHOULDER MILLING/TRENCHING 		

PAVEMENT MARKING LEGEND	
	PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
	PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
	PAVE STRIPING - THERMO 6 INCH YELLOW
	PAVE STRIPING - THERMO 6 INCH WHITE
	PAVE MARKING - THERMO CURV ARROW
	PAVE MARKING - THERMO MERGE ARROW
	PAVE MARKING - THERMO YELLOW NOSE CONE
	PAVE MARKING - THERMO YELLOW CROSS-HATCH
	PAVE MARKING - THERMO 6 INCH WHITE - 10FT LENGTH, 30FT GAPS
	PAVE MARKING - THERMO WHITE 6 IN 2 - 4 GAP

MATCHLINE STA 560+00

MATCHLINE STA 570+00

565+00







COMMONWEALTH OF KENTUCKY  
DEPARTMENT OF HIGHWAYS

DRAWING TITLE: US 119  
CENTERLINE BUFFER PROJECT  
STA. 570+00.01 TO STA. 580+00.01

0'  
100'  
200'  
400'

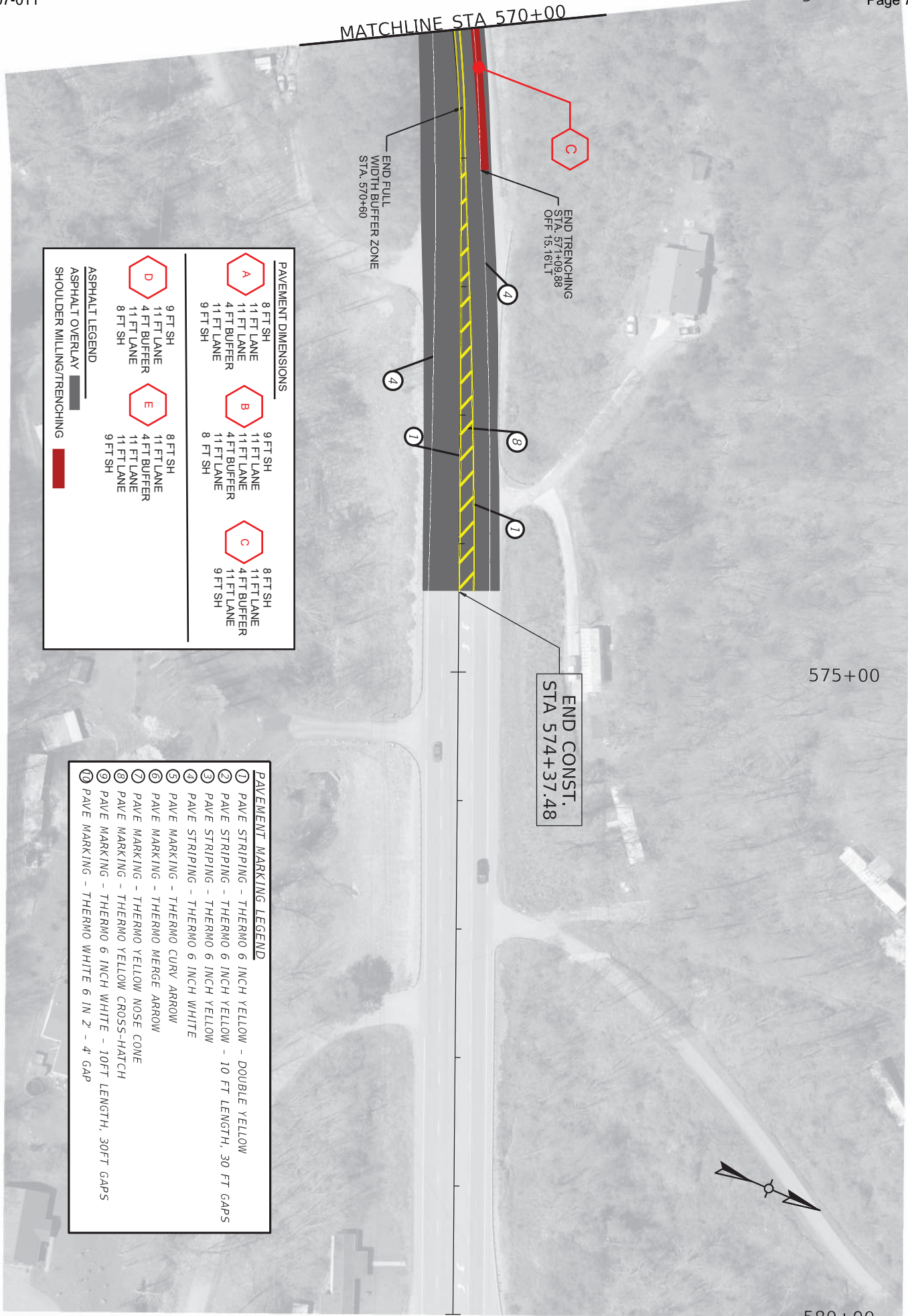
HORIZONTAL SCALE: 1"=100'

ITEM NO.  
SHEET NO.

