



**CALL NO. 321**

**CONTRACT ID. 242126**

**JEFFERSON COUNTY**

**FED/STATE PROJECT NUMBER FD04 056 1065 005-008**

**DESCRIPTION OUTER LOOP (KY 1065)**

**WORK TYPE JPC PAVEMENT**

**PRIMARY COMPLETION DATE 10/31/2024**

**LETTING DATE: March 21,2024**

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME March 21,2024. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

**NO PLANS ASSOCIATED WITH THIS PROJECT.**

**REQUIRED BID PROPOSAL GUARANTY:** Not less than 5% of the total bid.

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# **PART I**

## **SCOPE OF WORK**

**ADMINISTRATIVE DISTRICT - 05**

**CONTRACT ID - 242126**  
**FD04 056 1065 005-008**  
**COUNTY - JEFFERSON**  
**PCN - MP05610652401**  
**FD04 056 1065 005-008**

OUTER LOOP (KY 1065) (MP 5.470) BEGIN AT BRIARCLIFF ROAD EXTENDING EAST TO ASPHALT PAVEMENT  
JOINT 0.2 MILES WEST OF DONEGAL DRIVE (MP 7.717), A DISTANCE OF 02.24 MILES.JPC PAVEMENT  
GEOGRAPHIC COORDINATES LATITUDE 38:08:16.00 LONGITUDE 85:40:21.00  
ADT 24,927

**COMPLETION DATE(S):**  
COMPLETED BY 10/31/2024                      APPLIES TO ENTIRE CONTRACT

## **CONTRACT NOTES**

### **PROPOSAL ADDENDA**

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

### **BID SUBMITTAL**

Bidder must use the Department's electronic bidding software. The Bidder must download the bid file located on the Bid Express website ([www.bidx.com](http://www.bidx.com)) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

### **JOINT VENTURE BIDDING**

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

### **UNDERGROUND FACILITY DAMAGE PROTECTION**

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. When prescribed in said directives, the contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom shall be contacted through their individual Protection Notification Center. Non-compliance with these directives can result in the enforcement of penalties.

### **REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY**

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth ("certificate") from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

**For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity's solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.**

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

### **SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT**

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by email to [kytc.projectquestions@ky.gov](mailto:kytc.projectquestions@ky.gov). The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading "Questions & Answers" on the Construction Procurement website ([www.transportation.ky.gov/construction-procurement](http://www.transportation.ky.gov/construction-procurement)). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

### **HARDWOOD REMOVAL RESTRICTIONS**

The US Department of Agriculture has imposed a quarantine in Kentucky and several surrounding states, to prevent the spread of an invasive insect, the emerald ash borer. Hardwood cut in conjunction with the project may not be removed from the state. Chipping or burning on site is the preferred method of disposal.

### **INSTRUCTIONS FOR EXCESS MATERIAL SITES AND BORROW SITES**

Identification of excess material sites and borrow sites shall be the responsibility of the Contractor. The Contractor shall be responsible for compliance with all applicable state and federal laws and may wish to consult with the US Fish and Wildlife Service to seek protection under Section 10 of the Endangered Species Act for these activities.

### **ACCESS TO RECORDS**

The contractor, as defined in KRS 45A.030 (9) agrees that the contracting agency, the Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this contract for the purpose of financial audit or program review. Records and other prequalification information confidentially disclosed as part of the bid process shall not be deemed as directly pertinent to the contract and

shall be exempt from disclosure as provided in KRS 61.878(1)(c). The contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884.

In the event of a dispute between the contractor and the contracting agency, Attorney General, or the Auditor of Public Accounts over documents that are eligible for production and review, the Finance and Administration Cabinet shall review the dispute and issue a determination, in accordance with Secretary's Order 11-004.

### **BOYCOTT PROVISIONS**

If applicable, the contractor represents that, pursuant to [KRS 45A.607](#), they are not currently engaged in, and will not for the duration of the contract engage in, the boycott of a person or an entity based in or doing business with a jurisdiction with which Kentucky can enjoy open trade. **Note:** The term Boycott does not include actions taken for bona fide business or economic reasons, or actions specifically required by federal or state law.

If applicable, the contractor verifies that, pursuant to KRS 41.480, they do not engage in, and will not for the duration of the contract engage in, in energy company boycotts as defined by KRS 41.472.

### **LOBBYING PROHIBITIONS**

The contractor represents that they, and any subcontractor performing work under the contract, have not violated the agency restrictions contained in [KRS 11A.236](#) during the previous ten (10) years, and pledges to abide by the restrictions set forth in such statute for the duration of the contract awarded.

The contractor further represents that, pursuant to [KRS 45A.328](#), they have not procured an original, subsequent, or similar contract while employing an executive agency lobbyist who was convicted of a crime related to the original, subsequent, or similar contract within five (5) years of the conviction of the lobbyist.

Revised: 2/29/2024

## 1.0 BUY AMERICA REQUIREMENT.

Follow the “Buy America” provisions as required by 23 U.S.C. § 313 and 23 C.F.R. § 635.410. Except as expressly provided herein all manufacturing processes of steel or iron materials including but not limited to structural steel, guardrail materials, corrugated steel, culvert pipe, structural plate, prestressing strands, and steel reinforcing bars shall occur in the United States of America, including the application of:

- Coating,
- Galvanizing,
- Painting, and
- Other coating that protects or enhances the value of steel or iron products.

The following are exempt, unless processed or refined to include substantial amounts of steel or iron material, and may be used regardless of source in the domestic manufacturing process for steel or iron material:

- Pig iron,
- Processed, pelletized, and reduced iron ore material, or
- Processed alloys.

The Contractor shall submit a certification stating that all manufacturing processes involved with the production of steel or iron materials occurred in the United States.

Produce, mill, fabricate, and manufacture in the United States of America all aluminum components of bridges, tunnels, and large sign support systems, for which either shop fabrication, shop inspection, or certified mill test reports are required as the basis of acceptance by the Department.

Use foreign materials only under the following conditions:

- 1) When the materials are not permanently incorporated into the project; or
- 2) When the delivered cost of such materials used does not exceed 0.1 percent of the total Contract amount or \$2,500.00, whichever is greater.

The Contractor shall submit to the Engineer the origin and value of any foreign material used.

## 2.0 – BUILD AMERICA, BUY AMERICA (BABA)

Contractor shall comply with the Federal Highway Administration (FHWA) Buy America Requirement in 23 C.F.R. § 635.410 and all relevant provisions of the Build America, Buy America Act (BABA), contained within the Infrastructure Investment and Jobs Act, Pub. L. No. 117-58, §§ 70901-52 enacted November 15, 2021. The BABA requires iron, steel, manufactured products, and construction materials used in infrastructure projects funded by federal financial assistance to be produced in the United States. Comply with 2 C.F.R § 184.

BABA permits FHWA participation in the Contract only if domestic steel and iron will be used on the Project. To be considered domestic, all steel and iron used, and all products manufactured from steel and iron must be produced in the United States and all manufacturing processes, including application of a coating, for these materials must occur in the United States. Coating includes all processes that protect or enhance the value of the material to which the coating is applied. This requirement does not preclude a minimal use of foreign steel and iron materials, provided the cost of such materials does not exceed 0.1% of the total contract amount under the Contract or \$2,500.00 whichever is greater.

BABA permits FHWA participation in the Contract only if all “construction materials” as defined in the Act are made in the United States. The Buy America preference applies to the following construction materials



SPECIAL NOTE – BUY AMERICA REQUIREMENTS AND BUILD  
AMERICA, BUY AMERICA (BABA) ACT

10/26/2023

incorporated into infrastructure projects: non-ferrous metals; plastic and polymer-based products (including polyvinylchloride, composite building materials, and polymers used in fiber optic cables); glass (including optic glass); Fiber optic cable; optical fiber; lumber; engineered wood; and drywall. Contractor will be required to use construction materials produced in the United States on this Project. The Contractor shall submit a certification stating that all construction materials are certified to be BABA compliant.

Finally, BABA permits the continuation of FHWA's current general applicability waivers for manufactured products, raw materials, and ferryboat parts, but these waivers are subject to reevaluation, specifically the general applicability waiver for manufactured products.

The Contractor has completed and submitted, or shall complete and submit, to the Cabinet a Buy America/Build America, Buy America Certificate prior to the Cabinet issuing the notice to proceed, in the format below. After submittal, the Contractor is bound by its original certification.

A false certification is a criminal act in violation of 18 U.S.C. § 1001. The Contractor has the burden of proof to establish that it is in compliance.

At the Contractor's request, the Cabinet may, but is not obligated to, seek a waiver of Buy America requirements if grounds for the waiver exist under 23 C.F.R. § 635.410(c) or will comply with the applicable Buy America requirements if a waiver of those requirements is not available or not pursued by the Cabinet.

Please refer to the Federal Highway Administration's Buy America webpage for more information.

[Buy America - Construction Program Guide - Contract Administration - Construction - Federal Highway Administration \(dot.gov\)](#)

October 26, 2023 Letting

**BUY AMERICA / BUILD AMERICA, BUY AMERICA (ACT) MATERIALS CERTIFICATE OF COMPLIANCE**

The Contractor hereby certifies that it will comply with all relevant provisions of the Build America, Buy America Act, contained within the Infrastructure Investment and Jobs Act, Pub. L. NO. 117-58, §§ 70901-52, the requirements of 23 U.S.C. § 313, 23 C.F.R. § 635.410 and 2 C.F.R § 184.

Date Submitted:\_\_\_\_\_

Contractor:\_\_\_\_\_

Signature:\_\_\_\_\_

Printed Name:\_\_\_\_\_

Title:\_\_\_\_\_

**NOTE: THIS CERTIFICATION IS IN ADDITION TO ANY AND ALL REQUIREMENTS OUTLINED IN THE CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND/OR SPECIAL NOTES CONTAINED IN THE PROJECT PROPOSAL.**

## **SPECIAL NOTE FOR RECIPROCAL PREFERENCE**

### **RECIPROCAL PREFERENCE TO BE GIVEN BY PUBLIC AGENCIES TO RESIDENT BIDDERS**

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the electronic bidding software. Submittal of the Affidavit should be done along the bid in Bid Express.

April 30, 2018

### **SURFACING AREAS**

The Department estimates the mainline surfacing width to be varied 24 to 82 feet.

The Department estimates the total mainline area to be surfaced to be 15,339 square yards.

The Department estimates the shoulder width to be N/A foot on each side.

The Department estimates the total shoulder area to be surfaced to be N/A square yards.

### **INCIDENTAL SURFACING**

The Department has included in the quantities of asphalt mixtures established in the proposal estimated quantities required for resurfacing or surfacing mailbox turnouts, farm field entrances, residential and commercial entrances, curve widening, ramp gores and tapers, and road and street approaches, as applicable. Pave these areas to the limits as shown on Standard Drawing RPM-110-06 or as directed by the Engineer. In the event signal detectors are present in the intersecting streets or roads, pave the crossroads to the right of way limit or back of the signal detector, whichever is the farthest back of the mainline. Surface or resurface these areas as directed by the Engineer. The Department will not measure placing and compacting for separate payment but shall be incidental to the Contract unit price for the asphalt mixtures.

### **JPC RIDE QUALITY**

The Department will require the pavement to be straightedged per Standard Specification 501.

## SPECIAL NOTE FOR JPC INTERSECTION PAVEMENT

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### I. DESCRIPTION

Except as specified herein, construct Jointed Plain Concrete (JPC) intersection pavement in accordance with the Department's Standard and Supplemental Specifications, Special Provisions and Special Notes, and Standard and Sepia Drawings, current editions, and as directed by the Engineer. Section references are to the Standard Specifications. Furnish all materials, equipment, labor, and incidentals for:

- (1) Removing asphalt and/or concrete pavement and replacing with JPC Pavement; (2) Maintaining and controlling traffic; and (3) All other work specified as part of this contract.

