



CALL NO. 320

CONTRACT ID. 121345

LIVINGSTON COUNTY

FED/STATE PROJECT NUMBER FD04 SPP 070 0024 030-034

DESCRIPTION PADUCAH-TENNESSEE STATE LINE ROAD (I-24)

WORK TYPE ASPHALT REHAB INTERSTATE/PARKWAY

PRIMARY COMPLETION DATE 6/30/2013

LETTING DATE: September 14, 2012

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME September 14, 2012. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

REQUIRED BID PROPOSAL GUARANTY: Not less than 5% of the total bid.

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PART I

SCOPE OF WORK

CONTRACT ID - 121345 ADMINISTRATIVE DISTRICT - 01

PROJECT(S) IDENTIFICATION AND DESCRIPTION:

COUNTY - LIVINGSTON PCN - DE07000241245
FD04 SPP 070 0024 030-034
PADUCAH-TENNESSEE STATE LINE ROAD (I-24) MILL AND THIN ASPHALT OVER ON I-24 FROM MP 30.454 TO MP 33.660, A DISTANCE OF 3.21 MILES. ASPHALT REHAB INTERSTATE/PARKWAY. SYP NO. 01-02027.00.
GEOGRAPHIC COORDINATES LATITUDE 37^02'58" LONGITUDE 88^14'49"

COMPLETION DATE(S):
COMPLETION DATE - June 30, 2013
APPLIES TO ENTIRE CONTRACT

CONTRACT NOTES

PROPOSAL ADDENDA

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

BID SUBMITTAL

Bidder must use the Department's Expedite Bidding Program available on the Internet web site of the Department of Highways, Division of Construction Procurement. (www.transportation.ky.gov/contract)

The Bidder must download the bid file located on the Bid Express website (www.bidx.com) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

JOINT VENTURE BIDDING

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

UNDERGROUND FACILITY DAMAGE PROTECTION

The contractor is advised that the Underground Facility Damage Protection Act of 1994, became law January 1, 1995. It is the contractor's responsibility to determine the impact of the act regarding this project, and take all steps necessary to be in compliance with the provision of the act.

SPECIAL NOTE FOR PIPE INSPECTION

Contrary to Section 701.03.08 of the 2012 Standard Specifications for Road and Bridge Construction and Kentucky Method 64-114, certification by the Kentucky Transportation Center for prequalified Contractors to perform laser/video inspection is not required on this contract. It will continue to be a requirement for the Contractor performing any laser/video pipe inspection to be prequalified for this specialized item with the Kentucky Transportation Cabinet-Division of Construction Procurement.

REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth (“certificate”) from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity’s solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by fax (502) 564-7299 or email to kytc.projectquestions@ky.gov. The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading “Questions & Answers” on the Construction Procurement website (www.transportation.ky.gov/contract). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

ACCESS TO RECORDS

The contractor, as defined in KRS 45A.030 (9) agrees that the contracting agency, the Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this

contract for the purpose of financial audit or program review. Records and other prequalification information confidentially disclosed as part of the bid process shall not be deemed as directly pertinent to the contract and shall be exempt from disclosure as provided in KRS 61.878(1)(c). The contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884.

In the event of a dispute between the contractor and the contracting agency, Attorney General, or the Auditor of Public Accounts over documents that are eligible for production and review, the Finance and Administration Cabinet shall review the dispute and issue a determination, in accordance with Secretary's Order 11-004. (See attachment)

10/18/2011

SPECIAL NOTE FOR RECIPROCAL PREFERENCE

Reciprocal preference to be given by public agencies to resident bidders

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the Expedite Bidding Program. Submittal of the Affidavit should be done along with the bid in Bid Express.

03/01/2011

FUEL AND ASPHALT PAY ADJUSTMENT

The Department has included the Contract items Asphalt Adjustment and Fuel Adjustment for possible future payments at an established Contract unit price of \$1.00. The Department will calculate actual adjustment quantities after work is completed. If existing Contract amount is insufficient to pay all items on the contract with the adjustments, the Department will establish additional monies with a change order.

ASPHALT PAVEMENT RIDE QUALITY CATEGORY A

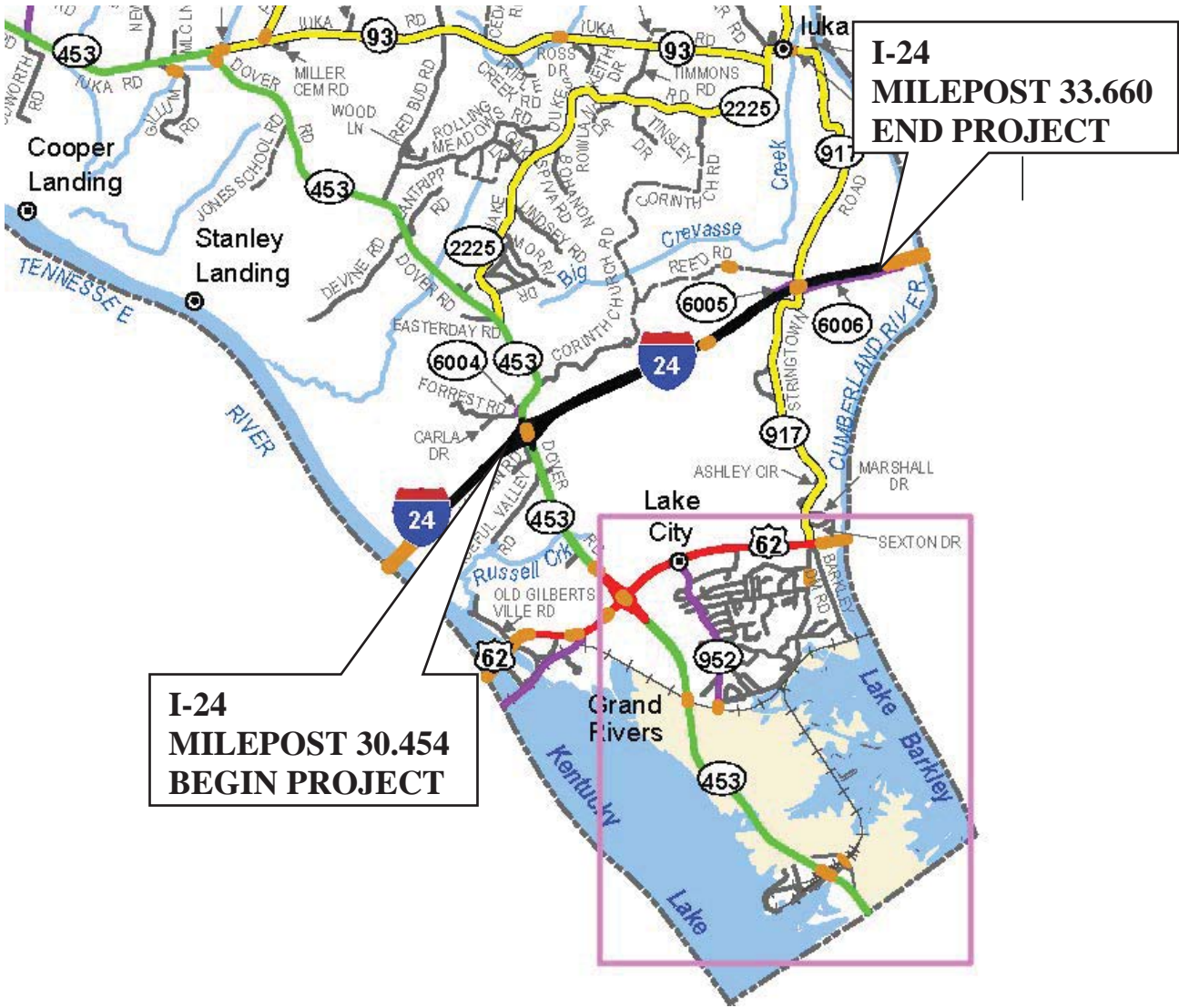
The Department will apply Pavement Rideability Requirements on this project in accordance with Section 410, Category A.

OPTION A

Be advised that the Department will accept compaction of asphalt mixtures furnished for driving lanes and ramps, at 1 inch (25mm) or greater, on this project according to OPTION A in accordance with Section 402 and Section 403 of the current Standard Specifications. The Department will require joint cores as described in Section 402.03.02 for surface mixtures only. The Department will accept compaction of all other asphalt mixtures according to OPTION B.

MATERIAL TRANSFER VEHICLE (MTV)

Provide and use a MTV in accordance with Sections 403.02.10 and 403.03.05.