R22

COUNTY OF  
HARLAN

Design: 23.00.01.11  
User: ADEGRUCIO



PAVEMENT DIMENSIONS		
<b>A</b>	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	9 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH
<b>B</b>	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>C</b>	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>D</b>	9 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 8 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH
<b>E</b>	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH	8 FT SH 11 FT LANE 4 FT BUFFER 11 FT LANE 9 FT SH

- PAVEMENT MARKING LEGEND**
- ① PAVE STRIPING - THERMO 6 INCH YELLOW - DOUBLE YELLOW
  - ② PAVE STRIPING - THERMO 6 INCH YELLOW - 10 FT LENGTH, 30 FT GAPS
  - ③ PAVE STRIPING - THERMO 6 INCH YELLOW
  - ④ PAVE STRIPING - THERMO 6 INCH WHITE
  - ⑤ PAVE MARKING - THERMO CURV ARROW
  - ⑥ PAVE MARKING - THERMO MERGE ARROW
  - ⑦ PAVE MARKING - THERMO YELLOW NOSE CONE
  - ⑧ PAVE MARKING - THERMO YELLOW CROSS-HATCH
  - ⑨ PAVE MARKING - THERMO 6 INCH WHITE - 10FT LENGTH, 30FT GAPS
  - ⑩ PAVE MARKING - THERMO WHITE 6 IN 2' - 4' GAP

**ASPHALT OVERLAY**  
SHOULDER MILLING/TRENCHING

**PART II**

**SPECIFICATIONS AND STANDARD DRAWINGS**

### **STANDARD SPECIFICATIONS**

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2019* and *Standard Drawings, Edition of 2020*.

### **SUPPLEMENTAL SPECIFICATIONS**

The contractor shall use the Supplemental Specifications that are effective at the time of letting. The Supplemental Specifications can be found at the following link:  
<http://transportation.ky.gov/Construction/Pages/Kentucky-Standard-Specifications.aspx>

## **SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS**

This Special Note will apply when indicated on the plans or in the proposal.

**1.0 DESCRIPTION.** Furnish, install, operate, and maintain variable message signs at the locations shown on the plans or designated by the Engineer. Remove and retain possession of variable message signs when they are no longer needed on the project.

## **2.0 MATERIALS.**

**2.1 General.** Use LED Variable Message Signs Class I, II, or III, as appropriate, from the Department's List of Approved Materials.

Unclassified signs may be submitted for approval by the Engineer. The Engineer may require a daytime and nighttime demonstration. The Engineer will make a final decision within 30 days after all required information is received.

### **2.2 Sign and Controls.** All signs must:

- 1) Provide 3-line messages with each line being 8 characters long and at least 18 inches tall. Each character comprises 35 pixels.
- 2) Provide at least 40 preprogrammed messages available for use at any time. Provide for quick and easy change of the displayed message; editing of the message; and additions of new messages.
- 3) Provide a controller consisting of:
  - a) Keyboard or keypad.
  - b) Readout that mimics the actual sign display. (When LCD or LCD type readout is used, include backlighting and heating or otherwise arrange for viewing in cold temperatures.)
  - c) Non-volatile memory or suitable memory with battery backup for storing pre-programmed messages.
  - d) Logic circuitry to control the sequence of messages and flash rate.
- 4) Provide a serial interface that is capable of supporting complete remote control ability through land line and cellular telephone operation. Include communication software capable of immediately updating the message, providing complete sign status, and allowing message library queries and updates.
- 5) Allow a single person easily to raise the sign to a satisfactory height above the pavement during use, and lower the sign during travel.
- 6) Be Highway Orange on all exterior surfaces of the trailer, supports, and controller cabinet.
- 7) Provide operation in ambient temperatures from -30 to + 120 degrees Fahrenheit during snow, rain and other inclement weather.
- 8) Provide the driver board as part of a module. All modules are interchangeable, and have plug and socket arrangements for disconnection and reconnection. Printed circuit boards associated with driver boards have a conformable coating to protect against moisture.
- 9) Provide a sign case sealed against rain, snow, dust, insects, etc. The lens is UV stabilized clear plastic (polycarbonate, acrylic, or other approved material) angled to prevent glare.
- 10) Provide a flat black UV protected coating on the sign hardware, character PCB, and appropriate lens areas.
- 11) Provide a photocell control to provide automatic dimming.