### II. MATERIALS

The Department will sample and test all materials according to the Department's sampling Manual. Make the materials available for sampling a sufficient time in advance of their use, to allow for the necessary time for testing, unless otherwise specified in these notes.

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Dense Graded Aggregate.** Do not furnish Crushed Stone Base in lieu of DGA.

**C. Jointed Plain Cement Concrete Pavement.** Use JPC Pavement 9 IN. At Contractor's request and at no additional cost to the Department, the Engineer may approve other high early strength rapid setting concrete. The Department will allow either central mixing or truck mixing.

**D. Joint Sealant.** Use hot poured elastic, no alternates.

**E. Traffic Signal Loops.** See Special Notes for Radar Presence Detectors.

### III. CONSTRUCTION METHODS

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Site Preparation.** Be responsible for all site preparation, including but not limited to, incidental excavation and backfilling; removal of all obstructions or any other items; disposal of materials; sweeping and removal of debris; shoulder preparation and restoration; temporary and permanent erosion and pollution control; final dressing, clean

up, and seeding; and all incidentals. Perform all Site Preparation only as approved or directed by the Engineer.

**C. Pavement Removal.** Consider pavement removal locations and dimensions shown on the drawings to be approximate only; The Engineer will determine actual locations at the time of construction and may add to the listed repairs if deemed necessary. **The contractor must give the Engineer at least a 7-day notice as to when removal will begin so that the intersection limits and slab removal locations can be marked.** Prior to removal, saw-cut existing asphalt and/or concrete pavement at locations directed by the Engineer to provide a neat edge where new concrete will adjoin existing pavement. Remove pavement for full depth repairs by a saw cut and lift method without disturbing the underlying DGA base or damaging the adjacent pavement remaining in place. Do not "pre-saw" in advance until ready for slab removal within more than 24 hours of removal. (The Engineer will not allow the slab to be sawed and then to remain under traffic for more than 24 hours after sawing.) Liquidated damages will apply 24 hours after sawing. Pre-saw only in locations directed by the Engineer. Do not hammer or break pavement by other means to facilitate removal. Do not oversaw into existing JPC Pavement not intended to be removed. The original nominal depth of the mainline JPC pavement is 9 inch as noted on the typical sections. However, the finished grade will be transitioned to match the adjacent pavement to remain in place. It is intended to not disturb the underlying soil; however, a quantity of DGA, Crushed Aggregate #2, and Geotextile Fabric Class 2, is included for undercutting very poor, soft, wet soils - to be used sparingly and only as directed by the Engineer. Undercutting will not be measured as a bid item and will be considered incidental to the items of work listed above.

**D. Concrete Pavement Replacement.** Prior to pavement removal and placing JPC Pavement, obtain the Engineer's approval of proposed method of construction for ensuring and establishing a smooth profile. Place the replacement JPC in a continuous operation in accordance with the Traffic Control Plan Phasing and as directed by the Engineer. Construct the replacement JPC Pavement with a minimum depth of 9 inches; however, transition the finished grade to match adjacent pavement that is to remain in place; therefore, the actual thickness of the pavement may be greater than 9 inches in some areas. Consolidate the concrete, strike off, machine finish with a vibrating or roller screed, and straightedge the plastic concrete with a straightedge conforming to Section 501.02.18. Test the profile of the finished pavement with a 12-foot straight edge according to Section 501.03.19. Provide positive drainage upon completion of construction.

**E. Joint Sealing.** Saw, clean, and seal transverse and longitudinal joints as shown on the standard drawings and as directed the Engineer.

**F. Traffic Signal Loops.** See Special Notes for Radar Presence Detectors.

**G. Disposal of Waste.** Dispose of all cuttings, debris, and other waste off the right-of-way at sites obtained by the Contractor at no additional cost to the Department. See Special Note for Waste and Borrow.

**H. Pavement Markings.** See traffic Control Plan.

**I. On-Site Inspection.** Prior to submitting a bid, make a thorough inspection of the site and become thoroughly familiar with the existing conditions so that the work can be expeditiously performed after a contract is awarded. The Department will consider submission of a bid as evidence of this inspection having been made. The Department will not honor any claims resulting from site conditions.

**J. Property Damage and Restoration.** Be responsible for all damage to public and/or private property resulting from the work. Repair or replace all damaged roadway features in like kind materials and design at no additional cost to the Department. Repair or replace damaged private property in like kind materials and design to the satisfaction of the owner.

**K. Caution.** Consider information shown on the drawings and in this proposal and the types and quantities of work listed are approximate only, and not as an accurate or complete evaluation of the material and conditions to be encountered during construction. The bidder must draw his own conclusion as to the conditions encountered. The Department does not give any guarantee as to the accuracy of the data and no claim will be considered for additional compensation if the conditions encountered are not in accordance with the information shown.

**L. Utility Clearance.** Determine the location of all underground and overhead utilities prior to construction. It is not anticipated that utility facilities will need to be relocated and/or adjusted; however, in the event that work does require relocation and/or adjustment, the utility companies will work concurrently with the Contractor while relocating their facilities.

**M. Final Dressing, Clean Up, and Seeding and Protection.** After all work is completed, remove all waste and debris from the construction sites. Remove all temporary shoulder widening and restore disturbed shoulders. Perform Class A final dressing on all disturbed areas. Sow disturbed earthen areas with Seed Mixture No. 1.

**N. Coordination of Work.** Be advised that other projects may be in progress within or in the near vicinity of this project. Take into consideration that the traffic control of those projects may affect this project and the traffic control of this project may affect those projects. Coordinate the work on this project with the work of the other contractors. In case of a conflict, the Engineer will determine the relative priority to give to work phasing on the various projects.

#### IV. METHOD OF MEASUREMENT

The Department will measure only the bid items listed. All other items required to complete the construction shall be incidental to the listed bid items.

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Site Preparation.** Other than the bid items listed, site preparation will not be measured for payment, but will be incidental to the other items of the work.

**C. Remove Pavement.** The Department will measure removed asphalt/concrete pavement in square yards.

**D. JPC Pavement-9 IN.**

**E. Joint Sealing.** The Department will not measure Joint Sealing for payment, but shall be incidental to the bid item JPC Pavement-9 IN.

**F. Install Radar Presence Detector Type A.** See Special Notes for Radar Presence Detectors.

**G. Smooth Dowels, Deformed Tie Bars, and Hook Bolts.** The Department will not measure smooth dowels, deformed tie bars and hook bolts, but will be incidental to JPC Pavement-9 IN.

**H. DGA Base.** The department will measure DGA base by the ton. Included for undercutting very poor, soft, wet soils - to be used sparingly and only as directed by the Engineer.

**I. Fabric-Geotextile Class 2.** The department will measure Fabric-Geotextile Class 2 by the SQYD. Included for undercutting very poor, soft, wet soils - to be used sparingly and only as directed by the Engineer.

**J. Crushed Aggregate #2.** The Department will measure Crushed Aggregate #2 by the ton. Included for undercutting very poor, soft, wet soils - to be used sparingly and only as directed by the Engineer.

**K. Staking.** The department will measure staking as one lump sum for the entire project.



#### IV. BASIS OF PAYMENT

The Department will make payment only for the bid items listed. All other items required to complete the construction shall be incidental to the bid items listed.

**A. Maintain and Control Traffic.** See Traffic Control Plan.

**B. Remove Pavement.** Payment at the contract unit price per square yard shall be full compensation for saw cutting, milling and texturing, and removing existing pavement (asphalt and/or concrete); disposing of waste and debris.

**C. JPC Pavement-9 IN.**

**D. Install Radar Presence Detector Type A.** Payment at the contract unit price per each shall be full compensation for the installation of radar presence detectors.

**E. DGA Base.** Payment at the contract unit price per square yard shall be full compensation for the DGA Base required when undercutting poor, soft, wet soils - to be used sparingly and only as directed by the Engineer.

**F. Fabric-Geotextile Class 2.** Payment at the contract unit price per square yard shall be full compensation for the fabric required when undercutting poor, soft, wet soils - to be used sparingly and only as directed by the Engineer.

**G. Crushed Aggregate #2.** Payment at the contract unit price per ton shall be full compensation for Crushed Aggregate #2's required when undercutting poor, soft, wet soils - to be used sparingly and only as directed by the Engineer.

**H. Staking.** Payment at the contract unit price per one lump sum shall be full compensation for all staking required for construction.

## **SPECIAL NOTE FOR PAVEMENT MARKING MODIFICATIONS**

Per Section 713.03.01 of the Standard Specifications, the Contractor shall still be required to submit a record of existing pavement markings prior to beginning resurfacing activities. The Department requests these records be submitted at least two weeks prior to milling or paving in order to coordinate any desired changes between the District Traffic Engineer and the Contractor. All changes will be returned to the Contractor to ensure the desired modifications can be performed during final surfacing. As the Contractor is responsible for implementing any pavement marking changes, it is highly recommended any questions are addressed to the Project Engineer prior to striping. Any incorrect markings will be removed and replaced with the proper markings at the Contractor's expense and in a manner approved by the Engineer.

### **Special Note for Utility Duct Bank at KY 61/KY 1065 Intersection**

Please be advised that there is a Utility Duct Bank located underneath of the WB approach of KY 1065 right as you get to the KY 61/KY 1065 intersection. Do not disturb this structure during any phase of construction.

## **SPECIAL NOTE FOR STAKING**

It is intended to replace the existing concrete surface at the same line and grade with new JPC Pavement. Therefore, a field survey of the existing pavement is required in order to establish the existing cross slopes, transitions and profile. Irregularities in the existing pavement are to be eliminated with the construction of a smooth line and grade of the new JPC pavement to ensure the best rideability possible.

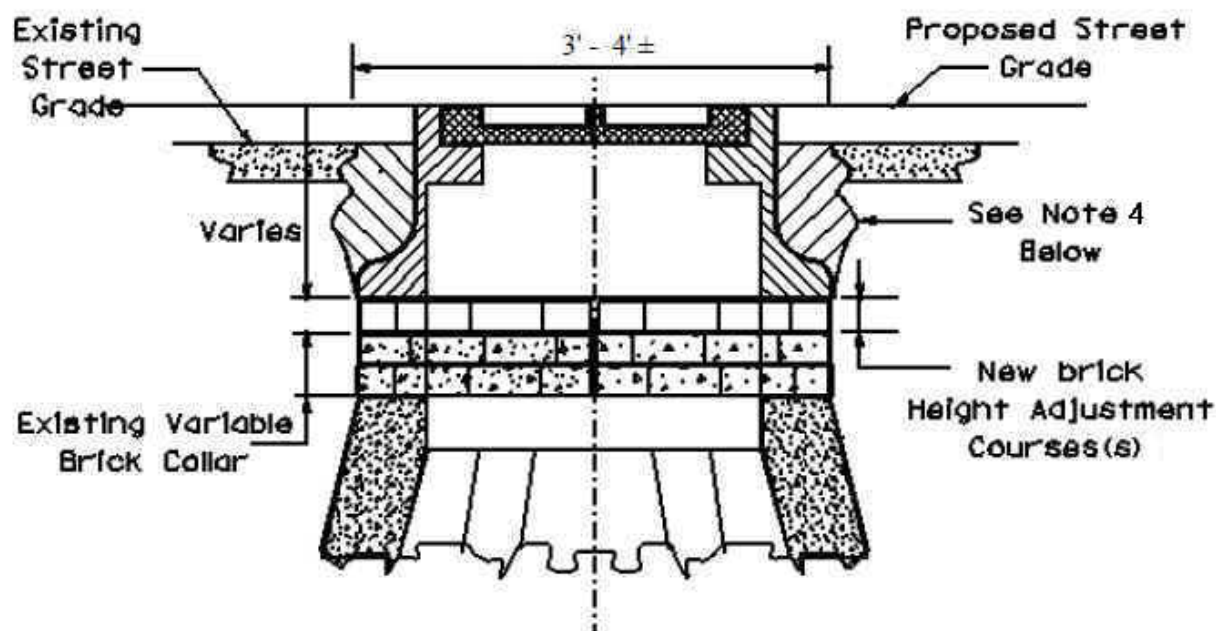
The Department will measure "Staking" as a Lump Sum item. Payment at the contract unit price shall be full compensation for all labor, materials, equipment and incidentals necessary to complete the survey and establish grade during construction of both intersections.