ITEM NUMBER: 1-2027.00

PROJECT NUMBER: FD04 SPP 070 0024 030-034

CONSTRUCTION NUMBER:

LETTING DATE: September 14, 2012

RECOMMENDED BY: Project Manager DATE:

PLAN APPROVED BY: State Highway Engineer DATE:

FHWA APPROVED BY: DATE:

MATCHLINE (SEE SHEET 2)

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00

RAMP B

RAMP A

225+00

135+00

2835+00

230+00

130+00

2830+00

POB 127+90.30

BEGIN PROJECT
STA. 2828+85.29
M.P. 30.470

235+00

RAMP B

2825+00

POE 239+98.93

2820+00

SCALE: 1"=200'

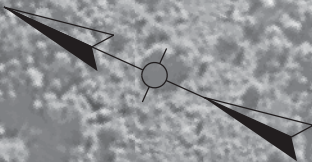
I-24
STATIONING INFORMATION SHEETS
(1 OF 9)

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



MATCHLINE (SEE SHEET 4)

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



2880+00

3880+00

2875+00

3875+00

2870+00

3870+00

2865+00

POE 466+00.85

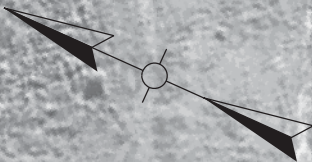
3865+00 465+00

SCALE: 1"=200'

MATCHLINE (SEE SHEET 2)

I-24
STATIONING INFORMATION SHEETS
(3 OF 9)

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00

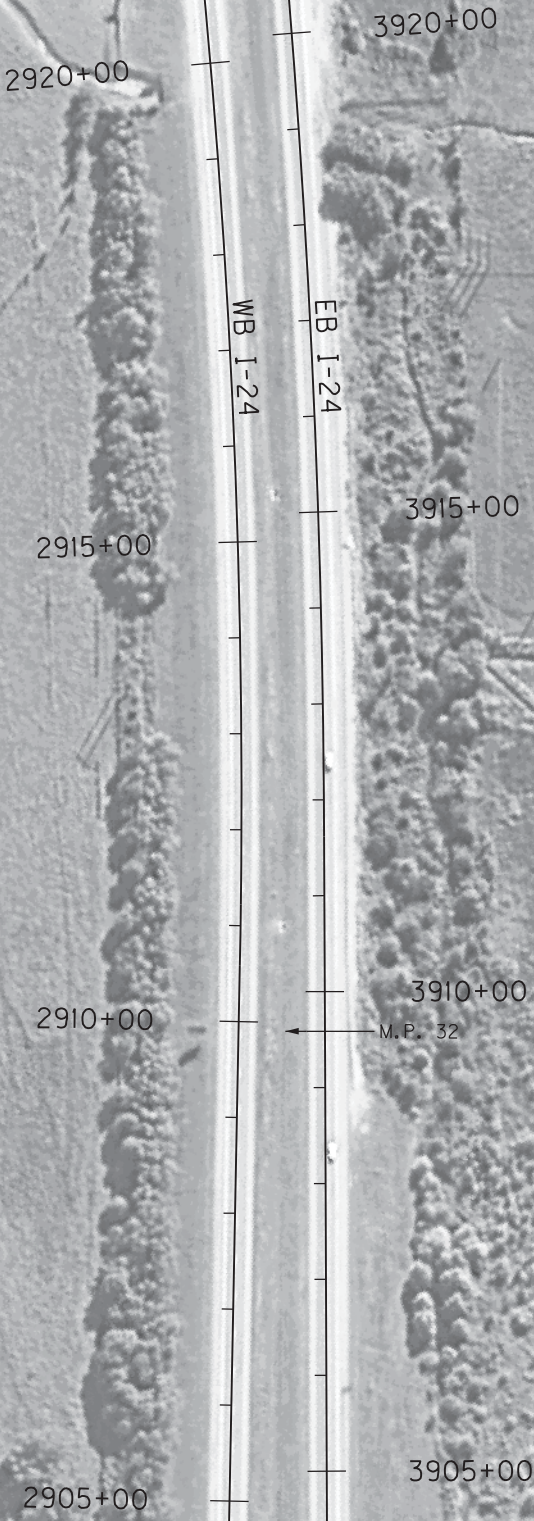
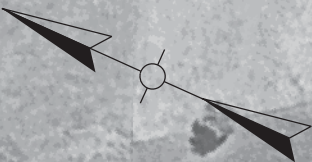


SCALE: 1"=200'

I-24
STATIONING INFORMATION SHEETS
(4 OF 9)

MATCHLINE (SEE SHEET 6)

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



SCALE: 1"=200'

MATCHLINE (SEE SHEET 4)

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



SCALE: 1"=200'

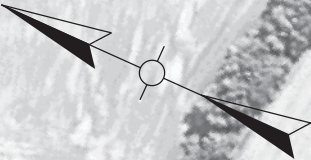
I-24
STATIONING INFORMATION SHEETS
(6 OF 9)

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



MATCHLINE (SEE SHEET 9

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



2985+00

2980+00

2975+00

2970+00

WB I-24

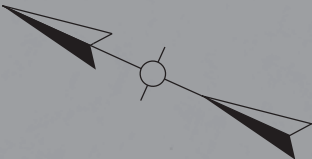
EB I-24

SCALE: 1"=200'

I-24
STATIONING INFORMATION SHEETS
(8 OF 9)

MATCHLINE (SEE SHEET 7

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



CUMBERLAND RIVER

3005+00

3000+00

END PROJECT
STA. 2997+50.54
M.P. 33.672

2995+00

WB I-24

EB I-24

2990+00

SCALE: 1"=200'

MATCHLINE (SEE SHEET 8)

I-24
STATIONING INFORMATION SHEETS
(9 OF 9)

COUNTY OF	ITEM NO.
LIVINGSTON	1-2027.00



NORMAL SECTION



SUPERELEVATED SECTION

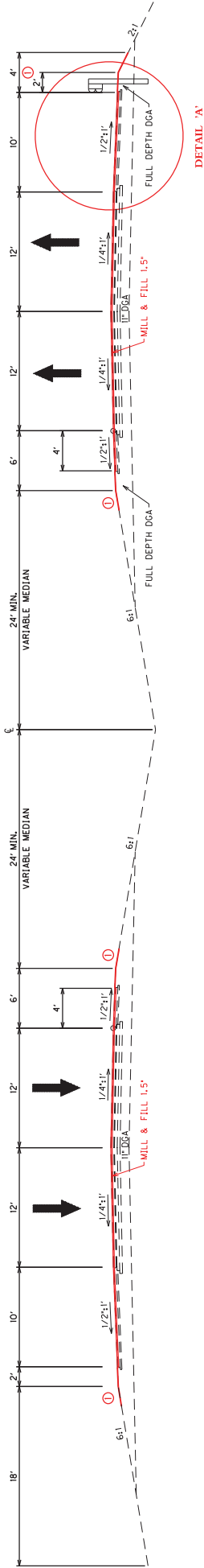


COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00

I-24

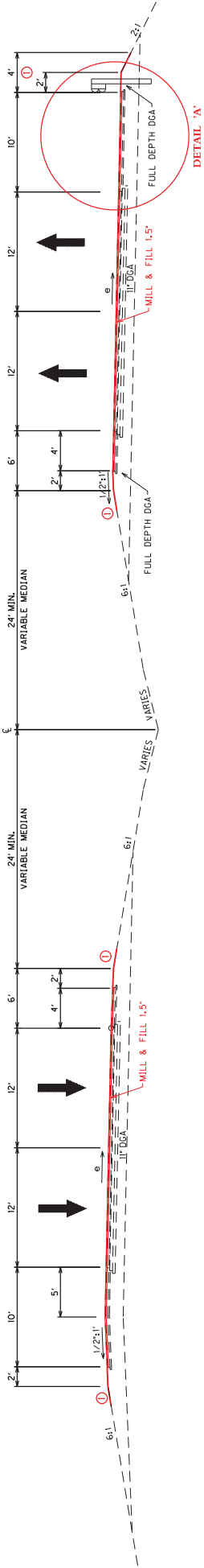
PROPOSED TYPICAL SECTIONS

M.P. 30.454 TO M.P. 33.660



WESTBOUND LANES

NORMAL SECTION



WESTBOUND LANES

EASTBOUND LANES

SUPERELEVATED SECTION

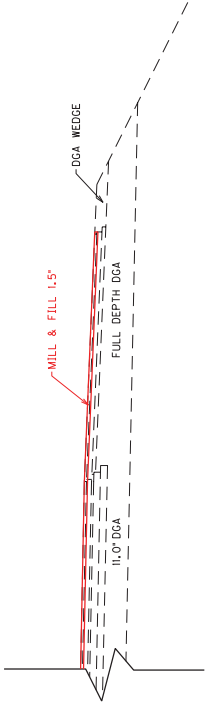
PAVEMENT REHABILITATION

DRIVING LANES & INSIDE SHOULDERS

SURFACE -- 1.5" CL4 ASPHALT SURFACE 0.5A PG76-22
1.5" ASPHALT MILLING AND TEXTURING

OUTSIDE SHOULDER

SURFACE -- 1.5" CL3 ASPHALT SURFACE 0.50 PG64-22
1.5" ASPHALT MILLING AND TEXTURING

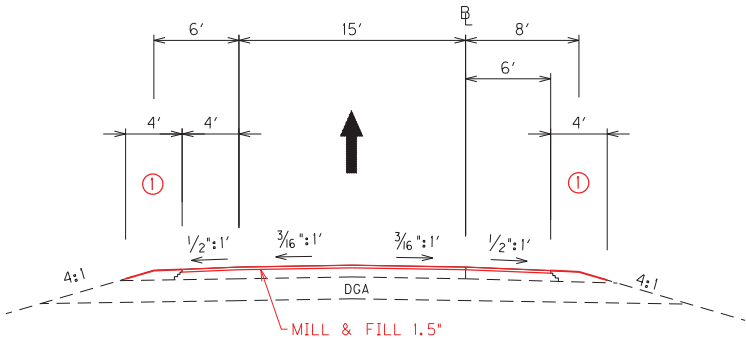


DETAIL 'A'

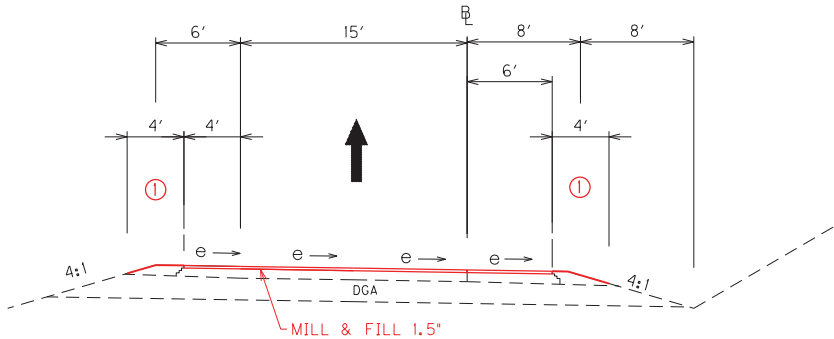
① ASPHALT SEAL
2 APPLICATIONS:
ASPHALT SEAL AGGREGATE - 20 LB/SY
ASPHALT SEAL COAT - 2.4 LB/SY