11

- 12) Allow an on-off flashing sequence at an adjustable rate.
- 13) Provide a sight to aim the message.
- 14) Provide a LED display color of approximately 590 nm amber.
- 15) Provide a controller that is password protected.
- 16) Provide a security device that prevents unauthorized individuals from accessing the controller.
- 17) Provide the following 3-line messages preprogrammed and available for use when the sign unit begins operation:

/KEEP/RIGHT/⇒⇒⇒/	/MIN/SPEED/**MPH/
/KEEP/LEFT/⇐⇐⇐/	/ICY/BRIDGE/AHEAD/ /ONE
/LOOSE/GRAVEL/AHEAD/	LANE/BRIDGE/AHEAD/
/RD WORK/NEXT/**MILES/	/ROUGH/ROAD/AHEAD/
/TWO WAY/TRAFFIC/AHEAD/	/MERGING/TRAFFIC/AHEAD/
/PAINT/CREW/AHEAD/	/NEXT/***/MILES/
/REDUCE/SPEED/**MPH/	/HEAVY/TRAFFIC/AHEAD/
/BRIDGE/WORK/***() FT/	/SPEED/LIMIT/**MPH/
/MAX/SPEED/**MPH/	/BUMP/AHEAD/
/SURVEY/PARTY/AHEAD/	/TWO/WAY/TRAFFIC/

\*Insert numerals as directed by the Engineer.

Add other messages during the project when required by the Engineer.

### 2.3 Power.

- 1) Design solar panels to yield 10 percent or greater additional charge than sign consumption. Provide direct wiring for operation of the sign or arrow board from an external power source to provide energy backup for 21 days without sunlight and an on-board system charger with the ability to recharge completely discharged batteries in 24 hours.

**3.0 CONSTRUCTION.** Furnish and operate the variable message signs as designated on the plans or by the Engineer. Ensure the bottom of the message panel is a minimum of 7 feet above the roadway in urban areas and 5 feet above in rural areas when operating. Use Class I, II, or III signs on roads with a speed limit less than 55 mph. Use Class I or II signs on roads with speed limits 55 mph or greater.

Maintain the sign in proper working order, including repair of any damage done by others, until completion of the project. When the sign becomes inoperative, immediately repair or replace the sign. Repetitive problems with the same unit will be cause for rejection and replacement.

Use only project related messages and messages directed by the Engineer, unnecessary messages lessen the impact of the sign. Ensure the message is displayed in either one or 2 phases with each phase having no more than 3 lines of text. When no message is needed, but it is necessary to know if the sign is operable, flash only a pixel.

When the sign is not needed, move it outside the clear zone or where the Engineer directs. Variable Message Signs are the property of the Contractor and shall be removed from the project when no longer needed. The Department will not assume ownership of these signs.

**4.0 MEASUREMENT.** The final quantity of Variable Message Sign will be

11  
the actual number of individual signs acceptably furnished and operated during the project. The Department will not measure signs replaced due to damage or rejection.

**5.0 PAYMENT.** The Department will pay for the Variable Message Signs at the unit price each. The Department will not pay for signs replaced due to damage or rejection. Payment is full compensation for furnishing all materials, labor, equipment, and service necessary to, operate, move, repair, and maintain or replace the variable message signs. The Department will make payment for the completed and accepted quantities under the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
02671	Portable Changeable Message Sign	Each

Effective June 15, 2012

## SPECIAL NOTE FOR BARCODE LABEL ON PERMANENT SIGNS

**1.0 DESCRIPTION.** Install barcode label on sheeting signs. Section references herein are to the Department's Standard Specifications for Road and Bridge Construction, current edition.

**2.0 MATERIALS.** The Department will provide the Contractor with a 2 inch x 1 inch foil barcode label for each permanent sheeting sign. A unique number will be assigned to each barcode label.

The Contractor shall contact the Operations and Pavement Management Branch in the Division of Maintenance at (502) 564-4556 to obtain the barcode labels.

**3.0 CONSTRUCTION.** Apply foil barcode label in the lower right quadrant of the sign back. Signs where the bottom edge is not parallel to the ground, the lowest corner of the sign shall serve as the location to place the barcode label. The barcode label shall be placed no less than one-inch and no more than three inches from any edge of the sign. The barcode must be placed so that the sign post does not cover the barcode label.

Barcodes shall be applied in an indoor setting with a minimum air temperature of 50°F or higher. Prior to application of the barcode label, the back of the sign must be clean and free of dust, oil, etc. If the sign is not clean, an alcohol swab shall be used to clean the area. The area must be allowed to dry prior to placement of the barcode label.

Data for each sign shall include the barcode number, MUTCD reference number, sheeting manufacturer, sheeting type, manufacture date, color of primary reflective surface, installation date, latitude and longitude using the North American Datum of 1983 (NAD83) or the State Plane Coordinates using an x and y ordinate of the installed location.

Data should be provided electronically on the TC 71-229 Sign Details Information and TC 71-230 Sign Assembly Information forms. The Contractor may choose to present the data in a different format provided that the information submitted to the Department is equivalent to the information required on the Department TC forms. The forms must be submitted in electronic format regardless of which type of form is used. The Department will not accept PDF or handwritten forms. These completed forms must be submitted to the Department prior to final inspection of the signs. The Department will not issue formal acceptance for the project until the TC 71-229 and TC-230 electronic forms are completed for all signs and sign assemblies on the project.

**4.0 MEASUREMENT.** The Department will measure all work required for the installation of the barcode label and all work associated with completion and submission of the sign inventory data (TC 71-229 and TC 71-230).

