



CALL NO. 335

CONTRACT ID. 252944

JEFFERSON COUNTY

FED/STATE PROJECT NUMBER FJ11 056 0065 B00214L

DESCRIPTION KENNEDY BRIDGE (I-65)

WORK TYPE BRIDGE REPAIRS

PRIMARY COMPLETION DATE 4/15/2026

LETTING DATE: January 23,2025

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN STANDARD TIME January 23,2025. Bids will be publicly announced at 10:00 AM EASTERN STANDARD TIME.

NO PLANS ASSOCIATED WITH THIS PROJECT.

REQUIRED BID PROPOSAL GUARANTY: Not less than 5% of the total bid.

TABLE OF CONTENTS

PART I	SCOPE OF WORK <ul style="list-style-type: none">PROJECT(S), COMPLETION DATE(S), & LIQUIDATED DAMAGESCONTRACT NOTESSTATE CONTRACT NOTESSPECIAL NOTE(S) APPLICABLE TO PROJECTRIGHT OF WAY CERTIFICATIONUTILITY IMPACT & RAIL CERTIFICATION NOTESPERMIT(S)
PART II	SPECIFICATIONS AND STANDARD DRAWINGS <ul style="list-style-type: none">STANDARD AND SUPPLEMENTAL SPECIFICATIONS
PART III	EMPLOYMENT, WAGE AND RECORD REQUIREMENTS <ul style="list-style-type: none">LABOR AND WAGE REQUIREMENTSEXECUTIVE BRANCH CODE OF ETHICSKENTUCKY EQUAL EMPLOYMENT OPPORTUNITY ACT OF 1978 LOCALITY / STATEPROJECT WAGE RATES / STATE FUNDED
PART IV	BID ITEMS

PART I
SCOPE OF WORK

ADMINISTRATIVE DISTRICT - 05

CONTRACT ID - 252944
FJ11 056 0065 B00214L
COUNTY - JEFFERSON
PCN - MB05600652501
FJ11 056 0065 B00214L

KENNEDY BRIDGE (I-65) SOUTHBOUND KENNEDY BRIDGE OVER OHIO RIVER AT MILEPOINT 137.19BRIDGE
REPAIRS SYP NO. 05-10074.00.
GEOGRAPHIC COORDINATES LATITUDE 38:15:52.00 LONGITUDE 85:44:37.00
ADT

COMPLETION DATE(S):

COMPLETED BY 04/15/2026	APPLIES TO ENTIRE CONTRACT (SEE SPECIAL NOTE)
60 CALENDAR Days	APPLIES TO PHASE 1B-US. PP O ONLY (SEE SPECIAL NOTE)
60 CALENDAR Days	APPLIES TO PHASE 2B-DS. PP O ONLY (SEE SPECIAL NOTE)
120 CALENDAR Days	APPLIES TO PHASE 1 TOTAL (SEE SPECIAL NOTE)
120 CALENDAR Days	APPLIES TO PHASE 2 TOTAL (SEE SPECIAL NOTE)

CONTRACT NOTES

INSURANCE

Refer to Kentucky Standard Specifications for Road and Bridge Construction, current edition.

PROPOSAL ADDENDA

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

BID SUBMITTAL

Bidder must use the Department's electronic bidding software. The Bidder must download the bid file located on the Bid Express website (www.bidx.com) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

JOINT VENTURE BIDDING

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

UNDERGROUND FACILITY DAMAGE PROTECTION

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. When prescribed in said directives, the contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom shall be contacted through their individual Protection Notification Center. Non-compliance with these directives can result in the enforcement of penalties.

REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth ("certificate") from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the

foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity's solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by email to kytc.projectquestions@ky.gov. The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading "Questions & Answers" on the Construction Procurement website (www.transportation.ky.gov/construction-procurement). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

HARDWOOD REMOVAL RESTRICTIONS

The US Department of Agriculture has imposed a quarantine in Kentucky and several surrounding states, to prevent the spread of an invasive insect, the emerald ash borer. Hardwood cut in conjunction with the project may not be removed from the state. Chipping or burning on site is the preferred method of disposal.

INSTRUCTIONS FOR EXCESS MATERIAL SITES AND BORROW SITES

Identification of excess material sites and borrow sites shall be the responsibility of the Contractor. The Contractor shall be responsible for compliance with all applicable state and federal laws and may wish to consult with the US Fish and Wildlife Service to seek protection under Section 10 of the Endangered Species Act for these activities.

ACCESS TO RECORDS

The state agency certifies that it is in compliance with the provisions of KRS 45A.150, "Access to contractor's books, documents, papers, records, or other evidence directly pertinent to the contract." The Contractor, as defined in KRS 45A.030, agrees that the contracting agency, the

Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this agreement for the purpose of financial audit or program review. The Contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884. Records and other prequalification information confidentially disclosed as part of the bid process shall not be deemed as directly pertinent to the agreement and shall be exempt from disclosure as provided in KRS 61.878(1)(c).

BOYCOTT PROVISIONS

If applicable, the contractor represents that, pursuant to [KRS 45A.607](#), they are not currently engaged in, and will not for the duration of the contract engage in, the boycott of a person or an entity based in or doing business with a jurisdiction with which Kentucky can enjoy open trade. **Note:** The term Boycott does not include actions taken for bona fide business or economic reasons, or actions specifically required by federal or state law.

If applicable, the contractor verifies that, pursuant to KRS 41.480, they do not engage in, and will not for the duration of the contract engage in, in energy company boycotts as defined by KRS 41.472.

LOBBYING PROHIBITIONS

The contractor represents that they, and any subcontractor performing work under the contract, have not violated the agency restrictions contained in [KRS 11A.236](#) during the previous ten (10) years, and pledges to abide by the restrictions set forth in such statute for the duration of the contract awarded.

The contractor further represents that, pursuant to [KRS 45A.328](#), they have not procured an original, subsequent, or similar contract while employing an executive agency lobbyist who was convicted of a crime related to the original, subsequent, or similar contract within five (5) years of the conviction of the lobbyist.

Revised: 1/1/2025

1.0 BUY AMERICA REQUIREMENT.

Follow the “Buy America” provisions as required by 23 U.S.C. § 313 and 23 C.F.R. § 635.410. Except as expressly provided herein all manufacturing processes of steel or iron materials including but not limited to structural steel, guardrail materials, corrugated steel, culvert pipe, structural plate, prestressing strands, and steel reinforcing bars shall occur in the United States of America, including the application of:

- Coating,
- Galvanizing,
- Painting, and
- Other coating that protects or enhances the value of steel or iron products.

The following are exempt, unless processed or refined to include substantial amounts of steel or iron material, and may be used regardless of source in the domestic manufacturing process for steel or iron material:

- Pig iron,
- Processed, pelletized, and reduced iron ore material, or
- Processed alloys.

The Contractor shall submit a certification stating that all manufacturing processes involved with the production of steel or iron materials occurred in the United States.

Produce, mill, fabricate, and manufacture in the United States of America all aluminum components of bridges, tunnels, and large sign support systems, for which either shop fabrication, shop inspection, or certified mill test reports are required as the basis of acceptance by the Department.

Use foreign materials only under the following conditions:

- 1) When the materials are not permanently incorporated into the project; or
- 2) When the delivered cost of such materials used does not exceed 0.1 percent of the total Contract amount or \$2,500.00, whichever is greater.

The Contractor shall submit to the Engineer the origin and value of any foreign material used.

2.0 – BUILD AMERICA, BUY AMERICA (BABA)

Contractor shall comply with the Federal Highway Administration (FHWA) Buy America Requirement in 23 C.F.R. § 635.410 and all relevant provisions of the Build America, Buy America Act (BABA), contained within the Infrastructure Investment and Jobs Act, Pub. L. No. 117-58, §§ 70901-52 enacted November 15, 2021. The BABA requires iron, steel, manufactured products, and construction materials used in infrastructure projects funded by federal financial assistance to be produced in the United States. Comply with 2 C.F.R § 184.

BABA permits FHWA participation in the Contract only if domestic steel and iron will be used on the Project. To be considered domestic, all steel and iron used, and all products manufactured from steel and iron must be produced in the United States and all manufacturing processes, including application of a coating, for these materials must occur in the United States. Coating includes all processes that protect or enhance the value of the material to which the coating is applied. This requirement does not preclude a minimal use of foreign steel and iron materials, provided the cost of such materials does not exceed 0.1% of the total contract amount under the Contract or \$2,500.00 whichever is greater.

BABA permits FHWA participation in the Contract only if all “construction materials” as defined in the Act are made in the United States. The Buy America preference applies to the following construction materials

SPECIAL NOTE – BUY AMERICA REQUIREMENTS AND BUILD
AMERICA, BUY AMERICA (BABA) ACT

10/26/2023

incorporated into infrastructure projects: non-ferrous metals; plastic and polymer-based products (including polyvinylchloride, composite building materials, and polymers used in fiber optic cables); glass (including optic glass); Fiber optic cable; optical fiber; lumber; engineered wood; and drywall. Contractor will be required to use construction materials produced in the United States on this Project. The Contractor shall submit a certification stating that all construction materials are certified to be BABA compliant.

Finally, BABA permits the continuation of FHWA's current general applicability waivers for manufactured products, raw materials, and ferryboat parts, but these waivers are subject to reevaluation, specifically the general applicability waiver for manufactured products.

The Contractor has completed and submitted, or shall complete and submit, to the Cabinet a Buy America/Build America, Buy America Certificate prior to the Cabinet issuing the notice to proceed, in the format below. After submittal, the Contractor is bound by its original certification.

A false certification is a criminal act in violation of 18 U.S.C. § 1001. The Contractor has the burden of proof to establish that it is in compliance.

At the Contractor's request, the Cabinet may, but is not obligated to, seek a waiver of Buy America requirements if grounds for the waiver exist under 23 C.F.R. § 635.410(c) or will comply with the applicable Buy America requirements if a waiver of those requirements is not available or not pursued by the Cabinet.

Please refer to the Federal Highway Administration's Buy America webpage for more information.

[Buy America - Construction Program Guide - Contract Administration - Construction - Federal Highway Administration \(dot.gov\)](#)

October 26, 2023 Letting

BUY AMERICA / BUILD AMERICA, BUY AMERICA (ACT) MATERIALS CERTIFICATE OF COMPLIANCE

The Contractor hereby certifies that it will comply with all relevant provisions of the Build America, Buy America Act, contained within the Infrastructure Investment and Jobs Act, Pub. L. NO. 117-58, §§ 70901-52, the requirements of 23 U.S.C. § 313, 23 C.F.R. § 635.410 and 2 C.F.R § 184.

Date Submitted:_____

Contractor:_____

Signature:_____

Printed Name:_____

Title:_____

NOTE: THIS CERTIFICATION IS IN ADDITION TO ANY AND ALL REQUIREMENTS OUTLINED IN THE CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND/OR SPECIAL NOTES CONTAINED IN THE PROJECT PROPOSAL.

SPECIAL NOTE FOR RECIPROCAL PREFERENCE

RECIPROCAL PREFERENCE TO BE GIVEN BY PUBLIC AGENCIES TO RESIDENT BIDDERS

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the electronic bidding software. Submittal of the Affidavit should be done along the bid in Bid Express.

April 30, 2018

**SPECIAL NOTES
DISTRICT NO. 5
BRIDGE STEEL REPAIRS
JEFFERSON COUNTY
SYP ITEM NUMBER 5-10074.00
CID 252944
BRIDGE NUMBER 056B00214L**

FJ11 056 0065 B00214L 137.19

Jefferson County ~ I-65 SB Bridge over Ohio River

Geographic Coordinates

Latitude 38° 15' 52'' (38.2645)

Longitude -85° 44' 37'' (-85.7435)

Description

Drawing #1 – 14525

Drawing #2 – 14744

Drawing #3 – 23792

Drawing #4 – 26886

SPECIAL NOTES

SPECIAL NOTE FOR CONTRACT COMPLETION DATE AND PENALTIES ON
BRIDGE REPAIR CONTRACTS

SPECIAL NOTE FOR TRAFFIC CONTROL ON BRIDGE REPAIR CONTRACTS

SPECIAL NOTE FOR PORTABLE QUEUE WARNING ALERT SYSTEM

SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS

SPECIAL NOTE FOR QUEUE PROTECTION VEHICLE

SPECIAL NOTE FOR TEMPORARY WORKSITE SPEED LIMIT SIGN ASSEMBLY

SPECIAL NOTE FOR TEMPORARY SUPPORT

SPECIAL NOTE FOR CONTRACT COORDINATION

SPECIAL NOTE FOR FINGER EXPANSION JOINT

PROJECT SITE MAP

John F Kennedy Bridge (I-65 SB) over Ohio River
Jefferson County
Louisville, Kentucky
056B00214L



SPECIAL NOTE FOR CONTRACT COMPLETION DATE AND LIQUIDATED DAMAGES ON BRIDGE REPAIR CONTRACT

I. COMPLETION DATE. The Contractor has the option of selecting the starting date for this Contract. Once selected, notify the Department in writing of the date selected at least two weeks prior to beginning work.

The work in PLAN SETS A and B is to be completed by April 15, 2026 at 5:00 AM

PLAN SETS A and B: An allotted number of calendar days are assigned to Phase 1 and 2 in this contract. The Contractor shall have 120 days to complete Phase 1A and Phase 1B. Only 60 consecutive days of the allotted 120 days may include the Phase 1B closures as shown on the conceptual MOT drawings at PP0. Similarly, the Contractor shall have up to 120 days to complete Phase 2A and Phase 2B. Only 60 consecutive days of the allotted 120 days may include the Phase 2B closures as shown on the conceptual MOT drawings at PP0. Phase 1 and 2 shall be performed consecutively.

Plan Set A – Truss Bearing Repair and Plan Set B – Remove and Replace Finger Joints	
PHASE	ALLOTTED DAYS
PHASE 1 Total	120
PHASE 1B – US, PP 0 Only	60
PHASE 2 Total	120
PHASE 2B – DS, PP 0 Only	60

Contrary to Kentucky Transportation Cabinet Department of Highways Standard Specifications for Road and Bridge Construction, Edition of 2019 (Standard Specifications), Section 108.07.03, the Engineer will begin charging calendar days for a bridge on the day the Contractor sets up long term traffic control. The Engineer will charge calendar days, regardless of holidays or seasonal weather limitations.

II. LIQUIDATED DAMAGES AND PENALTIES.

Liquidated damages will be assessed the Contractor in accordance with the Standard Specifications, Section 108.09, when either the allotted number of calendar days or the completion dates are exceeded.

PLAN SETS A and B work items completed by April 15, 2026 at 5:00 AM

Contrary to the Standard Specifications, liquidated damages will be assessed to the Contractor during the months of December, January, February and March when the contract time has expired. Contract time will be charged during these months.

All construction must be completed in accordance with the weather limitations specified in Section 606 and/or Section 601 as applicable. No extension of Contract time will be granted due to inclement weather or temperature limitations that occur due to starting work on the Contract or a structure late in the construction season.

In addition to the project liquidated damages as specified in Sections 108.09 and 112.03.15A of the Standard Specifications, each of the following penalties will apply cumulatively:

- (A) Penalties of \$25,000 per day for each day exceeding the allotted closure calendar days in Phase 1, 1A, 2, and 2A collectively.
- (B) Penalties of \$10,000 per day for each day exceeding the allotted PLAN SETS A and B completion date.

SPECIAL NOTE FOR TRAFFIC CONTROL ON BRIDGE REPAIR CONTRACTS

1. TRAFFIC CONTROL GENERAL

Except as provided herein, traffic shall be maintained in accordance with Kentucky Transportation Cabinet Department of Highways Standard Specifications for Road and Bridge Construction, Edition of 2019 (Standard Specifications), Section 112.

Contractor shall develop their own maintenance of traffic (MOT) plan that conforms to this note, the MOT scheme shown in the plan set and in accordance with the Manual for Uniform Traffic Control Devices (MUTCD), Current Edition, INDOT Specifications 105, 107 and 801, and requirements in the INDOT Interstate Highway Congestion Policy Exception Request (IHCP). The MOT shall be approved by the District prior to implementation. All lane closures must be approved by the Engineer at least 14 days prior to the closure.

Contrary to Standard Specifications, Section 106.01, traffic control devices used on this project may be new or used in new condition, at the beginning of the work and maintained in like new condition until completion of the work.

The posted speed limit in Indiana is 60 MPH and it will be reduced by 10 MPH in the immediate vicinity of the work zone area (see the attached "TEMPORARY WORKSITE SPEED LIMIT SIGN ASSEMBLY" for Special requirements of State of Indiana). The extent of these areas within the project limits will be restricted to the proximity of actual work areas as determined by the Engineer. Reduced speed limits and double fine zones will be in place only while lane closures or restricted lane and/or shoulder widths are in place. Notify the Engineer a minimum of 12 hours prior to using the double fine signs. Erect dual mounted "WARNING FINE DOUBLED IN WORK ZONE" signs at the beginning of the work zone. At the end of the work zone, erect dual mounted "END DOUBLE FINE" signs. Remove or cover the double fine signs when the highway work zone does not have workers present for more than a 12-hour period of time. Erect all required signs at the locations shown on the MOT Plan or as directed by the Engineer. Traffic Control will need to be coordinated with Indiana Department of Transportation.

2. ITEMS OF WORK

The various work items associated with this contract are identified in Plan Sets A, B and C.

Plan Set A – Addresses the repair of the truss bearings at Piers 1L and 6L.

Plan Set B – Addresses the finger joint replacement at PP's 0, 23, 23', and 0'.

The MOT identified includes all work items in Plan Set A in Plan Set B.

3. COORDINATION OF WORK

See Special Note for Coordination of Work.

4. OPENING WORK AREA TO TRAFFIC

The Contractor may open the new finger joints to traffic when concrete reaches the minimum break strength listed below and as approved by the Engineer.

MINIMUM BREAK STRENGTH OF 3,000 PSI : AT PP 0

MINIMUM BREAK STRENGTH OF 4,000 PSI : AT PP 23, PP 23', PP 0'

5. TRAFFIC COORDINATOR

Furnish a Traffic Coordinator as per Standard Specifications Section 112. The Traffic Coordinator shall inspect the project maintenance of traffic, at least three times daily, or as directed by the Engineer, during the Contractor's operations and at any time a lane closure is in place. The personnel shall have access on the project to a radio or telephone to be used in case of emergencies or accidents.

The Traffic Coordinator shall report all incidents throughout the work zone to the Engineer on the project. The Contractor shall furnish the name and telephone number where the Traffic Coordinator can be contacted at all times.

6. MOT RESTRICTIONS

No lane closures shall be allowed during the following events, unless noted otherwise, or as directed by the Engineer:

PLAN SETS A and B: MOT closure assumes shoulder plus three adjacent lanes.

- (a) Contrary to the National Holidays identified in Section 101 of the Standard Specifications, the MOT may remain in place, except Phase 1B and 2B are not allowed during the holidays, as directed by the Engineer.
- (b) KY Derby Festival Events – exact dates as directed by the Engineer. No lane closures allowed. (April 17, 2025 at 5:00am through May 4, 2025 at 11:59pm)
- (c) Kentucky State Fair – exact dates as directed by the Engineer.
(August 14, 2025 at 5:00am through August 24, 2025 at 11:59pm) – During this time must maintain 2 lanes of traffic at PP0 to access I-65 SB mainline. Phase 2B is not allowed.

7. SIGNS

The contractor is responsible for all signage during construction. The contractor shall adhere to the standard drawings and MUTCD for guidance. If, at any time, the engineer requests a change in the maintenance of traffic signage, the contractor shall implement the change within 8 hours. Failure to implement these changes within the required eight hours will result in penalties of \$5,000 per day. Signs should be placed no sooner than two weeks prior to placing lane closures. All installations shall be approved by the Engineer.

Additional traffic control signs in addition to normal lane closure signing detailed on the standard drawings or MUTCD may be required by the Engineer. Signage for reduced speed limits and double fine work zones will be furnished, relocated, and maintained by the contractor.

Contrary to section 112, individual signs will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged signs or signs directed to be replaced by the Engineer due to poor legibility or reflectivity will not be measured for payment.

Temporary signs include, but are not limited to, those shown in KYTC standard drawings TTC-120, TTC-145, TTC-146, TTD-130, and TTD-120-03. Contractor shall sign all closures in accordance with the MOT drawings and the current edition of the approved MUTCD.

The Contractor is to install warning signs for wide loads in advance of the bridge under the direction of the Engineer. Wide load detours will not be established on this project. Provide for passage of wide loads up to 16 feet. Wide loads may use a portion of the shoulder to allow for passage.

Remove or cover sign panels with conflicting direction guidance information on overhead sign trusses before and on the Kennedy Bridge, as directed by the Engineer.

8. ARROW PANELS

Use arrow panels as shown on the Standard Drawings or as directed by the Engineer. Flashing arrows will be paid for once, no matter how many times they are moved or relocated. The Department WILL NOT take possession of the flashing arrows upon completion of the work.

9. PAVEMENT MARKINGS

Temporary striping will be paid for by the appropriate bid items. Permanent and Temporary Striping shall be in accordance with Standard Specifications Section 112, except that:

1. Temporary Striping shall be 6" removal tape-white or yellow; and
2. Edge lines will be required for temporary striping; and
3. Temporary or permanent striping shall be in place before a lane is opened to traffic; and
4. Permanent striping shall be 6" water-based paint.

After all work is completed, or when approved by the Engineer, remove temporary striping, place permanent striping back to its original configuration, and repair any damaged inlaid pavement markers. Mobile operations may be utilized. This work item will be considered incidental to the "Maintain and Control Traffic" bid item. Any striping removal (temporary or permanent) shall be removed by water blasting. Water blasting will be considered incidental to the "Maintain and Control Traffic" bid item.

Conflicting pavement skip lines and/or solid lines through the length of the MOT for lane closures and other striping, as directed by the Engineer, shall be temporarily covered with 8" black removable tape.

The Contractor shall replace any temporary striping that becomes damaged or fails to adhere to the pavement before dark on the day of notification. Penalties shall be assessed to the Contractor at a rate of \$500 per day for failing to replace temporary striping within this time limit.

Cover or remove/replace conflicting Interstate shields in pavement to satisfaction of the Engineer.