TYPICAL SECTIONS RAMPS

COUNTY OF	ITEM NO.
LIVINGSTON	1-2027.00



NORMAL SECTION



SUPERELEVATED SECTION

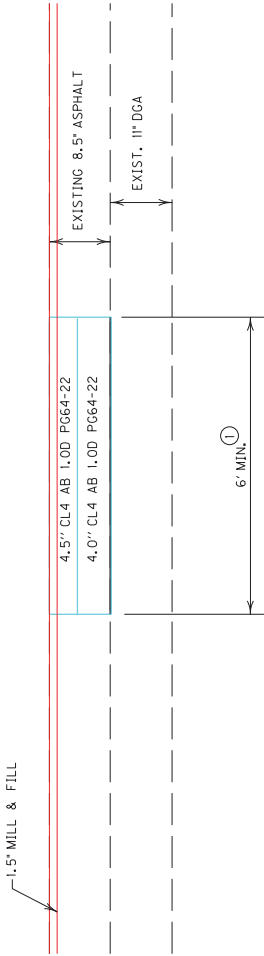
PAVEMENT REHABILITATION

DRIVING LANE & BOTH SHOULDERS

SURFACE -- 1.5" CL4 ASPHALT SURFACE 0.5A PG76-22
1.5" ASPHALT MILLING AND TEXTURE

① ASPHALT SEAL
2 APPLICATIONS:
ASPHALT SEAL AGGREGATE - 20 LB/SY
ASPHALT SEAL COAT - 2.4 LB/SY

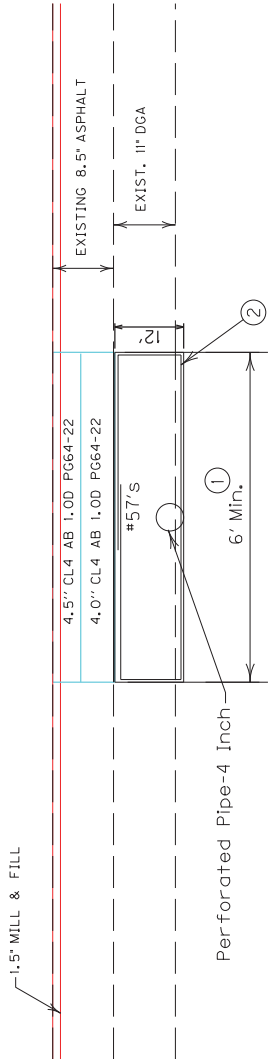
COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



MINOR PAVEMENT REPAIR DETAIL

- ① APPROXIMATE LOCATIONS ARE NOTED IN THE PROPOSAL. SPECIFIC LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. BEFORE REMOVAL OF THE EXISTING MATERIAL, SAW CUTS SHALL BE MADE ON EACH SIDE OF THE AREA TO BE REMOVED. THIS OPERATION SHALL BE PERFORMED ON THE OUTSIDE SHOULDER AND DRIVING LANES WHERE DIRECTED.
- AFTER ALL EXISTING MATERIAL HAS BEEN REMOVED THEN EACH COURSE OF BACKFILL MATERIAL SHALL BE COMPACTED TO THE PROPER DENSITY FOR THE MATERIAL BEING PLACED AS REQUIRED IN THE STANDARD SPECIFICATIONS.
- THE CONTRACT UNIT BID PRICE PER SQ YD FOR PAVEMENT REPAIR (MINOR) SHALL INCLUDE ROADWAY EXCAVATION, REMOVING PAVEMENT, SAW CUTTING, ASPHALT BASE AND ALL INCIDENTALS NECESSARY TO COMPLETE THE INSTALLATION AS DETAILED.

COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



MAJOR PAVEMENT REPAIR DETAIL

- 1

Approximate locations are noted in the proposal. Specific locations will be determined by the Engineer in the field. Before removal of the existing material, saw cuts shall be made on each side of the area to be removed. This operation shall be performed on the outside shoulder and driving lanes where directed.

After all existing material has been removed then each course of backfill material shall be compacted to the proper density for the material being placed as required in the Standard Specifications. The 4" perforated pipe shall not be wrapped and only coarse aggregate shall be used.

The Contract Unit Bid Price per SQ YD for Pavement Repair (Major) shall include Roadway Excavation, Removing Pavement, Saw Cutting, DGA, Perforated Pipe-4 Inch, Crushed Aggregate Size No. 57, Asphalt Base, Fabric-Geotextile Type IV and all incidentals necessary to complete the installation as detailed.

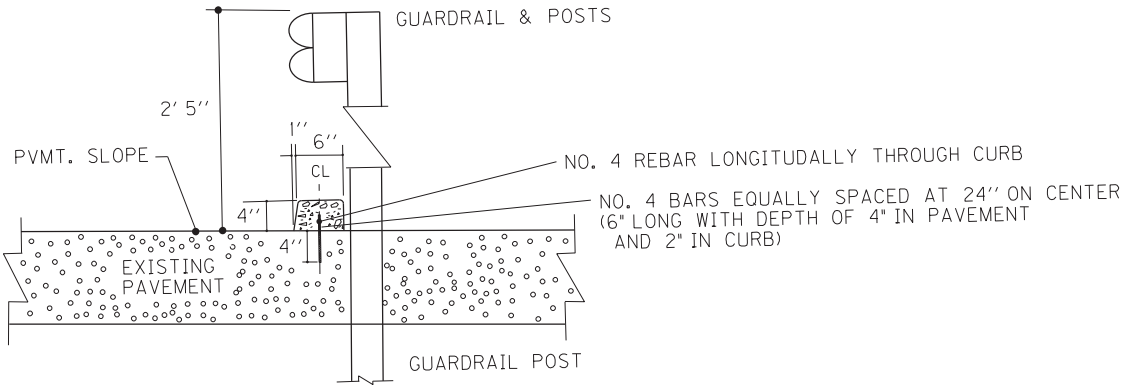
Place the Perforated Pipe at the low point of the digout.

The non-perforated outlet pipe and headwall will be paid for separately. Any excavation or backfill required will be incidental to the length of Non-perforated pipe.
- 2

Fabric-Geotextile Type IV required around coarse aggregate. Overlap fabric one foot.