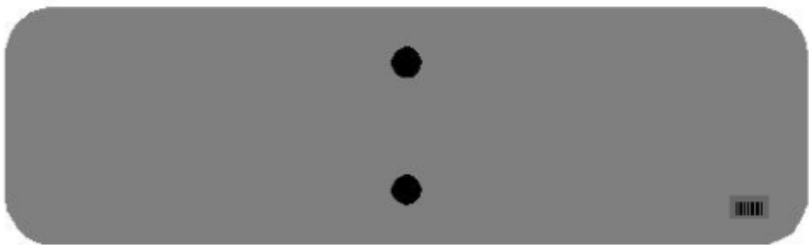
The installation of the permanent sign will be measured in accordance to Section 715.

**5.0 PAYMENT.** The Department will make payment for the completed and accepted quantities under the following:

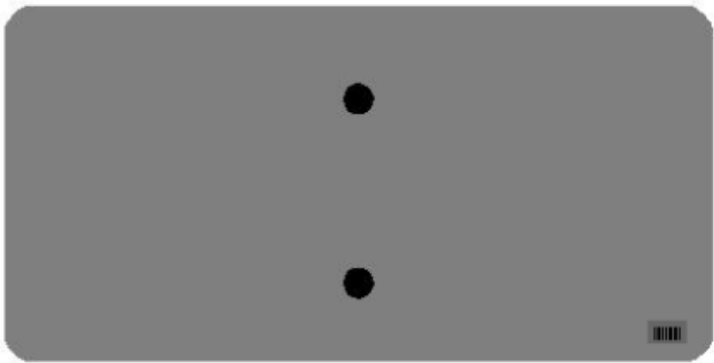
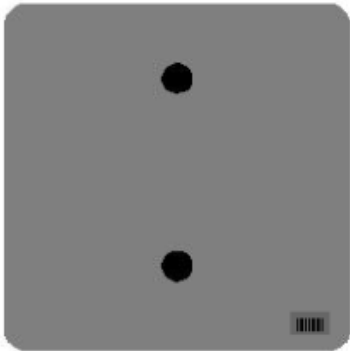
<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
24631EC	Barcode Sign Inventory	Each

The Department will not make payment for this item until all barcodes are installed and sign inventory is complete on every permanent sign installed on the project. The Department will make payment for installation of the permanent sign in accordance to Section 715. The Department will consider payment as full compensation for all work required under this special note.

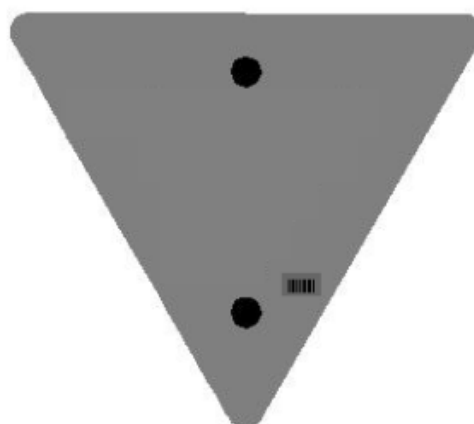
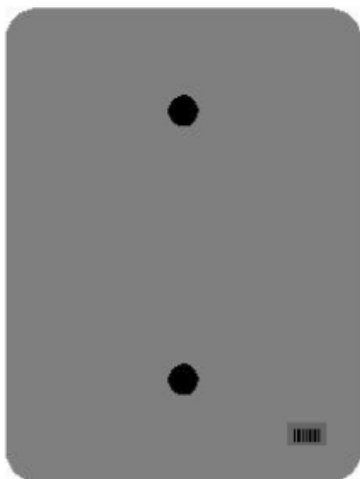
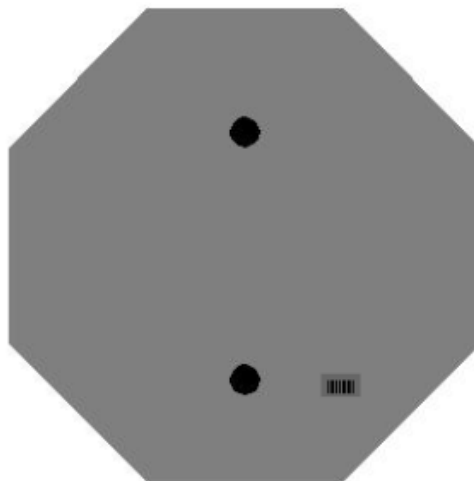
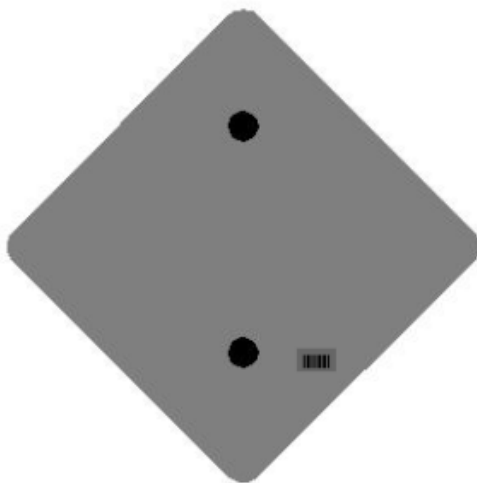
One Sign Post