10. PROJECT PHASING

This project has allotted calendar days for each phase. See Special Note for Contract Completion and Liquidated Damages.

Lane closures are required for the project. The minimum lane width shall be 12-feet.

Night work is allowed on this project. The method of lighting must be submitted to and approved by the Engineer prior to use.

PLAN SETS A and B:

MOT assumes closure of shoulder and adjacent three lanes or as approved by the Engineer.

BEFORE PHASE 1: Temporary Shoulder and Single Lane Closures will be permitted before the Phase 1 lane closure setup. The intent of these lane closures is to obtain field measurements for fabrication of new steel plates, new finger plate assemblies, new truss bearing components and temporary hold downs, as applicable.

The Contractor may install the temporary hold down components and install temporary jacking support. DO NOT stress the hold down threaded rods or engage the jack support system until the Phase 1 setup is installed. No other construction activities are permitted during this time, unless approved by the Engineer.

These short-term single lane closures will not count against the allotted number of calendar days for Phase 1 or Phase 2.

PHASE 1: For Phase 1A and 1B, close the three lanes (lanes 4 to 6) and shoulder on the west side of the Kennedy Bridge.

Phase 1A: Place the lane closure to panel point 14. The three open lanes will consist of two for SB I-65, and one for the I-64 and I-71 Ramps.

Phase 1B: Extend lane closure to Pier 1L (PP 0) to complete work activities at PP 0. The three open lanes will consist of two for SB I-65, and one for the I-64 and I-71 Ramps. During Phase 1B, close the I-64 WB ramp. After completion of Phase 1, immediately switch traffic control and begin Phase 2.

PHASE 2: For Phase 2A and 2B, close the three lanes (lanes 1 to 3) and shoulder on the east side of the Kennedy Bridge.

Phase 2A: Place the lane closure to panel point 14. The three open lanes will consist of one for SB I-65, one for the SB I-65 / I-64 and I-71 Ramps, and one for I-64 WB Ramp.

Phase 2B: Extend lane closure to Pier 1L (PP 0) to complete work activities at PP 0. The three open lanes will consist of one for SB I-65, one for the I-64 and I-71 Ramps, and one for I-64 WB Ramp.

After completion of Phase 2, or when approved by the Engineer, remove the traffic control setup and restripe the roadway to its original configuration, as directed by the Engineer.

Lane closures shall be in accordance with the MUTCD and Standard Drawings TTC-120, TTC-115 or TTC-125. Long term closures are allowed for all Plan Sets. Contrary to section 112, lane closures will NOT be measured for payment, but are considered incidental to "Maintain and Control Traffic".

During Phase 1 activities the entrance ramp from Court/Clark Streets may be closed.

11. PORTABLE CHANGEABLE MESSAGE SIGNS

The Portable Changeable Message Signs are being bid independently of the Queue Warning System and shall be used as directed by the Engineer. The contractor shall provide a minimum of three (3) Portable Changeable Message Signs located in advance of, or on the project at locations to be determined by the Engineer, at each phase of the project. Portable Changeable Message Signs shall be in operation at all times, during all lane closures and for the immediate seven (7) days prior to the closure. The message required to be provided shall be designated by the Engineer. In the event of damage or mechanical/electrical failure, the Contractor shall repair or replace the Portable Changeable Message Sign within 24 hours. PCMS will be paid for once, no matter how many times they are moved or relocated. The Department WILL NOT take possession of the PCMS upon completion of the work.

See the attached "SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS".

12. TEMPORARY CONCRETE BARRIERS

All long-term lane closures for Phases 1A, 1B, 2A and 2B shall require temporary Concrete Barrier Walls Type 9T (NO ALTERNATIVES ALLOWED). Temporary Concrete Barriers shall remain the Contractor's property and shall be removed from the construction site upon completion of construction. The temporary concrete barriers shall be in accordance with Standard Drawings RBM-115, RBM-120, and RBM-020.

13. DETOUR

During Phase 1B and closure of the I-64 WB ramp (Exit 137), the traffic control plan proposed by the contractor shall include a signed detour route for the ramp closure. See the attached Proposed Detour Route.

The traffic control plan along with the proposed detour plan will be delivered to the engineer 7 days prior to the pre-construction meeting. The proposed detour route shall meet the following:

- 1) An alternate detour route may be submitted to the Engineer for written approval.

- 2) The contractor must coordinate with other projects along the detour route to avoid ongoing construction projects along those routes.
- 3) The contractor must coordinate detour and schedule with the Engineer.
- 4) Sign detour per the standard drawings and MUTCD.

The traffic control plan must be submitted and approved to allow for coordination of the public information officer with the closure notification. The public must be notified of the proposed detour route when they are notified of the closure, 2 weeks before closure. All time and expenses necessary for the development of the detour plan(s) will be incidental to the lump sum bid item "Maintain and Control Traffic".

14. BARRELS

Barrels are to be used for channelization and tapers at locations not requiring temporary concrete barrier. Replacements for damaged barrels directed by the Engineer to be replaced due to poor condition or reflectivity. Barrels shall be incidental to the "Maintain and Control Traffic" bid item.

15. TEMPORARY LANE CLOSURES.

Temporary Shoulder and Single Lane Closures will be permitted before the Phase 1 lane closure setup. All closures shall be in accordance with Standard Drawing TTC-115. The closures will be determined by the Engineer. All closures shall be removed when not working. Contrary to section 112, lane closures will NOT be measured for payment, but are considered incidental to "Maintain and Control Traffic".

16. PORTABLE QUEUE WARNING ALERT SYSTEM (PLAN SETS A & B)

During work for PLAN SETS A & B, the contractor shall provide a portable, quickly deployable, real-time automated ITS queue warning alert system (PQWAS). The Contractor shall also provide the maintenance of the complete system for the duration of the project or as directed by the Project Engineer. The Department is willing to look at different technologies (i.e. allow the use of crowd sourcing data to be used in lieu of the portable radar sensors). Any changes must be submitted and approved by the Engineer.

The queue protection vehicles are intended to be used during morning and evening peaks, or as directed by the Engineer.

See the attached "SPECIAL NOTE FOR PORTABLE QUEUE WARNING ALERT SYSTEM".

17. LAW ENFORCEMENT OFFICERS (LEO'S)

Police support shall be a unit consisting of an off-duty police officer from any police force agency having lawful jurisdiction and police car equipped with externally mounted flashing blue lights. Officers may be asked to issue citations for traffic violations but will be considered incidental to the contract unit bid price for "Law Enforcement Officer". No additional compensation will be provided. The officers will be placed at the discretion of the Engineer. Police support will be measured on a per hour basis for each officer and each police vehicle.

18. PAYMENT

Unless listed as a bid item, payment will only be made for the following items:

1. **Portable Changeable Message Boards** – Each
2. **Conc. Barrier Wall, Type 9T** – LF
3. **Relocate Temp. Conc. Median Barrier** - LF
4. **Maintain and Control Traffic**- Lump Sum
5. **Portable Queue Warning Alert System** – Month
6. **Queue Warning PCMS** – Month
7. **Queue Warning Portable Radar Sensors** – Month
8. **Queue Protection Vehicle** – Hour
9. **Furnish Queue Protection Vehicles** – Month
10. **Pave Striping**-Temp Rem Tape-B – LF
11. **Pave Striping**-Temp Rem Tape-W – LF
12. **Pave Striping**-Temp Rem Tape-Y – LF
13. **Arrow Panel** – Each
14. **Temporary Signs** – SF
15. **Pave Striping** – DUR TY 1 – 6 IN W
16. **Pave Striping** – DUR TY 1 – 6 IN Y
17. **Delineator for Barrier** – White – Each
18. **Delineator for Barrier** – Yellow – Each
19. **Law Enforcement Officers** - Hour

All other items needed to maintain traffic in accordance with these contract documents and the approved traffic control plan shall be considered incidental to Maintain and Control Traffic. These items include, but are not limited to barricades, TMA trucks, temporary guardrail, cones, barrels, flaggers, etc.

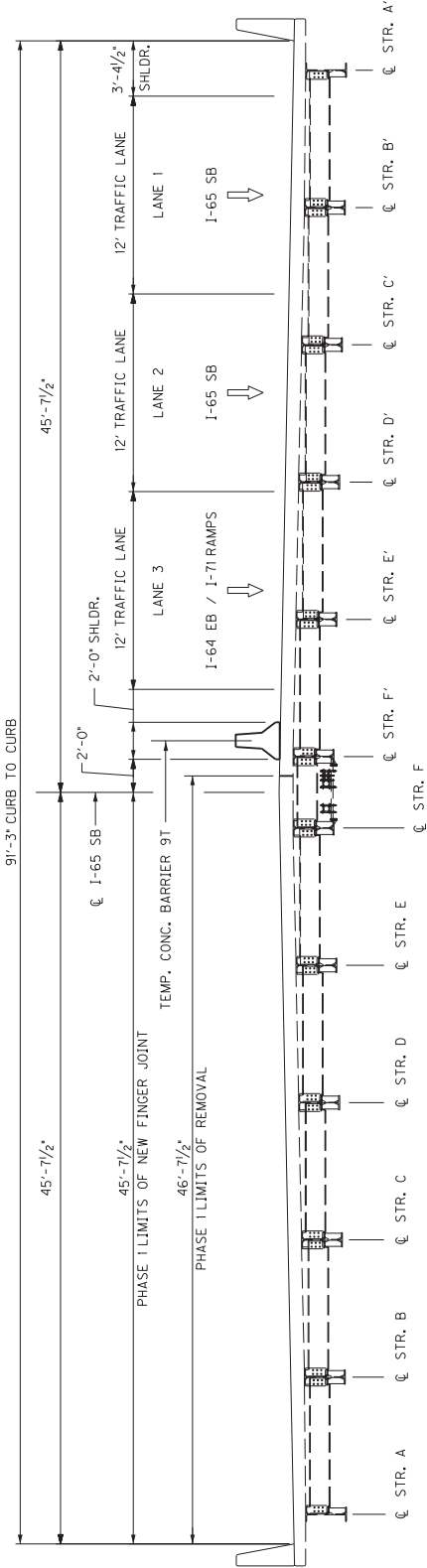
PHASE 1A: CLOSE WEST SIDE (LANES 4, 5, AND 6) OF BRIDGE TO TRAFFIC, UP TO PANEL POINT 14.

PHASE 1B: EXTEND LANE CLOSURES THROUGH PIER 1L (PP 0) TO COMPLETE FINGER JOINT REPLACEMENT AND TRUSS BEARING WORK AT PIER 1L (PP 0).

AFTER COMPLETING THE WORK AT PIER 1L (PP 0), REDUCE LANE CLOSURE BACK TO PANEL POINT 14, IF CONTINUING TO WORK ON THE OTHER FINGER JOINT LOCATIONS.

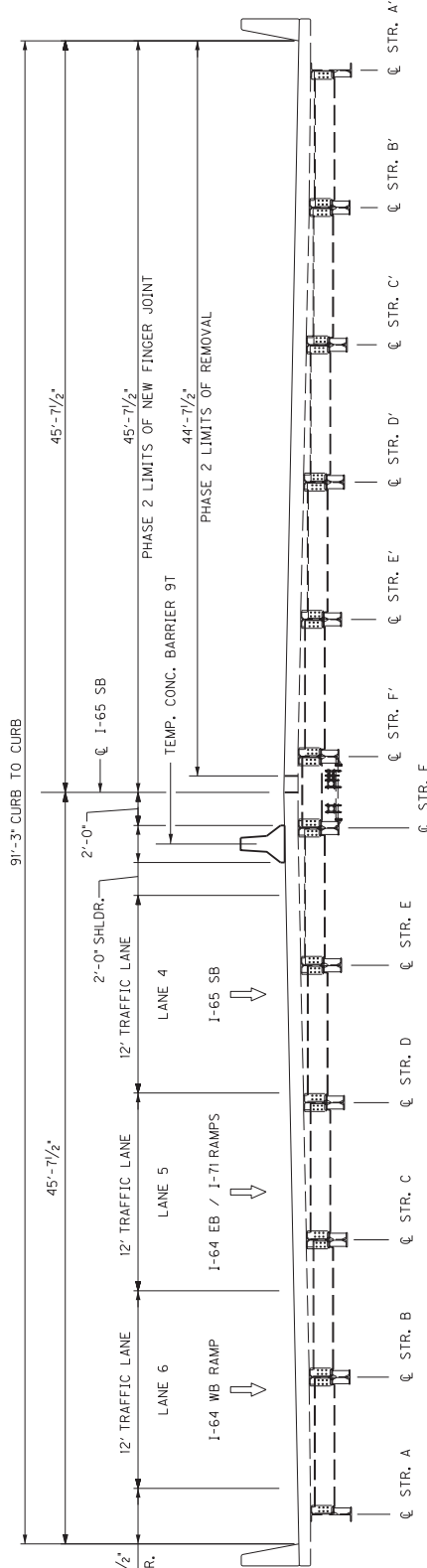
PRESENTED IS ONE SUGGESTED ORDER OF WORK FOR PHASE 1. THE CONTRACTOR HAS THE OPTION TO SELECT THE ORDER OF WORK. NOTIFY THE ENGINEER IN WRITING.

SWITCH MAINTENANCE OF TRAFFIC TO PHASE 2 IMMEDIATELY FOLLOWING PHASE 1, AS DIRECTED BY THE ENGINEER. COORDINATE PHASE 2 CLOSURE WITH THE I-65 CENTRAL CORRIDOR PROJECT. PHASE 2B TO COINCIDE WITH THE I-65 PROJECT FULL CLOSURE.



PHASE 1A & PHASE 1B

(LOOKING NORTH)



PHASE 2A & PHASE 2B

(LOOKING NORTH)

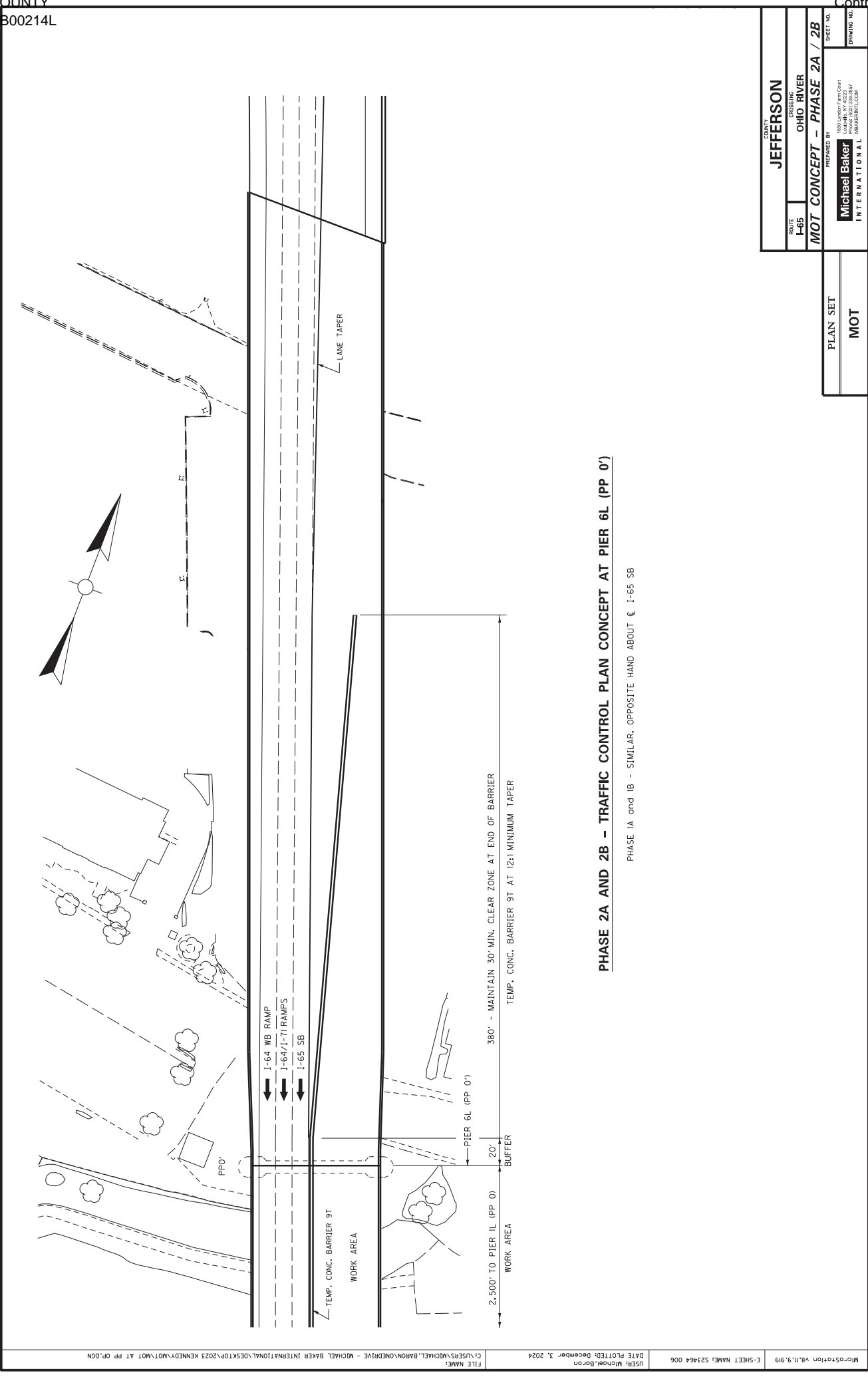
PHASE 2A: CLOSE EAST SIDE (LANES 1, 2, AND 3) OF BRIDGE TO TRAFFIC, UP TO PANEL POINT 14.

PHASE 2B: EXTEND LANE CLOSURES THROUGH PIER 1L (PP 0) TO COMPLETE FINGER JOINT REPLACEMENT AND TRUSS BEARING WORK AT PIER 1L (PP 0).

AFTER COMPLETING THE WORK AT PIER 1L (PP 0), REDUCE LANE CLOSURE BACK TO PANEL POINT 14, IF CONTINUING TO WORK ON THE OTHER FINGER JOINT LOCATIONS.

PRESENTED IS ONE SUGGESTED ORDER OF WORK FOR PHASE 2. THE CONTRACTOR HAS THE OPTION TO SELECT THE ORDER OF WORK. NOTIFY THE ENGINEER IN WRITING.

REMOVE MAINTENANCE OF TRAFFIC AND RESTORE TRAFFIC LANES TO ORIGINAL LAYOUT.



COUNTY	
JEFFERSON	
ROUTE	ORDERING
I-65	OHIO RIVER
MOT CONCEPT - PHASE 2A / 2B	
PREPARED BY	SHEET NO.
Michael Baker INTERNATIONAL	1650 Union Farm Court Louisville, KY 40228 502.261.1000 MBINTERNL.COM
DRAWING NO.	

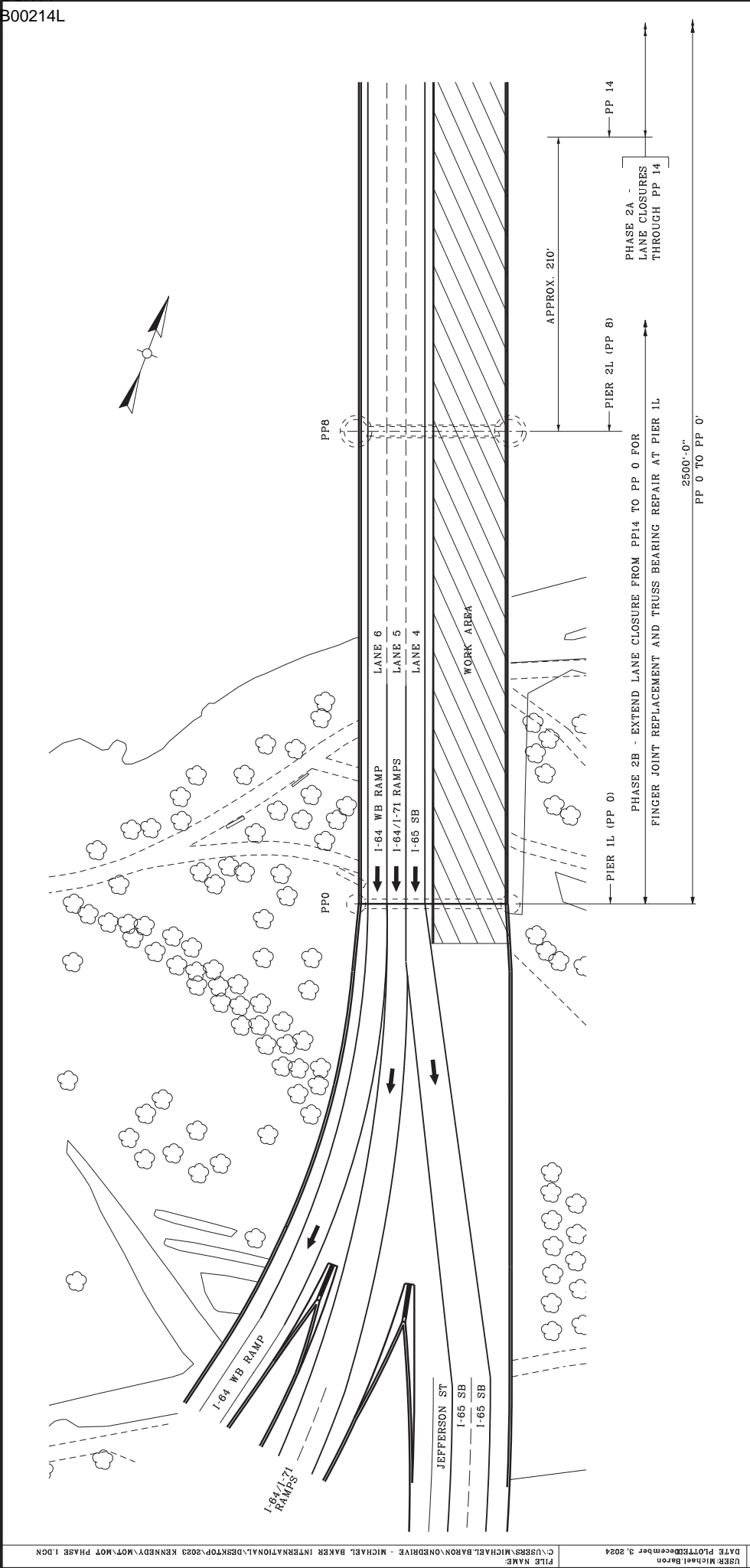
PLAN SET
MOT



NOTES

1. I-64 WB RAMP IS PERMITTED TO BE CLOSED DURING PHASE 1B WORK.

JEFFERSON		COUNTY	
ROUTE	I-65	CROSSING	
MOT CONCEPT - PHASE 1A / 1B			
PLAN SET		PREPARED BY Michael Baker INTERNATIONAL 1651 Avenue Park Drive Louisville, KY 40220 502.496.1000 WWW.MBI-INTL.COM	
MOT			



PHASE 2A AND 2B - TRAFFIC CONTROL PLAN CONCEPT

NOTE:
1. MAINTAIN TRAFFIC ACCESS FOR I-65 SB AND JEFFERSON STREET DURING PHASE 2A.
2. DURING PHASE 2B, CLOSE THE JEFFERSON STREET ACCESS.