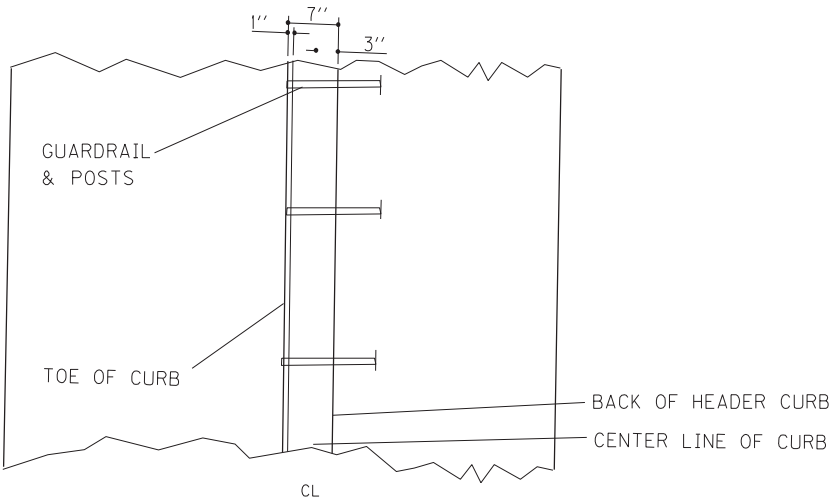
CROSS SECTION VIEW
SPECIAL CONCRETE
HEADER CURB

COUNTY OF	ITEM NO.
LIVINGSTON	1-2027.00



NOT TO SCALE

PLAN VIEW
SPECIAL CONCRETE
HEADER CURB



NOT TO SCALE

I-24 PAVEMENT REHABILITATION - MP 30.454 - MP 33.660 LIVINGSTON COUNTY ITEM NUMBERS: 1-2027.00 GENERAL SUMMARY			
ITEM NUMBER	ITEM	QUANTITY	UNIT
1	DGA BASE (11)	1500	TON
78	CRUSHED AGGREGATE SIZE NO. 2	24	TON
100	ASPHALT SEAL AGGREGATE	888	TON
103	ASPHALT SEAL COAT	107	TON
312	CL3 ASPH SURF 0.50D PG64-22	2841	TON
335	CL4 ASPH SURF 0.50A PG76-22	10298	TON
464	CULVERT PIPE - 24 IN (6)	24	LIN FT
1010	NON-PERFORATED PIPE - 4 INCH	508	LIN FT
1020	PERFORATED PIPE HEADWALL TYPE 1 - 4 INCH	9	EACH
1028	PERFORATED PIPE HEADWALL TYPE 3 - 4 INCH	7	EACH
1032	PERFORATED PIPE HEADWALL TYPE 4 - 4 INCH	8	EACH
1490	DROP BOX INLET TYPE 1 (6)	1	EACH
1505	DROP BOX INLET TYPE 5B (6)	1	EACH
1890	ISLAND HEADER CURB TYPE I	150	LIN FT
1904	REMOVE CURB	7526	LIN FT
1982	DELINEATOR FOR GUARDRAIL - WHITE	114	EACH
1983	DELINEATOR FOR GUARDRAIL -YELLOW	40	EACH
1984	DELINEATORS FOR BARRIER-WHITE	4	EACH
1985	DELINEATORS FOR BARRIER-YELLOW	4	EACH
2220	FLOWABLE FILL (5)	17	CU YD
2237	DITCHING (1)	33681	LIN FT
21802EN	GUARDRAIL-STEEL W BEAM-S FACE (7 FT POST)	12825.0	LIN FT
2352	GUARDRAIL-STEEL W BEAM-D FACE	625.0	LIN FT
2363	GUARDRAIL CONNECTOR TO BRIDGE END TY A	10	EACH
2365	CRASH CUSHION TY IX-A	4	EACH
2367	GUARDRAIL END TREATMENT TYPE 1 (8)	1	EACH
2369	GUARDRAIL END TREATMENT TYPE 2A	9	EACH
2381	REMOVE GUARDRAIL	13712.5	LIN FT
2387	GUARDRAIL CONNECTOR TO BRIDGE END TY A-1	7	EACH
2391	GUARDRAIL END TREATMENT TYPE 4A	3	EACH
2483	CHANNEL LINING CLASS II (2)	1278	TON
2484	CHANNEL LINING CLASS III (2)	388	TON
2562	SIGNS (3)	500	SQ FT
2568	MOBILIZATION	1	LUMP SUM
2569	DEMOBILIZATION	1	LUMP SUM
2599	FABRIC - GEOTEXTILE TYPE IV	100	SQ YD
2650	MAINTAIN AND CONTROL TRAFFIC	1	LUMP SUM
2671	PORTABLE CHANGEABLE MESSAGE SIGN (3)(10)	4	EACH
2677	ASPHALT PAVE MILLING & TEXTURING	13139	TON
2696	SHOULDER RUMBLE STRIPS-SAWED	82937	LF
2714	SHOULDERING (9)	15000	LIN FT
2775	ARROW PANEL (3)(10)	2	EACH
2676	MOBILIZATION FOR MILLING & TEXTURING	1	LUMP SUM
3262	CLEAN PIPE STRUCTURE	1	EACH
5950	EROSION CONTROL BLANKET (4)	54090	SQ YD
6407	SBM ALUM SHEET SIGNS .125 IN	100	SQ FT
6412	STEEL POST MILE MARKERS	6	EACH
6417	FLEXIBLE DELINEATOR POST-W	172	EACH
6418	FLEXIBLE DELINEATOR POST-Y	7	EACH
6549	PAVE STRIPING-TEMP REM TAPE-B	7200	LIN FT
6550	PAVE STRIPING-TEMP REM TAPE-W	7200	LIN FT
6551	PAVE STRIPING-TEMP REM TAPE-Y	3600	LIN FT
6592	PAVEMENT MARKER TYPE V-B W/R	543	EACH
6593	PAVEMENT MARKER TYPE V-B Y/R	103	EACH

I-24 PAVEMENT REHABILITATION - MP 30.454 - MP 33.660 LIVINGSTON COUNTY ITEM NUMBERS: 1-2027.00 GENERAL SUMMARY			
ITEM NUMBER	ITEM	QUANTITY	UNIT
6600	REMOVE PAVEMENT MARKER TYPE V	491	EACH
10020NS	FUEL ADJUSTMENT	17963	DOLLAR
10030NS	ASPHALT ADJUSTMENT	31654	DOLLAR
20071EC	JOINT ADHESIVE (12)	67720	LIN FT
20191ED	OBJECT MARKER TYPE 3	6	EACH
20366NN	REPLACE GRATE	8	EACH
20411ED	LAW ENFORCEMENT OFFICER	2000	HOURL
20757ED	PAVEMENT REPAIR (MINOR) (13)	3108	SQ YD
20757ED	PAVEMENT REPAIR (MAJOR) (13)	168	SQ YD
23143ED	KPDES PERMIT AND TEMPORARY EROSION CONTROL	1	LUMP SUM
21533EN	EMBANKMENT (7)	34	CU YD
24189ER	DURABLE WATERBORNE MARKING-6 IN W	47737	LIN FT
24190ER	DURABLE WATERBORNE MARKING-6 IN Y	38111	LIN FT
24191ER	DURABLE WATERBORNE MARKING-12 IN W	1194	LIN FT
	SPECIAL CONCRETE HEADER CURB	7376	LIN FT

- (1) Ditching is intended for repair to the eroded and/or poorly draining areas throughout the project as directed by the engineer. Any embankment required is incidental to ditching.
- (2) Any excavation and Fabric-Geotextile Type I required to place the Channel Lining Class III is incidental to the lining.
- (3) The quantity for these items includes initial placement. Any relocation required will not be paid for directly, but will be considered incidental to maintain and control traffic.
- (4) To be used as directed by the Engineer
- (5) The drainage summary includes flowable fill to repair voids near drainage structures. The remaining quantity is to repair locations beneath the shoulder or other structures.
- (6) Removal of existing structure or pipe is incidental.
- (7) Contrary to the Standard Specifications, payment will be based on measured quantity **NOT** plan quantity.
- (8) A quantity of Guardrail End Treatment Type 1 is included for bidding purposes.
- (9) Shouldering quantity for shaping the DGA shoulder along guardrail replacement areas and other project areas as directed by the Engineer.
- (10) Flashing Arrows and Portable Changeable Message Signs to become the property of the Department.
- (11) DGA Base for shoulder wedge throughout the project as directed by the Engineer.
- (12) Longitudinal Pavement Joint Adhesive for Centerline and Outside shoulder joints.
- (13) Includes an additional 10% for continuing pavement deterioration.

NOTE: Quantities from all summaries have been carried over and included in this General Summary

I-24 LIVINGSTON COUNTY PAVEMENT REHABILITATION, MILE POST 30.454 TO 33.660 ITEM NUMBER 1-2027.00			
PAVING AREAS MAINLINE		PAVING AREAS KY 453 RAMPS	
ITEM	TOTAL	ITEM	TOTAL
INSIDE SHOULDER AND DRIVING LANES 1.5" CL4 ASPH SURF 0.50A PG76-22	S.Y. 104322	KY 453 - RAMP A (EB EXIT) 1.5" CL4 ASPH SURF 0.50A PG76-22 ASPHALT PAVE MILLING & TEXTURING	S.Y. 3567 3567
OUTSIDE SHOULDER 1.5" CL3 ASPH SURF 0.50D PG64-22	S.Y. 34434	KY 453 - RAMP B (WB ENT.) 1.5" CL4 ASPH SURF 0.50A PG76-22 ASPHALT PAVE MILLING & TEXTURING	S.Y. 6619 6619
ASPHALT PAVE MILLING & TEXTURING PROJECT LIMITS (1.5")	S.Y. 138756	KY 453 - RAMP C (WB EXIT) 1.5" CL4 ASPH SURF 0.50A PG76-22 ASPHALT PAVE MILLING & TEXTURING	S.Y. 3812 3812
ASPHALT SEAL ASPHALT SEAL COAT (2 APPLICATIONS) ASPHALT SEAL AGGREGATE (2 APPLICATIONS)	S.Y. 35564 35564	KY 453 - RAMP D (EB ENT.) 1.5" CL4 ASPH SURF 0.50A PG76-22 ASPHALT PAVE MILLING & TEXTURING	S.Y. 6507 6507
		ASPHALT SEAL ASPHALT SEAL COAT (2 APPLICATIONS) ASPHALT SEAL AGGREGATE (2 APPLICATIONS)	S.Y. 8819 8819

PAVING SUMMARY

ALL QUANTITIES HAVE BEEN CARRIED OVER AND INCLUDED IN THE GENERAL SUMMARY

I-24					
LIVINGSTON COUNTY					
PAVEMENT REHABILITATION, MILEPOST 30.454 TO 33.660					
ITEM NO. 1-2027.00					
PAVEMENT REPAIR LOCATIONS					
BEGIN MILEPOST	END MILEPOST	WIDTH OF REPLACEMENT (FT)	LENGTH OF REPLACEMENT (FT)	AREA OF REPLACEMENT (SQ YD)	DESCRIPTION
EASTBOUND LANES					
30.47	30.54	24	356	948	MINOR REPAIR BOTH LANES
30.60	30.62	12	114	152	MINOR REPAIR OUTSIDE LANE
31.13	31.14	12	82	109	MINOR REPAIR OUTSIDE LANE
31.18	31.20	12	89	118	MINOR REPAIR OUTSIDE LANE
31.31	31.31	12	6	8	MINOR REPAIR OUTSIDE LANE
31.52	31.54	12	113	151	MINOR REPAIR OUTSIDE LANE
31.54	31.55	12	6	8	MINOR REPAIR INSIDE LANE
31.90	31.91	12	18	25	MINOR REPAIR OUTSIDE LANE
33.11	33.12	12	21	27	MINOR REPAIR OUTSIDE LANE
33.20	33.21	12	35	46	MINOR REPAIR OUTSIDE LANE
33.22	33.23	12	25	33	MINOR REPAIR OUTSIDE LANE
33.52	33.54	24	130	346	MINOR REPAIR OUTSIDE LANE
WESTBOUND LANES					
33.61	33.52	12	487	649	MINOR REPAIR OUTSIDE LANE
33.52	33.51	12	55	73	MAJOR REPAIR OUTSIDE LANE
33.49	33.49	12	6	8	MAJOR REPAIR OUTSIDE LANE
33.48	33.48	12	6	8	MAJOR REPAIR OUTSIDE LANE
33.45	33.45	12	6	8	MAJOR REPAIR OUTSIDE LANE
32.60	32.59	12	30	40	MAJOR REPAIR OUTSIDE LANE
32.16	32.15	12	11	15	MINOR REPAIR OUTSIDE LANE
32.16	32.15	12	11	15	MAJOR REPAIR OUTSIDE LANE
32.13	32.12	12	23	31	MINOR REPAIR OUTSIDE LANE
32.08	32.06	12	93	124	MINOR REPAIR OUTSIDE LANE
31.99	31.99	12	20	27	MINOR REPAIR OUTSIDE LANE
31.21	31.20	12	6	8	MINOR REPAIR INSIDE LANE
TOTAL MINOR REPAIR (SQ. YD.)					2825
TOTAL MAJOR REPAIR (SQ. YD.)					152