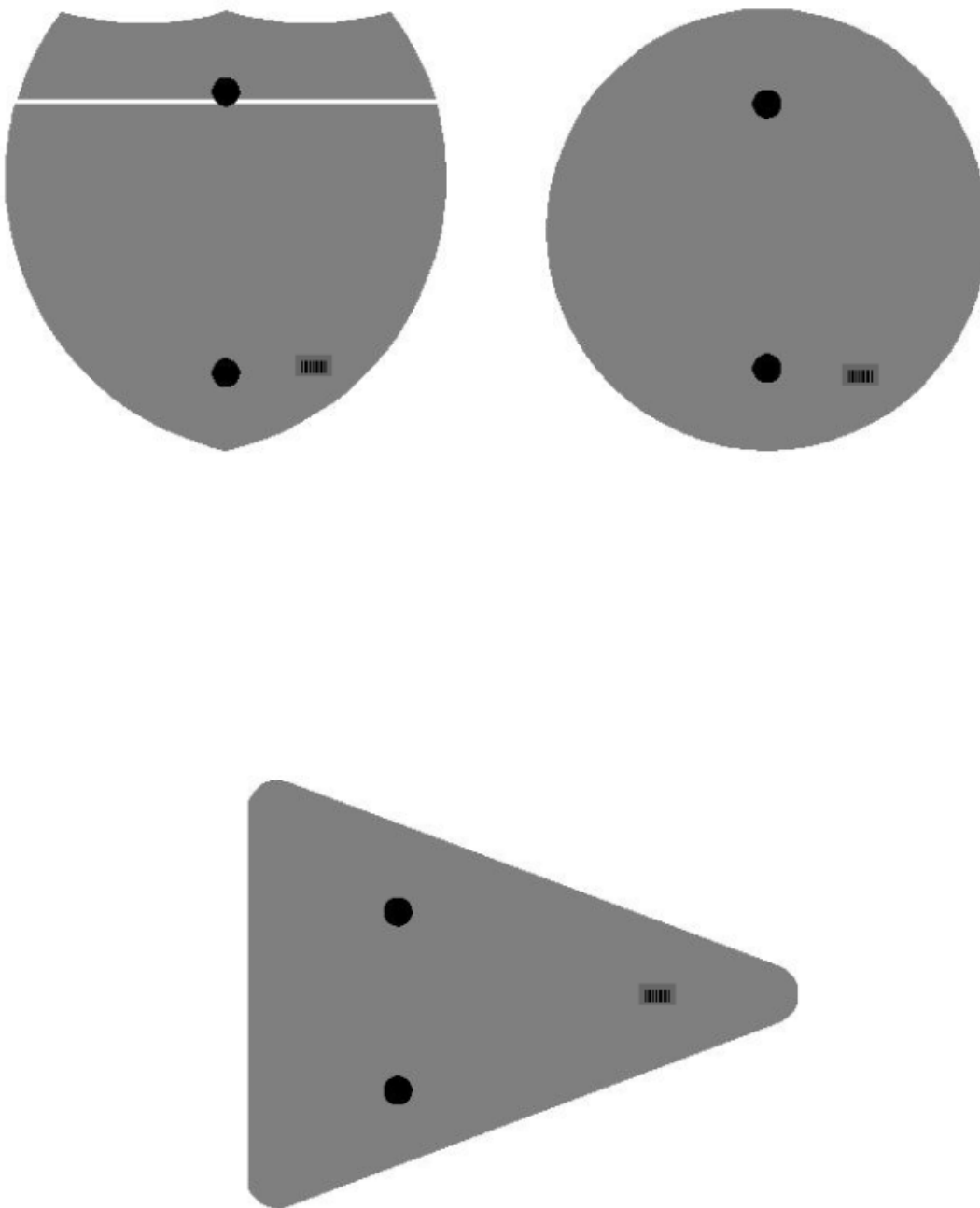
↑  
2" Wide Post



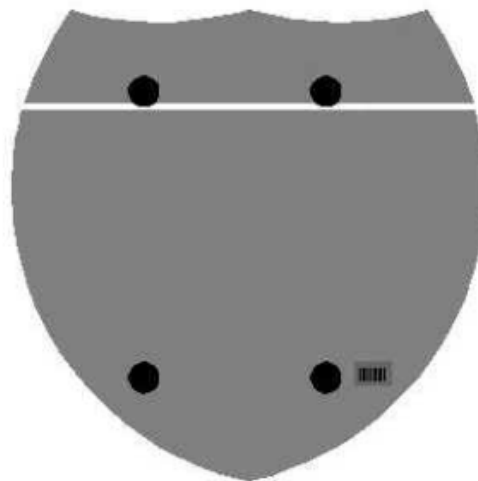
## One Sign Post



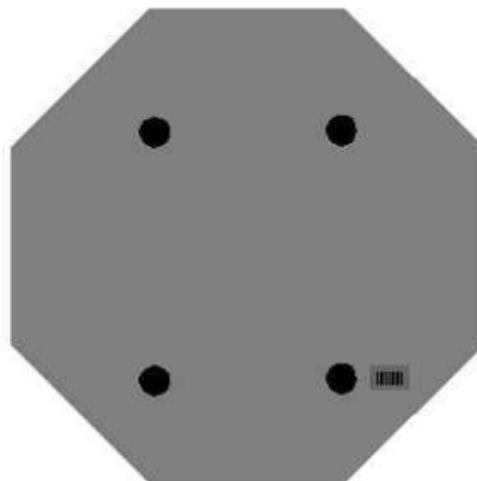
### One Sign Post



### Double Sign Post

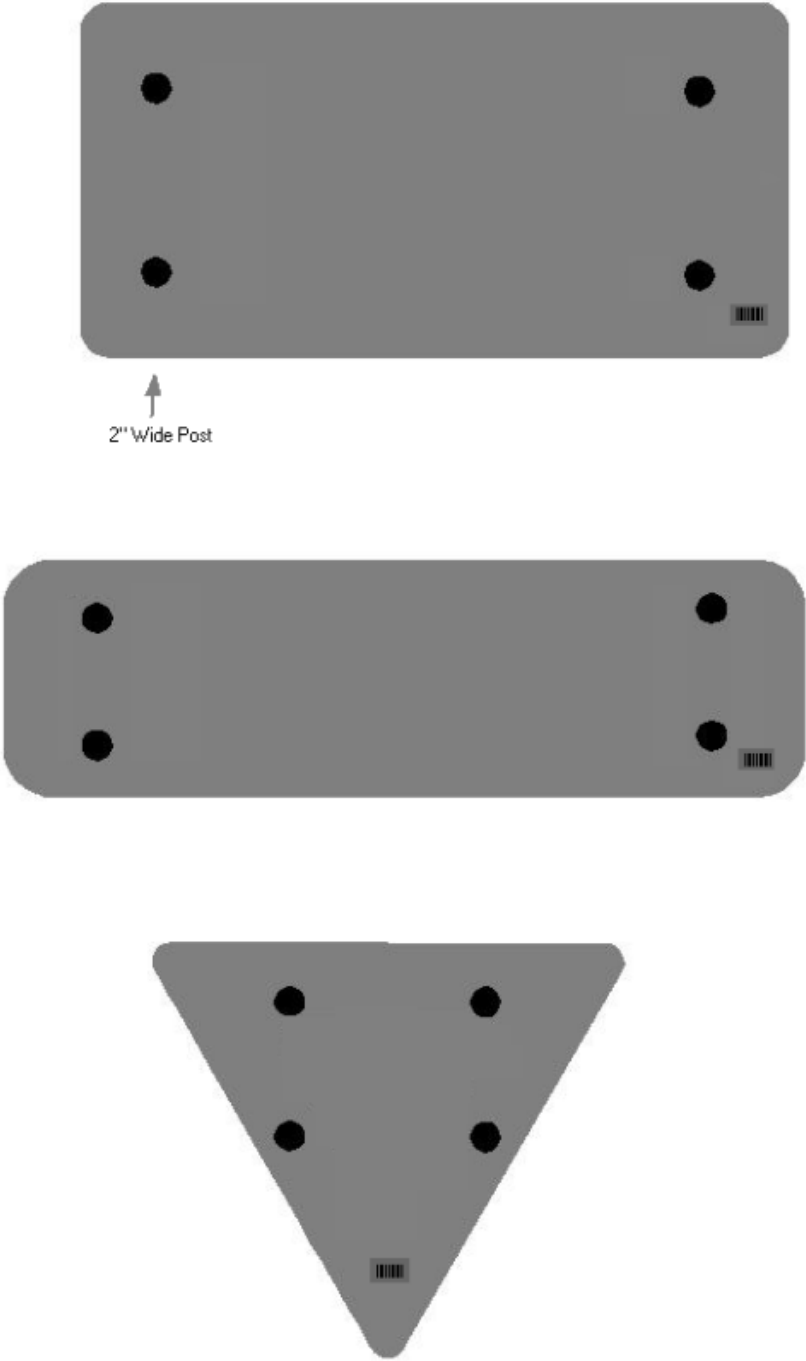


Interstate  
Shield



48" Stop

2 Post Signs





2020 STANDARD DRAWINGS THAT APPLY

<b>ROADWAY</b>	
<b>~ PAVEMENT ~</b>	
<u>MEDIANS, CURBS, APPROACHES, ENTRANCES, ETC.</u>	
APPROACHES, ENTRANCES, AND MAIL BOX TURNOUT .....	RPM-110-07
<b>~ GENERAL ~</b>	
<u>CURVE WIDENING AND SUPERELEVATION</u>	
CURVE WIDENING AND SUPERELEVATION TRANSITIONS.....	RGS-001-07
SUPERELEVATION FOR MULTILANE PAVEMENTS .....	RGS-002-06
<u>MISCELLANEOUS STANDARDS</u>	
MISCELLANEOUS STANDARDS.....	RGX-001-06
<b>TRAFFIC</b>	
<b>~ PERMANENT ~</b>	
TYPICAL MARKINGS FOR TURN LANES PAGE 1.....	TPM-206
<u>RUMBLE STRIPS</u>	
CENTERLINE RUMBLE STRIPS 6 INCH STRIPING .....	Sepia 027
EDGE LINE RUMBLE STRIPS PLACEMENT DETAILS .....	Sepia 028
EDGE LINE RUMBLE STRIP DETAILS TWO LANE ROADWAYS.....	Sepia 029
EDGE LINE RUMBLE STRIP NOTES TWO LANE ROADWAYS NOTES .....	Sepia 029N
<b>~ TEMPORARY ~</b>	
<u>TRAFFIC CONTROL</u>	
LANE CLOSURE TWO-LANE HIGHWAY.....	TTC-100-05
SHOULDER CLOSURE .....	TTC-135-03
<u>DEVICES</u>	
PAVEMENT CONDITION WARNING SIGNS .....	TTD-125-03
<u>STRIPING OPERATIONS</u>	
MOBILE OPERATION FOR DURABLE STRIPING CASE I .....	TTS-120-02
MOBILE OPERATION FOR DURABLE STRIPING CASE IV .....	TTS-135-02

## **PART III**

### **EMPLOYMENT, WAGE AND RECORD REQUIREMENTS**

**TRANSPORTATION CABINET  
DEPARTMENT OF HIGHWAYS**

**LABOR AND WAGE REQUIREMENTS  
APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS**

- I. Application
- II. Nondiscrimination of Employees (KRS 344)

**I. APPLICATION**

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual because of his race, color, religion, national origin, sex, disability or age forty (40) and over, in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