COUNTY		JEFFERSON	
ROUTE	I-65	CROSSING	OHIO RIVER
MOT CONCEPT - PHASE 2A / 2B			
PLAN SET		PREPARED BY	
MOT		Michael Baker INTERNATIONAL	

Special Note for Portable Queue Warning Alert System

1.0 Description

This item shall consist of furnishing, installing, relocating, operating, servicing, and removing various components of a portable, quickly deployable, real-time automated ITS queue warning alert system (PQWAS), in accordance with the standard specifications and this special provision. The Contractor shall also provide the maintenance of the complete system for the duration of the project or as directed by the Project Engineer. The Department is willing to look at different technologies (i.e. allow the use of crowd sourcing data to be used in lieu of the portable radar sensors). Any changes to the below requirements must be submitted and approved by the Engineer.

2.0 Materials

Materials shall be in accordance as follows:

All materials used shall meet the manufacturer's specifications and recommendations.

All PQWAS materials installed on the project shall be provided by the Contractor in excellent quality condition, shall be corrosion resistant and in strict accordance with all of the details shown within Contractor's Plans approved by KYTC. The Contractor shall maintain an adequate inventory of parts and replacement units to support maintenance and repair of the PQWAS. Pre-deployment is a condition of the system's acceptance and is based on the successful performance demonstration for a (5) day continuous period in accordance to this specification and as set forth in the plans. Ensure compliance to all FCC and Department specifications.

The Contractor shall maintain this system and shall be locally available to service and maintain system components, move portable devices as necessary and respond to emergency situations. The Contractor has oversight responsibility for directing placement of devices in the project area. The Contractor is to be accessible seven (7) days a week and twenty-four (24) hours a day while the system is deployed. The Contractor shall provide contact information for the system's coordinator and others responsible for maintenance of the system prior to installation of the system. Furnish a System Coordinator for monitoring the PQWAS throughout all periods of deployment.

A. General Capabilities and Performance Requirements

1. Overall PQWAS capabilities and performance requirements include the following:
 - a. Furnish a system capable of providing advance traffic information to motorists when there is a queueing of traffic due to congestion resulting from lane reductions, emergency events or other conditions. The condition-responsive notification to the motorist occurs with the use of Portable Changeable Message Signs (PCMS) in accordance to the below capabilities and performance requirements, activated through real-time traffic data collected downstream of the PCMS locations. This equipment must

be a packaged system, pre-programmed and operates as a stand-alone PQWAS meeting this specification. Conditions might exist that require relocation of the portable sensors at any given time, the sensors shall be portable and shall not require re-calibration in the field for fast deployments. Due to the potential need to replace damaged sensors or to change the position of one or more sensors at any given time, sensors must be interchangeable and relocatable by an unskilled laborer. The system must continue to function if as many as half the sensors fail to function.

- b. Provide a PQWAS that consists of the following field equipment: portable radar sensors and portable changeable message signs (PCMS). Provide a system capable of withstanding inclement weather conditions while continuing to provide adequate battery power. The portable radar sensor battery, in a stand-alone state and without a solar panel for recharging, shall be capable of keeping power and capable of sending data for (10) consecutive days or longer. The system shall notify drivers of real-time queue events via specifically placed PCMS units up stream of the work zone. All predetermined/preprogrammed messages are to be approved by KYTC. The number and location of portable radar sensors and PCMS units shall be as directed by the Project Engineer. The decision to deploy or relocate field equipment is made by the Project Engineer and instrumented through the System Coordinator. The decision for equipment removal is made by the Project Engineer after work is complete. The sensors and PCMS units shall be identifiable via global positioning system (GPS) and shall contain an accelerometer to detect and alert of unauthorized movement.
- c. The portable radar sensor shall be capable of collecting traffic speed data. The processed data is used to remotely control PCMS units to display user definable, Engineer approved and locally stored messages. The message trigger state thresholds for slow and stopped speeds shall be user configurable and revisable in less than {1} hour from the Project Engineer's request. Weekly Traffic Data Reports shall be presented to the Project Engineer and shall include speed data per sensor location, travel times, and queue lengths in graphical and numerical formats. In the event the Project Engineer requires a report, other than a weekly report, for any reason; then the Contractor shall provide report within (48) hours of request. Unlimited data reports shall be included within price of system. Sensors shall require no calibration adjustments in the field. Sensor should begin transmitting data within (30) seconds of being turned on. Satellite (SAT) communications will be required when cellular service does not provide continuous communications. Contractor shall identify the most trustworthy cellular provider within the project area.
- d. Data shall be accessible through a website and the Contractor shall provide a username and password for protection. The website shall be accessible seven (7) days a week and twenty - four (24) hours a day. The website shall provide historical & real-time data in graphical and numerical formats and shall have the capability of being integrated within the Department's Traffic Management Center (if requested). The website should be compatible to most hand held devices. Data shall be saved on the manufacturer's network for up to (5) years from the deployment date of system and shall be provided at the request

of the Department at any time within the (5) year window. The use of the website shall be included within the price of system.

- e. Warning Alerts: queue events, low battery voltage warnings, sensor movement alerts, high and low speed alerts shall be provided via cellular text messaging and/or via email messaging at the request of select Contractor personnel and KYTC officials.
- f. The PQWAS system shall have the capabilities to provide alternate route messaging on specifically placed portable changeable message units and/or fixed Variable Message Systems (VMS). The intent of this service is to provide alternate route messaging to motorists before entering the project limits from all directions and giving them appropriate time to adjust their routes. Alternative routes shall be predefined and approved by KYTC. Additional PCMS units may be required for alternate route messaging and will be as per Section 5.0 of this note. KYTC's Traffic Management Center will provide detour messages via fixed VMS units during the term of the project.

B. Portable Radar Sensor Capabilities and Performance Requirements

The PQWAS shall include portable radar sensors (PRD) to monitor and detect queue events.

- 1. The Radar Sensor shall be FHWA accepted to meet NCHRP 350 test requirements
- 2. The Radar Sensor shall be locatable at all times via an internal Global Positioning System (GPS) and shall be capable of Cellular or SAT Communications.
- 3. The Radar Sensor shall have a dry-cell battery capable of powering the system for (10) consecutive days or longer
- 4. The Radar sensor shall be K-Band technology and have a line of sight up to 200 linear feet without obstruction
- 5. The Radar sensor shall have the ability to be charged in the field through adaptable solar recharging technology in the case the sensor is utilized for more than 10 consecutive days

C. PCMS Capabilities and Performance Requirements

The PQWAS shall include portable changeable message signs (PCMS) designated to relay automated messaging of queue events, alternate route messages, and caution for the work area defined by the project limits. PCMS placements shall meet the requirements set forth by the Cabinet in each direction of the National Highway System (NHS).

- 1. The PCMS unit shall be a Full Matrix 24 rows x 50 columns and shall be capable of 1 line, 2 line or 3 line messages
- 2. The PCMS unit shall be legible from a distance over twelve hundred feet (1200')
- 3. The height and size of characters shall be 18" to 58"
- 4. The PCMS shall be capable of storing up to 199 pre-programmed messages and up to 199 user-defined messages
- 5. The PCMS shall have a weather tight control cabinet with back lit LCD handheld controller.
- 6. The PCMS shall utilize a hydraulic lift to raise the unit to display height
- 7. The PCMS unit shall include solar recharging ports to allow for recharging of the portable radar sensors when they are not deployed.
- 8. The PCMS shall be NTCIP compliant and shall have an active Modem with active cellular service.

9. The user shall have the ability to communicate and override the PCMS remotely in the event of an emergency, Amber Alert, etc.
10. The PCMS unit shall have a docking station to include safety rails that allow a commercial safety strap to tie down the portable radar sensors while in transport. The docking station shall hold-up to (4) sensors safely and securely at all times

3.0 Construction Requirements

All communication costs include cellular telephone services, FCC licensing, wireless data networks, satellite and internet subscription charges, and battery charging and maintenance. Additional to these requirements, the Contractor shall assume all responsibility for any and all damaged equipment due to crashes, vandalism, and adverse weather that may occur during the contract period.

The PQWAS shall operate continuously (24 hours/ 7 Days) when deployed on the project. The system is in a constant "data collection" mode when deployed. The Contractor shall provide technical support for the PQWAS for all periods of operation.

In the event communication is lost with any component of the PQWAS, provide a means and staff to manually program a PCMS message. If communication is lost for more the 10 consecutive minutes, the system shall revert to a fail-safe ROADWORK/# MILES/AHEAD message displayed on the PCMS units until communication is restored.

System Operator, local control function and remote management operation must be password protected.

The PQWAS shall be capable of acquiring traffic information and selecting messages automatically without operator intervention after system utilization. The lag time between changes in threshold ranges and the posting of the appropriate PCMS message(s) shall be no greater than (60) seconds. The system operation and accuracy must not be appreciably degraded by inclement weather or degraded visibility conditions including precipitation, fog, darkness, excessive dust, and road debris.

The system shall be capable of storing ad-hoc messages created by the System Coordinator and logging this action when overriding any default or automatic advisory message.

The PQWAS communication system shall incorporate an error detection/correction mechanism to insure the integrity of all traffic conditions data and motorists information messages. Any required configuration of the PQWAS communication system shall be performed automatically during system initialization.

The system's acceptance is based on the successful performance demonstration of PQWAS for a (5) day continuous period in accordance to this specification and as set forth in the plans. Ensure compliance to all FCC and Department specifications.

4.0 Equipment Maintenance.

Maintain system components in good working condition at all times. Repair or replace damaged or malfunctioning components, at no cost to the Department, as soon as possible and within (12) hours of notification by the Engineer. Periodically clean PCMS units if necessary.

5.0 Measurement. The Department will measure each item below in Months. For partial months the Department will pay in 0.25 increments based on the number of calendar days in the below table.

Partial Month Payment Schedule	
Days	Increment
0-7 days	0.25
8-14 days	0.50
15-21 days	0.75
22-31 days	1.00

5.1 Portable Queue Warning Alert System includes cellular (SAT communications will be required if cellular is not available), all supporting field equipment, website, and unlimited data reports accessible by the Engineer. It will be measured by the number of months authorized by the Engineer for use on the project.

5.2 Queue Warning PCMS will be measured by each individual unit multiplied by the number of months authorized by the Engineer for use on the project.

5.3 Queue Warning Portable Radar Sensors will be measured by each individual unit multiplied by the number of months authorized by the Engineer for use on the project. Queue Warning Portable Radar Sensors will not be measured for payment if the Contractor utilizes a system operating on crowd sourcing data. Crowd sourcing data systems will only be allowed as approved by the engineer and will be considered incidental to Portable Queue Warning Alert System.

6.0 Payment.

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
26136EC	Portable Queue Warning Alert System	Month
26137EC	Queue Warning PCMS	Month
26138EC	Queue Warning Portable Radar Sensors	Month

SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS

This Special Note will apply when indicated on the plans or in the proposal.

1.0 DESCRIPTION. Furnish, install, operate, and maintain variable message signs at the locations shown on the plans or designated by the Engineer. Remove and retain possession of variable message signs when they are no longer needed on the project.

2.0 MATERIALS.

2.1 General. Use LED Variable Message Signs Class I, II, or III, as appropriate, from the Department's List of Approved Materials.

Unclassified signs may be submitted for approval by the Engineer. The Engineer may require a daytime and nighttime demonstration. The Engineer will make a final decision within 30 days after all required information is received.

2.2 Sign and Controls. All signs must:

- 1) Provide 3-line messages with each line being 8 characters long and at least 18 inches tall. Each character comprises 35 pixels.
- 2) Provide at least 40 preprogrammed messages available for use at any time. Provide for quick and easy change of the displayed message; editing of the message; and additions of new messages.
- 3) Provide a controller consisting of:
 - a) Keyboard or keypad.
 - b) Readout that mimics the actual sign display. (When LCD or LCD type readout is used, include backlighting and heating or otherwise arrange for viewing in cold temperatures.)
 - c) Non-volatile memory or suitable memory with battery backup for storing pre-programmed messages.
 - d) Logic circuitry to control the sequence of messages and flash rate.
- 4) Provide a serial interface that is capable of supporting complete remote control ability through land line and cellular telephone operation. Include communication software capable of immediately updating the message, providing complete sign status, and allowing message library queries and updates.
- 5) Allow a single person easily to raise the sign to a satisfactory height above the pavement during use, and lower the sign during travel.
- 6) Be Highway Orange on all exterior surfaces of the trailer, supports, and controller cabinet.
- 7) Provide operation in ambient temperatures from -30 to + 120 degrees Fahrenheit during snow, rain and other inclement weather.
- 8) Provide the driver board as part of a module. All modules are interchangeable, and have plug and socket arrangements for disconnection and reconnection. Printed circuit boards associated with driver boards have a conformable coating to protect against moisture.
- 9) Provide a sign case sealed against rain, snow, dust, insects, etc. The lens is UV stabilized clear plastic (polycarbonate, acrylic, or other approved material) angled to prevent glare.
- 10) Provide a flat black UV protected coating on the sign hardware, character PCB, and appropriate lens areas.
- 11) Provide a photocell control to provide automatic dimming.

11

- 12) Allow an on-off flashing sequence at an adjustable rate.
- 13) Provide a sight to aim the message.
- 14) Provide a LED display color of approximately 590 nm amber.
- 15) Provide a controller that is password protected.
- 16) Provide a security device that prevents unauthorized individuals from accessing the controller.
- 17) Provide the following 3-line messages preprogrammed and available for use when the sign unit begins operation:

/KEEP/RIGHT/⇒⇒⇒/	/MIN/SPEED/**MPH/
/KEEP/LEFT/⇐⇐⇐/	/ICY/BRIDGE/AHEAD/ /ONE
/LOOSE/GRAVEL/AHEAD/	LANE/BRIDGE/AHEAD/
/RD WORK/NEXT/**MILES/	/ROUGH/ROAD/AHEAD/
/TWO WAY/TRAFFIC/AHEAD/	/MERGING/TRAFFIC/AHEAD/
/PAINT/CREW/AHEAD/	/NEXT/**/MILES/
/REDUCE/SPEED/**MPH/	/HEAVY/TRAFFIC/AHEAD/
/BRIDGE/WORK/**0 FT/	/SPEED/LIMIT/**MPH/
/MAX/SPEED/**MPH/	/BUMP/AHEAD/
/SURVEY/PARTY/AHEAD/	/TWO/WAY/TRAFFIC/

*Insert numerals as directed by the Engineer.

Add other messages during the project when required by the Engineer.

2.3 Power.

- 1) Design solar panels to yield 10 percent or greater additional charge than sign consumption. Provide direct wiring for operation of the sign or arrow board from an external power source to provide energy backup for 21 days without sunlight and an on-board system charger with the ability to recharge completely discharged batteries in 24 hours.

3.0 CONSTRUCTION. Furnish and operate the variable message signs as designated on the plans or by the Engineer. Ensure the bottom of the message panel is a minimum of 7 feet above the roadway in urban areas and 5 feet above in rural areas when operating. Use Class I, II, or III signs on roads with a speed limit less than 55 mph. Use Class I or II signs on roads with speed limits 55 mph or greater.

Maintain the sign in proper working order, including repair of any damage done by others, until completion of the project. When the sign becomes inoperative, immediately repair or replace the sign. Repetitive problems with the same unit will be cause for rejection and replacement.

Use only project related messages and messages directed by the Engineer, unnecessary messages lessen the impact of the sign. Ensure the message is displayed in either one or 2 phases with each phase having no more than 3 lines of text. When no message is needed, but it is necessary to know if the sign is operable, flash only a pixel.

When the sign is not needed, move it outside the clear zone or where the Engineer directs. Variable Message Signs are the property of the Contractor and shall be removed from the project when no longer needed. The Department will not assume ownership of these signs.

4.0 MEASUREMENT. The final quantity of Variable Message Sign will be the actual number of individual signs acceptably furnished and operated during

11
the project. The Department will not measure signs replaced due to damage or rejection.

5.0 PAYMENT. The Department will pay for the Variable Message Signs at the unit price each. The Department will not pay for signs replaced due to damage or rejection. Payment is full compensation for furnishing all materials, labor, equipment, and service necessary to, operate, move, repair, and maintain or replace the variable message signs. The Department will make payment for the completed and accepted quantities under the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
02671	Portable Changeable Message Sign	Each

Effective June 15, 2012

Special Note for Traffic Queue Protection Vehicle

1.0 DESCRIPTION. Furnish, Operate, and Maintain Traffic Queue Protection Vehicle at locations and times described herein. The Queue Protection Vehicle is expected to alert motorists (inside and outside of project limits) of all stopped traffic caused by construction activities or incidents within the project limits.

2.0 MATERIALS. The contractor shall provide a minimum of one (1) queue protection vehicle for each traveling direction where traffic flow is reduced or modified in a manner where a queue could occur. One (1) additional queue protection vehicle shall be onsite in reserve. The Traffic Queue Protection Vehicle must fulfill the following minimum requirements:

1. A truck mounted attenuators that meets or exceeds NCHRP TL-3 requirements.
2. Four (4) round yellow strobe lights (with auto-dimmers) positioned rear facing
 - Two (2) mounted under rear bumper
 - Two (2) mounted at cab level
 - Visibility of strobe lights can not be deterred by attenuator
3. One (1) standard cab mounted light bar.
4. A truck mounted message board with a minimum of 3 Lines and 8 Characters per line.
5. Four Hour National Traffic Incident Management (TIM) Responder Training for Queue Truck Operators.

3.0. CONSTRUCTION. A queue will be defined as anytime that traffic traveling through the project is reduced to a speed of twenty (20) miles per hour or less. The following procedures will be followed when a traffic queue occurs until free flow traffic conditions are present:

- The queue protection vehicle shall be positioned no further than ½ mile upstream from the back of the slow moving traffic.
- The queue protection vehicle shall be positioned on the shoulder and clear of the traveled way so as not to impede traffic.
- The queue protection vehicle shall relocate as needed to maintain approximately ½ mile distance from the back of the slow moving traffic.
- The 2nd queue protection vehicle shall be held in reserve, on site, and support the primary vehicle if conditions prevent repositioning by reverse. This vehicle shall not be paid for idle time.
- Queue Protection Vehicles shall be kept in project limits during planned lane closures and other project activities expected to cause a queue. One Queue Protection Vehicle shall remain on the project at all times available to respond to incidents within the project limits in a timely manner.
- Queue length estimates and traffic conditions shall be reported to the KYTC project engineer or designee at the following periods:
 1. At 30 minute intervals
 2. At significant changes
 3. When free flow traffic is achieved
- The KYTC project engineer or designee will document all daily queue reports and provide these logs to the Director of Maintenance and Director of Construction at the end of each month.

The Queue Protection Vehicle shall be mobilized by the Project Engineer or designee for planned construction activities. For unplanned incidents mobilization should be initiated by the first person (KYTC’s or Contractor’s project staff) receiving notification of the queue.

4. MEASUREMENT.

4.01 Queue Protection Vehicle. The Department will measure the time from when the vehicle is in position protecting the queue until either free flow traffic is achieved or the vehicle is no longer protecting the queue, whichever occurs first. Idle time will not be paid. The Department will not measure mobilization, removal, maintenance, labor, fuel, or any additional items but will consider them all incidental to this item of work.

4.02 Furnish Queue Protection Vehicles. The Department will measure the quantity by each month the Engineer requires to have the Contractor furnish vehicles as defined in ‘2.0 Materials’ of this Special Note. The Department will not measure mobilization, removal, labor, fuel, or any additional items but will consider them all incidental to this item of work. Partial Months will be calculated as shown in the table below.

Partial Month Payment Schedule	
Days	Increment
0-7 days	0.25
8-14 days	0.50
15-21 days	0.75
22-31 days	1.00

5. PAYMENT.

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
25075EC	Queue Protection Vehicle	Hour
25117EC	Furnish Queue Protection Vehicles	Month

TEMPORARY WORKSITE SPEED LIMIT SIGN ASSEMBLY

This shall consist of furnishing and placing portable speed limit signs as shown on the plans or as directed in areas of work activity. The worksite speed limit flashing strobe lights shall be activated when the worksite speed limit is in effect. This shall be only where and while work is actually in progress and workers are present. Each strobe light shall be visible through a range of 120 degrees Fahrenheit when viewed facing the sign and shall have a minimum effective luminance of 2100 cd effective according to the manufacturer's literature, which shall be provided to the Engineer prior to use.

Wherever a permanent speed limit sign exists within the limits controlled by the worksite speed limit sign assemblies, additional worksite speed limit sign assemblies shall be placed next to the permanent signs or the permanent signs shall be covered.

The worksite speed limit shall not be used for the entire length of a roadway under construction unless there is actual work activity for the entire length of such roadway. It shall not be activated at the beginning of the day, for the entire day, if actual work is not being done all day in the work area.

The worksite speed zone signage shall be placed and maintained by the Contractor. The worksite speed limit will be 55 mph, or 10 mph below the posted speed limit for the roadway under construction, whichever is lower.

A worksite reduced speed advance warning sign assembly shall be placed in advance of the first worksite speed limit sign assembly when the reduction in speed limit is greater than 15 mph.

The Department will **NOT** measure for payment, but are incidental to "Maintain and Control Traffic", lump sum.