## **SPECIAL PROVISION FOR WASTE AND BORROW SITES**

Obtain U.S. Army Corps of Engineer's approval before utilizing a waste or borrow site that involves "Waters of the United States". The Corps of Engineers defines "Waters of the United States" as perennial or intermittent streams, ponds or wetlands. The Corps of Engineers also considers ephemeral streams, typically dry except during rainfall but having a defined drainage channel, to be jurisdictional waters. Direct questions concerning any potential impacts to "Waters of the United States" to the attention of the appropriate District Office for the Corps of Engineers for a determination prior to disturbance. Be responsible for any fees associated with obtaining approval for waste and borrow sites from the U.S. Army Corps of Engineer or other appropriate regulatory agencies.

1-296 Waste & Borrow Sites  
01/02/2012

**SPECIAL NOTE FOR MANHOLE ADJUSTMENT  
LOUISVILLE METROPOLITAN SEWER DISTRICT (MSD)**



NOT TO SCALE

**CONSTRUCTION NOTES:**

1. The Engineer will determine whether manhole adjustments are to be made prior to or after JPC pavement removal.
2. Reuse existing frames, grates, and covers. Use other materials conforming to MSD's Technical Specifications. Obtain these specifications from MSD's Engineering Division Office, 400 South Sixth Street, Louisville, KY 40202.
3. Report any missing or damaged frame, grate, or cover discovered by the Contractor to MSD's inspector. Obtain replacement hardware items from MSD's Storage Yard, 151 Cabel Street, Louisville, KY, upon presentation of an MSD inspector's validated Stores Requisition and exchange for the damaged hardware items.
4. Adjust manhole frame and grates with brick and mortar as shown on the drawing or as directed by the Engineer. Do not use wood shims or blocks to adjust or reset the frame height. Fill cross-hatched area with concrete having a minimum 28-day compressive strength of 2000 psi. Use first class workmanship in conformity with MSD's Technical Specifications.
5. Adjust catch basin frames and grates in similar manner as manhole adjustments as approved by the Engineer.
6. Louisville MSD and/or the Engineer may inspect manhole and/or catch basin frame height adjustments. Make corrections as directed by the Engineer at no additional cost to the Department.
7. The Department will measure and pay Adjust Manhole Frame to Grade according to Sections 403.04.02 and 403.05.01 or Sections 408.04.04 and 408.05.02 as applicable.

### **SPECIAL NOTE FOR TYPICAL SECTION DIMENSIONS**

Consider the dimensions shown on the typical sections for pavement and shoulder widths and thickness' to be nominal or typical dimensions. The Engineer may direct or approve varying the actual dimensions to be constructed to fit existing conditions. Do not widen existing pavement or shoulders unless specified elsewhere in this proposal or directed by the engineer.

1-3725 Typical Section Dimensions  
01/02/2012

## SPECIAL NOTE FOR SIDEWALK RAMPS & DETECTABLE WARNINGS

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### GENERAL

Unless otherwise stated in the contract, or as directed by or with prior approval from the Engineer, construct Sidewalk Ramps and Detectable Warnings in accordance with Sections 505 and 720; Supplemental Specifications; Standard Drawings RGX-040-03, RPM-150-08, RPM-152-08, RPM-170-09, and RPM-172-07; current editions, as applicable. In lieu of the Detectable Warnings shown on Standard Drawing RGX-040-03, the Department will also allow the use of any Detectable Warnings listed as Phase XI on the [Kentucky Product Evaluation List](http://www.ktc.uky.edu/kytc/kypel/allevvaluations.php) (<http://www.ktc.uky.edu/kytc/kypel/allevvaluations.php>). For Detectable Warnings as shown on Standard Drawing RGX-040-03, saw cut existing sidewalks, curb and gutter, and pavement, if present, as shown on the detail and reconstruct sidewalk ramps with detectable warnings as directed or approved by the Engineer. For Detectable Warnings from the Kentucky Product Evaluation List, install according to the manufacturer's recommendations. Unless specified otherwise in the Contract, construct sidewalk with 4" nominal minimum required thickness; however, if the existing sidewalk thickness is found to be greater or less than the thickness specified, transition the thickness as directed by the Engineer.

Except as required by the work, do not disturb drainage pipe, catch basins, and other roadway features, appurtenances and installations. Restore any roadway features, appurtenances, and installations damaged by the work in like kind materials and design at no additional cost to the Department. Dispose of all waste off the right of way at sites obtained by the Contractor at no additional cost to the Department (see Special Note for Waste and Borrow).

### MEASUREMENT & PAYMENT

**SIDEWALK RAMPS** – The Department will measure Sidewalk Ramps in accordance with Section 505.04.01 and Standard Drawing RPM-170-09, current editions; however, contrary to Sections 505.04.05 and 505.04.06, the Department will not measure Roadway Excavation or Embankment in Place, but shall be incidental to the Sidewalk. Accept payment at the Contract unit price per square yard as full compensation for all labor, materials, equipment, and incidentals required for removal and disposal of existing sidewalk and curb and gutter, excavation and embankment, construction of the sidewalk ramps, reconstruction of the adjacent curb and/or sidewalk as necessary to install the sidewalk ramps, and restoration of disturbed features in accordance with these notes or as directed by the Engineer.

**DETECTABLE WARNINGS** – The Department will measure Detectable Warnings in accordance with Section 505.04.04 and Standard Drawings RGX-040-03 and RPM-170-09, current editions. The Department will make payment according to Section 505.05.

**HANDRAIL** – The Department will measure and make payment for Handrail in accordance with Section 720.05 and Standard Drawing RPM-172-07, current editions.

## **TRAFFIC CONTROL PLAN FD04 056 1065 004-008**

### **TRAFFIC CONTROL GENERAL**

Except as provided herein, traffic shall be maintained in accordance with the current Standard Specifications and the Standard Drawings, current editions. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic". All lane closures used on the project will be in compliance with the appropriate Standard Drawings. (NOTE: Any lane closures used on the project shall be done utilizing traffic drums with a maximum spacing of 25 ft.) Do NOT use cones for lane closures or shoulder closures.

Contrary to Section 106.01, traffic control devices used on this project may be new, or used in like new condition, at the beginning of the work and maintained in like new condition until completion of the work. Traffic control devices used on this project and the Temporary Traffic Control Plan shall conform to the current edition of the *Manual on Uniform Traffic Control Devices*.

District Traffic Personnel will be responsible for the movement of traffic signals to accommodate the changing traffic control scheme for the duration of the project. Contractor shall notify the Project Engineer and District Traffic Engineer of plans to switch the traffic control phasing 48 hours prior to the operation.

### **PROJECT PHASING & CONSTRUCTION PROCEDURES**

Maintain a minimum of one traffic lane (mainline) in each direction at all times during construction. The clear lane width shall be 12 Feet. Maintain one lane of traffic during construction in accordance with Standard Drawing No. TTC-115-02, and the attached detail drawing.

### **CONSTRUCTION PHASING**

**The contractor must develop their own plan for construction phasing and submit it to the Engineer for approval prior to the beginning of work. No work is to be performed until the Engineer has approved the contractor's submittal for construction phasing.**

**Night work will be allowed on this project. The method of lighting for night work will require written approval from the Engineer prior to its use.**

**Locations listed in the proposal or shown on sketch map are approximate only; the Engineer will determine exact locations at time of construction.**

## **LANE CLOSURES**

Limit the lengths of lane closures to only that needed for actual operations in accordance with the phasing specified herein, or as directed by the Engineer. Lane closures must be monitored 24 hours a day by the contractor in order to provide safe travel for the general public and to ensure protection of the work zone.

## **SIGNS**

Sign posts and splices shall be compliant with NCHRP 350 or MASH. Manufacturer's documentation validating this compliance shall be provided to the Engineer prior to installation. Signs, including any splices, shall be installed according to manufacturer's specifications and installation recommendations. Contrary to section 112.04.02, only long-term signs (signs intended to be continuously in place for more than 3 days) will be measured for payment. Short-term signs (signs intended to be left in place for 3 days or less) will not be measured for payment but will be incidental to Maintain and Control Traffic.

## **BARRICADES**

Barricades used in lieu of barrels and cones for channelization or delineation will be incidental to Maintain and Control Traffic according to Section 112.04.01. Barricades used to protect pavement removal areas will be bid as each according to Section 112.04.04.

Individual barricades will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged barricades or barricades directed to be replaced by the Engineer due to poor legibility or reflectivity will not be measured for payment.

## **CHANGEABLE MESSAGE SIGNS**

Provide changeable message signs in advance of and within the project at locations to be determined by the Engineer. If work is in progress concurrently in both directions or if more than one lane closure is in place in the same direction of travel, provide additional changeable message signs as directed by the Engineer. Place changeable message signs one mile in advance of the anticipated queue at each lane closure. As the actual queue lengthens and/or shortens relocate or provide additional changeable message signs so that traffic has warning of slowed or stopped traffic at least one mile but not more than two miles before reaching the end of the actual queue. The locations designated may vary as the work progresses. The messages required to be provided shall be designated by the Engineer. In the event of damage or mechanical/electrical failure, the Contractor shall repair or replace the Changeable Message Sign within 24 hours. The Department will measure for payment the maximum number of changeable message signs in concurrent use at the same time on a single day on all sections of the contract. Individual changeable message signs will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged changeable message signs directed by the Engineer to be replaced due to poor condition or readability will not be measured for payment.



## **ARROW PANEL**

Use arrow panels as shown on the Standard Drawings or as directed by the Engineer. The Department will measure for payment the maximum number of arrow panels in concurrent use at the same time on a single day on all sections of the contract. Individual arrow panels will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged arrow panels directed by the Engineer to be replaced due to poor condition or readability will not be measured for payment. Arrow panels will remain the property of the Contractor after construction is complete.

## **TRAFFIC COORDINATOR**

Designate an employee to be traffic coordinator conforming to the requirements of Section 112.03.12. Designate an employee to serve as Traffic Coordinator. The Traffic Coordinator will inspect the project maintenance of traffic once every two hours during the Contractor's operations and at any time a lane closure is in place. The Traffic Coordinator will report all incidents throughout the work zone to the Engineer on the project. The Contractor will furnish the name and telephone number where the Traffic Coordinator can be contacted at all times. During any period when a lane closure is in place, the Traffic Coordinator will arrange for personnel to be present on the project at all times to inspect the traffic control, maintain the signing and devices, and relocate variable message boards as queue lengths change. The personnel will have access on the project to a radio or telephone to be used in case of emergencies or accidents.

## **TEMPORARY ENTRANCES**

The Contractor will not be required to provide continuous access to farms, single family, duplex, or triplex residential properties during working hours; however, provide reasonable egress and ingress to each such property when actual operations are not in progress at that location. The time during which a farm or residential entrance is blocked shall be the minimum length of time required for actual operations, shall not be extended for the Contractor's convenience, and in no case shall exceed six (6) hours. Notify all residents twenty-four hours in advance of any driveway or entrance closings and make any accommodations necessary to meet the access needs of disabled residents.

Except as allowed by the Phasing as specified above, maintain direct access to all side streets and roads, schools, churches, commercial properties and apartments or apartment complexes of four or more units at all times.

Payment will be allowed at the unit price bid for all asphalt materials required to construct and maintain any temporary entrances which may be necessary to provide temporary access; however, no direct payment will be allowed for aggregates, excavation and/or embankment needed. The Engineer will determine the type of surfacing material, asphalt or aggregate, to be used at each entrance.