Revised: January 25, 2017

**II. NONDISCRIMINATION OF EMPLOYEES**

**AN ACT OF THE KENTUCKY  
GENERAL ASSEMBLY TO PREVENT  
DISCRIMINATION IN EMPLOYMENT  
KRS CHAPTER 344  
EFFECTIVE JUNE 16, 1972**

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (forty and above); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age forty (40) and over. The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, except that such a notice or advertisement may indicate a preference, limitation, or specification based on religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, when religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, is a bona fide occupational qualification for employment.

## EXECUTIVE BRANCH CODE OF ETHICS

The Executive Branch Code of Ethics created by Kentucky Revised Statutes (KRS) Chapter 11A, effective July 14, 1992, establishes the ethical standards that govern the conduct of all executive branch employees. The Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (7) provides:

A present or former public servant listed in KRS 11A.010(9)(a) to (g) shall not, within one (1) year following termination of his or her office or employment, accept employment, compensation, or other economic benefit from any person or business that contracts or does business with, or is regulated by, the state in matters in which he was directly involved during the last thirty-six (36) months of his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, or for which he received, prior to his state employment, a professional degree or license, provided that, for a period of one (1) year, he or she personally refrains from working on any matter in which he was directly involved during the last thirty-six (36) months of his or her tenure in state government. This subsection shall not prohibit the performance of ministerial functions, including but not limited to filing tax returns, filing applications for permits or licenses, or filing incorporation papers, nor shall it prohibit the former officer or public servant from receiving public funds disbursed through entitlement programs.