SPECIAL NOTE FOR TEMPORARY SUPPORT

The end supports at L0 and L0' are subject to uplift due to loading of the truss spans. The south approach girders at panel point 0 bear on the truss floorbeam and eliminate the dead load uplift, while panel point 0' is subject to dead load uplift. The live load on the truss spans can produce either positive or negative reaction at the support, depending on location of the load. A hold down pin and link are used to connect the superstructure to a fixed bearing anchored to the substructure (see Figure 1). Steel link plates with large steel pins connect holes in the L0 and L0' gusset plates and bearings. The link plates support the superstructure, provides restraint to vertical loads, including uplift forces due to dead load and live load, and allows thermal movements associated with temperature change.

The proposed retrofit for the truss bearings requires the pin and link system to be bypassed using a temporary support to remove the vertical loads, including uplift, from the bearing to make repairs to the pin and link assembly. The temporary support may be accomplished using various solutions, or combination of solutions, which includes but not limited to Concept A: a hold-down concept (as shown in the plans), Concept B: a counterweight concept which includes loaded vehicles to counteract uplift force at or near the end supports, or Concept C: a counterweight concept to counteract uplift due to dead load and live load as previously used during the ORB project (as shown in the plans). In addition to the temporary support for the uplift forces, a temporary jacking support must be used concurrently at the end floorbeams to accommodate the vertical dead and live loads (as shown in the plans). The floorbeam is to be jacked to remove the floorbeam dead load off the link plate under repair to transfer to the jacking support. Positive bearing reaction due to maintenance of traffic (MOT) live load on the end anchor span must also bypass the link plate and transfer the load to the jacking support.

The Contractor has the option for selecting the temporary support concept. Prior to preparation of working drawings, Contractor shall verify in the field conditions and dimensions as necessary to perform the work. In accordance with Standard Specification 601.03.11, submit to the Engineer for approval, working drawings and applicable calculations. For example, with Concept B, show vehicle placement on the deck and design calculations for counterweight option. Such documents shall be prepared, sealed, and signed by an engineer licensed to practice in the Commonwealth of Kentucky.

See the Plan Set A drawings for additional information.

Figure 1 - DOWNSTREAM TRUSS BEARING AT PIER 1L



SPECIAL NOTE FOR CONTRACT COORDINATION

The Contractor is advised that other projects may be in progress within or in the near vicinity of this project. The traffic control of those projects may affect this project, and the traffic control of this project may affect those projects. The Contractor will coordinate the work on this project with the work of the other contractors. In case of conflict, the Engineer will determine the relative priority to give to work phasing on the various projects. See Section 105.06.

SPECIAL NOTE FOR FINGER EXPANSION JOINTS

These Notes or designated portion thereof, apply where so indicated on the plans, proposals, or bidding instruction.

Description Perform all work in accordance with the Department's current Standard Specifications, and applicable Supplemental Specifications, the attached plans, and these Notes. Section references are to the Standard Specifications.

This work consists of: (1) Perform all work in two phases (2) Furnish all labor, materials, tools and equipment; (3) Remove existing finger joint expansion system and drain trough; (4) Prepare remaining structure and install new finger expansion joint (5) Prepare blockout and place concrete around and under the joint; (6) Finish and cure the new concrete; (7) Maintain and control traffic; (8) Any other work specified as part of this contract

Materials Materials shall conform to the following

- A. Structural Steel. Steel plate, bars and shapes shall conform to AASHTO M270 (ASTM A709), Grade 50. No aluminum components or hardware shall be used. Galvanize steel components in accordance with ASTM A123.
- B. Welded Studs. Welded studs for anchorage purposes shall conform to ASTM A108.
- C. Bolts. Bolts, nuts, and washers shall be in accordance with ASTM F3125, Grade 325, Type 1, ASTM A563 Grade DH and ASTM F436, respectively, and shall be hot-dipped galvanized.
- D. Neoprene Trough and Drainage Collector System. Fabric reinforced drainage trough shall be polychloroprene (Neoprene) of the thickness no less than ¼". Trough shall be reinforced with one or two plies of tightly woven polyester or nylon fabric and shall be supplied and installed in one continuous length. The inside surface of the trough shall be smooth to promote self-removal of foreign material during normal joint operation.

The new drain trough shall make use of the existing drainage collector system with additions and modifications as required to correspond with the flowlines. The new trough hang line is lower than the hang line of the existing trough. When the existing trough flow line profile is superimposed on the new trough hang line, the new trough sag conflicts with the structural steel below at Pier 1L (panel point 0), panel point 23 and panel point 23'. The new trough flowline shall be detailed to achieve an 8% minimum slope and avoid contact with the structural steel below.

At Pier 6L (panel point 0') the existing trough shop drawings may be considered a reference for panel point 0' in the new trough profile layout.

At Pier 1L (panel point 0), panel point 23 and panel point 23', low points may be required in each bay between stringers to ensure an 8% minimum trough slope and clear the steel below. Each of those low points requires a catch basin with downspout connected to a lateral run of pipe with a 2% minimum slope. Match new catch basins with the existing catch basin.

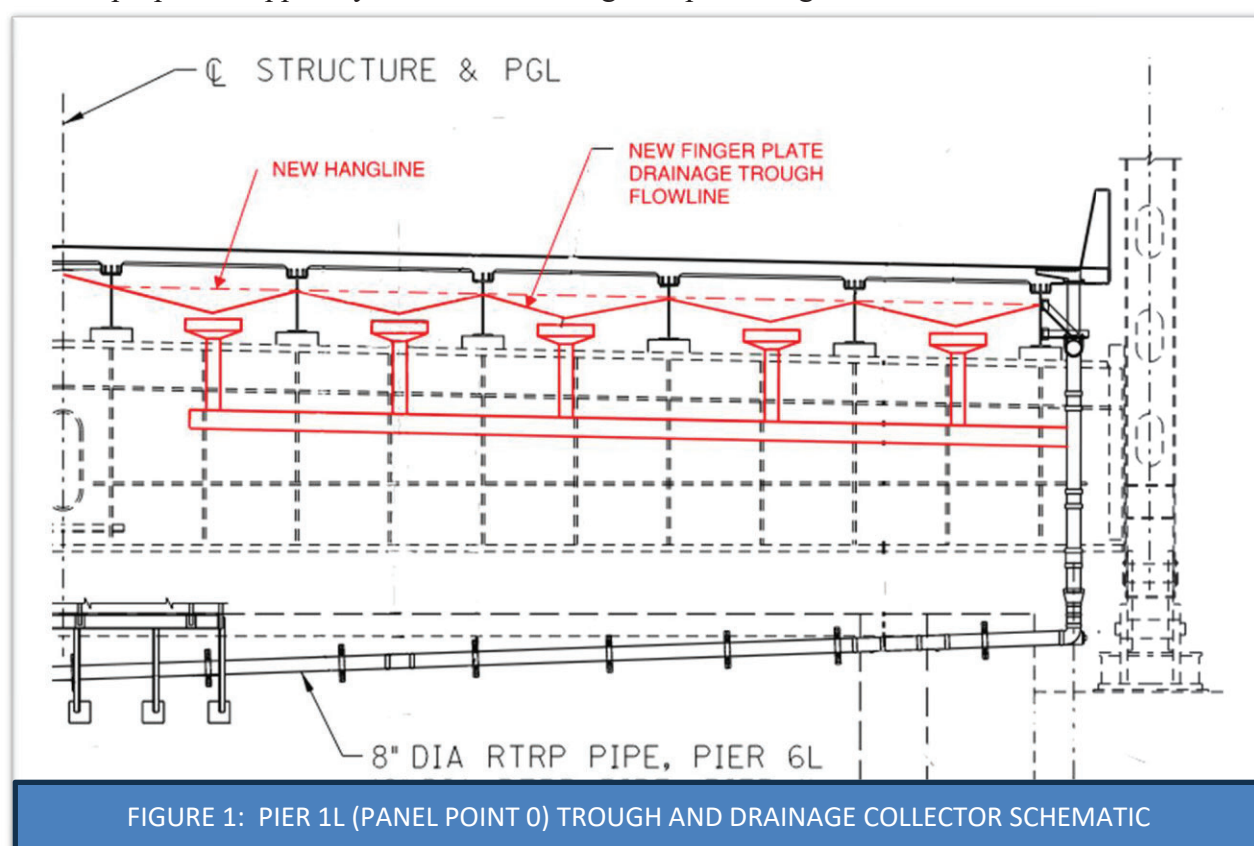
For a schematic representation of the new trough flow line profile and drainage trough collector concept at the three locations, see Figures 1, and 2.

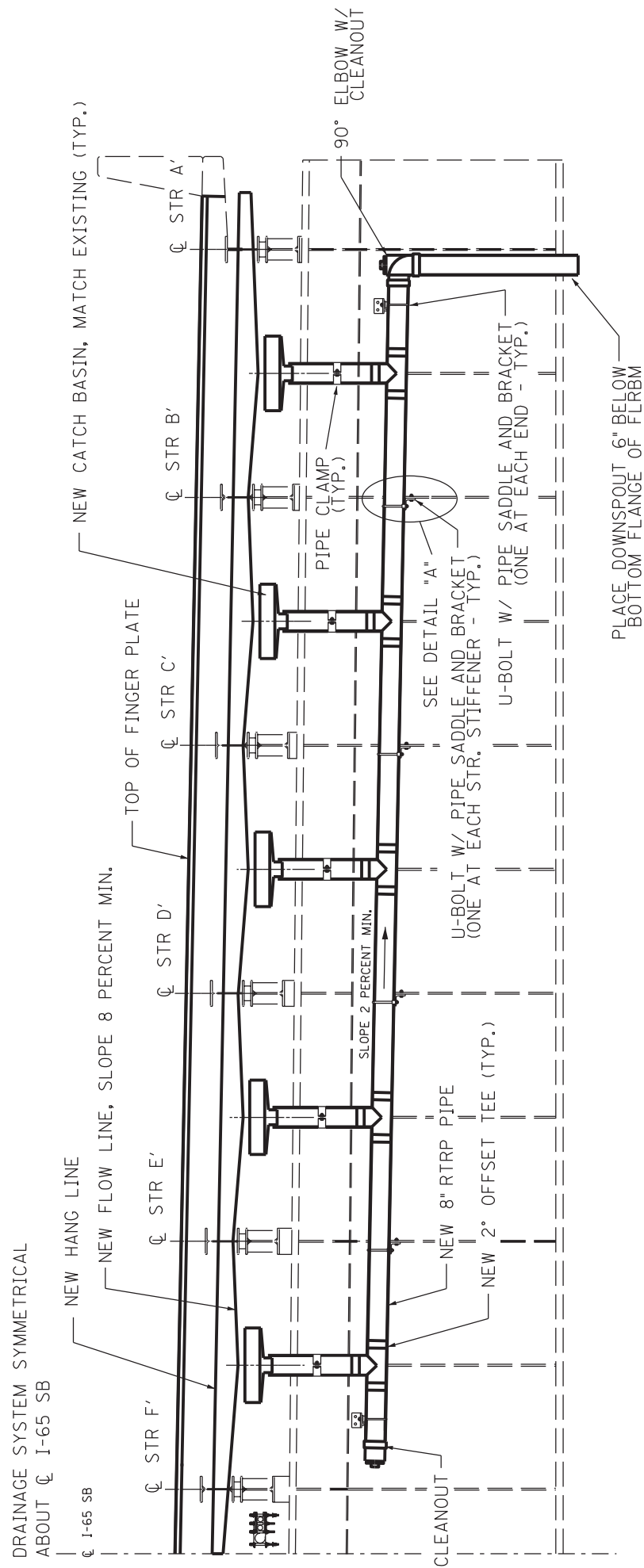
Unless noted otherwise, the new drainage pipe, fittings, catch basins, pipe clamps, and support brackets to match existing components in-kind, or approved equals.

For existing details at finger plate expansion joints, see "056B00214L As-built Section 2 – ORB Downtown I-65 SB Rehab Plans Combined" slab drainage on sheets 2S7045, 2S7046, 2S7047 & 2S7047A. For locations of drainage downspouts and drainage collector system, see as-built slab reinforcing Plans on sheets 2S7032 thru 2S7037. For existing finger joint shop drawings, see General Notes "Plans of Existing Structures".

See the "For Information Only" sheets in the proposal plan set for select sheets from the existing Section 2 – ORB Downtown I-65 SB plan set.

Remove, clean, and store the existing catch basins for reuse. The new catch basins shall be supported to provide a positive flow into the downspout and prevent the free end from deflecting when filled with debris. See Figure 2 for a proposed concept. The Contractor may select a different supporting system per manufacturers recommendations. Submit the proposed support system with the trough shop drawings.





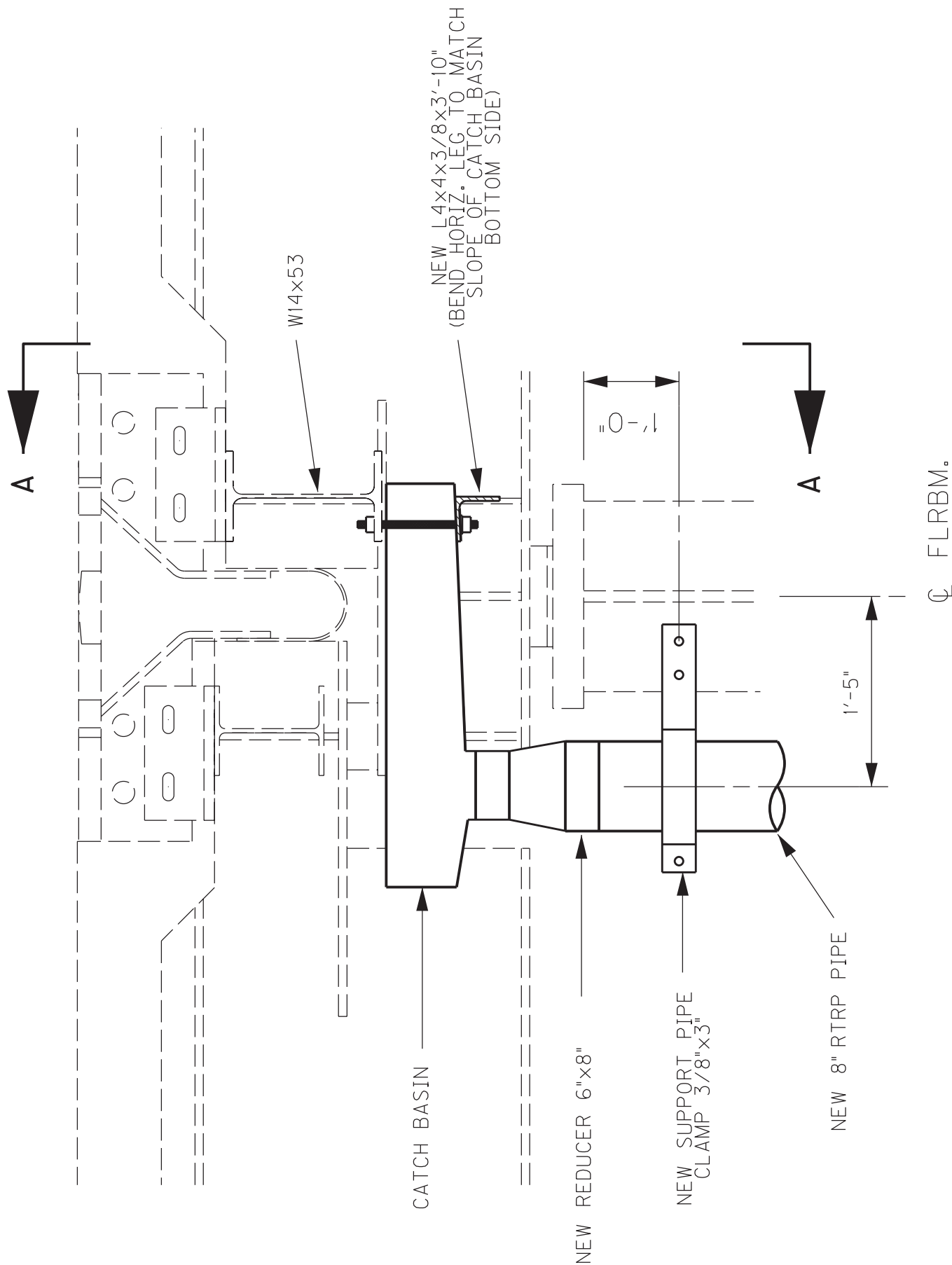


FIGURE 2A:

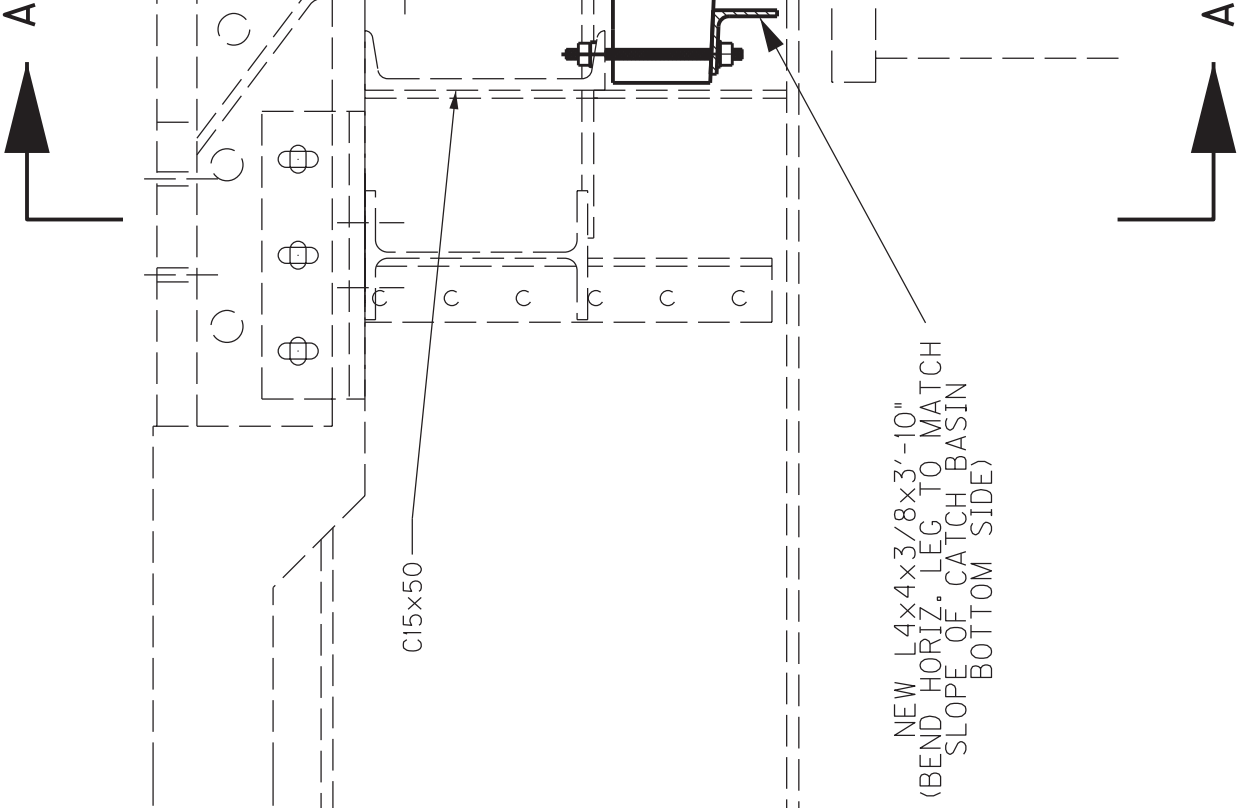
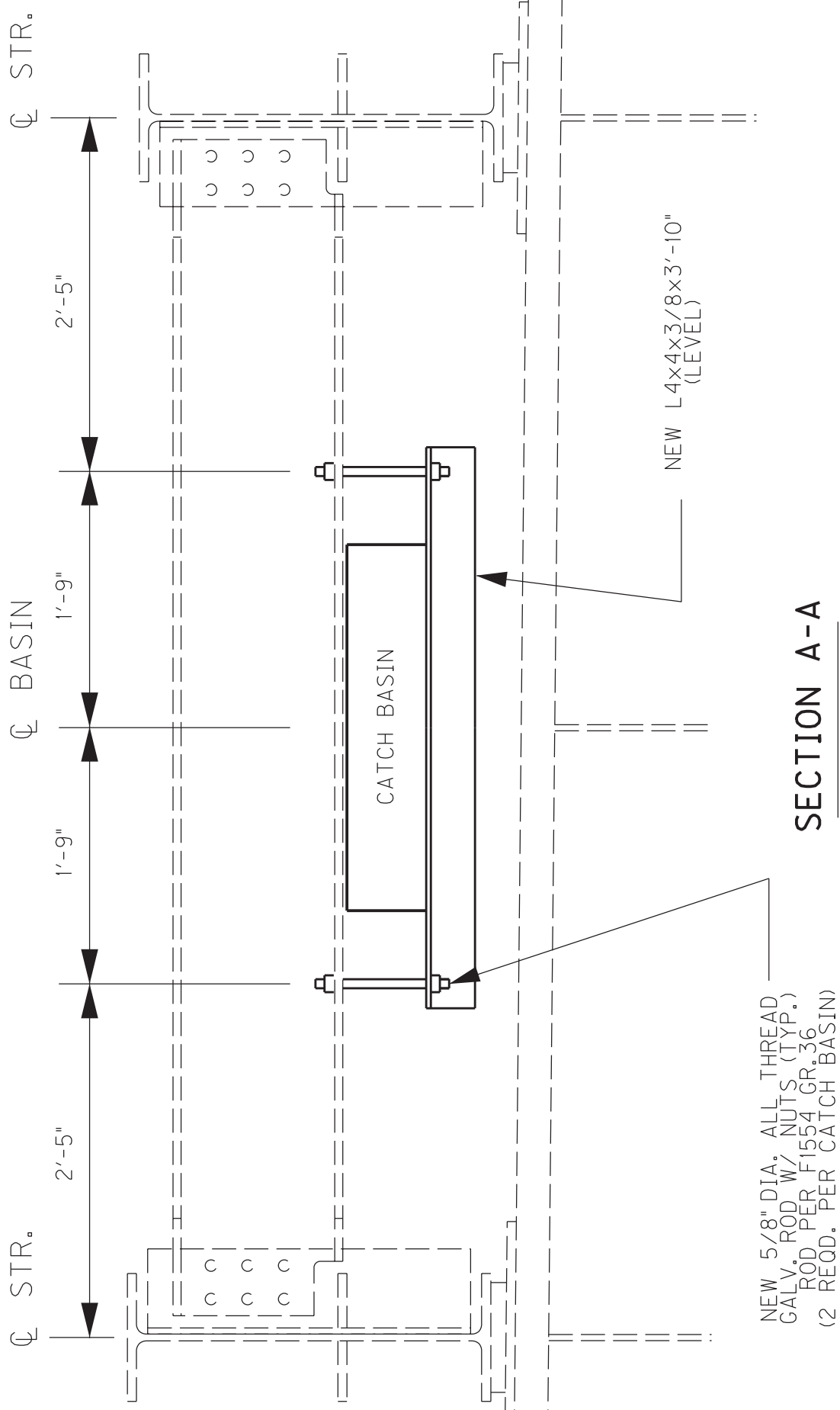


FIGURE 2B:

SECTION AT L23



(JOINT NOT SHOWN FOR CLARITY)

FIGURE 2C:

E. Neoprene/fabric composite material shall comply with the following:

PHYSICAL PROPERTY	ASTM TEST	VALUE
Density		75 psf minimum
Hardness (Type A Durometer)	D2240	50 to 75 points
Tensile Strength, both directions	D378	800 lbs/in. minimum
Elongation @ ultimate tensile strength	D412	35% maximum
Tear (Die C)	ASTM S624	120 lbs/in. minimum
Low Temperature Brittleness (22 hrs. @ -20°F, then wrapped around a 3 inch mandrel)	ASTM D2137	No cracks
Ozone Resistance (20% Strain) 100 pphm in air (100 hrs. @ 100°F)	D1149	No cracks

F. Drainage Trough Hardware. Fasteners, washers and nuts shall be stainless steel conforming to ASTM A240, ASTM F593 and ASTM F594.