## PAVEMENT MARKINGS

Contrary to Section 112.03.10, remove or cover the lenses of raised pavement markers that do not conform to the traffic control scheme in use during night time hours, or as directed by the Engineer. Replace or uncover lenses before a closed lane is reopened to traffic. No direct payment will be made for removing and replacing or covering and uncovering the lenses, but shall be incidental to “Maintain and Control Traffic”.

Place temporary and permanent striping according to Sections 112 and 748, except that:

Temporary or Permanent striping or tape shall be in place before a lane is opened to traffic.

Any pavement striping that conflicts with the traffic control phasing must be removed and is incidental to Maintain and Control Traffic.

If the Contractor’s operations or phasing requires temporary markings that must be subsequently removed from the final surface course, Temporary Removable Tape shall be used. This removable tape will be measured separately.

## PAVEMENT EDGE DROP-OFFS

A pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation shall not have an elevation difference greater than 1½”. Warning signs (MUTCD W8-9 or W8-9A, or W8-11) shall be placed in advance of and at 1500’ intervals throughout the drop-off area. Dual posting on both sides of the traveled way shall be required. All transverse transitions between newly surfaced pavement and the existing pavement areas that traffic may cross shall be wedged with asphalt mixture for leveling and wedging. Remove wedges prior to placement of the final surface course.

Pavement edges that traffic is not expected to cross, except accidentally, shall be treated as follows:

Less than 2” – No protection required.

2” to 4” – Place plastic drums, vertical panels, or barricades every 50 feet. Cones may be used in place of plastic drums, panels, and barricades during daylight working hours. Wedge with DGA or asphalt mixtures with a 1:1 or flatter slope in daylight hours, or 3:1 or flatter slope during nighttime hours, when work is not active in the drop-off area.

4 inches and greater –Protect with a lane or shoulder closure using drums or barricades; cones will not be allowed for lane or shoulder closures for drop-offs 4 inches or greater. Place drums or barricades with spacing **not to exceed 25 feet**.

Place Type III Barricades facing oncoming traffic at each drop off. If for any reason traffic must be maintained less than 5 feet from the drop off, wedge with crushed stone with 3:1 or flatter slope when work is not actively in progress in the drop-off area. Once excavation begins, work continuously to construct crushed stone backfill and DGA base to eliminate the drop-off. Drop-offs greater than 4 inches within 5 feet of traffic will not be allowed during non-working hours.

## USE AND PLACEMENT OF CHANGEABLE MESSAGE SIGNS

The following policy is based upon current Changeable Message Signs (CMS) standards and practice from many sources, including the Federal Highway Administration (FHWA), other State Departments of Transportation, and Traffic Safety Associations. It is understood that each CMS installation or use requires individual consideration due to the specific location or purpose. However, there will be elements that are constant in nearly all applications. Accordingly these recommended guidelines bring a level of uniformity, while still being open to regional experience and engineering judgment.

### **Application**

The primary purpose of CMS is to advise the driver of unexpected traffic and routing situations. Examples of applications where CMS can be effective include:

- Closures (road, lane, bridge, ramp, shoulder, interstate)
- Changes in alignment or surface conditions
- Significant delays, congestion
- Construction/maintenance activities (delays, future activities)
- Detours/alternative routes
- Special events with traffic and safety implications
- Crash/incidents
- Vehicle restrictions (width, height, weight, flammable)
- Advance notice of new traffic control devices
- Real-time traffic conditions (must be kept up to date)
- Weather /driving conditions, environmental conditions, Roadway Weather Information Systems
- Emergency Situations
- Referral to Highway Advisory Radio (if available)
- Messages as approved by the County Engineer's Office

### **CMS should not be used for:**

- Replacement of static signs (e.g. road work ahead), regulatory signage (e.g. speed limits), pavement markings, standard traffic control devices, conventional warning or guide signs.
- Replacement of lighted arrow board
- Advertising (Don't advertise the event unless clarifying "action" to be taken by driver – e.g. Speedway traffic next exit)
- Generic messages
- Test messages (portable signs only)
- Describe recurrent congestion (e.g. rush hour)
- Public service announcements (not traffic related)

## **Messages**

Basic principles that are important to providing proper messages and insuring the proper operation of a CMS are:

- Visible for at least ½ mile under ideal daytime and nighttime conditions
- Legible from all lanes a minimum of 650 feet
- Entire message readable twice while traveling at the posted speed
- Nor more than two message panels should be used (three panels may be used on roadways where vehicles are traveling less than 45 mph). A panel is the message that fits on the face of the sign without flipping or scrolling.
- Each panel should convey a single thought; short and concise
- Do not use two unrelated panels on a sign
- Do not use the sign for two unrelated messages
- Should not scroll text horizontally or vertically
- Should not contain both the words left and right
- Use standardized abbreviations and messages
- Should be accurate and timely
- Avoid filler/unnecessary words and periods (hazardous, a, an, the)
- Avoid use of speed limits
- Use words (not numbers) for dates

## **Placement**

Placement of the CMS is important to insure that the signs is visible to the driver and provides ample time to take any necessary action. Some of the following principles may only be applicable to controlled access roadways. The basic principles of placement for a CMS are:

- When 2 signs are needed, place on same side of roadway and at least 1,000 feet apart
- Place behind semi-rigid/rigid protection (guardrail, barrier) or outside of the clear zone
- Place 1,000 feet in advance of work zone; at least one mile ahead of decision point
- Normally place on right side of roadway; but should be placed closest to the affected lane so that either side is acceptable
- Signs should not be dual mounted (one on each side of roadway facing same direction)
- Point trailer hitch downstream
- Secure to immovable object to prevent thief (if necessary)
- Do not place in sags or just beyond crest
- Check for reflection of sun to prevent the blinding of motorist
- Should be turned ~3 degrees outward from perpendicular to the edge of pavement
- Bottom of sign should be 7 feet above the elevation of edge of roadway
- Should be removed when not in use
-

Standard Abbreviations

The following is a list of standard abbreviations to be used on CMS.

<u>Word</u>	<u>Abbrev.</u>	<u>Example</u>
Access	ACCS	ACCIDENT AHEAD/USE ACCS RD NEXT RIGHT
Alternate	ALT	ACCIDENT AHEAD/USE ALT RTE NEXT RIGHT
Avenue	AVE	FIFTH AVE CLOSED/DETOUR NEXT LEFT
Blocked	BLKD	FIFTH AVE BLKD/MERGE LEFT
Boulevard	BLVD	MAIN BLVD CLOSED/USE ALT RTE
Bridge	BRDG	SMITH BRDG CLOSED/USE ALT RTE
Cardinal Directions	N, S, E, W	N I75 CLOSED/ DETOUR EXIT 30
Center	CNTR	CNTR LANE CLOSED/MERGE LEFT
Commercial	COMM	OVRSZ COMM VEH/USE I275
Condition	COND	ICY COND POSSIBLE
Congested	CONG	HVY CONG NEXT 3 MI
Construction	CONST	CONST WORK AHEAD/EXPECT DELAYS
Downtown	DWNTN	DWNTN TRAF USE EX 40
Eastbound	E-BND	E-BND I64 CLOSED/DETOUR EXIT 20
Emergency	EMER	EMER VEH AHEAD/PREPARE TO STOP
Entrance, Enter	EX, EXT	DWNTN TRAF USE EX 40
Expressway	EXPWY	WTRSN EXPWY CLOSED/DETOUR EXIT 10
Freeway	FRWY, FWY	GN SYNDR FWY CLOSED/DETOUR EXIT 15
Hazardous Materials	HAZMAT	HAZMAT IN ROADWAY/ALL TRAF EXIT 25
Highway	HWY	ACCIDENT ON AA HWY/EXPECT DELAYS
Hour	HR	ACCIDENT ON AA HWY/2 HR DELAY
Information	INFO	TRAF INFO TUNE TO 1240 AM
Interstate	I	E-BND I64 CLOSED/DETOUR EXIT 20
Lane	LN	LN CLOSED/MERGE LEFT
Left	LFT	LANE CLOSED/MERGE LFT
Local	LOC	LOC TRAF USE ALT RTE
Maintenance	MAINT	MAINT WRK ON BRDG/SLOW
Major	MAJ	MAJ DELWAYS I75/USE ALT RTE
Mile	MI	ACCIDENT 3 MI AHEAD/ USE ALT RTE
Minor	MNR	ACCIDENT 3 MI MNR DELAY

Minutes	MIN	ACCIDENT 3 MI/30 MIN DELAY
Northbound	N-BND	N-BND I75 CLOSED/ DETOUR EXIT 50
Oversized	OVRSZ	OVRSZ COMM VEH/USE I275 NEXT RIGHT
Parking	PKING	EVENT PKING NEXT RGT
Parkway	PKWY	CUM PKWAY TRAF/DETOUR EXIT 60
Prepare	PREP	ACCIDENT 3 MIL/PREP TO STOP
Right	RGT	EVENT PKING NEXT RGT
Road	RD	HAZMAT IN RD/ALL TRAF EXIT 25
Roadwork	RDWK	RDWK NEXT 4 MI/POSSIBLE DELAYS
Route	RTE	MAJ DELAYS I75/USE ALT RTE
Shoulder	SHLDR	SHLDR CLOSED NEXT 5 MI
Slippery	SLIP	SLIP COND POSSIBLE/ SLOW SPD
Southbound	S-BND	S-BND I75 CLOSED/DETOUR EXIT 50
Speed	SPD	SLIP COND POSSIBLE/ SLOW SPD
Street	ST	MAIN ST CLOSED/USE ALT RTE
Traffic	TRAF	CUM PKWAY TRAF/DETOUR EXIT 60
Vehicle	VEH	OVRSZ COMM VEH/USE I275 NEXT RIGHT
Westbound	W-BND	W-BND I64 CLOSED/DETOUR EXIT 50
Work	WRK	CONST WRK 2MI/POSSIBLE DELAYS

Certain abbreviations are prone to inviting confusion because another word is abbreviated or could be abbreviated in the same way. DO NO USE THESE ABBREVIATIONS.

<u>Abbrev.</u>	<u>Intended Word</u>	<u>Word Erroneously Given</u>
ACC	Accident	Access (Road)
CLRS	Clears	Colors
DLY	Delay	Daily
FDR	Feeder	Federal
L	Left	Lane (merge)
LOC	Local	Location
LT	Light (traffic)	Left
PARK	Parking	Park
POLL	Pollution (index)	Poll
RED	Reduce	Red
STAD	Stadium	Standard
TEMP	Temporary	Temperature
WRNG	Warning	Wrong

TYPICAL MESSAGES

The following is a list of typical messages used on CMS. The list consists of the reason or problem that you want the driver to be aware of and the action that you want the driver to take.