KRS 11A.040 (9) states:

A former public servant shall not represent a person or business before a state agency in a matter in which the former public servant was directly involved during the last thirty-six (36) months of his tenure, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not to obtain private benefits.

If you have worked for the executive branch of state government within the past year, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, 1025 Capital Center Drive, Suite 105, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Revised: March 11, 2025

### **Kentucky Equal Employment Opportunity Act of 1978**

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:  
**<https://www.eProcurement.ky.gov>**.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **[finance.contractcompliance@ky.gov](mailto:finance.contractcompliance@ky.gov)** or by phone at 502-564-2874.

# EMPLOYEE RIGHTS UNDER THE FAIR LABOR STANDARDS ACT

THE UNITED STATES DEPARTMENT OF LABOR WAGE AND HOUR DIVISION

## FEDERAL MINIMUM WAGE

# \$7.25

 PER HOUR

BEGINNING JULY 24, 2009

### OVERTIME PAY

At least  $1\frac{1}{2}$  times your regular rate of pay for all hours worked over 40 in a workweek.

### CHILD LABOR

An employee must be at least **16** years old to work in most non-farm jobs and at least **18** to work in non-farm jobs declared hazardous by the Secretary of Labor.

Youths **14** and **15** years old may work outside school hours in various non-manufacturing, non-mining, non-hazardous jobs under the following conditions:

#### *No more than*

- **3** hours on a school day or **18** hours in a school week;
- **8** hours on a non-school day or **40** hours in a non-school week.