FABRICATION

- A. Tooth plate assembly may be fabricated in two pieces with a joint at the roadway crown (Option 1.) Alternatively, plate assembly may be fabricated in four pieces provided that segment joints occur at the crown line and within 1'-0" of roadway lane lines (Option 2.) Tooth plates for each assembly shall be cut from a single plate by burning such that, after the plate is cut and the toothed plates placed in the same relative position as before burning, no part of the cut shall be wider than 1/8 in. Drill vent holes and machine or grind tapers into the plate. Upon completion, the tooth plate shall be galvanized per ASTM A-123.
- B. The joint segments shall be manufactured and delivered in pairs of independent elements with parallel gaps between teeth. Each side of adjacent units of finger expansion joints shall be fully assembled in the shop to ensure that full joint closure can be attained without binding of fingers. After joint segments have been fully assembled to nominal joint dimensions and approved, they shall be match marked and scored (the upper surface of the finger plates shall be permanently scored to provide two or more parallel lines in both directions) to aid in proper field installation.
- C. Finger joint assembly shall be welded in accordance with the latest edition of American Welding Society (AWS) D1.5
- D. After galvanizing the fabricator shall prepare the ends of the expansion joint for field welding.
- E. Finger joint components shall be hot dip galvanized in accordance with ASTM A-123.
- F. After field welding and installation, any areas of damage galvanized shall be painted with galvanizing repair paint in accordance with ASTM A-780.

SAMPLING AND TESTING

An independent laboratory, to ensure compliance with these provisions, shall test each lot of composite neoprene/fabric sheeting. Two certified copies of the qualification test data indicating that the tested materials comply with these provisions shall be submitted to KYTC. The sample from each lot shall be one 6-inch piece, 2-foot long.

IDENTIFICATION, SHIPPING AND HANDLING

Expansion joint openings shall be preset prior to shipment and assembled with temporary shipping angles at maximum 5'-0" centers. Fabricator shall show details of all shipping and erection temporary attachments on the shop drawings.

INSTALLATION

Installation of the expansion dam shall be to the lines and grades shown on the plans and in accordance with Contract Documents and shop drawings. Expansion joint system shall be shipped to job site preassembled for units either side of crown line (Option 1) or for the fabricated segment lengths (Option 2).

Align the finger plate or sliding plate joint assemblies in position and check the expansion opening. The expansion opening must be adjusted for temperature prior to bolting, welding or placing concrete on each side of the joint. The bulkhead plate joint at the crown shall be field welded prior to Phase 2 concreting.

Test fit the finger plates or sliding plates with all the armoring and anchorages in place. Install the finger joint centered over the expansion gap, for both fingerplates and sliding plates. Verify that the joint is in plane and sloped per the roadway. For Fabrication Option 2 align adjacent finger plate assemblies using temporary angles bolted to the assemblies. Make sure the fingers do not rub during the full range of temperature movement.

The Engineer will confirm the procedure, opening and alignment prior to concrete placement. Cast and cure the expansion joint blockout per KYTC specifications. Place concrete under the expansion dams, vibrate until the concrete is forced through air holes, and strike off excess concrete. After the concrete has cured, clean air holes and fill with an approved sealer.

Install the fabric trough and the finger or sliding plates according to the Contract Documents and shop drawings. Do not splice the drain trough, unless indicated. If splices are indicated, use splices vulcanized by the manufacturer. Field laps required shall be trimmed and the two sections securely bonded together. Do not use longitudinal splices.

SUBMITTALS

Submit shop drawings, for each location, type and model of expansion device used. Shop drawings shall include, but not be limited to, the following:

- A. Complete details of all components and sections showing all materials used in the expansion joints.
- B. A listing of all applicable KYTC, ASTM and AASHTO specifications.
- C. Name and address of the manufacturer, and location of the fabrication plant.
- D. Name and telephone number of the manufacturer's representative who will be responsible for coordination of production, inspection, sampling and testing.
- E. Welding procedures used in the expansion joint assembly manufacture clearly described and detailed.
- F. Table of longitudinal offsets for installation at varying temperatures. Use 60°F as the mean temperature.

Joint shop drawings and neoprene trough shop drawings shall be coordinated to ensure that joints and troughs will fit when field assembled. Fabrication shall not commence until the approved shop drawings are in the hands of the Inspector and fabricator and KYTC has authorized fabrication.



KENTUCKY TRANSPORTATION CABINET

Department of Highways


DIVISION OF RIGHT OF WAY & UTILITIES

TC 62-226

Rev. 01/2016

Page 1 of 1

RIGHT OF WAY CERTIFICATION

<input checked="" type="checkbox"/>	Original	<input type="checkbox"/>	Re-Certification	RIGHT OF WAY CERTIFICATION	
ITEM #		COUNTY		PROJECT # (STATE)	PROJECT # (FEDERAL)
5-10074.00		Jefferson		FD52 1100 056 1564601	STPBRO 0503(260)
PROJECT DESCRIPTION					
BRIDGE PROJECT IN JEFFERSON COUNTY ON (056B00214L) I-65 SB AT OHIO RIVER					
<input checked="" type="checkbox"/>	No Additional Right of Way Required				
Construction will be within the limits of the existing right of way. The right of way was acquired in accordance to FHWA regulations under the Uniform Relocation Assistance and Real Property Acquisitions Policy Act of 1970, as amended. No additional right of way or relocation assistance were required for this project.					
<input type="checkbox"/>	Condition # 1 (Additional Right of Way Required and Cleared)				
All necessary right of way, including control of access rights when applicable, have been acquired including legal and physical possession. Trial or appeal of cases may be pending in court but legal possession has been obtained. There may be some improvements remaining on the right-of-way, but all occupants have vacated the lands and improvements, and KYTC has physical possession and the rights to remove, salvage, or demolish all improvements and enter on all land. Just Compensation has been paid or deposited with the court. All relocations have been relocated to decent, safe, and sanitary housing or that KYTC has made available to displaced persons adequate replacement housing in accordance with the provisions of the current FHWA directive.					
<input type="checkbox"/>	Condition # 2 (Additional Right of Way Required with Exception)				
The right of way has not been fully acquired, the right to occupy and to use all rights-of-way required for the proper execution of the project has been acquired. Some parcels may be pending in court and on other parcels full legal possession has not been obtained, but right of entry has been obtained, the occupants of all lands and improvements have vacated, and KYTC has physical possession and right to remove, salvage, or demolish all improvements. Just Compensation has been paid or deposited with the court for most parcels. Just Compensation for all pending parcels will be paid or deposited with the court prior to AWARD of construction contract					
<input type="checkbox"/>	Condition # 3 (Additional Right of Way Required with Exception)				
The acquisition or right of occupancy and use of a few remaining parcels are not complete and/or some parcels still have occupants. All remaining occupants have had replacement housing made available to them in accordance with 49 CFR 24.204. KYTC is hereby requesting authorization to advertise this project for bids and to proceed with bid letting even though the necessary right of way will not be fully acquired, and/or some occupants will not be relocated, and/or the just compensation will not be paid or deposited with the court for some parcels until after bid letting. KYTC will fully meet all the requirements outlined in 23 CFR 635.309(c)(3) and 49 CFR 24.102(j) and will expedite completion of all acquisitions, relocations, and full payments after bid letting and prior to AWARD of the construction contract or force account construction.					
Total Number of Parcels on Project		EXCEPTION (S) Parcel #		ANTICIPATED DATE OF POSSESSION WITH EXPLANATION	
Number of Parcels That Have Been Acquired					
Signed Deed					
Condemnation					
Signed ROE					
Notes/ Comments (Text is limited. Use additional sheet if necessary.)					
LPA RW Project Manager			Right of Way Supervisor		
Printed Name				Printed Name	
Signature				Signature	
Date				Tom Boykin <small>Digitally signed by Tom Boykin Date: 2023.12.11 10:23:15 -05'00'</small>	
Right of Way Director			FHWA		
Printed Name				Printed Name	
Signature		 <small>Digitally signed by Kelly Divine Date: 2023.12.11 11:50:34 -06'00'</small>		Signature	
Date				Date	

UTILITIES AND RAIL CERTIFICATION NOTE

Jefferson County

No federal number available STBRO 0503(260)

FD52 056 0065 136-137

Bridge Project in Jefferson County on I-65 SB at Ohio River

ITEM NUMBER: 05-10074

PROJECT NOTES ON UTILITIES

The contractor should be aware that there is UTILITY WORK INCLUDED IN THIS ROAD CONSTRUCTION CONTRACT. The Contractor shall review the GENERAL UTILITY NOTES AND INSTRUCTIONS which may include KYTC Utility Bid Item Descriptions, utility owner supplied specifications, plans, list of utility owner preapproved subcontractors, and other instructions. Utility contractors may be added via addendum if KYTC is instructed to do so by the utility owner. Potential contractors must seek prequalification from the utility owner. Any revisions must be sent from the utility owner to KYTC a minimum of one week prior to bid opening.

For all projects under 2000 Linear feet which require a normal excavation locate request pursuant to KRS 367.4901-4917, the awarded contractor shall field mark the proposed excavation or construction boundaries of the project (also called white lining) using the procedure set forth in KRS 367.4909(9)(k). For all projects over 2000 linear feet, which are defined as a “Large Project” in KRS 367.4903(18), the awarded contractor shall initially mark the first 2000 linear feet minimally of proposed excavation or construction boundaries of the project to be worked using the procedure set forth in KRS 367.4909(9)(k). This temporary field locating of the project excavation boundary shall take place prior to submitting an excavation location request to the underground utility protection Kentucky Contact Center. For large projects, the awarded contractor shall work with the impacted utilities to determine when additional white lining of the remainder of the project site will take place. This provision shall not alter or relieve the awarded contractor from complying with requirements of KRS 367.4905 to 367.4917 in their entirety.

Please Note: The information presented in this Utility Note is informational in nature and the information contained herein is not guaranteed.

The contractor will be responsible for contacting all utility facility owners on the subject project to coordinate his activities. The contractor will coordinate his activities to minimize and, where possible, avoid conflicts with utility facilities. Due to the nature of the work proposed, it is unlikely to conflict with the existing utilities beyond minor facility adjustments. Where conflicts with utility facilities are unavoidable, the contractor will coordinate any necessary relocation work with the facility owner and Resident Engineer. The Kentucky Transportation Cabinet maintains the right to remove or alter portions of this contract if a utility conflict occurs. The utility facilities as noted in the previous section(s) have been determined using data garnered by varied means and with varying degrees of accuracy: from the facility owners, a result of S.U.E., field inspections, and/or reviews of record drawings. The facilities

UTILITIES AND RAIL CERTIFICATION NOTE

<p>Jefferson County No federal number available STBRO 0503(260) FD52 056 0065 136-137 Bridge Project in Jefferson County on I-65 SB at Ohio River ITEM NUMBER: 05-10074</p>
--

defined may not be inclusive of all utilities in the project scope and are not Level A quality, unless specified as such. It is the contractor's responsibility to verify all utilities and their respective locations before excavating.

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. The contractor is instructed to contact KY 811 for the location of existing underground utilities. Contact shall be made a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY 811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom are to be contacted through their individual Protection Notification Center. It may be necessary for the contractor to contact the County Court Clerk to determine what utility companies have facilities in the area. Non-compliance with these directives can result in the enforcement of penalties.

Utility coordination efforts determined that no significant utility relocation work is required to complete the project. Any work pertaining to these utility facilities is defined in the bid package and is to be carried out as instructed by the Kentucky Transportation Cabinet. The contractor will be responsible for any coordination or adjustments that are discussed or quantified in the proposal.

UTILITIES AND RAIL CERTIFICATION NOTE

Jefferson County

No federal number available STBRO 0503(260)

FD52 056 0065 136-137

Bridge Project in Jefferson County on I-65 SB at Ohio River

ITEM NUMBER: 05-10074

NOTE: DO NOT DISTURB THE FOLLOWING FACILITIES LOCATED WITHIN THE PROJECT DISTURB LIMITS

The Contractor is fully responsible for protection of all utilities listed above

THE FOLLOWING FACILITY OWNERS ARE RELOCATING/ADJUSTING THEIR FACILITIES WITHIN THE PROJECT LIMITS AND WILL BE COMPLETE PRIOR TO CONSTRUCTION

N/A

THE FOLLOWING FACILITY OWNERS HAVE FACILITIES TO BE RELOCATED/ADJUSTED BY THE OWNER OR THEIR SUBCONTRACTOR AND IS TO BE COORDINATED WITH THE ROAD CONTRACT

N/A

THE FOLLOWING FACILITY OWNERS HAVE FACILITIES TO BE RELOCATED/ADJUSTED BY THE ROAD CONTRACTOR AS INCLUDED IN THIS CONTRACT

N/A

RAIL COMPANIES HAVE FACILITIES IN CONJUNCTION WITH THIS PROJECT AS NOTED

☒ No Rail Involvement ☐ Rail Involved ☐ Rail Adjacent

UTILITIES AND RAIL CERTIFICATION NOTE

Jefferson County
No federal number available STBRO 0503(260)
FD52 056 0065 136-137
Bridge Project in Jefferson County on I-65 SB at Ohio River
ITEM NUMBER: 05-10074

UTILITIES AND RAIL CERTIFICATION NOTE

Jefferson County

No federal number available STBRO 0503(260)

FD52 056 0065 136-137

Bridge Project in Jefferson County on I-65 SB at Ohio River

ITEM NUMBER: 05-10074

AREA FACILITY OWNER CONTACT LIST

- | | |
|--|---|
| 1. All Points Broadband
Manager
148 Citizens Blvd
Simpsonville, KY 40067 | DJ Anderson, Regional Operations

Cell (502) 390-6806
DAnderson@allpointsbroadband.com
Vince West, Regional Operations |
| 2. Atmos Energy
105 Hudson Blvd
Shelbyville, KY 40065

2850 Russellville Rd.
Bowling Green, KY 42101 | Jake Basham
Cell (270) 779-7381
Jake.Basham@atmosenergy.com

Taylor Collins
(270)-577-4582 |
| 3. AT&T KY
1340 E. John Rowan Blvd

Bardstown, KY 40004 | Scott Roche
Office (502) 348-4528

Cell (502) 827-4703

SR8832@att.com |
| 4. AT&T Legacy
OH, MI, KY, WV
7555 E. Pleasant Valley Road, Suite 140
Independence, OH 44131

AT&T Corporation, Long Haul | Bill Taggart, Sr. Tech Project Manager

WT2462@att.com
Office (614) 868-2511
Cell (614) 370-5414
Don Garr, Cell (502) 741-8374
DonGarr@outlook.com |
| 5. Center Point Energy
2520 Lincoln Drive
Clarksville, IN 47129 | Jeff Donnelly, Transmission Department
Office (812) 431-5670
Jeff.Donnelly@centerpointenergy.com
Chris Wood, Engineering Manager |

UTILITIES AND RAIL CERTIFICATION NOTE

Jefferson County

No federal number available STBRO 0503(260)

FD52 056 0065 136-137

Bridge Project in Jefferson County on I-65 SB at Ohio River

ITEM NUMBER: 05-10074

6. Charter Communications
4701 Commerce Crossings Drive
Louisville, KY 40229

Nathen L Howerton
Office (502) 357-4318
Cell (502) 639-6838
Nathen.Howerton@charter.com
Kevin Mercer
Office (502) 357-4724
Cell (502) 817-5055
Kevin.Mercer@charter.com
Michael York
Cell (502) 548-1632
Michael.York@charter.com

Facility Map Requests to Nathen-Kevin-Mike

7. Crown Castle Network Operations
10300 Ormsby Park Place, Suite 501
1578
Louisville, KY 40223

Mona Couch
Office (317) 249-2029, Cell (317) 439-
Mona.Couch@crowncastle.com

Lisa Clark, Construction Manager
(810) 223-3780
Lisa.Clark@crowncastle.com

8. Crown Castle Fiber
3310 Ruckreigel Parkway
Jeffersontown, KY 40299

Lisa Clark, Construction Manager
(810) 223-3780
Lisa.Clark@crowncastle.com
Craig Brown, Manager-Network
Construction Office (317)
819-8648, Cell (585) 402-8342
Craig.Brown@crowncastle.com

9. Indiana Utilities Corporation
123 West Chestnut Street
Corydon, Indiana 47112

Scott Schmitt
Office (812) 738-3235
Cell (812) 972-0539
ScottS@indianautilitiescorp.com
Corey Thatcher, Field Technician

UTILITIES AND RAIL CERTIFICATION NOTE

<p>Jefferson County No federal number available STBRO 0503(260) FD52 056 0065 136-137 Bridge Project in Jefferson County on I-65 SB at Ohio River ITEM NUMBER: 05-10074</p>

Office (812) 738-3235
Cell (812) 267-6936
CoreyT@indianautilitiescorp.com
Kevin Kinney
Ron Timberlake

10. Intercarrier Networks, LLC
Subsidiary of Great Plains Communications
1600 Great Plains Center
PO Box 500
Blair, NE 68008

Finley Engineering Company, Inc.
2456 Fortune Drive, Suite 105
Lexington, KY 40509

CWulf@gpcom.com
Nancye Allison, Consultant
Office (859) 231-9831
Cell (502) 541-3442
N.Allison@finleyusa.com
Dustin Nobbe
(812) 209-9012
DNobbe@gpcom.com

11. Jefferson County Public Schools (JCPS)
C B Young
Building 6

3001 Crittenden Drive
Louisville, KY 40209

Jeff Hardy
Office (502) 485-7975
Cell (502) 379-9315
Jeff.Hardy@Jefferson.kyschools.us
JDHardy70@gmail.com

Scott McMahan (Team Fishel)
Office (502) 456-2900
Cell (502) 664-9312

12. Kentucky Wired, KCNA
500 Mero Street – Suite 1-1 NC #59
Manager
Frankfort, KY 40601

Ledcor, Service Provider
LTS Kentucky Managed Technical Services LLC

Erick Johnson
Executive Advisor/Operations Project

Office (502) 330-0679
Erick.Johnson@ky.gov

Nick Blake
Operations Manager

UTILITIES AND RAIL CERTIFICATION NOTE

<p>Jefferson County No federal number available STBRO 0503(260) FD52 056 0065 136-137 Bridge Project in Jefferson County on I-65 SB at Ohio River ITEM NUMBER: 05-10074</p>
--

2008 Mercer Rd. 2nd floor
Lexington, KY 40511

Office (859) 286-8158
Cell (859) 447-0781
Nick.Blake@Ledcor.com

13. LG&E KU (Electric)
820 West Broadway
Louisville, KY 40202
LG&E Emergency Number (502) 589-1444
LG&E and KU Emergency Number 1-800-331-7370

Caroline Justice
Office (502) 627-3708
Caroline.Justice@LGE-KU.com

14. LG&E (Gas)
820 West Broadway
Louisville, KY 40202
Gas Emergency Number (502) 589-5511
LG&E and KU Emergency Number 1-800-331-7370

Caroline Justice
Office (502) 627-3708
Caroline.Justice@LGE-KU.com

15. Louisville Water Company
550 South Third Street
Louisville, KY 40202

Pat Howard, PE
Office (502) 569-3615
Cell (502) 287-7568
PHoward@lwcky.com

16. Lumen Technologies Inc.
11857 Commonwealth Drive
Louisville, KY 40299

Tim Sewell
Tim.Sewell@lumen.com
(502) 389-4812 / (502) 295-0940

17. MCI (See Verizon)

18. Marathon Pipeline, LLC
539 South Main Street, Room X-05-018
Findlay, OH 45840

Landon Morris
Senior Right of Way Specialist
Cell: (419) 957-7792

UTILITIES AND RAIL CERTIFICATION NOTE

<p>Jefferson County No federal number available STBRO 0503(260) FD52 056 0065 136-137 Bridge Project in Jefferson County on I-65 SB at Ohio River ITEM NUMBER: 05-10074</p>
--

or

19. Metropolitan Sewer District
700 West Liberty Street
Louisville, KY 40203-1911

LrMorris@marathonpetroleum.com

Taylor Friesz
Office (502) 540-6163
Cell (502) 530 5192
Taylor.Friesz@louisvillemsd.org
Greg Powell
Greg.Powell@LouisvilleMSD.org