The Milepost references listed above are given as distances measured from Milepost 32

I-24

Item Number: 1-2027.00

PROJECT TOTAL	12,825.0	625.0	13,712.5	1	9
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* Milepost distances have been measured from Milepost 32

Location	MILE POST	Type	Pipe Size	Reset Grate*	Replace Grate	Channel Lining CL III	Channel Lining CL II	Clean Inlet/ Outlet*	Clean Pipe*	Clean Pipe Structure	DROP BOX INLET TYPE 1	DROP BOX INLET TYPE 5B	24" CULVERT PIPE	EMBANKMENT	FLOWABLE FILL
Units				Each	Ton	Ton	Each	Lin. Ft.	Each	EACH	LF	CY	CY		
Item Number				20366NN	2484	2483	Incidental	3262	1490	1505	464	21533EN	2220		
RT I-24 A	30.67	DBI	18"				1						1	1.0	
I-24 A (Median)	30.67	DBI	18" / 18" / 24"				4						1		
I-24 A (Median)	30.70	DBI	18" / 18"				4						1		
I-24 MEDIAN	30.79	DBI	18"				1								
LT I-24 WB	30.90	DBI	15" / 24"				1						1		
LT I-24 WB	31.08	HEADWALL	48"				1								
LT I-24 WB	31.22	HEADWALL	42"				1								
LT I-24 WB	31.38	HEADWALL	36"				1								
I-24 MEDIAN	31.40	DBI	36" / 36"		1		1						2		
LT I-24 WB	31.53	HEADWALL	48"				1								
LT I-24 WB	31.67	HEADWALL	30"				1								
I-24 MEDIAN	31.67	DBI	30" / 30"				1						3		
LT I-24 WB	32.02	HEADWALL	15"	1			1								
I-24 MEDIAN	32.02	DBI	15"				1						2		
LT I-24 WB	32.10	HEADWALL	15"				1								
I-24 MEDIAN	32.19	DBI	15"		1		1						2		
LT I-24 WB	32.34	HEADWALL	24"		1		1								
LT I-24 WB	32.42	HEADWALL	24"				1								
I-24 MEDIAN	32.42	DBI	24" / 24"				1						1		
I-24 MEDIAN	32.56	DBI	24" / 24"				2						1		
LT I-24 WB	32.57	HEADWALL	24"	2		2	1								
I-24 MEDIAN	32.69	HEADWALL	24"				2						1		
RT I-24 WB	32.69	DBI	24"				1								
I-24 MEDIAN	32.93	CBI					1								
LT I-24 WB	32.99	HEADWALL	48"				1								
RT I-24 EB	30.82	DBI	18" / 24"				1						1	1.0	
I-24 MEDIAN	30.92	DBI	24" / 24"				1						1		
RT I-24 EB	31.23	HEADWALL	42"				1								
LT I-24 EB	31.41	DBI	36" / 36"		1		2						1		
RT I-24 EB	31.43	HEADWALL	36"		1		1								
I-24 MEDIAN	31.53	DBI	24"				1						1		
RT I-24 EB	31.56	HEADWALL	48"				1								
RT I-24 EB	31.68	HEADWALL	36"		1	5	1								
RT I-24 EB	31.88	HEADWALL	24"				1								
RT I-24 EB	32.34	HEADWALL				1	1								
RT I-24 EB	32.43	HEADWALL	24"				1								
RT I-24 EB	32.56	HEADWALL	24"				1								
RT I-24 EB	32.92	HEADWALL	15"				1								
RT I-24 EB	32.96	EROSION				5									
RT I-24 EB	32.97	HEADWALL	48"				1		1						
I-24 B (Median)	33.10	DBI	15"				1						1		
LT I-24 B	33.10	HEADWALL	15"				1								
RT I-24 B	33.19	HEADWALL	36"				1	1							
LT I-24 B	33.21	HEADWALL	36"				1								
I-24 B (Median)	33.24	DBI	24"				1						1		
LT I-24 B	33.24	HEADWALL	30"				1								
RT I-24 B	33.28	HEADWALL	36"				1	1							
LT I-24 B	33.29	HEADWALL	36"				1								
I-24 B (Median)	33.40	DBI	15"				1								
LT I-24 B	33.41	HEADWALL	15"				1								
RT I-24 B	33.43	HEADWALL	48"				1								
LT I-24 B	33.43	HEADWALL	48"				1								
LT I-24 B	33.52	FLUME				115	1								
RT I-24 B	33.52	FLUME					1								
I-24 B (Median)	33.55	DBI	15"				1								
LT I-24 B	33.59	FLUME				225	1								
RT Ramp A	129+86	DBI	18"				1						1		
RT Ramp A	136+20	DITCH				305									
RT Ramp B	216+80	HEADWALL	18"				1								
RT Ramp B	222+49	HEADWALL	24"			15	1								
RT Ramp B	228+63	HEADWALL	18"				1								
RT Ramp B	232+62	HEADWALL	18"				1								
LT Ramp B	219+00	DITCH				75									
LT Ramp B	221+56	DBI	24"							1	8	4			
RT Ramp C	350+56	HEADWALL	18"				1								
RT Ramp C	362+93	DITCH				600									
RT Ramp D	443+59	DITCH				250									
LT Ramp D	448+01	DBI	24" / 24"							1	16	6			
RT Ramp D	448+49	HEADWALL		1			1								
LT Ramp D	452+55	DBI	24" / 24"				1						1	1.0	
RT Ramp D	453+20	HEADWALL	24"		1		1								
RT Ramp D	460+91	HEADWALL	48"			35									
LT KY 453	244+58	CBI				7	1								
LT KY 453	244+73	EROSION													
LT KY 453	247+86	HEADWALL					1								
RT KY 453	248+22	CBI					1								
RT KY 453	250+70	EROSION				6									
RT KY 453	252+83	EROSION				20									
RT KY 453	253+06	HEADWALL	15"				1								
TOTAL				3	8	388	1,278	77	2	1	1	1	24	34	3

* Quantities for "Reset Grate", "Clean Inlet/Outlet" and "Clean Pipe" quantities are shown for information purposes only and are considered incidental to the bid item "Ditching".

**I-24 LIVINGSTON COUNTY
MP 30.454 TO MP 33.660
FD04 SPP 070 0024 030-034
Item No. 1-2027.00**

**THIS PROJECT IS A FULLY
CONTROLLED ACCESS HIGHWAY**

I. DESCRIPTION

Perform all work in accordance with the Department's 2012 Standard Specifications, Supplemental Specifications, any applicable Special Provisions, and applicable Standard and Sepia Drawings, except as hereafter specified. Article references are to the Standard Specifications. Furnish all materials, labor, equipment, and incidentals for the following work:

(1) Maintain and Control Traffic; (2) Remove and replace Guardrail and Guardrail End treatments at the locations listed and/or as directed by the Engineer; (3) Type V pavement markers; (4) Asphalt Pavement Milling and Texturing; (5) Asphalt Surface and Asphalt Base at locations listed and/or as directed by the Engineer; and (6) All other work specified as part of this contract.

II. MATERIALS

Except as specified in these notes or on the drawings, all materials will be according to the Standard Specifications and applicable Special Provisions and Special Notes. The Department will sample and test all materials according to Department's Sampling Manual and the Contractor will have the materials available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing, unless otherwise specified in these notes.

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Dense Graded Aggregate.** Crushed Stone Base may not be furnished in lieu of DGA.
- C. **Pavement Markings -6 inch Paint.** Use Durable Waterborne Marking 6-inch for permanent striping (12 inch at entrance and exit ramp tapers).
- D. **Channel Lining Class II & III.** Channel lining will be limestone and is to be placed at pipe outlets with significant erosion and in ditch repair locations as directed by the Engineer.
- E. **Erosion Control Blanket.** Erosion control blanket is to be placed in all ditching areas when ditching is complete, on slope stabilization areas, or as directed by the Engineer. Use Seed Mixture No. 1

III. CONSTRUCTION METHODS

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Site Preparation.** Be responsible for all site preparation. Do not disturb existing signs. This item will include, but is not limited to, incidental excavation and backfilling; removal of all obstructions or any other items; disposal of materials; sweeping and removal of debris; shoulder preparation and restoration, temporary and permanent erosion and pollution control; and all incidentals. Site preparation will be only as approved or directed by the Engineer. Other than the bid items listed, no direct payment will be made for site preparation, but will be incidental to the other items of work.
- C. **Disposal of Waste.** Dispose of all cuttings, debris, and other waste off the right-of-way at approved sites obtained by the Contractor at no additional cost to the Department. The contractor will be responsible for obtaining any necessary permits for this work. Temporary openings in the right of way fence for direct access to waste sites off the right of way or for access to other public roads will not be allowed. No separate payment will be made for the disposal of waste and debris from the project or obtaining the necessary permits, but will be incidental to the other items of the work.
- D. **Final Dressing, Clean Up, and Seeding and Protection.** After all work is completed, completely remove all debris from the job site. Perform Class A Final Dressing on all disturbed areas. Sow disturbed earthen areas with Seed Mixture No. I. These items are incidental to other items in the contract.
- E. **Guardrail.** Remove and replace guardrail and guardrail End Treatments listed in the Guardrail Summary or as directed by the Engineer. Guardrail, End Treatments and Terminal Sections are listed by mile points and/or stations and quantities are approximate only. Actual locations will be determined by the Engineer at the time of construction. Grade and reshape shoulders to proper template for new End Treatment. Utilize DGA for embankment when required for new end treatments. Remove any existing guardrail with a lane closure in place.