<u>Reason/Problem</u>	Action
ACCIDENT	ALL TRAFFIC EXIT RT
ACCIDENT/XX MILES	AVOID DELAY USE XX
XX ROAD CLOSED	CONSIDER ALT ROUTE
XX EXIT CLOSED	DETOUR
BRIDGE CLOSED	DETOUR XX MILES
BRIDGE/(SLIPPERY, ICE, ETC.)	DO NOT PASS
CENTER/LANE/CLOSED	EXPECT DELAYS
DELAY(S), MAJOR/DELAYS	FOLLOW ALT ROUTE
DEBRIS AHEAD	KEEP LEFT
DENSE FOG	KEEP RIGHT
DISABLED/VEHICLE	MERGE XX MILES
EMER/VEHICLES/ONLY	MERGE LEFT
EVENT PARKING	MERGE RIGHT
EXIT XX CLOSED	ONE-WAY TRAFFIC
FLAGGER XX MILES	PASS TO LEFT
FOG XX MILES	PASS TO RIGHT
FREEWAY CLOSED	PREPARE TO STOP
FRESH OIL	REDUCE SPEED
HAZMAT SPILL	SLOW
ICE	SLOW DOWN
INCIDENT AHEAD	STAY IN LANE
LANES (NARROW, SHIFT, MERGE, ETC.)	STOP AHEAD
LEFT LANE CLOSED	STOP XX MILES
LEFT LANE NARROWS	TUNE RADIO 1610 AM
LEFT 2 LANES CLOSED	USE NN ROAD
LEFT SHOULDER CLOSED	USE CENTER LANE
LOOSE GRAVEL	USE DETOUR ROUTE
MEDIAN WORK XX MILES	USE LEFT TURN LANE
MOVING WORK ZONE, WORKERS IN ROADWAY	USE NEXT EXIT
NEXT EXIT CLOSED	USE RIGHT LANE
NO OVERSIZED LOADS	WATCH FOR FLAGGER
NO PASSING	
NO SHOULDER	
ONE LANE BRIDGE	
PEOPLE CROSSING	
RAMP CLOSED	
RAMP (SLIPPERY, ICE, ETC.)	
RIGHT LANE CLOSED	
RIGHT LANE NARROWS	
RIGHT SHOULDER CLOSED	
ROAD CLOSED	
ROAD CLOSED XX MILES	
ROAD (SLIPPERY, ICE, ETC.)	
ROAD WORK	
ROAD WORK (OR CONSTRUCTION) (TONIGHT, TODAY, TOMORROW, DATE)	



ROAD WORK XX MILES  
SHOULDER (SLIPPERY, ICE, SOFT, BLOCKED, ETC.)  
NEW SIGNAL XX MILES  
SLOW 1 (OR 2) - WAY TRAFFIC  
SOFT SHOULDER  
STALLED VEHICLES AHEAD  
TRAFFIC BACKUP  
TRAFFIC SLOWS  
TRUCK CROSSING  
TRUCKS ENTERING  
TOW TRUCK AHEAD  
UNEVEN LANES  
WATER ON ROAD  
WET PAINT  
WORK ZONE XX MILES  
WORKERS AHEAD

## **INSTALL RADAR PRESENCE DETECTOR TYPE A**

*Install Radar Presence Detector Type A* shall consist of installation of a pole mounted radar presence sensor, sensor mounting bracket, sensor cables, interface boxes, lead-in cable, connectors (furnished by contractor), and controller interface assembly. Radar Presence Detector Type A bid item shall include all labor required to provide a functional detection system. Radar Presence Detector Type A shall be installed and wired in accordance with the manufacturer's instructions. After the detector is installed and before the detector is powered on, the contractor shall coordinate with District Traffic Division's representatives to schedule a time to perform the detector setup. The contractor shall double check to verify that all wiring is correctly installed and connected before scheduling the setup work. Representatives from KYTC and/or the manufacturer or salesrepresentative will assist with setup and calibration. The contractor shall provide a bucket truck and operators at this time for final aiming of the sensors. The contractor shall provide individuals capable of operating the setup software and learning the setup process so that future installations may be completed without assistance from others.

October 12, 2023

JEFFERSON



KY 1065 Concrete Rehab Pavement Markings General Summary							
Item No:		Federal No:		State No:			
ITEM	DESCRIPTION	SUPPLEMENTAL DESCRIPTION	UNIT	Slab Replacements	KY 61 INT	KY 2052 INT	PROJECT TOTALS
6515	PAVE STRIPING-PERM PAINT-6 IN		LF	4600	3400	4900	12900
6517	PAVE STRIPING-PERM PAINT-12 IN		LF		240	100	340
6547	PAVE STRIPING-THERMO-12 IN Y	(ON CONCRETE)	LF		40		40
6565	PAVE MARKING-THERMO X-WALK-6 IN	(ON CONCRETE)	LF		540	550	1090
6565	PAVE MARKING-THERMO X-WALK-6 IN	(ON ASPHALT)	LF		380	190	570
6568	PAVE MARKING-THERMO STOP BAR-24IN	(ON ASPHALT)	LF		96	40	136
6568	PAVE MARKING-THERMO STOP BAR-24IN	(ON CONCRETE)	LF		112	144	256
6574	PAVE MARKING-THERMO CURV ARROW	(ON ASPHALT)	EACH			2	2
6610	INLAID PAVEMENT MARKER-MW		EACH		40	38	78
6611	INLAID PAVEMENT MARKER-MY		EACH		22		22
6612	INLAID PAVEMENT MARKER-BY		EACH			22	22
22520EN	PAVE MARKING-THERMO YIELD BAR-36 IN	(ON CONCRETE)	LF		72		72
23254E5717	PAVE MARK TY 1 TAPE DOTTED LANE EXT		LF		210	95	305
23261EC	PAVE MARK-THERMO-X-WALK-24 IN	(ON CONCRETE)	LF		80		80
23270E5717	PAVE MARK TY 1 TAPE-CURV ARROW		EACH		16	12	28
24683ED	PAVE MARKING-THERMO DOTTED LANE EXTEN	(ON ASPHALT)	LF		150		150
26146E5717	PAVE MARK TY 1 TAPE LANE REDUCTION ARROW		EACH			2	2
26166E5717	PAVE MARK TY 1 TAPE CHEVRON		SQFT			100	100

\*Ty 1 Tape bid items includ pavement preparation and removal of existing markings.

MANHOLE SUMMARY

Jefferson County	
FD04 056 1065 004-008	
Location	Manhole (EA)
KY 2052/KY 1065	3
KY 61/KY 1065	1
Total	4

Jefferson County  
RADAR PRESENCE DETECTOR SUMMARY  
FD04 056 1065 004-008

MPT.	INTERSECTION	RADAR PRESENCE DETECTOR TYPE A
6.055	KY 61	EA 4
6.405	NOLTEMAYER WYNDE	4
7.655	SHEPHERSVILLE RD/KY 2052	4
TOTAL		12

NOTES:

Sidewalks & Detectable Warnings Summary  
Jefferson County

FD04 056 1065 004-008

Intersection	Mile Point	Ramp Location	4" Sidewalk SQYD	Detectable Warning (NEW) SQFT	Detectable Warning (Retrofit) SQFT
Shephersville Rd. (KY 2052)	7.655	NE	-	-	44
		NW	-	-	20
		SE	13.00	20.00	-
		SW	14.00	30.00	-
PROJECT TOTALS			27	50	64

FULL DEPTH CONCRETE REPAIR SUMMARY

Eastbound					
MP	Length	Width	SQYD		Location
5.547	65	12	86.67		Left Lane
5.547	28	12	37.33		Right Lane
5.598	32	12	42.67		Left lane
5.664	29	12	38.67		Right Lane
5.751	16	12	21.33		Right Lane
5.882	30	12	40.00		Fast lane
5.902	35	24	93.33		Fast and Slow Lanes
5.92	18	12	24.00		Right Lane
5.944	30	12	40.00		Both lanes
6.149	22	12	29.33		Right Lane
6.181	35	12	46.67		Right Lane
6.247	35	12	46.67		Right Lane
6.383	58	24	154.67		Fast and Slow Lanes
6.441	24	12	32.00		Right Lane
6.489	22	12	29.33		Right Lane
6.517	28	12	37.33		Right Lane
6.524	22	12	29.33		Left Lane
6.555	25	12	33.33		Right Lane
6.584	50	12	66.67		Right Lane
6.616	71	12	94.67		Right Lane
6.641	28	12	37.33		Right Lane
6.65	56	12	74.67		Right Lane
6.668	29	12	38.67		Right Lane
6.722	32	12	42.67		Left Lane
6.796	83	12	110.67		Right Lane
6.826	45	12	60.00		Right Lane
6.851	55	24	146.67		Center of Intersection
6.933	30	12	40.00		Right Lane
6.955	18	12	24.00		Left Lane
6.99	28	12	37.33		Right Lane
7.001	25	12	33.33		Right Lane
7.04	22	12	29.33		Left Lane
7.049	28	12	37.33		Right Lane
7.123	25	24	66.67		Both lanes
7.145	242	12	322.67		Left Lane
7.181	18	12	24.00		Right Lane
7.219	90	24	240.00		Fast and Slow Lanes
7.316	25	12	33.33		Left Lane
7.341	35	12	46.67		Right Lane
7.363	72	12	96.00		Right Lane
7.43	47	12	62.67		Left Lane
7.443	25	24	66.67		Both lanes
7.505	32	24	85.33		Both lanes
		Total	2780.00		