20. Mid - Valley Pipeline Company
4910 Limaburg Road
Burlington, KY 41005

Richard (Todd) Calfee
Office (859) 371-4469
Cell (859) 630-8271
Fax (866) 699-1185
Todd.Calfee@energytransfer.com

21. Texas Gas Transmission, LLC
2332 Hwy 60 West

Hardinsburg, KY 40143

610 W 2nd Street

Howard K. Menser Jr., Area Manager
Office (502) 491-0251

Cell (502) 396-2418

Howard.Menser@bwpipelines.com

22. T-Mobile (Sprint) Fiber Optics
3007 Tucumcari Dr.
Sardinia, OH 45171

Steven T. Hughes
Office (513) 459-5796
Cell (814) 553-2300
Steven.Hughes1@T-Mobile.com

23. TRIMARC
Public Safety & Transportation Systems
901 West Main Street
Louisville, KY 40202

Todd Hood
Office (502) 290-7201
Cell (270) 307-7456
Todd.Hood@peraton.com

24. Verizon
730 West Henry Street
Indianapolis, IN 46225

Dean Boyers
Office (615) 777-7855
Cell (615) 507-5287

UTILITIES AND RAIL CERTIFICATION NOTE

Jefferson County

No federal number available STBRO 0503(260)

FD52 056 0065 136-137

Bridge Project in Jefferson County on I-65 SB at Ohio River

ITEM NUMBER: 05-10074

Verizon
Ops
2421 Holloway Road
Louisville, KY 40299

Dean.Boyers@verizon.com
Jeffrey Tucker, Engr III Spec-Ntwk Eng &

Office (502) 830-1827
Cell (502) 593-5585

Jeffrey.Tucker@verizon.com
Ronnie Kuerzi, Eng IV Spec-Ntwk Eng &

Ops

Cell (502) 780-2748

Ronald.Kuerzi@verizon.com

25. Windstream
111 S. Main Street
Elizabethtown, KY 42701

James Galvin
Office (270) 765-1818
Cell (270) 748-9249
James.Galvin@windstream.com
Steve Johnson, Sr Engineer OSP
Office (859) 357-6209

Windstream

Steve Johnson, Sr Engineer OSP
130 W New Circle Road, Suite 170
Lexington, KY 40505

Cell (859) 321-2035
Steve.Johnson@windstream.com
Ronald (Todd) Moore
(502) 818-7605
Ronald.Moore@windstream.com

KENTUCKY TRANSPORTATION CABINET
Department of Highways
DIVISION OF ENVIRONMENTAL ANALYSIS
CATEGORICAL EXCLUSION DETERMINATIONTC 58-48
Rev. 12/2020
Page 1 of 1**1. PROJECT SUMMARY**

Item #: 05-10074	Project Sponsor: KYTC
Route(s): I-65 SB	County: Jefferson
Project Description: BRIDGE PROJECT IN JEFFERSON COUNTY ON the Kennedy (056B00214L) I-65 SB AT OHIO RIVER	



2. ENVIRONMENTAL DETERMINATION

Functional Area	Determination	Comments/Commitments/Mitigation
Public and Resource Agency Controversy	No	
Total acreage of fee simple ROW	0	All work on the bridge in existing ROW
Number of Total Relocations	0	All work on the bridge in existing ROW
Environmental Justice Impacts	No	
Section 106: Architectural Historic	No Adverse Effect	SHPO concurrence 12/20/2023
Section 106: Archaeological Resources	No Effect	SHPO concurrence 12/13/2023
Section 4(f)	De minimis	No adverse effect on the Kennedy Bridge JFCB-722
Section 6(f)	No 6(f) Properties	
Noise	Not a Type I	Bridge Repairs
Air Quality Impacts	No	FY2023-26 TIP KIPDA# 3028 Pg 200/255
Hazardous Materials Impacts	No	AAsbestos Inspections performed 12/18/2023
Section 7: T&E Species	No Effect	No Effect Form 12/19/2023
Anticipated Feet of Stream Impacts	0	All work on the bridge above the water
Anticipated Acreage of Wetland Impacts	0	
Anticipated Permits	Yes	10-day eNOI (Form DEP 7036) to DAQ
Other: Air Quality		STIP Amendment 2021.005
Other:		
Other:		

Based on the criteria listed above, in review of the most recent Categorical Exclusion Agreement between KYTC and FHWA, the subject project is determined to be considered a Categorical Exclusion, Level CEMP.

3. ENVIRONMENTAL DOCUMENT APPROVAL

Based on the information obtained during the environmental review process and included as attachments to this form, the project is determined to be a Categorical Exclusion under 23 CFR part 771 pursuant to the National Environmental Policy Act and complies with all other applicable environmental laws, regulations, and Executive Orders. The project action does not individually or cumulatively have a significant effect on the natural and human environment.

 District Environmental Coordinator	12/20/2023 Date	 Project Manager	12/20/23 Date
_____ Environmental Project Manager	_____ Date	_____ Director of Environmental Analysis	_____ Date
_____ Recommended by FHWA	_____ Date	_____ Federal Highway Administration	_____ Date

KYTC Item No: 5-10074

County: Jefferson

Route: Interstate 65

KYTC Historic Architectural Investigation Form

Project Description: The proposed project consists of maintenance for the bridge and will not result in major alterations to the bridge. The proposed maintenance consists of truss bearing repairs; finger joint repairs; and truss repairs. All repairs will be in-kind replacements. All work will be within the existing bridge footprint and right-of-way.

Project Type listed in Attachment 1 (in Section 106 Programmatic Agreement)?

☐ Yes

☒ No (Continue)

Project Type listed in Attachment 2 (in Section 106 Handbook)?

☒ Yes (List project activity types) #11 Bridge rehabilitation

☐ No (This project is not considered a small scale project under the Section 106 Programmatic Agreement. This checklist cannot be used. Process with full baseline or joint memorandum)

☐ No (However, SHPO has agreed that this project may be documented using the Historic Architectural Investigation Form)

Project Area of Potential Effect is defined as:

☐ Within 150 feet of project centerline (Small Scale Project - within existing corridor)

☐ Within view shed of project (Discuss):

☒ Other (Discuss): Within the existing footprint of the bridge.

Are there Historical Resources within the project APE (per KHC database)?

☒ Yes

☐ No

☐ N/A (Explain):

KYTC Item No: 5-10074

County: Jefferson

Route: Interstate 65

Are there Historical Resources (50 years old or older) identified within the project APE based on field investigations?

☒ Yes

☐ No

Date of Field Investigation: December 11, 2023

Investigator Name(s): Amanda Abner/Stephanie Lechert

Discuss Basis for finding

(Historic Mapping, PVA, Building Permit, Date of Construction, Deed/Title, etc.):

KHC database; Site Photos; Google Earth; Google Street View; Historicaerials.com; USGS historic aerial imagery; KYTC records

NRHP listed or potentially eligible sites/districts (> 50 years old) are:

☒ Present within the APE (Continue)

☐ No Properties Eligible within APE

Sections below to be completed by KYTC Architectural Historian

Discuss eligibility determinations (criteria, integrity):

See pages 6-7.

Determination of Effect (when eligible sites have been identified):

☐ No Historic Properties Affected

☒ No Adverse Effect (May result in Section 4(f) *De minimis* finding – Document appropriately)

☐ Adverse Effect

KYTC Item No: 5-10074

County: Jefferson

Route: Interstate 65

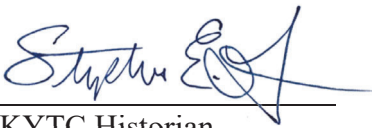
Discuss No Effect/No Adverse Effect Determination:

See pages 6-7.

☐ No Historic Properties Affected

☒ No Adverse Effect to Historic Properties


As Determined By:



12/14/23

KYTC Historian

Date



12/20/2023

SHPO Representative

Date

☒ Map Showing APE and Identified Historic Resources

☒ Individual Site Maps

☒ Photographs

☒ Project Plans

☒ KHC Site Survey Forms

☐ Other (Describe): _____

☒ Copy EPM

☒ Copy DEC

☐ Copy DEA Project File

☒ Copy FHWA (w/De minimis Memo if appropriate)

☒ Copy SHPO



Figure 1. Project location map.

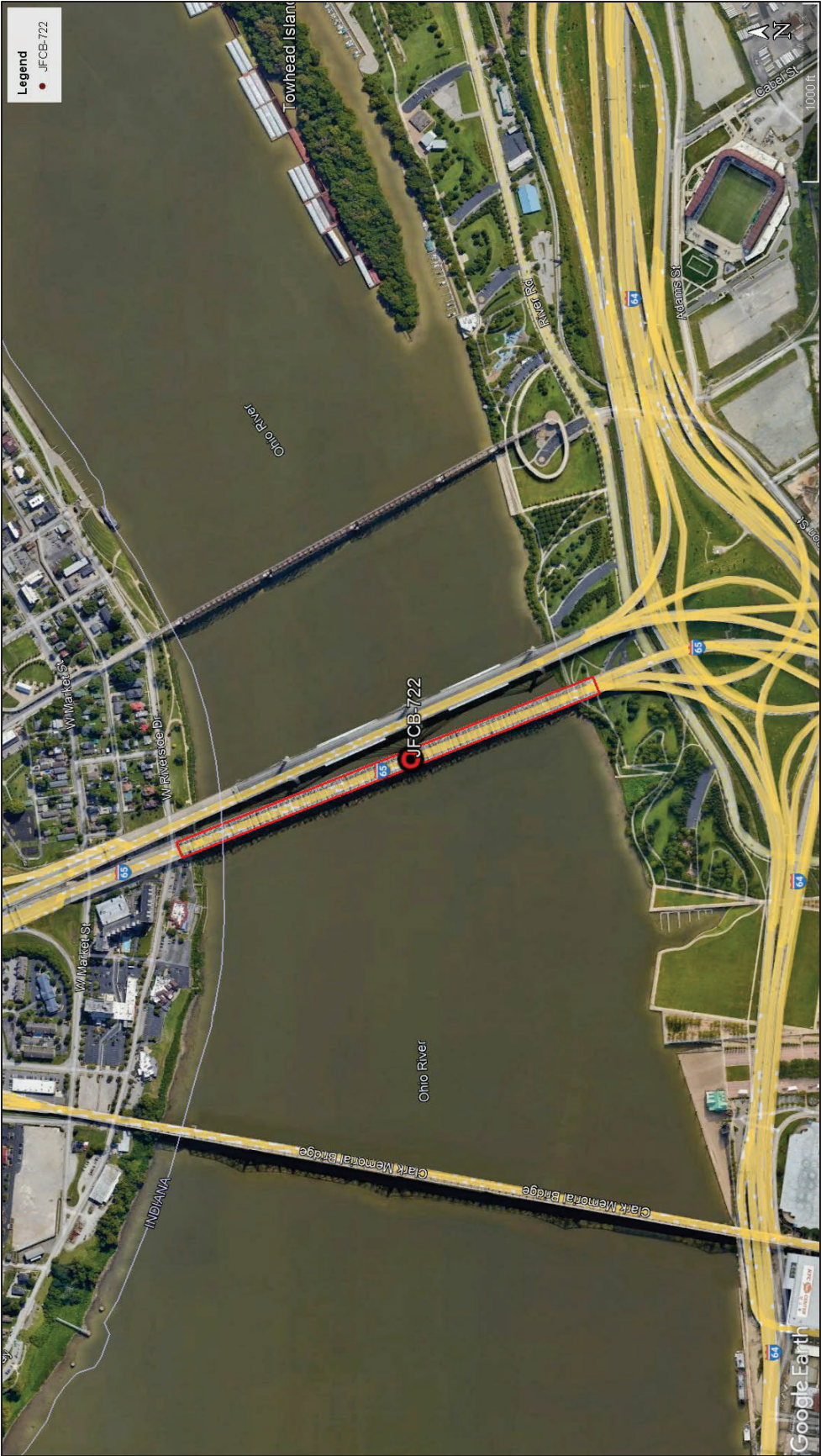


Figure 2. Site location map. The red box outlines the extends of JFCB-722.



Figure 3. JFCB-722, the John F. Kennedy Memorial Bridge, facing north. The Abraham Lincoln Bridge is in the background on the right side of the photograph.

Site JFCB-722 (Bridge No. 056B00214L), formally named the John F. Kennedy Memorial Bridge, is a 2,498.30-foot-long, 5-span, single-deck cantilever thru-truss bridge built between 1961 and 1963. The bridge connecting Louisville, Kentucky and Jeffersonville, Indiana was opened in December 1963, and named for President John F. Kennedy, who had recently been assassinated. "On November 26, 1963, it was named the John Fitzgerald Kennedy Memorial Bridge before opening less than 2 weeks later on December 6" (Weingroff 2023). Site JFCB-722, designed by Hazelet & Erdal Consulting Engineers of Louisville, Kentucky, originally carried both northbound and southbound Interstate 65 (I-65), but now only carries the six lanes of southbound I-65 with the 2016 construction of the Abraham Lincoln Bridge, which now carries northbound I-65. Site JFCB-722 was previously documented in 2009 during the Louisville Bridges project.

Hazelet & Erdal Consulting Engineers was formed "in 1936 as a successor to the Scherzer Rolling Lift Bridge Company" (Linville and Allen 2016). Hazelet & Erdal were known for bridge and roadway design, including the neighboring Sherman Minton Bridge. The firm was eventually acquired by BRW, which was acquired by Dames & Moore Group in 1996, which was later acquired by URS Corporation in 1999, and finally acquired by AECOM in 2014. (AECOM 2014; Bolton 1999; Covell 2019; Journal of Commerce Staff 1996; Kirkham 2014; Linville and Kent 2016).

Site JFCB-722 is eligible for the NRHP under Criterion C for its design as a cantilever thru truss bridge which carries Interstate 65 over the Ohio River between Indiana and Kentucky. While the bridge is named in honor of John F. Kennedy, it has no other connection to the former president. Site JFCB-722 was designed by engineering firm Hazelet & Erdal Consulting Engineers, no research currently available

connects the bridge design to a significant engineer. Further research at a later date may provide additional insight. Therefore, JFCB-722 is not significant under Criterion B for the NRHP. While JFCB-722 is also provides an river crossing between Kentucky and Ohio on the Interstate 65 corridor, further research should be conducted to determine if the I-65 corridor's impact on commerce. Therefore, JFCB-722 is not eligible under Criterion A at this time. Site JFCB-722 has had in-period maintenance and rehabilitation work during its existence. While the materials have experienced some very minor impacts due to the maintenance and rehabilitation, the integrity of location, design, setting, feeling, workmanship, and association remain unaffected. Overall, the trusses, all of the piers, and the bridge and road alignment remain the same as when they were originally constructed in 1963. In summary, JFCB-722 is eligible under Criterion C.

The proposed project consists of maintenance for the bridge and will not result in major alterations to the bridge. The proposed maintenance consists of truss bearing repairs; finger joint repairs; and truss repairs. All repairs will be in-kind replacements. All work will be within the existing bridge footprint and right-of-way. Therefore, the project will result in No Adverse Effect to JFCB-722.



Figure 4. JFCB-722, underside of the John F. Kennedy Memorial Bridge from the Kentucky banks of the Ohio River, facing north.

References Cited

AECOM

2014 AECOM completes acquisition of URS Corporation. Available at <https://aecom.com/press-releases/aecom-announced-today-that-the-company-has-completed-its-acquisition-of-urs-corporation-with-broad-support-from-stakeholders-following-approval-of-the-merger-agreement-by-urs-stockholders-and/>. Accessed December 14, 2023.

Bolton, Andrew

1999 Second URS buy creates UK stronghold. Available at: <https://www.newcivilengineer.com/archive/second-urs-buy-creates-uk-stronghold-01-06-1999/>. Accessed December 14, 2023.

Covell, Jeffery L.

2019 Dames & Moore, Inc. Available at <https://www.encyclopedia.com/books/politics-and-business-magazines/dames-moore-inc>. Accessed December 14, 2023.

Journal of Commerce Staff

1996 Dames & Moore Buys BRW Group. Available at https://www.joc.com/article/dames-moore-buys-brw-group_19960612.html. Accessed December 14, 2023.

Kirkham, Chris

2014 Aecom finalizes \$6-billion acquisition of engineering design rival URS. Available at <https://www.latimes.com/business/la-fi-aecom-urs-20141017-story.html>. Accessed December 14, 2023.

Linville, Nick and Allen Kent

2016 "Photographs, Written Historical and Descriptive Data." Historic American Engineering Record State Route A1A, Simpson Creek Bridge (FDOT Bridge No. 720072) HAER NO. FL-40-B. Available at <https://tile.loc.gov/storage-services/master/pnp/habshaer/fl/fl0900/fl0924/data/fl0924data.pdf>. Accessed December 14, 2023.

Weingroff, Richard F.

2023 A Moment in Time: President John F. Kennedy Cuts a Ribbon. Available at <https://highways.dot.gov/highway-history/general-highway-history/moment-time-president-john-f-kennedy-cuts-ribbon>. Accessed December 14, 2023.

Archaeology

KYTC Item No: 5-10074.00

County: Jefferson

Route: I-65 SB

KYTC Archaeological Investigation Form

Project Description: Bridge repair project on the Kennedy Bridge in Louisville, I-65 SB.
Activities include bearing repairs, joint repairs, and steel repairs. All work will be on the bridge and within existing ROW.

USGS Quad Name: Jeffersonville

USGS Date: 1993

Coordinates (Project center point) LAT: 38.2643 LONG: -85..7434

Project Type listed in Attachment 1 (in Section 106 Programmatic Agreement)?☐ Yes (list project activity types) _____☒ No (Continue)**Project Type listed in Attachment 2 (in Section 106 Handbook)?**☒ Yes (list project activity types) #21 bridge maintenance _____**Are all new or existing ROW areas previously disturbed?**☒ Yes (Describe disturbance or basis for conclusion. Attach photos or maps):All work will occur on the existing bridge, within existing ROW.

KYTC Item No: 5-10074.00

County: Jefferson

Route: I-65 SB

No Historic Properties Affected

As Determined By:

Susan Neumeyer

December 11, 2023

KYTC Representative

Stephanie Dooley

December 13, 2023

SHPO Representative

Date

(Concurrence is assumed if no response is received within 30 days)

Attachments

☒ Project Plans (show date on plans)

☐ Photos

☐ Mapping

☐ Other:

☒ Copy EPM

☒ Copy DEC

☐ Copy DEA Archaeologist

☐ Copy SHPO

If the project plans change then additional archaeological survey may be required. If human remains are discovered or a previously unidentified archaeological site is encountered, work must cease and the KYTC Division of Environmental Analysis be notified immediately.

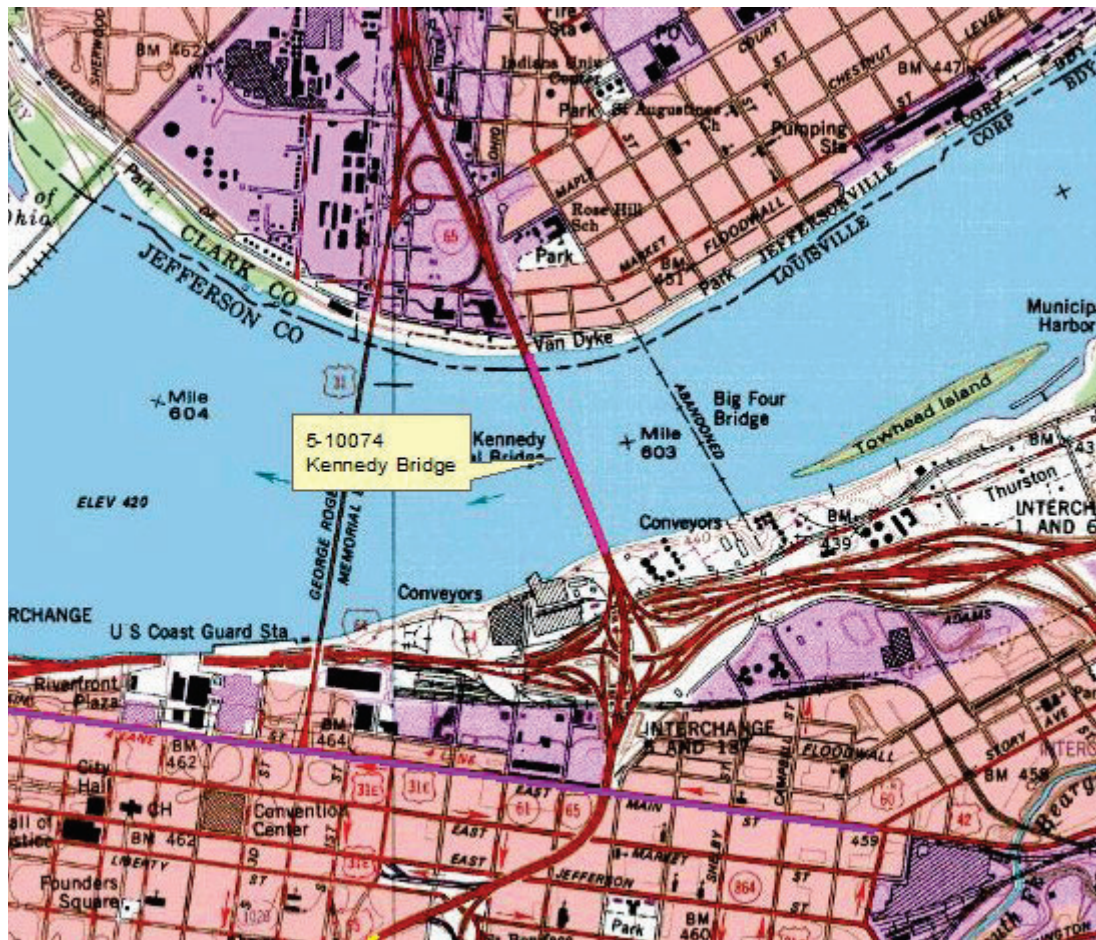
No archaeological work is warranted for this project. All work will occur on the bridge in existing ROW.

The OSA GIS was not consulted as there will be no ground disturbing activities.