Do not leave the area unprotected. After the guardrail is removed, a shoulder closure shall remain in place until the guardrail is replaced in that area. To minimize safety hazards, guardrail removal is to be performed at the latest practical time prior to initiating the paving operation in an area and re-installation is to begin within 5 calendar days from the time that the final surface course is completed and shall be pursued until completion. If guardrail installation is not started within 5 calendar days after paving operations ends, Liquidated Damages will be charged as outlined in Section 108 of the 2012 Standard Specifications.

The Contractor shall deliver existing salvaged guardrail system materials to the Guardrail and Sign Center on Wilkinson Blvd in Frankfort, KY and shall be neatly stacked in accordance with section 719.03.07 of the Standard Specifications. Contact Section Supervisor at (502) 564-8187 to schedule the delivery of material. Deliver the material between the hours of 8:00AM and 3:00PM, Monday through Friday. There is a guardrail delivery verification sheet which must be completed. The Contractor, engineer, and Central Sign/Guardrail Center representative must all sign off on this sheet before payment may be made.

F. **Pavement Striping and Pavement Markers.** Permanent striping will be in accordance with Section 112, except that:

- (1). Striping will be 6" in width, except 12" in gore area;
- (2). Permanent striping will be in place before a lane is opened to traffic; and
- (3). Permanent striping will be 6" Durable Waterborne Marking.

G. **On-Site Inspection.** Each Contractor submitting a bid for this work will make a thorough inspection of the site prior to submitting a bid and will thoroughly familiarize himself with existing conditions so that the work can be expeditiously performed after a contract is awarded. Submission of a bid will be considered evidence of this inspection having been made. Any claims resulting from site conditions will not be honored by the Department.

H. **Caution:** Information shown on the drawings and in this proposal and the types and quantities of work listed are not to be taken as an accurate or complete evaluation of the material and conditions to be encountered during construction. The bidder must draw his own conclusions as to the conditions encountered. The Department does not give any guarantee as to the accuracy of the data and no claim will be considered for additional compensation if the conditions encountered are not in accordance with the information above.

I. **Utility Clearance.** It is not anticipated that utility facilities will need to be relocated and/or adjusted; however, in the event that it is discovered that the work does require that utilities be relocated and/or adjusted, the utility companies will work concurrently with the Contractor while relocating their facilities.

IV. METHOD OF MEASUREMENT

A. **Maintain and Control Traffic.** See Traffic Control Plan.

B. **Site Preparation.** Other than the bid items listed, site preparation will not be measured for payment, but will be incidental to the other items of work.

C. **Dense Graded Aggregate.** DGA will be used for shoulder slope washouts.

D. **Raised Pavement Markers and Permanent Striping.** Permanent striping Durable Waterborne Marking (6" and 12") is measured per linear foot. See Traffic Control Plan. Type V Pavement Markers are measured as each.

E. **Erosion Control.** Erosion control items not listed as bid items will not be measured for payment, but will be considered incidental to the "lump sum" price for the bid item "KPDES Permit and Temporary Erosion Control".

F. **Erosion Control Blanket.** Erosion Control Blanket is measured by square yard and is to be used in ditching areas and slope stabilization areas as directed by the Engineer.

- G. **Embankment.** Embankment is measured by cubic yard and is to be placed in drop box, headwall, and pipe repair/extension locations, slope stabilization areas and as directed by the Engineer. Contrary to the Standard Specifications, payment will be based on measured quantity **NOT** plan quantity.

V. BASIS OF PAYMENT

No direct payment will be made other than for the bid items listed. All other items required to complete the construction will be incidental to the bid items listed. Existing signs damaged by the Contractor will be replaced by the Contractor at his expense.

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Site Preparation.** Other than the bid items listed, no direct payment will be allowed for site preparation, but will be incidental to the other items of work.
- C. **Dense Grade Aggregate.** See Section 302 of the Standard Specifications.
- D. **Raised Pavement Markers and Permanent Striping.** See Traffic Control Plan.

**NOTES APPLICABLE TO PROJECT
PAVEMENT REHABILITATION
I-24 LIVINGSTON COUNTY
Item No. 1-2027.00**

-
1. A summary of pavement repair locations is provided. The Engineer will determine the ultimate locations that will be repaired based upon the condition of the pavement at the time the repairs are accomplished. The repair locations listed may be lengthened, shortened, or eliminated completely if the conditions are such that modification of the locations would be deemed desirable by the Department.
 2. The dimensions shown on the typical section for pavement and shoulder widths and thickness are nominal or typical dimensions. The actual dimensions to be constructed may be varied to fit existing conditions as directed or approved by the Engineer. It is not intended that existing pavement or shoulders be widened unless specified in the Proposal.
 3. The contractor is to be advised locations of low wires may exist. The following locations are approximate:

I-24 M.P. 30.93 (Sta. 3852+90 & 2853+10)
I-24 M.P. 31.45 (Sta. 3880+30 & 2879+60)
I-24 M.P. 32.11 (Sta. 3915+30 & 2915+60)
I-24 M.P. 32.19 (Sta. 3919+60 & 2915+80)
I-24 M.P. 32.90 (Sta. 3957+00 & 2957+30)

CAUTION: Other Locations may exist. These and all other utilities should be avoided on this project. If any utility is impacted, it will be the contractor's responsibility to contact the affected utility and cover any costs associated with the impact.

4. A quantity of "flowable fill" is provided to repair voids caused by erosion around drainage or other structures. These and any other areas with similar erosion issues shall be filled with "flowable fill" as directed by the Engineer. Payment for this work shall be per cubic yard of "flowable fill" and will be based on quantities measured by the field Engineer.
5. Damaged or missing signs shall be replaced as directed by the Engineer. Per Section 715 of the 2012 Edition of the Standard Specifications for Road and Bridge Construction, payment for sign replacement will be made by "square feet" of "SBM Aluminum Sheet Signs" and shall include all materials, labor and equipment necessary to complete the installation of the new signs unless otherwise noted in this proposal or directed by the Engineer. Removal of the existing signs will be considered incidental to bid items to construct new signs.
6. All "green" milepost signs shall be replaced with this project. Payment for these signs will be made by "each" for the bid item "Steel Milepost Marker".
7. Any delineator posts or roadway signs that are damaged during construction due to construction operations are to be replaced at the contractor's expense. Payment for this work will be considered incidental to the contract.

8. A quantity of "Shouldering" has been included to clear road debris from shoulder edges to allow water to sheet flow over the shoulder. Payment for this work shall be by "linear foot" of the bid item "Shouldering". Payment for this work shall include all materials, labor and equipment necessary to remove all foreign debris from the shoulders and reshape the shoulders to "normal" condition as directed by the Engineer. DGA, Emulsified Asphalt RS-2, Asphalt Seal Aggregate, and removing guardrail will be paid separately from this item of work. Any other items of work necessary to complete this item of work as directed by the Engineer will be considered incidental to "Shouldering".
9. A quantity of "Asphalt Seal Aggregate" and "Emulsified Asphalt RS-2" is provided to stabilize the areas beyond the paved shoulders. Placement shall be as directed by the Engineer.
10. Guardrail, End Treatments, and Terminal Sections to be replaced are listed by mileposts and/or stations. Exact placement to be approved by the Engineer on construction.
11. Remove any existing guardrail with a lane closure in place. Do not leave the area unprotected. After the guardrail is removed, a shoulder closure shall remain in place until the guardrail is replaced in that area. The Contractor shall deliver existing salvaged guardrail system materials to the Central Sign Shop and Recycle Center in Frankfort, KY (502-564-8187) between the hours of 8:00AM and 3:00PM, Monday through Friday and shall be neatly stacked in accordance with section 719.03.07 of the standard specifications. Contractor, engineer, and Central Sign Shop and Recycle Center representative must all sign off on this sheet before payment may be made.
12. All existing asphalt and concrete curb is to be removed and replaced with concrete curb. Approximate locations are shown on the Guardrail Summary. Payment for removal of existing curb, regardless of the types, shall be per "linear foot" of "Remove Curb". Payment for the installation of new curb shall be per "linear foot" of either "Special Concrete Header Curb" or "Island Header Curb" as indicated in the Guardrail Summary.
13. A quantity of Channel Lining Class II and Class III has been included to be applied to eroded areas around drainage outlets and for some of the areas that are to be ditched. The actual limits of ditching and/or channel lining shall be as directed and/or approved by the Engineer. Geotextile Fabric Type I will not be measured for payment and will be considered incidental to "Channel Lining Class II" and "Channel Lining Class III".
14. Any light equipment that is damaged during construction is to be replaced at the contractor's expense.
15. The cleaning of existing pipe culvert inlets and outlets 36 inches or less in diameter are incidental to the bid item for "Ditching" in accordance with Section 209.03.01 of the 2012 Edition of the Standard Specifications for Road and Bridge Construction. Any pipe larger than 36 inches will be paid at the contract unit bid price for "Clean Pipe Structure".
16. The replacement of any existing drainage structure shall include the removal of said structures. The removal will be considered incidental to the contract.
17. The drainage summary lists locations where existing grates have been dislodged from their proper position. The contractor will be required to "re-set" the existing grates. "Resetting Grates" will be considered incidental to the bid item "Ditching". Grates that have been damaged and which need to be replaced will be paid for under the bid item "Replace Grate" and will be paid for by "each".

18. Delineators shall meet the requirements of Section 830 and 838 of the Standard Specifications.
Delineators shall be placed in accordance with Section 3F of the M.U.T.C.D.
19. The use of a Material Transfer Vehicle (MTV) will be required on this project.

**TRAFFIC CONTROL PLAN
LIVINGSTON COUNTY
I-24
FD04 SPP 070 0024 030-034
Item No. 1-2027.00**

<p>THIS PROJECT IS A FULLY CONTROLLED ACCESS HIGHWAY</p>

TRAFFIC CONTROL GENERAL

Except as provided herein, "Maintain and Control Traffic" shall be in accordance with the 2012 Standard Specifications and the Standard Drawings, current editions. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic". All lane closures used on the Project will be in compliance with the appropriate Standard Drawings. Do NOT use cones for lane closures or shoulder closures.