Also, work may not begin before **7 a.m.** or end after **7 p.m.**, except from June 1 through Labor Day, when evening hours are extended to **9 p.m.** Different rules apply in agricultural employment.

### TIP CREDIT

Employers of "tipped employees" must pay a cash wage of at least \$2.13 per hour if they claim a tip credit against their minimum wage obligation. If an employee's tips combined with the employer's cash wage of at least \$2.13 per hour do not equal the minimum hourly wage, the employer must make up the difference. Certain other conditions must also be met.

### ENFORCEMENT

The Department of Labor may recover back wages either administratively or through court action, for the employees that have been underpaid in violation of the law. Violations may result in civil or criminal action.

Employers may be assessed civil money penalties of up to \$1,100 for each willful or repeated violation of the minimum wage or overtime pay provisions of the law and up to \$11,000 for each employee who is the subject of a violation of the Act's child labor provisions. In addition, a civil money penalty of up to \$50,000 may be assessed for each child labor violation that causes the death or serious injury of any minor employee, and such assessments may be doubled, up to \$100,000, when the violations are determined to be willful or repeated. The law also prohibits discriminating against or discharging workers who file a complaint or participate in any proceeding under the Act.

### ADDITIONAL INFORMATION

- Certain occupations and establishments are exempt from the minimum wage and/or overtime pay provisions.
- Special provisions apply to workers in American Samoa and the Commonwealth of the Northern Mariana Islands.
- Some state laws provide greater employee protections; employers must comply with both.
- The law requires employers to display this poster where employees can readily see it.
- Employees under 20 years of age may be paid \$4.25 per hour during their first 90 consecutive calendar days of employment with an employer.
- Certain full-time students, student learners, apprentices, and workers with disabilities may be paid less than the minimum wage under special certificates issued by the Department of Labor.

For additional information:



# 1-866-4-USWAGE

(1-866-487-9243)

TTY: 1-877-889-5627



# WWW.WAGEHOUR.DOL.GOV

**PART IV**

**BID ITEMS**

254107

Section: 0001 - PAVING

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0010	00212		CL2 ASPH BASE 1.00D PG64-22	3,852.00	TON		\$	
0020	00307		CL2 ASPH SURF 0.38B PG64-22	4,030.00	TON		\$	
0030	00339		CL3 ASPH SURF 0.38D PG64-22	2,181.00	TON		\$	
0040	00356		ASPHALT MATERIAL FOR TACK	42.00	TON		\$	

Section: 0002 - ROADWAY

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0050	02562		TEMPORARY SIGNS	200.00	SQFT		\$	
0060	02585		EDGE KEY	71.00	LF		\$	
0070	02650		MAINTAIN & CONTROL TRAFFIC (HARLAN US 119 HSIP)	1.00	LS		\$	
0080	02671		PORTABLE CHANGEABLE MESSAGE SIGN	2.00	EACH		\$	
0090	02697		EDGE LINE RUMBLE STRIPS	34,680.00	LF		\$	
0100	02726		STAKING (HARLAN US 119 HSIP)	1.00	LS		\$	
0110	06511		PAVE STRIPING-TEMP PAINT-6 IN	106,979.00	LF		\$	
0120	06542		PAVE STRIPING-THERMO-6 IN W	36,117.00	LF		\$	
0130	06543		PAVE STRIPING-THERMO-6 IN Y	70,861.00	LF		\$	
0140	06569		PAVE MARKING-THERMO CROSS-HATCH	2,698.00	SQFT		\$	
0150	06574		PAVE MARKING-THERMO CURV ARROW	14.00	EACH		\$	
0160	06578		PAVE MARKING-THERMO MERGE ARROW	6.00	EACH		\$	
0170	10020NS		FUEL ADJUSTMENT	16,171.00	DOLL	\$1.00	\$	\$16,171.00
0180	10030NS		ASPHALT ADJUSTMENT	40,617.00	DOLL	\$1.00	\$	\$40,617.00
0190	20458ES403		CENTERLINE RUMBLE STRIPS	34,680.00	LF		\$	
0200	20748ED		SHOULDER MILLING/TRENCHING	9,337.00	SQYD		\$	
0210	21289ED		LONGITUDINAL EDGE KEY	14,005.00	LF		\$	
0220	23608EC		YELLOW PAINT FOR MEDIAN SAFETY NOSE	251.00	SQFT		\$	

Section: 0003 - SIGNING

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0230	06407		SBM ALUM SHEET SIGNS .125 IN	22.00	SQFT		\$	
0240	06410		STEEL POST TYPE 1	44.00	LF		\$	
0250	21373ND		REMOVE SIGN	1.00	EACH		\$	
0260	24631EC		BARCODE SIGN INVENTORY	4.00	EACH		\$	

Section: 0004 - DEMOBILIZATION

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0270	02569		DEMOBILIZATION	1.00	LS		\$	



254107

PROPOSAL BID ITEMS

Report Date 7/16/25