KYTC Item No: 5-10074.00

County: Jefferson

Route: I-65 SB



KENTUCKY

STATEWIDE TRANSPORTATION IMPROVEMENT PROGRAM (STIP) For FY 2021-2024

AMENDMENT #2021.005

I. Proposed Action:

The Kentucky Transportation Cabinet (KYTC) hereby submits the attached resolution from the Kentuckiana Regional Planning and Development Agency (KIPDA) Transportation Policy Committee stating their approval of the Fiscal Years 2023-2026 Transportation Improvement Program (TIP). The KYTC requests inclusion of the TIP in the KYTC's FY 2021-2024 Statewide Transportation Improvement Program (STIP).

Location: Louisville/Jefferson County, KY-Indiana Metropolitan Planning Area

II. Additional Remarks:

Attached is a copy of the Resolution, Self-Certification, Conformity Determination, and Governor's Designee approval letter.

III. Amendment Approval:

Amendment Recommended for Approval:

Jill Lamb for Ron Rigney 7/10/2023
Kentucky Transportation Cabinet Date
Ronald B. Rigney, Director
Division of Program Management

Approval of STIP Amendment:

JOHN D
BALLANTYNE Digitally signed by JOHN D
BALLANTYNE
Date: 2023.07.18 12:11:49 -04'00'
Federal Highway Administration Date

KENTUCKY

STATEWIDE TRANSPORTATION IMPROVEMENT PROGRAM (STIP)
For FY 2021-2024

AMENDMENT #2021.005

I. Proposed Action:

The Kentucky Transportation Cabinet (KYTC) hereby submits the attached resolution from the Kentuckiana Regional Planning and Development Agency (KIPDA) Transportation Policy Committee stating their approval of the Fiscal Years 2023-2026 Transportation Improvement Program (TIP). The KYTC requests inclusion of the TIP in the KYTC's FY 2021-2024 Statewide Transportation Improvement Program (STIP).

Location: Louisville/Jefferson County, KY-Indiana Metropolitan Planning Area

II. Additional Remarks:

Attached is a copy of the Resolution, Self-Certification, Conformity Determination and Governor's Designee approval letter.

III. Amendment Approval:

Amendment Recommended for Approval:

Approval of STIP Amendment:

Jill Lamb for Ron Rigney 7/10/2023
Kentucky Transportation Cabinet Date
Ronald B. Rigney, Director
Division of Program Management

Robert Sachnin 7/17/23
Federal Transit Administration Date

ADOPTED
MAY 2023

FY 2023 – FY 2026

TRANSPORTATION IMPROVEMENT PROGRAM

LOUISVILLE/JEFFERSON COUNTY, KY-IN
METROPOLITAN PLANNING AREA



K I P D A
Kentuckiana Regional Planning
& Development Agency



Area Agency on Aging and Independent Living
Area Development District
Metropolitan Planning Organization



**A Resolution of the
Kentuckiana Regional Planning and Development Agency
Transportation Policy Committee adopting the
FY 2023 - FY 2026 Transportation Improvement Program**

Whereas, the Kentuckiana Regional Planning and Development Agency (KIPDA) Transportation Policy Committee is designated by the governors of the State of Indiana and the Commonwealth of Kentucky under state and federal laws as the Metropolitan Planning Organization (MPO) for the Louisville/Jefferson County KY-IN Planning Area encompassing Clark and Floyd counties and a portion of Harrison County in Indiana, and Bullitt, Jefferson, and Oldham counties and a portion of Shelby County in Kentucky; and,

Whereas, consistent with federal and state mandates, states' environmental requirements, and with the KIPDA Transportation Policy Committee's Memorandum of Agreement, *Participation Plan*, *Title VI: Environmental Justice Plan*, *Project Management Guidebook*, *Complete Streets Policy*, and other operating procedures, the KIPDA Transportation Policy Committee has worked with local, state, and federal jurisdictions and agencies in a continuing, cooperative, and comprehensive planning process; and has incorporated the work of local governments, and the suggestions of citizens, businesses, and interests throughout the MPA in this document; and

Whereas, the *FY 2023 - FY 2026 Transportation Improvement Program for the Louisville/Jefferson County (KY-IN) Metropolitan Planning Area* is a subset of *Connecting Kentuckiana 2050*, the Louisville/Jefferson County (KY-IN) Metropolitan Transportation Plan, which has been determined to conform to the State Implementation Plans of Indiana and Kentucky; and,

Whereas, the *FY2023 -FY 2026 Transportation Improvement Program for the Louisville/Jefferson County (KY-IN) Metropolitan Planning Area* is fiscally constrained. The Kentucky and Indiana non-dedicated funded projects identified in this TIP have been requested by the Indiana Department of Transportation (INDOT) and the Kentucky Transportation Cabinet (KYTC). The required funds will become part of the Kentucky FY 2021 - 2024 Statewide Transportation Improvement Program (STIP), and the Indiana FY 2022 - 2026 STIP, respectively, and will become part of the end of fiscal year, fiscal constraint recalculations; and,

Now, therefore be it resolved, by the Transportation Policy Committee of KIPDA that the *FY 2023 - FY 2026 Transportation Improvement Program for the Louisville/Jefferson County (KY-IN) Metropolitan Planning Area* is adopted by official action at the May 25, 2023, meeting. This action is contingent upon and effective when a planning conformity finding is made by the appropriate federal agencies; and,

11520 Commonwealth Drive
Louisville, KY 40299
www.kipda.org

Phone: 502.266.6084
Fax: 502.266.5047
TDD: 800.648.6056





Area Agency on Aging and Independent Living
Area Development District
Metropolitan Planning Organization



Be it further resolved, that the KIPDA staff is authorized to transmit the *FY 2023 – FY 2026 Transportation Improvement Program for the Louisville/Jefferson County (KY-IN) Metropolitan Planning Area* to the governors of the State of Indiana and the Commonwealth of Kentucky, and to the Indiana Department of Transportation and the Kentucky Transportation Cabinet in compliance with Federal and state requirements.


Whereas, the KIPDA Transportation Policy Committee is to certify that *Connecting Kentuckiana 2050* complies with all of the applicable requirements of the Federal Transit Act, Clean Air Act, Americans with Disabilities Act, Civil Rights Act, Federal Transportation Act, and all other applicable state and federal laws; and,

Whereas, *Connecting Kentuckiana 2050*, will serve as the KIPDA Metropolitan Transportation Plan under federal law contingent upon and effective when a conformity finding is made by the appropriate federal agencies; and,

Now, therefore let it be resolved, that the KIPDA Transportation Policy Committee adopts the *Connecting Kentuckiana 2050 Metropolitan Transportation Plan* to serve as the KIPDA MPO official Metropolitan Transportation Plan; and,

Let it be further resolved that KIPDA staff is authorized to transmit *Connecting Kentuckiana 2050* to the Governors of the State of Indiana and the Commonwealth of Kentucky, and to the Indiana Department of Transportation and the Kentucky Transportation Cabinet in compliance with federal and state requirements.

Adopted by the KIPDA Transportation Policy Committee on the 25th day of May 2023.


Bernie Bowling,
St. Mathews City Council,
KIPDA Transportation
Policy Committee, Chair


Andy Rush,
KIPDA Transportation Division,
Director

Kentucky Maintenance Projects

Sponsor Agency: KYTC

Project Name: I-65 Bridge at Standiford Lane

KIPDA ID:	State ID/DES #:	Open to Public:	County/Counties:	Project Cost:	AQ Analysis Status:
3104	5-10083.00	2027	Jefferson	\$3,663,000	Exempt

Project Description:

Bridge project in Jefferson County on (056B00390N) I-65 at Standiford Lane.

Justification:

Increase safety for all users. Maintain the existing transportation network in a good state of repair.

Phase:	Year:	Funding Category:	Federal:	State/Local:	Total:
D	2025	BRO	\$266,400	\$66,600	\$333,000
C	2025	BRO	\$2,664,000	\$666,000	\$3,330,000
Total			\$2,930,400	\$732,600	\$3,663,000

Sponsor Agency: KYTC

Project Name: I-65 Bridge at US 60A (Eastern Parkway)

KIPDA ID:	State ID/DES #:	Open to Public:	County/Counties:	Project Cost:	AQ Analysis Status:
3112	5-10065.00	2028	Jefferson	\$6,094,000	Exempt

Project Description:

Bridge project in Jefferson County on (056B00180N) I-65 at US 60A (Eastern Parkway).

Justification:

Increase safety for all users. Maintain the existing transportation network in a state of good repair.

Phase:	Year:	Funding Category:	Federal:	State/Local:	Total:
D	2026	BRO	\$443,200	\$110,800	\$554,000
C	2026	BRO	\$4,432,000	\$1,108,000	\$5,540,000
Total			\$4,875,200	\$1,218,800	\$6,094,000

Sponsor Agency: KYTC

Project Name: I-65 SB Bridge at Ohio River

KIPDA ID:	State ID/DES #:	Open to Public:	County/Counties:	Project Cost:	AQ Analysis Status:
3028	5-10074.00	2025	Jefferson	\$1,320,000	Exempt

Project Description:

Bridge project in Jefferson County on (056B00214L) I-65 Southbound at Ohio River.

Justification:

Increase safety for all users. Maintain the existing transportation network in a state of good repair.

Phase:	Year:	Funding Category:	Federal:	State/Local:	Total:
D	2023	Bridge	\$96,000	\$24,000	\$120,000
C	2023	BRO	\$960,000	\$240,000	\$1,200,000
Total			\$1,056,000	\$264,000	\$1,320,000

and there is no suitable bat habitat present under this bridge, there will be no effect on roosting habitat for Gray bats.

Indiana and Northern long-eared bat roosting habitat includes any tree > 5” (IB) and > 3” dbh (NLEB) that possesses any or all of the following characteristics: exfoliating bark, dead or dying trunk/limbs, cavities, and fissures. Commuting Indiana and Northern long-eared bat habitat includes areas connecting foraging habitat to roosting habitat. This includes fencerows, tree lines, and open canopy breaks in forest habitat. Indiana and Northern long-eared bat foraging habitat includes closed to semi-open forested habitat and forested edges, including floodplain, riparian, lowland, and upland forests. Winter habitat for Indiana and Northern long-eared bats includes caves or deep mines including coal and limestone operations. There is forested habitat in proximity to the bridge, but there is no tree removal associated with this project, so there will be no impacts to roosting or commuting habitat for these species. This project does impact a bridge but given there will only be maintenance activities for this project and there is no suitable bat habitat present under this bridge, there will be no effect on roosting habitat for these bat species. The Ohio River is located underneath the John F. Kennedy bridge which provides foraging habitat for both bat species. Given this project is only providing maintenance to both the joints and bearings, it will have no impact on foraging habitat for these species.

Mussels: The listed freshwater mussel species can vary heavily in habitat preferences. Generally, they can prefer anywhere from small streams to large rivers depending on suitable habitat present. They require perennial flowing water of good quality with heterogeneous substrate mixtures typically consisting of gravel/cobble/sand. There is suitable habitat for listed mussel species in the Ohio River under this bridge, but the maintenance nature of this project would keep from any impacts to the river below the bridge. Thus, there will be no effects on the listed mussel species.

Determinations: no effect for:

Gray Bat	<i>Myotis grisescens</i>
Indiana Bat	<i>Myotis sodalis</i>
Northern Long-eared Bat	<i>Myotis septentrionalis</i>
Clubshell	<i>Pleurobema clava</i>
Fanshell	<i>Cyprogenia stegaria</i>
Northern Riffleshell	<i>Epioblasma rangiana</i>
Orangefoot Pimpleback	<i>Plethobasus cooperianus</i>
Pink Mucket	<i>Lampsilis abrupta</i>
Rabbitsfoot	<i>Theliderma cylindrica</i>
Ring Pink	<i>Obovaria retusa</i>
Rough Pigtoe	<i>Pleurobema plenum</i>

The project has been assessed in accordance with the provisions of Section 7 of the Endangered Species Act. As a designated representative of the FHWA, the KYTC has determined that the project will have No Effect on any listed species or their critical habitat, and further Section 7(a)(2) consultation with the Service is not required.

Makayla Beckner
KYTC Signature

12/19/2023
Date

Makayla Beckner
Print Name

E.A.T.S. Milestones updated

MCB
Name

12/19/2023
Date



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Kentucky Ecological Services Field Office
J C Watts Federal Building, Room 265
330 West Broadway
Frankfort, KY 40601-8670
Phone: (502) 695-0468 Fax: (502) 695-1024
Email Address: kentuckyes@fws.gov



In Reply Refer To:
Project Code: 2024-0027546
Project Name: 5-10074

December 18, 2023

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the

12/18/2023

2

human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<https://www.fws.gov/sites/default/files/documents/endangered-species-consultation-handbook.pdf>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts, see <https://www.fws.gov/program/migratory-bird-permit/what-we-do..>

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures, see <https://www.fws.gov/library/collections/threats-birds>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of Executive Order 13186, please visit <https://www.fws.gov/partner/council-conservation-migratory-birds>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. Please include the Consultation Code in the header of

12/18/2023

3

this letter with any request for consultation or correspondence about your project that you submit to our office.

Note: IPaC has provided all available attachments because this project is in multiple field office jurisdictions.

Attachment(s):

- Official Species List
- USFWS National Wildlife Refuges and Fish Hatcheries
- Bald & Golden Eagles
- Migratory Birds
- Wetlands

OFFICIAL SPECIES LIST

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Kentucky Ecological Services Field Office

J C Watts Federal Building, Room 265
330 West Broadway
Frankfort, KY 40601-8670
(502) 695-0468

This project's location is within the jurisdiction of multiple offices. However, only one species list document will be provided for all offices. The species and critical habitats in this document reflect the aggregation of those that fall in each of the affiliated office's jurisdiction. Other offices affiliated with the project:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

12/18/2023

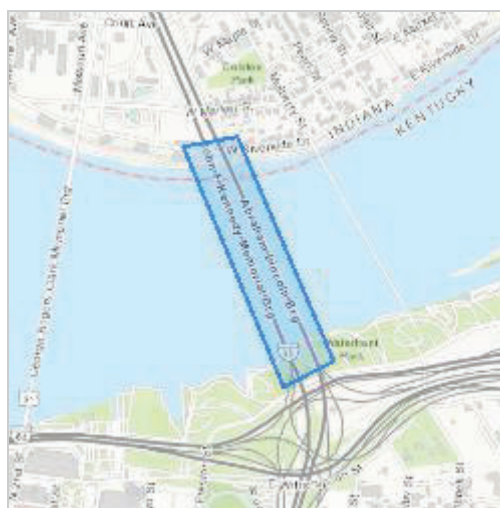
4

PROJECT SUMMARY

Project Code: 2024-0027546
Project Name: 5-10074
Project Type: Bridge - Maintenance
Project Description: BRIDGE PROJECT IN JEFFERSON COUNTY ON (056B00214L) I-65
SB AT OHIO RIVER

Project Location:

The approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@38.2647867,-85.7432560957654,14z>



Counties: Indiana and Kentucky

ENDANGERED SPECIES ACT SPECIES

There is a total of 14 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

MAMMALS

NAME	STATUS
<div>Gray Bat <i>Myotis grisescens</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/6329 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/6422.pdf</div>	Endangered
<div>Indiana Bat <i>Myotis sodalis</i></div> <div>There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/6422.pdf</div>	Endangered
<div>Northern Long-eared Bat <i>Myotis septentrionalis</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9045 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/6422.pdf</div>	Endangered
<div>Tricolored Bat <i>Perimyotis subflavus</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/10515</div>	Proposed Endangered

BIRDS

NAME	STATUS
Whooping Crane <i>Grus americana</i> Population: U.S.A. (AL, AR, CO, FL, GA, ID, IL, IN, IA, KY, LA, MI, MN, MS, MO, NC, NM, OH, SC, TN, UT, VA, WI, WV, western half of WY) No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/758	Experimental Population, Non- Essential

CLAMS	
NAME	STATUS
<div>Clubshell <i>Pleurobema clava</i></div> <div>Population: Wherever found; Except where listed as Experimental Populations No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/3789 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/5639.pdf</div>	Endangered
<div>Fanshell <i>Cyprogenia stegaria</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/4822 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/5639.pdf</div>	Endangered
<div>Northern Riffleshell <i>Epioblasma rangiana</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/527 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/5639.pdf</div>	Endangered
<div>Orangefoot Pimpleback (pearlymussel) <i>Plethobasus cooperianus</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/1132 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/5639.pdf</div>	Endangered
<div>Pink Mucket (pearlymussel) <i>Lampsilis abrupta</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/7829 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/5639.pdf</div>	Endangered
<div>Rabbitsfoot <i>Quadrula cylindrica cylindrica</i></div> <div>There is final critical habitat for this species. Your location does not overlap the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5165 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/5639.pdf</div>	Threatened
<div>Ring Pink (mussel) <i>Obovaria retusa</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/4128 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/5639.pdf</div>	Endangered

NAME	STATUS
<div>Rough Pigtoe <i>Pleurobema plenum</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/6894 General project design guidelines: https://ipac.ecosphere.fws.gov/project/JKB3OMH5FNGYRG3U4EQ2AUISPM/documents/generated/5639.pdf</div>	Endangered

INSECTS

NAME	STATUS
<div>Monarch Butterfly <i>Danaus plexippus</i></div> <div>No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743</div>	Candidate

CRITICAL HABITATS

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

YOU ARE STILL REQUIRED TO DETERMINE IF YOUR PROJECT(S) MAY HAVE EFFECTS ON ALL ABOVE LISTED SPECIES.

USFWS NATIONAL WILDLIFE REFUGE LANDS AND FISH HATCHERIES

Any activity proposed on lands managed by the [National Wildlife Refuge](#) system must undergo a 'Compatibility Determination' conducted by the Refuge. Please contact the individual Refuges to discuss any questions or concerns.

THERE ARE NO REFUGE LANDS OR FISH HATCHERIES WITHIN YOUR PROJECT AREA.

BALD & GOLDEN EAGLES

Bald and golden eagles are protected under the Bald and Golden Eagle Protection Act¹ and the Migratory Bird Treaty Act².

Any person or organization who plans or conducts activities that may result in impacts to bald or golden eagles, or their habitats³, should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the ["Supplemental Information on Migratory Birds and Eagles"](#).

1. The [Bald and Golden Eagle Protection Act](#) of 1940.

2. The [Migratory Birds Treaty Act](#) of 1918.

3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

There are bald and/or golden eagles in your project area.

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

NAME	BREEDING SEASON
<div>Bald Eagle <i>Haliaeetus leucocephalus</i></div> <div>This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities.</div> <div>https://ecos.fws.gov/ecp/species/1626</div>	Breeds Sep 1 to Jul 31
<div>Golden Eagle <i>Aquila chrysaetos</i></div> <div>This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities.</div> <div>https://ecos.fws.gov/ecp/species/1680</div>	Breeds elsewhere

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read "[Supplemental Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

Breeding Season (■)

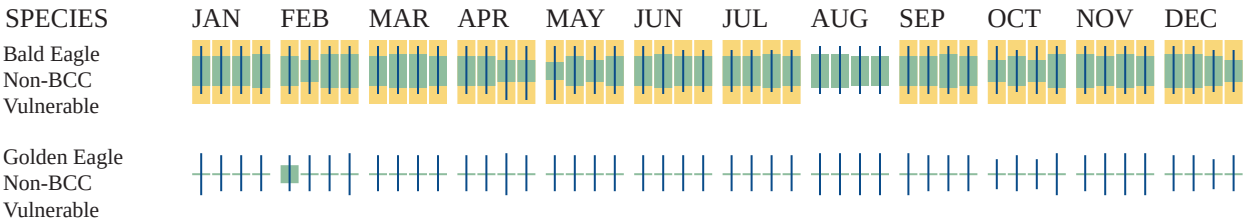
Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

Survey Effort (|)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)

A week is marked as having no data if there were no survey events for that week.



- Additional information can be found using the following links:
- Eagle Management <https://www.fws.gov/program/eagle-management>
 - Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
 - Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
 - Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

MIGRATORY BIRDS

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats³ should follow appropriate regulations and consider implementing appropriate conservation measures, as described in the links below. Specifically, please review the "[Supplemental Information on Migratory Birds and Eagles](#)".

-
1. The [Migratory Birds Treaty Act](#) of 1918.
 2. The [Bald and Golden Eagle Protection Act](#) of 1940.
 3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

For guidance on when to schedule activities or implement avoidance and minimization measures to reduce impacts to migratory birds on your list, see the PROBABILITY OF PRESENCE SUMMARY below to see when these birds are most likely to be present and breeding in your project area.

12/18/2023

11

NAME	BREEDING SEASON
Bald Eagle <i>Haliaeetus leucocephalus</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1626	Breeds Sep 1 to Jul 31
Black-billed Cuckoo <i>Coccyzus erythrophthalmus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9399	Breeds May 15 to Oct 10
Bobolink <i>Dolichonyx oryzivorus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9454	Breeds May 20 to Jul 31
Cerulean Warbler <i>Dendroica cerulea</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/2974	Breeds Apr 23 to Jul 20
Chimney Swift <i>Chaetura pelagica</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9406	Breeds Mar 15 to Aug 25
Eastern Whip-poor-will <i>Antrostomus vociferus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/10678	Breeds May 1 to Aug 20
Field Sparrow <i>Spizella pusilla</i> This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9446	Breeds Mar 1 to Aug 15
Golden Eagle <i>Aquila chrysaetos</i> This is not a Bird of Conservation Concern (BCC) in this area, but warrants attention because of the Eagle Act or for potential susceptibilities in offshore areas from certain types of development or activities. https://ecos.fws.gov/ecp/species/1680	Breeds elsewhere
Henslow's Sparrow <i>Ammodramus henslowii</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/3941	Breeds May 1 to Aug 31
Kentucky Warbler <i>Oporornis formosus</i> This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9443	Breeds Apr 20 to Aug 20

NAME	BREEDING SEASON
<div>Lesser Yellowlegs <i>Tringa flavipes</i></div> <div>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9679</div>	Breeds elsewhere
<div>Prairie Warbler <i>Dendroica discolor</i></div> <div>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9513</div>	Breeds May 1 to Jul 31
<div>Prothonotary Warbler <i>Protonotaria citrea</i></div> <div>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9439</div>	Breeds Apr 1 to Jul 31
<div>Red-headed Woodpecker <i>Melanerpes erythrocephalus</i></div> <div>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9398</div>	Breeds May 10 to Sep 10
<div>Rusty Blackbird <i>Euphagus carolinus</i></div> <div>This is a Bird of Conservation Concern (BCC) only in particular Bird Conservation Regions (BCRs) in the continental USA https://ecos.fws.gov/ecp/species/9478</div>	Breeds elsewhere
<div>Wood Thrush <i>Hylocichla mustelina</i></div> <div>This is a Bird of Conservation Concern (BCC) throughout its range in the continental USA and Alaska. https://ecos.fws.gov/ecp/species/9431</div>	Breeds May 10 to Aug 31

PROBABILITY OF PRESENCE SUMMARY

The graphs below provide our best understanding of when birds of concern are most likely to be present in your project area. This information can be used to tailor and schedule your project activities to avoid or minimize impacts to birds. Please make sure you read "[Supplemental Information on Migratory Birds and Eagles](#)", specifically the FAQ section titled "Proper Interpretation and Use of Your Migratory Bird Report" before using or attempting to interpret this report.