Contrary to Section 106.01, traffic control devices used on this project may be new, or used in like new condition at the beginning of the work and maintained in like new condition until completion of the work. Traffic control devices will conform to current MUTCD.

Reduce the speed limit in work areas to 55 miles per hour (35 miles per hour for ramps) and establish double fines for work zone speeding violations. The extent of these areas within the project limits will be restricted to the proximity of actual work areas as determined by the Engineer. Notify the Engineer a minimum of 12 hours prior to using the double fine signs. At the beginning of the work zone, the "WARNING FINE DOUBLED IN WORK ZONE" signs will be dual mounted. At the end of the work zone, the "END DOUBLE FINE" signs will be dual mounted as well. Remove or cover the signs when the highway work zone does not have workers present for more than a two-hour period of time. Payment for the signs will be at the unit bid price for signs erected. Any relocation or covering of the signs will be incidental to "Maintain and Control Traffic," lump sum.

If night work is utilized on this project, obtain approval from the Engineer for the method of lighting prior to its use.

Traffic Control Plan
Livingston County
I-24
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PROJECT PHASING & CONSTRUCTION PROCEDURES

No lane closures will be allowed during the following days and hours unless otherwise directed and/or approved by the Engineer:

Thanksgiving	6:00 A.M. November 21, 2012 to 6:00 A.M. November 25, 2012
Christmas & New Year’s	6:00 A.M. December 24, 2012 to 6:00 A.M. January 2, 2013
KEA (Spring Break)	To be determined by the Department.
Easter	6:00 A.M. March 30, 2013 to 6:00 A.M. April 1, 2013
Memorial Day	6:00 A.M. May 26, 2013 to 6:00 A.M. May 28, 2013
Independence Day	6:00 A.M. July 3, 2013 to 6:00 A.M. July 5, 2013
Labor Day	6:00 A.M. August 31, 2013 to 6:00 A.M. September 3, 2013

Traffic may be reduced to one lane in each direction all other times.

NOTE: Other projects may be occurring in the area at the same time. Coordination with area projects shall be coordinated to minimize disruption to the traveling public.

Approximate pavement repair locations are listed in the proposal. The Engineer will determine the exact location at the time of construction. Once removal of pavement at a particular repair location has begun, work continuously to complete the work and eliminate the “hole” within the time a lane closure is allowed. Place Type III Barricades immediately in front of pavement removal areas. Type III Barricades will not be measured for payment and will be considered incidental to “Maintain and Control Traffic.”

Access to all ramps at all interchanges on the project shall be maintained at all times unless otherwise directed by the Engineer. All diversions to access ramps in areas of lane closures shall be approved by the Engineer prior to implementing the particular lane closure

Note that lane shifts are required throughout the project. See the Exhibits for lane locations and widths. Stripe according to the MUTCD.

During the days and hours when a lane closure is allowed, implement the following procedures: Maintain traffic as specified in the phasing notes and typical sections. Any other work not requiring traffic lane widths to be restricted due to barrels or equipment encroaching into the driving lanes can be done during the remaining hours when two lanes of traffic must be maintained. Please refer to the “Special Note for Fixed Completion Date and Liquidated Damages” for damage rates per hour associated with failure to maintain the required number of lanes during the specified time period. Liquidated Damages, at the rate specified per hour in the “Special Note for Fixed Completion Date and Liquidated Damages”, will be assessed for each hour two lanes of traffic is not maintained.

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Livingston County
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The contractor must notify the Engineer at least fourteen (14) days prior to the beginning of each construction phase in either direction.

SHOULDER PREPARATION AND RESTORATION

The clear lane width will be 11 feet; however, make provisions for the passage of wide loads up to 16'. Use a lane closure all times when work is performed in the lane or adjacent shoulder. Shoulders used as temporary roadways will be inspected by the Engineer and if deemed necessary by the Engineer, repaired with asphalt mixture for level & wedging as directed prior to opening to traffic. Perform any maintenance of the shoulder as deemed necessary by the Engineer in order to maintain traffic. Remove failed materials and perform additional patching as directed by the Engineer during the time the shoulder is used as a travel lane. Patch and remove any foreign debris on the shoulders as directed by the Engineer. Remove existing striping by water blasting. Remove both edge lines throughout the project. Paint temporary edge lines through the lane closure.

The shoulders are to be inspected and low spots refilled to the satisfaction of the Engineer prior to placing traffic on the shoulders. Daytime shoulder closures will be permitted to repair the stabilized shoulders. Install delineators for the existing guardrail and bridges before shifting traffic onto the shoulders. All work required for shoulder preparation and restoration is incidental to Maintenance of Traffic.

Traffic Control Plan
Livingston County
I-24
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PHASE I

Close the inside lanes to traffic. Perform all pavement repairs (minor and major) for inside shoulders and driving lane.

NOTE on pavement repair operations: Once the pavement has been removed, the contractor must work continuously until the pavement has been replaced. The Engineer may restrict the number of locations being repaired at the same time. Pavement repairs must be completed 2 weeks prior to any general milling & filling pavement operations on those specific repair locations.

PHASE II

Shift traffic to the inside lanes and shoulders and close the outside lanes to traffic. Perform all pavement repairs (minor and major) for outside shoulders and driving lane.

NOTE on pavement repair operations: Once the pavement has been removed, the contractor must work continuously until the pavement has been replaced. The Engineer may restrict the number of locations being repaired at the same time. Pavement repairs must be completed 2 weeks prior to any general milling & filling pavement operations on those specific repair locations.

PHASE III

Close the inside lanes to traffic. Mill 1.5 inches of surface pavement and place 1.5 inches of surface pavement for inside shoulders and driving lane. Perform all median work during Phase III.

PHASE IV

Shift traffic to the inside lanes and close the outside lanes to traffic. Mill 1.5 inches and place 1.5 inches of surface pavement on outside shoulders and outside driving lanes. Perform all roadside work during Phase IV. All ramp work will be completed during this phase.

PHASE V – PERMANENT STRIPING

After all other work is completed, place permanent striping. Mobile operations may be utilized. In addition to newly paved areas, place permanent striping on bridge decks within the project limits.

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LANE CLOSURES

Limit the lengths of lane closures to only that needed for actual operations in accordance with the phasing specified herein, or as directed by the Engineer with a minimum of one mile between successive lane closures. Contrary to section 112, lane closures will **NOT** be measured for payment, but are considered incidental to “Maintain and Control Traffic,” lump sum.

SIGNS

Additional traffic control signs in addition to normal lane closure signing detailed on the Standard Drawings may be required by the Engineer. Additional signs needed for lane closures may include, but are not limited to, dual mounted TRUCKS USE LEFT/RIGHT LANE, LEFT/RIGHT LANE CLOSED 1 MILE, LEFT/RIGHT LANE CLOSED 2 MILES, LEFT/RIGHT LANE CLOSED 3 MILES, SLOWED/STOPPED TRAFFIC AHEAD. Signage for reduced speed limits and double fine work zones will be furnished, relocated, and maintained by the Contractor.

Contrary to section 112, Individual signs will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged signs or signs directed to be replaced by the Engineer due to poor legibility or reflectivity will not be measured for payment.

A quantity of signs has been included for lane shifts, “Roadwork Ahead” signs on entrance ramps, and extra double fine signs and speed limit signs between interchanges to be paid only once no matter how many times they are moved or relocated.

FLASHING ARROWS

Provide flashing arrow panels in advance of or on the project at locations to be determined by the Engineer. The arrow panels shall be in operation at all times. In the event of damage or mechanical failure, immediately repair or replace the arrow panels. The Department will measure for payment the maximum number of arrow panels in concurrent use at the same time on a single day on all sections of the contract. Individual arrow panels will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged arrow panels directed by the Engineer to be replaced due to poor condition will not be measured for payment. Arrow panels will become the property of the Department after construction is complete. The arrow panels must be new or like new condition and shall be delivered to the Livingston County Maintenance Facility.

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PORTABLE CHANGEABLE MESSAGE SIGNS

Provide portable changeable message signs (PCMS) in advance of or on the project at locations to be determined by the Engineer. The Engineer will designate the locations and messages to be provided. Unless directed otherwise by the Engineer, use messages and abbreviations according to the Policy for the Use and Placement of Changeable Message Signs. The PCMS shall be in operation at all times. In the event of damage or mechanical failure, immediately repair or replace the PCMS. The Department will measure for payment the maximum number of signs in concurrent use at the same time on a single day on all sections of the contract. Individual signs will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged signs directed by the Engineer to be replaced due to poor condition or readability will not be measured for payment. PCMS will become the property of the Department after construction is complete. The PCMS must be new or like new condition and shall be delivered to the Livingston County Maintenance Facility.

TRUCK MOUNTED ATTENUATORS

Furnish and install MUTCD approved truck mounted attenuators (TMA) in advance of work areas when workers are present less than 12 feet from traffic. If there is less than 500 feet between work sites, only a single TMA will be required at a location directed by the Engineer. Locate the TMAs at the individual work sites and move them as the work zone moves within the project limits. All details of the TMA installations shall be approved by the Engineer. TMA will not be measured for payment, but are incidental to "Maintain and Control Traffic," lump sum. The Department **WILL NOT** take possession of the TMAs upon completion of the work.

PAVEMENT MARKINGS

If lane closures are in place during nighttime hours, remove or cover the lenses of raised pavement markers that do not conform to the traffic control scheme in use, or as directed by the Engineer. Replace or uncover lenses before a closed lane is reopened to traffic. No direct payment will be made for removing and replacing or covering and uncovering the lenses, but will be incidental to "Maintain and Control Traffic," lump sum.