\*Does not include replacement areas at intersections with KY 61 and KY 2052

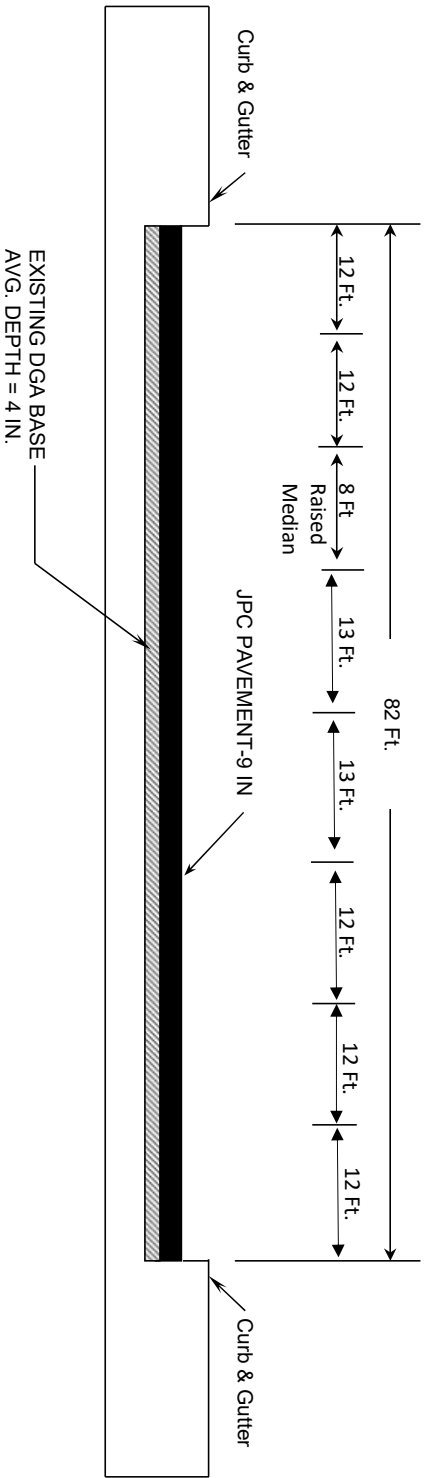


FULL DEPTH CONCRETE REPAIR SUMMARY

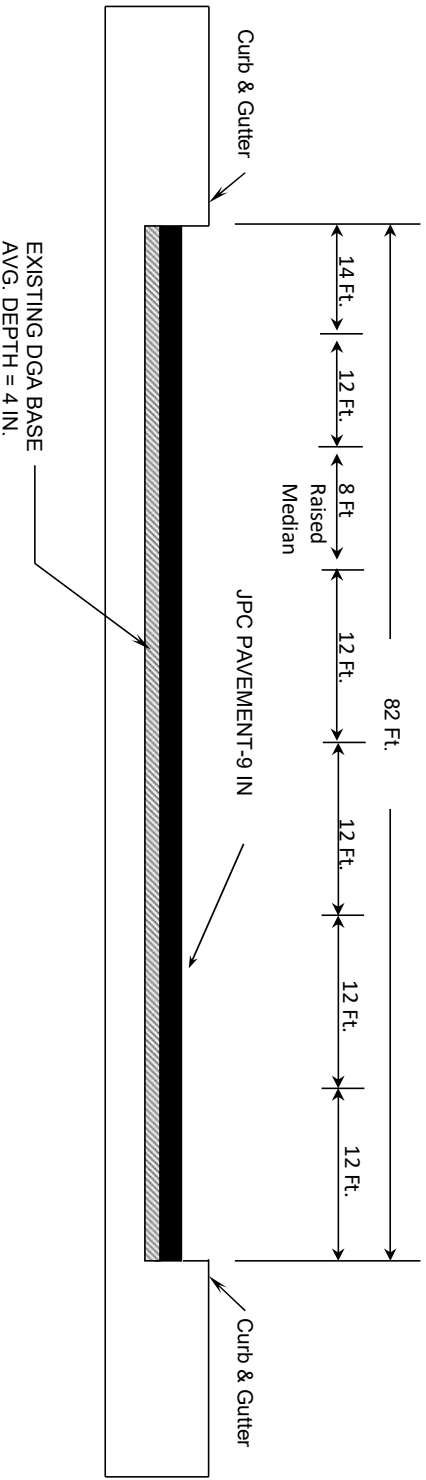
Westbound					
MP	Length	Width	SQYD		Location
7.575	56	24	149.33		Both Lanes
7.553	119	12	158.67		Right lane
7.522	45	12	60.00		Right lane
7.522	29	12	38.67		Left Lane
7.439	48	12	64.00		Right lane
7.345	20	12	26.67		Right lane
7.344	30	12	40.00		Right lane
7.221	22	24	58.67		Both Lanes
7.187	21	12	28.00		Right lane
7.151	37	24	98.67		Fast and Slow Lanes
7.085	21	24	56.00		Fast and Slow Lanes
7.028	55	12	73.33		Right lane
6.919	24	24	64.00		Fast and Slow Lanes
6.911	22	24	58.67		Fast and Slow Lanes
6.86	24	24	64.00		Fast and Slow Lanes
6.79	31	24	82.67		Fast and Slow Lanes
6.6	32	24	85.33		Both Lanes
6.582	23	12	30.67		Right lane
6.552	24	12	32.00		Right lane
6.43	30	12	40.00		Right lane
6.362	48	12	64.00		Right lane
6.342	22	12	29.33		Right lane
6.295	61	12	81.33		Right lane
6.295	35	12	46.67		Left Lane
6.187	45	24	120.00		Both Lanes
6.141	24	24	64.00		Both Lanes
5.996	22	12	29.33		Left Lane
5.989	14	12	18.67		Left Lane
5.954	22	12	29.33		Left Lane
5.946	16	12	21.33		Left Lane
5.925	24	24	64.00		Both Lanes
5.896	22	12	29.33		Left Lane
5.89	42	24	112.00		Both Lanes
5.845	16	12	21.33		Left Lane
5.835	16	12	21.33		Left Lane
5.779	14	24	37.33		Both Lanes
5.694	14	24	37.33		Both Lanes
5.605	68	24	181.33		Both Lanes
		Total	2318.00		

Does not include replacement areas at intersections with KY 61 and KY 2052

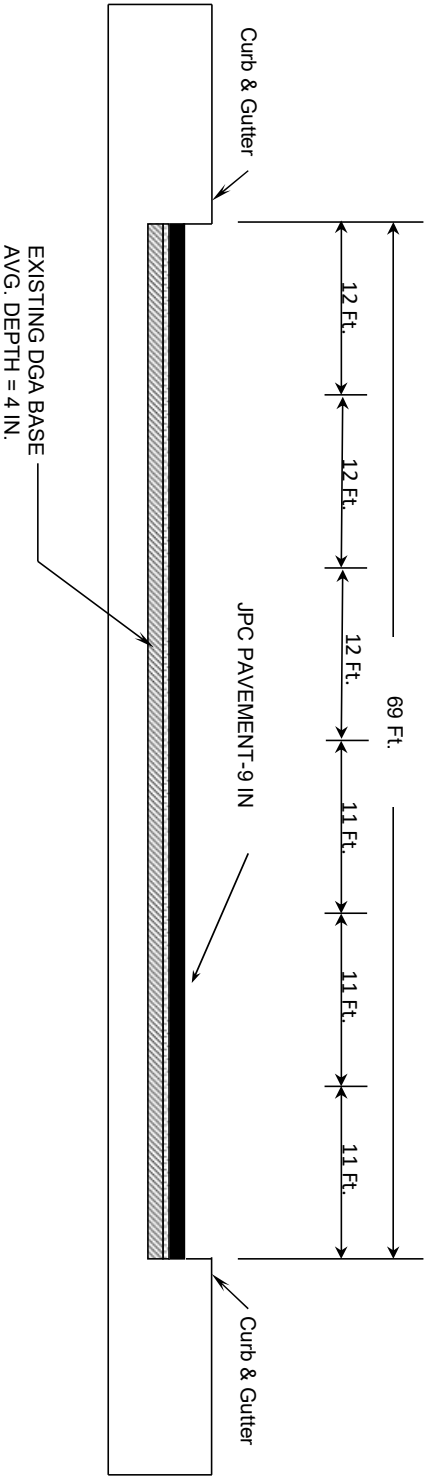
Jefferson County  
FD04 056 1065 004-008  
TYPICAL SECTION  
KY 1065/KY 61 EB APPROACH



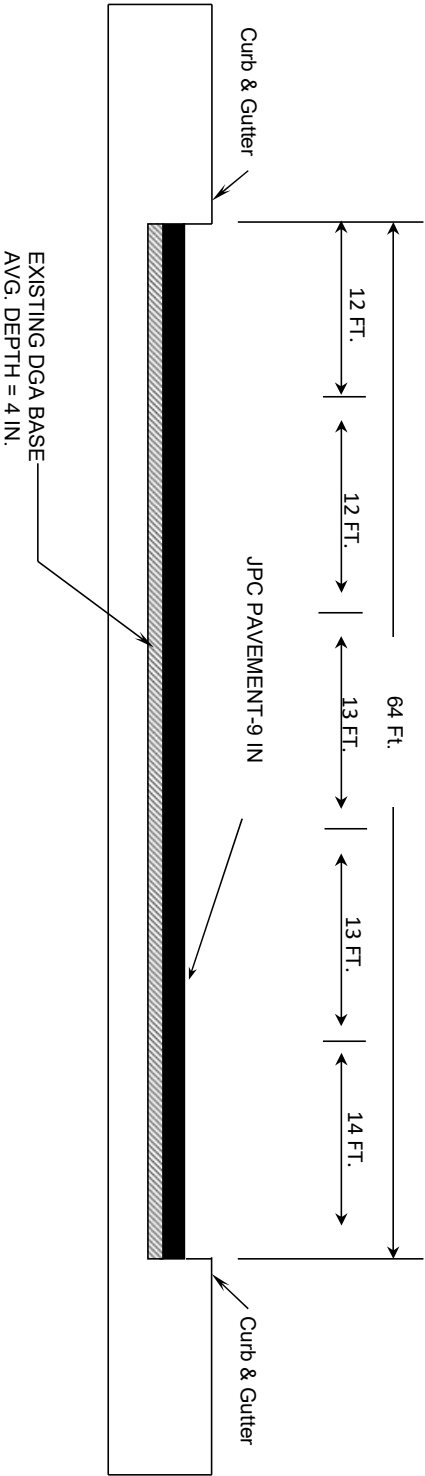
Jefferson County  
FD04 056 1065 004-008  
TYPICAL SECTION  
KY 1065/KY 61 WB APPROACH



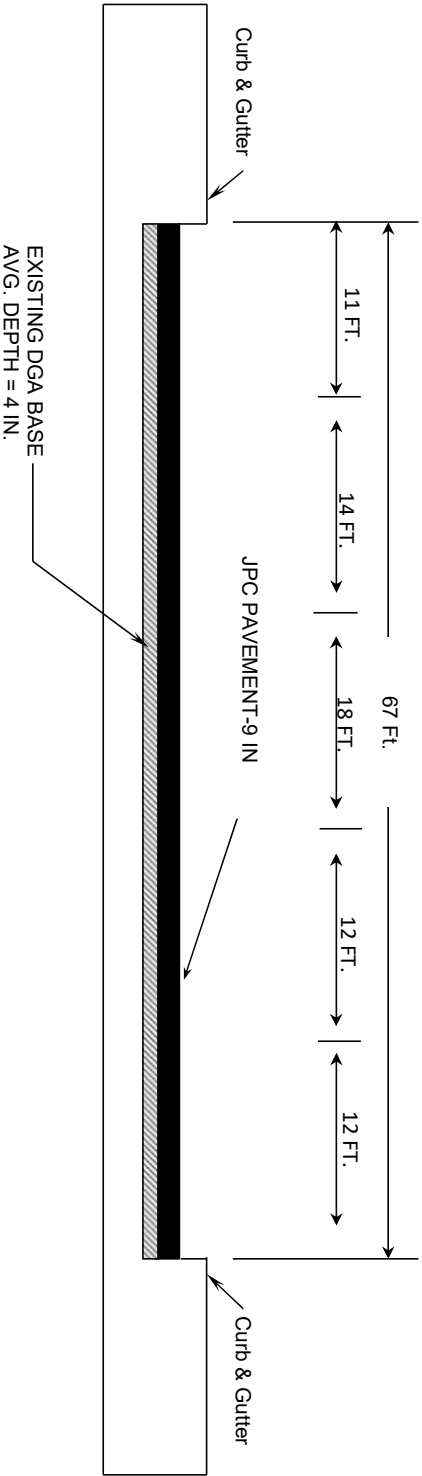
Jefferson County  
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TYPICAL SECTION  
KY 1065/KY 2052 EB APPROACH



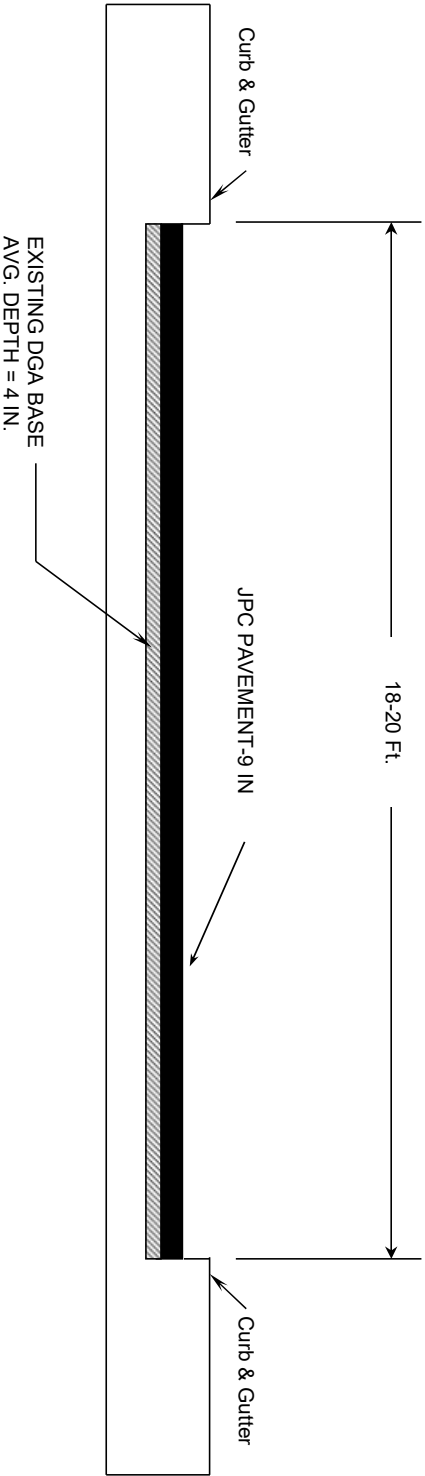
Jefferson County  
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TYPICAL SECTION  
KY 1065/KY 2052 SB Approach