Probability of Presence (■)

Green bars; the bird's relative probability of presence in the 10km grid cell(s) your project overlaps during that week of the year.

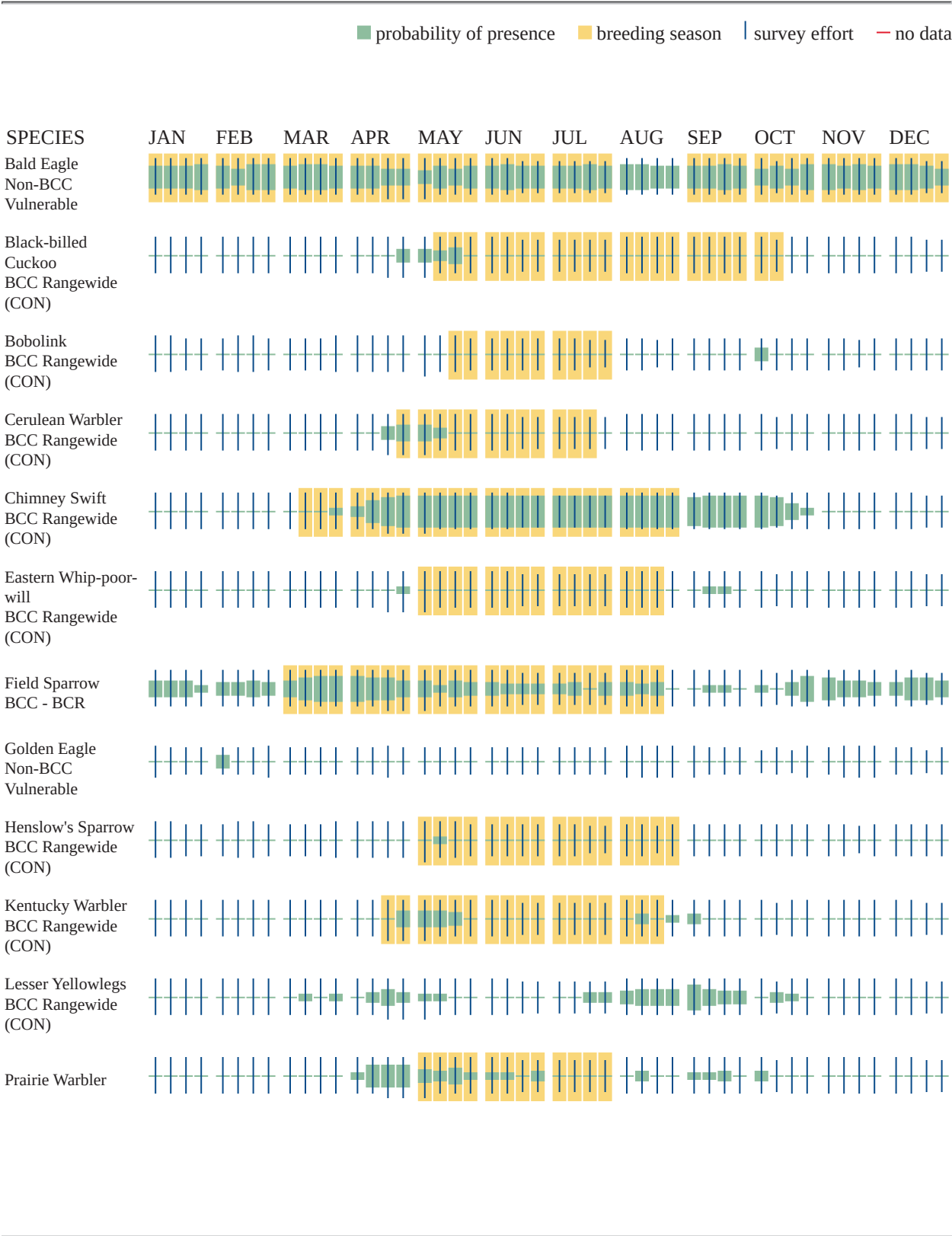
Breeding Season (■)

Yellow bars; liberal estimate of the timeframe inside which the bird breeds across its entire range.

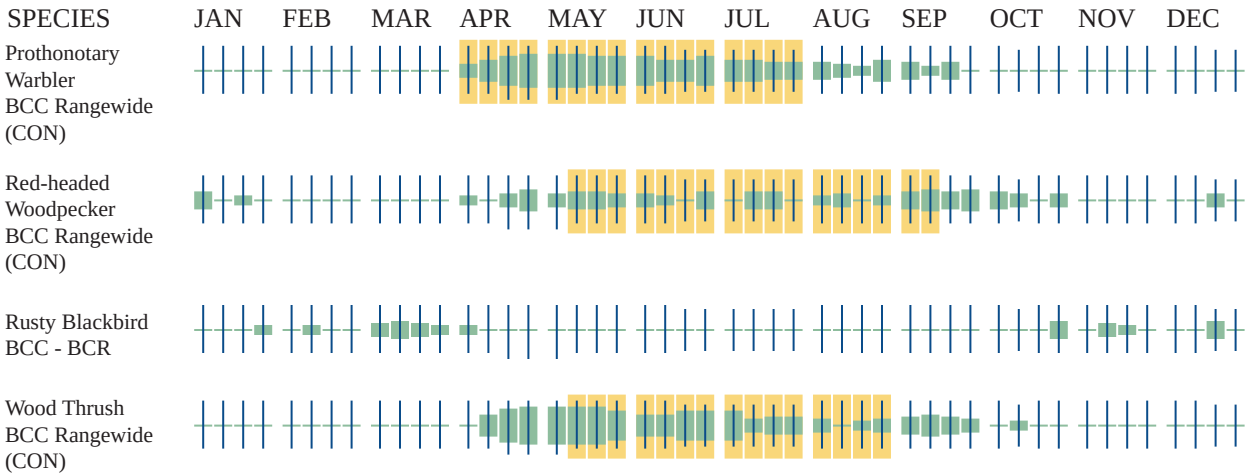
Survey Effort (l)

Vertical black lines; the number of surveys performed for that species in the 10km grid cell(s) your project area overlaps.

No Data (—)
A week is marked as having no data if there were no survey events for that week.



BCC Rangewide
(CON)



Additional information can be found using the following links:

- Eagle Management <https://www.fws.gov/program/eagle-management>
- Measures for avoiding and minimizing impacts to birds <https://www.fws.gov/library/collections/avoiding-and-minimizing-incidental-take-migratory-birds>
- Nationwide conservation measures for birds <https://www.fws.gov/sites/default/files/documents/nationwide-standard-conservation-measures.pdf>
- Supplemental Information for Migratory Birds and Eagles in IPaC <https://www.fws.gov/media/supplemental-information-migratory-birds-and-bald-and-golden-eagles-may-occur-project-action>

WETLANDS

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

- RIVERINE
- R2UBH

IPAC USER CONTACT INFORMATION

Agency: Kentucky Transportation Cabinet
Name: Makayla Beckner
Address: 200 Mero Street
City: FRANKFORT
State: KY
Zip: 40622
Email: makayla.beckner@ky.gov
Phone: 5027825029

UNITED STATES COAST GUARD REQUIRMENTS

The Contractor shall submit their work plan for approval with the United States Coast Guard for approval 14 days prior to the pre-construction conference with the completed following form.

U.S. Department of
Homeland Security

United States
Coast Guard



Commander
Eighth Coast Guard District

1222 Spruce Street, 2.102D
St. Louis, MO 63103-2832
Staff Symbol: dwb
Phone: (314) 269-2381
Email: rob.e.mccaskey@uscg.mil

16591.1/603.1 OHR
December 19, 2023

Mr. Tom Mathews
Kentucky Transportation Cabinet
Division Of Maint. Bridge Preservation Branch
200 Mero Street
Frankfort, KY
40622

Subj: John F. Kennedy Memorial Highway Bridge, MILE 603.1, Ohio River

Dear Mr. Mathews:

This letter is confirming that we received the contractor's Michael Baker International, work plan informing us of the work to be conducted on the subject bridge. Since the work does not result in the permanent encroachment on the navigational clearances or change in the configuration of the structure you may proceed with the work as described. The work plan is approved provided the following precautionary measures are taken during the performance of the work:

Condition 1: Before work proceeds, the name and phone number of the person who may be contacted on a 24-hour basis to respond to an emergency at the work site must be provided to this office. The emergency responder must be able to react to and immediately correct any problems that result if equipment becomes loose and extends into the navigation spans.

Condition 2: All proposed changes or deviations from these conditions shall be submitted to the Coast Guard Bridge Branch, St. Louis, Missouri for consideration. Drawings related to proposed changes are to be presented legibly in an 8 ½" x 11" format.

Condition 3: All work shall be performed so the free flow of navigation is not unreasonably interfered with and the navigation depths are not impaired.

Condition 4: Positive precautions shall be taken to prevent the accidental dropping of spark-producing, flame-producing, lighted or other damaging objects onto barges or vessels passing beneath the bridge. All flame-cutting, welding, and similar spark-producing operations shall be ceased over the channel when vessels are passing beneath the bridge.

Condition 5: Nothing can be allowed to drop into the river. If any condition exists that could result in any material deposition into the river, precautions such as positioning a "catch" barge or containment system beneath the work site shall be taken to ensure that nothing falls into the river. Any containment that is installed shall not extend more than 2 feet below low steel following the contours of the bridge.

Subj: JFK memorial Hwy Bridge, MILE 603.1
Ohio River

16591.1/603.1 OHR
December 19, 2023

Condition 6: The work should not be allowed to interfere with displaying navigation lights on the bridge at night or when visibility is less than one mile.

Condition 7: There are several factors, which may impact navigation and require further restrictions to work barge management. If unsafe conditions prevail due to high water levels, swift currents, or strong winds, this approval may need to be modified.

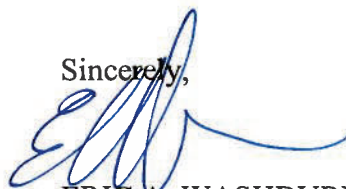
Condition 8: The Federal Water Pollution Control Act, as amended, prohibits the discharge of oil (including oil based paints) or hazardous substances into the waters of the United States. The law requires any person in charge of a vessel or facility from which oil or a hazardous substance is discharged to immediately report the discharge to the U.S. Coast Guard National Response Center, 800-424-8802 (toll free). The owner/operator of a vessel or facility from which the pollutant is discharge is subject to a civil penalty of up to \$10,000 and is liable for cleanup costs, if any.

Condition 9: Any containment system used must not extend more than 5 feet below low steel following the contour of the bridge and may be required to be raised in the event of high water.

This office must be kept informed on the status of this work to enable us to issue cautionary notices to mariners. Please furnish us the call sign, operating frequency and name of the onsite personnel so that the information can be included in our notices. Please complete the enclosed Project Information Record and return it so we can contact the person responsible for compliance with this letter.

Please contact Mr. Rob McCaskey at the above phone number to provide work status updates or for additional information.

Sincerely,



ERIC A. WASHBURN
Bridge Administrator, Western Rivers
By direction of the District Commander

Enclosure: Project Information Form

Copy: CG SECTOR OHR w/o encl
CDRUSAED Louisville w/o encl

PROMPTLY COMPLETE AND PROVIDE COPY TO:

Commander (dwb)
Eighth Coast Guard District
1222 Spruce Street
St. Louis, MO 63103-2832

(314) 269-2381
FAX (314) 269-2737

PROJECT INFORMATION RECORD

NAME OF BRIDGE: _____ RIVER/MILE: _____

PROJECT: _____

PERMITTEE: _____

RESIDENT ENGINEER OR INSPECTOR: _____
(NAME)

ADDRESS: _____

PHONE: _____
(OFFICE) (HOME)

SUBCONTRACTOR: _____

SUBCONTRACTOR'S MAN IN CHARGE: _____

ALTERNATES: _____

START DATE: _____ EXPECTED COMPLETION DATE: _____

NAME OR WORK BOAT ON JOB: _____

RADIO CALL SIGN AND FREQUENCIES: _____

HOURS/DAYS OF OPERATION: _____

(SIGNATURE)

PART II

SPECIFICATIONS AND STANDARD DRAWINGS

STANDARD SPECIFICATIONS

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2019* and *Standard Drawings, Edition of 2020*.

SUPPLEMENTAL SPECIFICATIONS

The contractor shall use the Supplemental Specifications that are effective at the time of letting. The Supplemental Specifications can be found at the following link:
<http://transportation.ky.gov/Construction/Pages/Kentucky-Standard-Specifications.aspx>

PART III

EMPLOYMENT, WAGE AND RECORD REQUIREMENTS

**TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS**

**LABOR AND WAGE REQUIREMENTS
APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS**

- I. Application
- II. Nondiscrimination of Employees (KRS 344)

I. APPLICATION

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual because of his race, color, religion, national origin, sex, disability or age forty (40) and over, in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

Revised: January 25, 2017

II. NONDISCRIMINATION OF EMPLOYEES

**AN ACT OF THE KENTUCKY
GENERAL ASSEMBLY TO PREVENT
DISCRIMINATION IN EMPLOYMENT
KRS CHAPTER 344
EFFECTIVE JUNE 16, 1972**

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (forty and above); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age forty (40) and over. The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, except that such a notice or advertisement may indicate a preference, limitation, or specification based on religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, when religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, is a bona fide occupational qualification for employment.

EXECUTIVE BRANCH CODE OF ETHICS

In the 1992 regular legislative session, the General Assembly passed and Governor Brereton Jones signed Senate Bill 63 (codified as KRS 11A), the Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (7) provides:

No present or former public servant shall, within six (6) months following termination of his office or employment, accept employment, compensation, or other economic benefit from any person or business that contracts or does business with, or is regulated by, the state in matters in which he was directly involved during the last thirty-six (36) months of his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, or for which he received, prior to his state employment, a professional degree or license, provided that, for a period of six (6) months, he personally refrains from working on any matter in which he was directly involved during the last thirty-six (36) months of his tenure in state government. This subsection shall not prohibit the performance of ministerial functions, including but not limited to filing tax returns, filing applications for permits or licenses, or filing incorporation papers, nor shall it prohibit the former officer or public servant from receiving public funds disbursed through entitlement programs.

KRS 11A.040 (9) states:

A former public servant shall not represent a person or business before a state agency in a matter in which the former public servant was directly involved during the last thirty-six (36) months of his tenure, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not as a way to obtain private benefits.

If you have worked for the executive branch of state government within the past six months, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, 1025 Capital Center Drive, Suite 104, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Revised: May 23, 2022

Kentucky Equal Employment Opportunity Act of 1978

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:
<https://www.eProcurement.ky.gov>.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **finance.contractcompliance@ky.gov** or by phone at 502-564-2874.

EMPLOYEE RIGHTS UNDER THE FAIR LABOR STANDARDS ACT

THE UNITED STATES DEPARTMENT OF LABOR WAGE AND HOUR DIVISION

FEDERAL MINIMUM WAGE

\$7.25 PER HOUR

BEGINNING JULY 24, 2009

- OVERTIME PAY** At least 1½ times your regular rate of pay for all hours worked over 40 in a workweek.
- CHILD LABOR** An employee must be at least **16** years old to work in most non-farm jobs and at least **18** to work in non-farm jobs declared hazardous by the Secretary of Labor.
- Youths **14** and **15** years old may work outside school hours in various non-manufacturing, non-mining, non-hazardous jobs under the following conditions:
- No more than*
- **3** hours on a school day or **18** hours in a school week;
 - **8** hours on a non-school day or **40** hours in a non-school week.
- Also, work may not begin before **7 a.m.** or end after **7 p.m.**, except from June 1 through Labor Day, when evening hours are extended to **9 p.m.** Different rules apply in agricultural employment.
- TIP CREDIT** Employers of “tipped employees” must pay a cash wage of at least \$2.13 per hour if they claim a tip credit against their minimum wage obligation. If an employee’s tips combined with the employer’s cash wage of at least \$2.13 per hour do not equal the minimum hourly wage, the employer must make up the difference. Certain other conditions must also be met.
- ENFORCEMENT** The Department of Labor may recover back wages either administratively or through court action, for the employees that have been underpaid in violation of the law. Violations may result in civil or criminal action.
- Employers may be assessed civil money penalties of up to \$1,100 for each willful or repeated violation of the minimum wage or overtime pay provisions of the law and up to \$11,000 for each employee who is the subject of a violation of the Act’s child labor provisions. In addition, a civil money penalty of up to \$50,000 may be assessed for each child labor violation that causes the death or serious injury of any minor employee, and such assessments may be doubled, up to \$100,000, when the violations are determined to be willful or repeated. The law also prohibits discriminating against or discharging workers who file a complaint or participate in any proceeding under the Act.
- ADDITIONAL INFORMATION**
- Certain occupations and establishments are exempt from the minimum wage and/or overtime pay provisions.
 - Special provisions apply to workers in American Samoa and the Commonwealth of the Northern Mariana Islands.
 - Some state laws provide greater employee protections; employers must comply with both.
 - The law requires employers to display this poster where employees can readily see it.
 - Employees under 20 years of age may be paid \$4.25 per hour during their first 90 consecutive calendar days of employment with an employer.
 - Certain full-time students, student learners, apprentices, and workers with disabilities may be paid less than the minimum wage under special certificates issued by the Department of Labor.

For additional information:



1-866-4-USWAGE

(1-866-487-9243)

TTY: 1-877-889-5627



WWW.WAGEHOUR.DOL.GOV

PART IV

BID ITEMS

Section: 0001 - BRIDGES - B00214L KENNEDY BR OVER OHIO RI-TRUSS BEARING REPA

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0010	23171EC		CONCRETE REPAIR - PIER 6L DS MASONRY PLATE	1.00	LS		\$	
0020	24879EC		STEEL REPAIR - WIND LOCK AT PIER 6L	1.00	EACH		\$	
0030	24879EC		STEEL REPAIR - TRUSS PIN REPLACEMENT - PIER 1L DS	1.00	EACH		\$	
0040	24879EC		STEEL REPAIR - TRUSS PIN REPLACEMENT - PIER 1L US	1.00	EACH		\$	
0050	24879EC		STEEL REPAIR - TRUSS PIN REPLACEMENT - PIER 6L DS	1.00	EACH		\$	
0060	24879EC		STEEL REPAIR - TRUSS PIN REPLACEMENT - PIER 6L US	1.00	EACH		\$	

Section: 0002 - BRIDGES - B00214L KENNEDY BR OVER OHIO RI-FINGER JOINT REPAIR

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0070	24430EC		REM AND REPLACE FINGER EXPANSION JOINT - JOINT L0	1.00	EACH		\$	
0080	24430EC		REM AND REPLACE FINGER EXPANSION JOINT - JOINT L0'	1.00	EACH		\$	
0090	24430EC		REM AND REPLACE FINGER EXPANSION JOINT - JOINT L23	1.00	EACH		\$	
0100	24430EC		REM AND REPLACE FINGER EXPANSION JOINT - JOINT L23'	1.00	EACH		\$	

Section: 0003 - TRAFFIC CONTROL - PLAN SET A & B

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0110	01984		DELINEATOR FOR BARRIER - WHITE	30.00	EACH		\$	
0120	01985		DELINEATOR FOR BARRIER - YELLOW	30.00	EACH		\$	
0130	02003		RELOCATE TEMP CONC BARRIER	2,920.00	LF		\$	
0140	02562		TEMPORARY SIGNS	971.00	SQFT		\$	
0150	02650		MAINTAIN & CONTROL TRAFFIC	1.00	LS		\$	
0160	02671		PORTABLE CHANGEABLE MESSAGE SIGN	3.00	EACH		\$	
0170	02775		ARROW PANEL	3.00	EACH		\$	
0180	03171		CONCRETE BARRIER WALL TYPE 9T	2,920.00	LF		\$	
0190	06549		PAVE STRIPING-TEMP REM TAPE-B	5,430.00	LF		\$	
0200	06550		PAVE STRIPING-TEMP REM TAPE-W	9,575.00	LF		\$	
0210	06551		PAVE STRIPING-TEMP REM TAPE-Y	7,685.00	LF		\$	
0220	06556		PAVE STRIPING-DUR TY 1-6 IN W	3,960.00	LF		\$	
0230	06557		PAVE STRIPING-DUR TY 1-6 IN Y	3,000.00	LF		\$	
0240	20411ED		LAW ENFORCEMENT OFFICER	240.00	HOURL		\$	
0250	25075EC		QUEUE PROTECTION VEHICLE	960.00	HOURL		\$	
0260	25117EC		FURNISH QUEUE PROTECTION VEHICLES	8.00	MONT		\$	

252944

PROPOSAL BID ITEMS

Page 2 of 2

Report Date 12/19/24

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0270	26136EC		PORTABLE QUEUE WARNING ALERT SYSTEM	8.00	MONT		\$	
0280	26137EC		QUEUE WARNING PCMS	24.00	MONT		\$	
0290	26138EC		QUEUE WARNING PORTABLE RADAR SENSORS	24.00	MONT		\$	

Section: 0004 - DEMOBILIZATION &/OR MOBILIZATION

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0300	02568		MOBILIZATION	1.00	LS		\$	
0310	02569		DEMOBILIZATION	1.00	LS		\$	