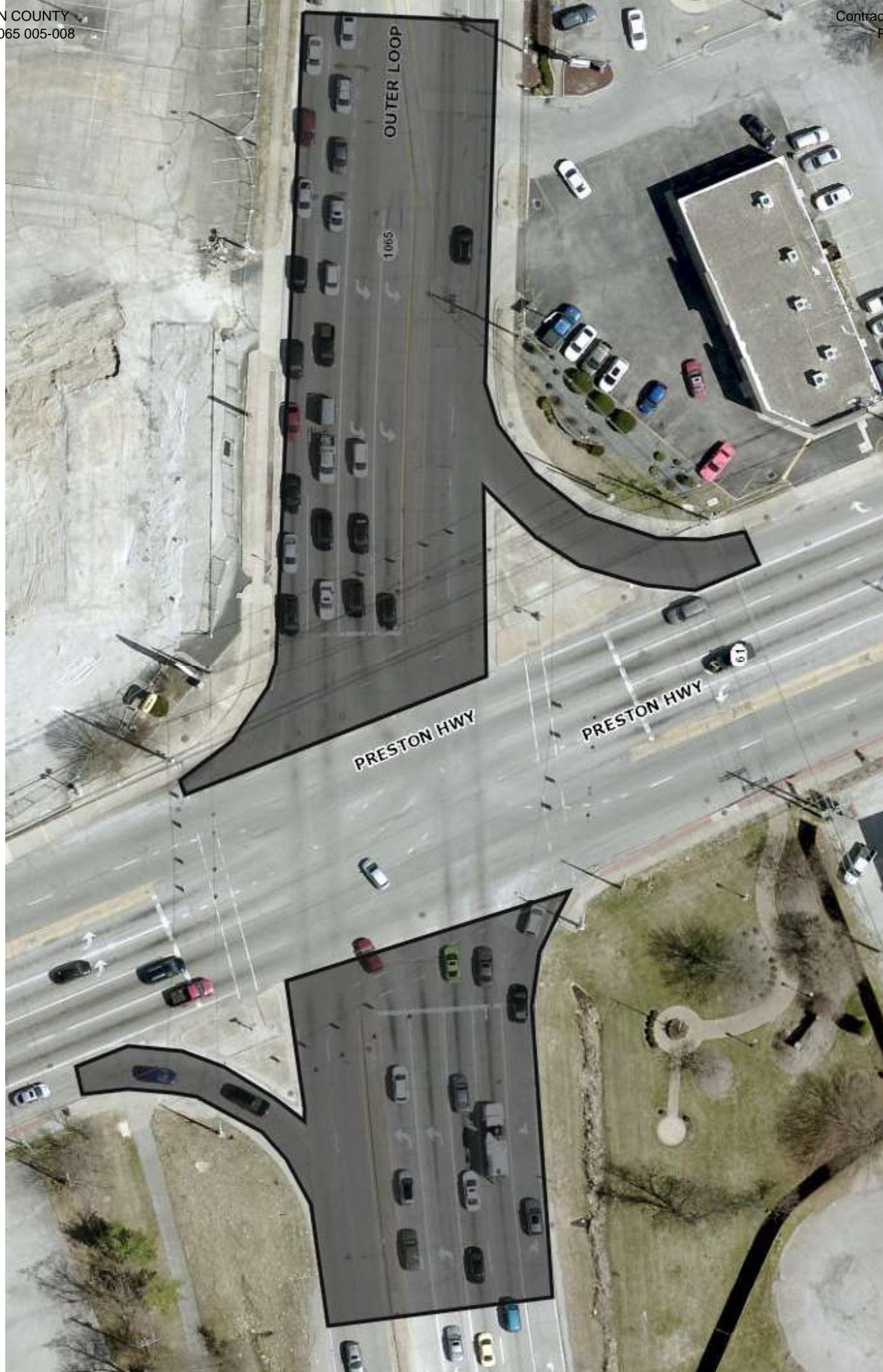


Jefferson County  
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TYPICAL SECTION  
KY 1065/KY 2052 WB Approach

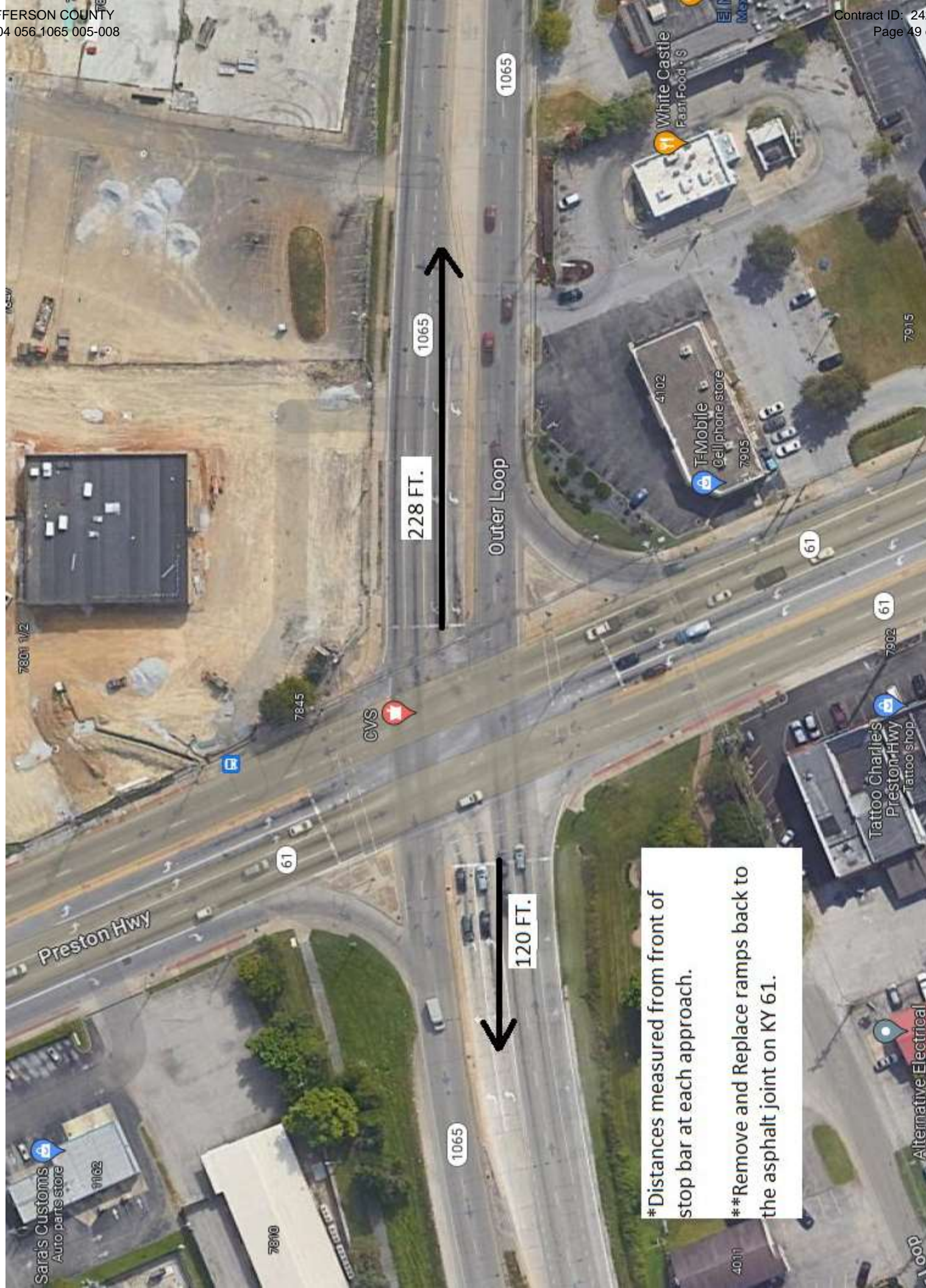


Jefferson County  
FD04 056 1065 004-008  
TYPICAL SECTION  
Ramps at KY 61/KY 1065









\*Distances measured from front of stop bar at each approach.  
\*\*Remove and Replace ramps back to the asphalt joint on KY 61.









**PART II**

**SPECIFICATIONS AND STANDARD DRAWINGS**

### **STANDARD SPECIFICATIONS**

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2019* and *Standard Drawings, Edition of 2020*.

### **SUPPLEMENTAL SPECIFICATIONS**

The contractor shall use the Supplemental Specifications that are effective at the time of letting. The Supplemental Specifications can be found at the following link:  
<http://transportation.ky.gov/Construction/Pages/Kentucky-Standard-Specifications.aspx>

## **SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS**

This Special Note will apply when indicated on the plans or in the proposal.

**1.0 DESCRIPTION.** Furnish, install, operate, and maintain variable message signs at the locations shown on the plans or designated by the Engineer. Remove and retain possession of variable message signs when they are no longer needed on the project.

## **2.0 MATERIALS.**

**2.1 General.** Use LED Variable Message Signs Class I, II, or III, as appropriate, from the Department's List of Approved Materials.

Unclassified signs may be submitted for approval by the Engineer. The Engineer may require a daytime and nighttime demonstration. The Engineer will make a final decision within 30 days after all required information is received.

### **2.2 Sign and Controls.** All signs must:

- 1) Provide 3-line messages with each line being 8 characters long and at least 18 inches tall. Each character comprises 35 pixels.
- 2) Provide at least 40 preprogrammed messages available for use at any time. Provide for quick and easy change of the displayed message; editing of the message; and additions of new messages.
- 3) Provide a controller consisting of:
  - a) Keyboard or keypad.
  - b) Readout that mimics the actual sign display. (When LCD or LCD type readout is used, include backlighting and heating or otherwise arrange for viewing in cold temperatures.)
  - c) Non-volatile memory or suitable memory with battery backup for storing pre-programmed messages.
  - d) Logic circuitry to control the sequence of messages and flash rate.
- 4) Provide a serial interface that is capable of supporting complete remote control ability through land line and cellular telephone operation. Include communication software capable of immediately updating the message, providing complete sign status, and allowing message library queries and updates.
- 5) Allow a single person easily to raise the sign to a satisfactory height above the pavement during use, and lower the sign during travel.
- 6) Be Highway Orange on all exterior surfaces of the trailer, supports, and controller cabinet.
- 7) Provide operation in ambient temperatures from -30 to + 120 degrees Fahrenheit during snow, rain and other inclement weather.
- 8) Provide the driver board as part of a module. All modules are interchangeable, and have plug and socket arrangements for disconnection and reconnection. Printed circuit boards associated with driver boards have a conformable coating to protect against moisture.
- 9) Provide a sign case sealed against rain, snow, dust, insects, etc. The lens is UV stabilized clear plastic (polycarbonate, acrylic, or other approved material) angled to prevent glare.
- 10) Provide a flat black UV protected coating on the sign hardware, character PCB, and appropriate lens areas.
- 11) Provide a photocell control to provide automatic dimming.

- 12) Allow an on-off flashing sequence at an adjustable rate.
- 13) Provide a sight to aim the message.
- 14) Provide a LED display color of approximately 590 nm amber.
- 15) Provide a controller that is password protected.
- 16) Provide a security device that prevents unauthorized individuals from accessing the controller.
- 17) Provide the following 3-line messages preprogrammed and available for use when the sign unit begins operation:

/KEEP/RIGHT/⇒⇒⇒/	/MIN/SPEED/**MPH/
/KEEP/LEFT/⇐⇐⇐/	/ICY/BRIDGE/AHEAD/ /ONE
/LOOSE/GRAVEL/AHEAD/	LANE/BRIDGE/AHEAD/
/RD WORK/NEXT/**MILES/	/ROUGH/ROAD/AHEAD/
/TWO WAY/TRAFFIC/AHEAD/	/MERGING/TRAFFIC/AHEAD/
/PAINT/CREW/AHEAD/	/NEXT/***/MILES/
/REDUCE/SPEED/**MPH/	/HEAVY/TRAFFIC/AHEAD/
/BRIDGE/WORK/***() FT/	/SPEED/LIMIT/**MPH/
/MAX/SPEED/**MPH/	/BUMP/AHEAD/
/SURVEY/PARTY/AHEAD/	/TWO/WAY/TRAFFIC/

\*Insert numerals as directed by the Engineer.  
Add other messages during the project when required by the Engineer.

2.3 Power.

- 1) Design solar panels to yield 10 percent or greater additional charge than sign consumption. Provide direct wiring for operation of the sign or arrow board from an external power source to provide energy backup for 21 days without sunlight and an on-board system charger with the ability to recharge completely discharged batteries in 24 hours.

**3.0 CONSTRUCTION.** Furnish and operate the variable message signs as designated on the plans or by the Engineer. Ensure the bottom of the message panel is a minimum of 7 feet above the roadway in urban areas and 5 feet above in rural areas when operating. Use Class I, II, or III signs on roads with a speed limit less than 55 mph. Use Class I or II signs on roads with speed limits 55 mph or greater.

Maintain the sign in proper working order, including repair of any damage done by others, until completion of the project. When the sign becomes inoperative, immediately repair or replace the sign. Repetitive problems with the same unit will be cause for rejection and replacement.

Use only project related messages and messages directed by the Engineer, unnecessary messages lessen the impact of the sign. Ensure the message is displayed in either one or 2 phases with each phase having no more than 3 lines of text. When no message is needed, but it is necessary to know if the sign is operable, flash only a pixel.

When the sign is not needed, move it outside the clear zone or where the Engineer directs. Variable Message Signs are the property of the Contractor and shall be removed from the project when no longer needed. The Department will not assume ownership of these signs.

**4.0 MEASUREMENT.** The final quantity of Variable Message Sign will be

11  
the actual number of individual signs acceptably furnished and operated during the project. The Department will not measure signs replaced due to damage or rejection.

**5.0 PAYMENT.** The Department will pay for the Variable Message Signs at the unit price each. The Department will not pay for signs replaced due to damage or rejection. Payment is full compensation for furnishing all materials, labor, equipment, and service necessary to, operate, move, repair, and maintain or replace the variable message signs. The Department will make payment for the completed and accepted quantities under the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
02671	Portable Changeable Message Sign	Each

Effective June 15, 2012



2020 KENTUCKY STANDARD DRAWINGS

CURVE WIDENING AND SUPERELEVATION TRANSITIONS .....	RGS-001-07
SUPERELEVATION FOR MULTILANE PAVEMENT .....	RGS-002-06
MISCELLANEOUS STANDARDS .....	RGX-001-06
APPROACHES, ENTRANCES, AND MAIL BOX TURNOUT .....	RPM-110-07
LANE CLOSURE MULTI-LANE HIGHWAY CASE I.....	TTC-115-04
LANE CLOSURE MULTI-LANE HIGHWAY CASE II .....	TTC-120-04
SHOULDER CLOSURE .....	TTC-135-02
PAVEMENT CONDITION WARNING SIGNS.....	TTD-125-02
MOBILE OPERATION FOR PAINT STRIPING CASE III.....	TTS-110-02
MOBILE OPERATION FOR PAINT STRIPING CASE IV .....	TTS-115-02
DETECTABLE WARNINGS .....	RGX-040-03
CONCRETE ENTRANCE PAVEMENT AND SIDEWALK.....	RPM-150-08
CONCRETE ENTRANCE PAVEMENT AND SIDEWALK.....	RPM-152-08
SIDEWALK RAMPS .....	RPM-170-09
CURB AND GUTTER, CURBS AND VALLEY GUTTER .....	RPM-100-11
CONCRETE PAVEMENT JOINT DETAILS.....	RPS-010-11
CONCRETE PAVEMENT JOINT DETAILS.....	RPS-010-11

## **PART III**

### **EMPLOYMENT, WAGE AND RECORD REQUIREMENTS**

**TRANSPORTATION CABINET  
DEPARTMENT OF HIGHWAYS**

**LABOR AND WAGE REQUIREMENTS  
APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS**

- I. Application
- II. Nondiscrimination of Employees (KRS 344)

**I. APPLICATION**

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual because of his race, color, religion, national origin, sex, disability or age forty (40) and over, in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

Revised: January 25, 2017

**II. NONDISCRIMINATION OF EMPLOYEES**

**AN ACT OF THE KENTUCKY  
GENERAL ASSEMBLY TO PREVENT  
DISCRIMINATION IN EMPLOYMENT  
KRS CHAPTER 344  
EFFECTIVE JUNE 16, 1972**

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (forty and above); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age forty (40) and over. The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, except that such a notice or advertisement may indicate a preference, limitation, or specification based on religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, when religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, is a bona fide occupational qualification for employment.

## EXECUTIVE BRANCH CODE OF ETHICS

In the 1992 regular legislative session, the General Assembly passed and Governor Brereton Jones signed Senate Bill 63 (codified as KRS 11A), the Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (7) provides:

No present or former public servant shall, within six (6) months following termination of his office or employment, accept employment, compensation, or other economic benefit from any person or business that contracts or does business with, or is regulated by, the state in matters in which he was directly involved during the last thirty-six (36) months of his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, or for which he received, prior to his state employment, a professional degree or license, provided that, for a period of six (6) months, he personally refrains from working on any matter in which he was directly involved during the last thirty-six (36) months of his tenure in state government. This subsection shall not prohibit the performance of ministerial functions, including but not limited to filing tax returns, filing applications for permits or licenses, or filing incorporation papers, nor shall it prohibit the former officer or public servant from receiving public funds disbursed through entitlement programs.

KRS 11A.040 (9) states:

A former public servant shall not represent a person or business before a state agency in a matter in which the former public servant was directly involved during the last thirty-six (36) months of his tenure, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not as a way to obtain private benefits.

If you have worked for the executive branch of state government within the past six months, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, 1025 Capital Center Drive, Suite 104, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Revised: May 23, 2022

### **Kentucky Equal Employment Opportunity Act of 1978**

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:  
**<https://www.eProcurement.ky.gov>**.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **[finance.contractcompliance@ky.gov](mailto:finance.contractcompliance@ky.gov)** or by phone at 502-564-2874.

# EMPLOYEE RIGHTS UNDER THE FAIR LABOR STANDARDS ACT

THE UNITED STATES DEPARTMENT OF LABOR WAGE AND HOUR DIVISION

## FEDERAL MINIMUM WAGE

**\$7.25** PER HOUR

BEGINNING JULY 24, 2009

### OVERTIME PAY

At least 1½ times your regular rate of pay for all hours worked over 40 in a workweek.

### CHILD LABOR

An employee must be at least **16** years old to work in most non-farm jobs and at least **18** to work in non-farm jobs declared hazardous by the Secretary of Labor.

Youths **14** and **15** years old may work outside school hours in various non-manufacturing, non-mining, non-hazardous jobs under the following conditions:

**No more than**

- **3** hours on a school day or **18** hours in a school week;
- **8** hours on a non-school day or **40** hours in a non-school week.

Also, work may not begin before **7 a.m.** or end after **7 p.m.**, except from June 1 through Labor Day, when evening hours are extended to **9 p.m.** Different rules apply in agricultural employment.

### TIP CREDIT

Employers of “tipped employees” must pay a cash wage of at least \$2.13 per hour if they claim a tip credit against their minimum wage obligation. If an employee’s tips combined with the employer’s cash wage of at least \$2.13 per hour do not equal the minimum hourly wage, the employer must make up the difference. Certain other conditions must also be met.

### ENFORCEMENT

The Department of Labor may recover back wages either administratively or through court action, for the employees that have been underpaid in violation of the law. Violations may result in civil or criminal action.

Employers may be assessed civil money penalties of up to \$1,100 for each willful or repeated violation of the minimum wage or overtime pay provisions of the law and up to \$11,000 for each employee who is the subject of a violation of the Act’s child labor provisions. In addition, a civil money penalty of up to \$50,000 may be assessed for each child labor violation that causes the death or serious injury of any minor employee, and such assessments may be doubled, up to \$100,000, when the violations are determined to be willful or repeated. The law also prohibits discriminating against or discharging workers who file a complaint or participate in any proceeding under the Act.

### ADDITIONAL INFORMATION

- Certain occupations and establishments are exempt from the minimum wage and/or overtime pay provisions.
- Special provisions apply to workers in American Samoa and the Commonwealth of the Northern Mariana Islands.
- Some state laws provide greater employee protections; employers must comply with both.
- The law requires employers to display this poster where employees can readily see it.
- Employees under 20 years of age may be paid \$4.25 per hour during their first 90 consecutive calendar days of employment with an employer.
- Certain full-time students, student learners, apprentices, and workers with disabilities may be paid less than the minimum wage under special certificates issued by the Department of Labor.

For additional information:



**1-866-4-USWAGE**

(1-866-487-9243)

TTY: 1-877-889-5627



**WWW.WAGEHOUR.DOL.GOV**

## **PART IV**

## **INSURANCE**

Refer to  
*Kentucky Standard Specifications for Road and Bridge Construction,*  
current edition

**PART V**

**BID ITEMS**



Report Date 2/23/24

Section: 0001 - ROADWAY

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0010	00001		DGA BASE	355.00	TON		\$	
0020	00078		CRUSHED AGGREGATE SIZE NO 2	1,060.00	TON		\$	
0030	01791		ADJUST MANHOLE FRAME TO GRADE	4.00	EACH		\$	
0040	02014		BARRICADE-TYPE III	20.00	EACH		\$	
0050	02058		REMOVE PCC PAVEMENT	15,339.00	SQYD		\$	
0060	02073		JPC PAVEMENT-9 IN	15,339.00	SQYD		\$	
0070	02562		TEMPORARY SIGNS	700.00	SQFT		\$	
0080	02603		FABRIC-GEOTEXTILE CLASS 2	1,534.00	SQYD		\$	
0090	02650		MAINTAIN & CONTROL TRAFFIC	1.00	LS		\$	
0100	02671		PORTABLE CHANGEABLE MESSAGE SIGN	4.00	EACH		\$	
0110	02720		SIDEWALK-4 IN CONCRETE	27.00	SQYD		\$	
0120	02726		STAKING	1.00	LS		\$	
0130	02775		ARROW PANEL	4.00	EACH		\$	
0140	06515		PAVE STRIPING-PERM PAINT-6 IN	12,900.00	LF		\$	
0150	06517		PAVE STRIPING-PERM PAINT-12 IN	340.00	LF		\$	
0160	06547		PAVE STRIPING-THERMO-12 IN Y	40.00	LF		\$	
0170	06565		PAVE MARKING-THERMO X-WALK-6 IN	1,660.00	LF		\$	
0180	06568		PAVE MARKING-THERMO STOP BAR-24IN	392.00	LF		\$	
0190	06574		PAVE MARKING-THERMO CURV ARROW	2.00	EACH		\$	
0200	06610		INLAID PAVEMENT MARKER-MW	78.00	EACH		\$	
0210	06611		INLAID PAVEMENT MARKER-MY	22.00	EACH		\$	
0220	06612		INLAID PAVEMENT MARKER-BY	22.00	EACH		\$	
0230	22520EN		PAVE MARKING-THERMO YIELD BAR-36 IN	72.00	LF		\$	
0240	23158ES505		DETECTABLE WARNINGS (NEW)	50.00	SQFT		\$	
0250	23158ES505		DETECTABLE WARNINGS (RETROFIT)	64.00	SQFT		\$	
0260	23254ES717		PAVE MARK TY 1 TAPE DOTTED LANE EXT	305.00	LF		\$	
0270	23261EC		PAVE MARK-THERMO-X-WALK-24 IN	80.00	LF		\$	
0280	23270ES717		PAVE MARK TY 1 TAPE-CURV ARROW	28.00	EACH		\$	
0290	24683ED		PAVE MARKING-THERMO DOTTED LANE EXTEN	150.00	LF		\$	
0300	24880EC		REMOVE PAVEMENT MARKER	122.00	EACH		\$	
0310	26119EC		INSTALL RADAR PRESENCE DETECTOR TYPE A	12.00	EACH		\$	
0320	26146ES717		PAVE MARK TY 1 TAPE LANE REDUCTION ARROW	2.00	EACH		\$	
0330	26166ES717		PAVE MARK TY 1 TAPE CHEVRON	100.00	SQFT		\$	

Section: 0002 - DEMOBILIZATION

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0340	02568		MOBILIZATION	1.00	LS		\$	
0350	02569		DEMOBILIZATION	1.00	LS		\$	