Place temporary and permanent striping in accordance with Section 112, except that:

1. Temporary and permanent striping will be 6" in width
2. If the contractor's operations or phasing requires temporary markings which must be subsequently removed from the ultimate pavement, an approved removable lane tape will be used; however removable tape will be measured and paid as Pavement Striping-Temporary Paint 6"
3. Edge lines will be required for temporary striping

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4. Existing, temporary, or permanent striping will be in place before a lane is opened to traffic
5. Place permanent striping on bridge decks and pavement within the project limits.
6. Permanent striping will be Durable Waterborne Markings

Should the Contractor change the existing striping pattern, the Contractor is to restripe the roadway back to its original configuration after a certain period of time especially if no work is anticipated for a period of time (i.e. Winter shutdown).

PAVEMENT EDGE DROP-OFFS

Pavement edge drop-offs will be protected by a lane or shoulder closure. Lane closures will be protected with plastic drums, vertical panels, or barricades as shown on the Standard Drawings.

Pavement edges that traffic is not expected to cross, except accidentally, shall be treated as follows:

Less than 2" – Protect with a lane closure.

2" to 4" – Protect with a lane closure. Place plastic drums, vertical panels, or barricades every 50 feet. Cones may not be used in place of plastic drums, panels, and barricades at any time. Construct a wedge with compacted cuttings from milling, trenching, or asphalt mixtures with a 3:1 or flatter slope, when work is not active in the drop-off area. Place Type III Barricades at the beginning of the lane closures, and place additional Type III Barricades spaced at 2,500 feet during the time the lane closure is in place.

Greater than 4" – Pavement Repair areas – In areas where pavement is to be removed, work should proceed continuously so that traffic is exposed to a drop-off for the minimum amount of time necessary to bring the pavement back up to existing grade. Barrel spacing should be 20 feet and appropriate lighting should be utilized to illuminate the area during nighttime operations.

Guardrail Installation – Guardrail installation is to take place one week after the general milling & filling paving operations are completed. All areas from which guardrail is removed shall be protected by a shoulder closure or other method approved by the Engineer until the new guardrail is installed. A maximum of seven calendar days will be allowed between the removal of a guardrail section and the installation of new guardrail at that same location.

TRAFFIC COORDINATOR

Designate an employee to be traffic coordinator. The designated Traffic Coordinator must be

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certified by the American Traffic Safety Services Association (ATSSA). The Traffic Coordinator will inspect the project maintenance of traffic once every two hours during the Contractor's operations and at any time a lane closure is in place. The Traffic Coordinator will report all incidents throughout the work zone to the Engineer on the project. The Contractor will furnish the name and telephone number where the Traffic Coordinator can be contacted at all times.

During any period when a lane closure is in place, the Traffic Coordinator will arrange for personnel to be present on the project at all times to inspect the traffic control, maintain the signing and devices, and relocate portable changeable message boards as queue lengths change. The personnel will have access on the project to a radio or telephone to be used in case of emergencies or accidents.

COORDINATION OF WORK

The Contractor is advised that other projects may be in progress within or in the near vicinity of this project. The traffic control of those projects may affect this project and the traffic control of this project may affect those projects. The Contractor will coordinate the work on this project with the work of the other contractors. In case of conflict, the Engineer will determine the relative priority to give to work phasing on the various projects.

CONTRACTOR'S AND CONTRACTOR'S EMPLOYEES' VEHICLES

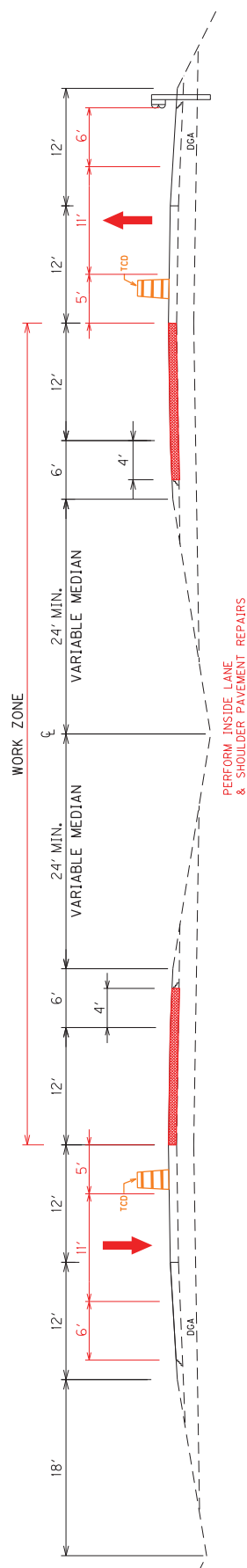
Do not use or allow employees to use median crossovers at any time except when inside lanes are closed for construction. In all other phases of construction, change vehicular direction of travel only at interchanges.

LAW ENFORCEMENT OFFICERS (LEO'S)

Police support shall be a unit consisting of an off-duty police officer from any police force agency having lawful jurisdiction and a police car equipped with externally mounted flashing blue lights. Officers may be asked to issue citations for traffic violations, but will be considered incidental to the contract unit bid price for "Law Enforcement Officer". No additional compensation will be provided. The officers will be placed at the discretion of the Engineer. Police support will be measured and paid on a per hour basis for each officer and police vehicle.

I-24 MAINTENANCE OF TRAFFIC TYPICAL SECTIONS

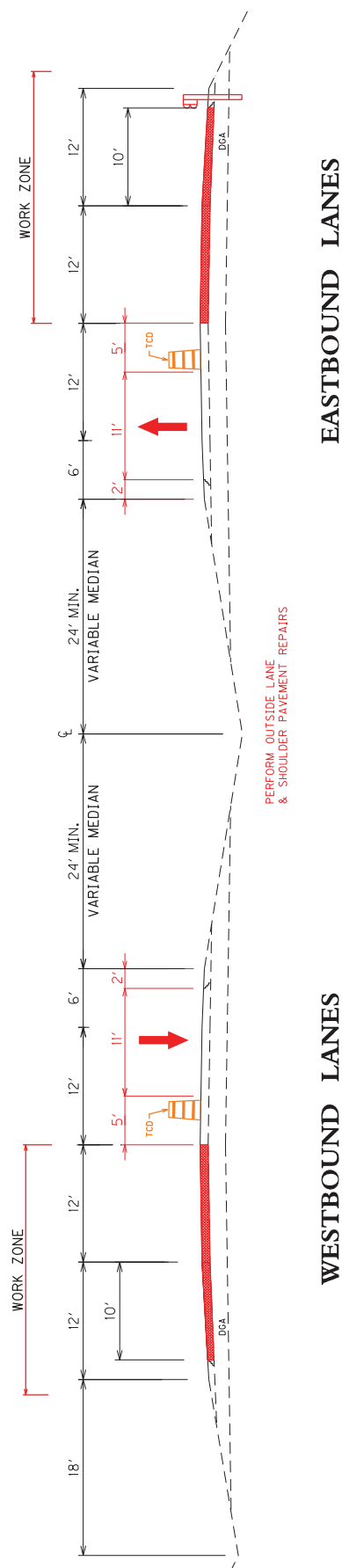
COUNTY OF	ITEM NO.
LIVINGSTON	I-2027.00



EASTBOUND LANES

PHASE I

WESTBOUND LANES



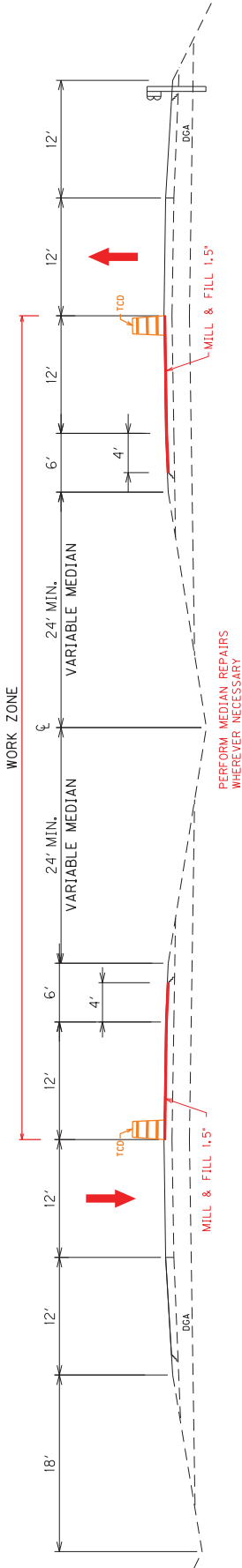
PHASE II

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MAINTENANCE OF TRAFFIC

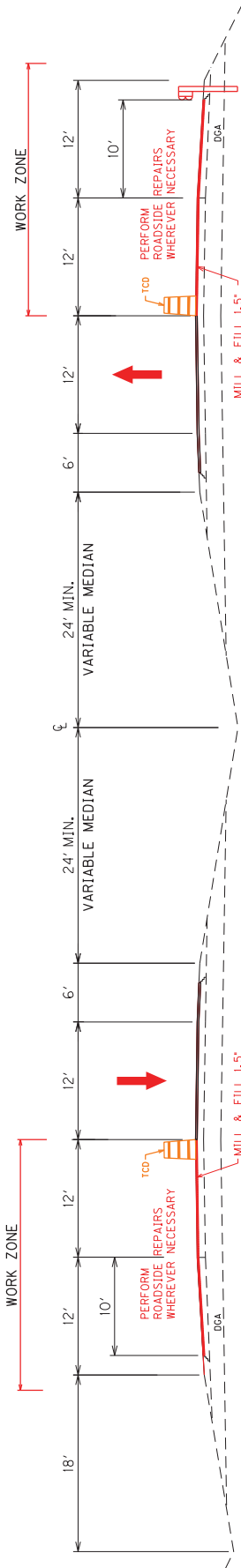
TYPICAL SECTIONS



EASTBOUND LANES

WESTBOUND LANES

PHASE III



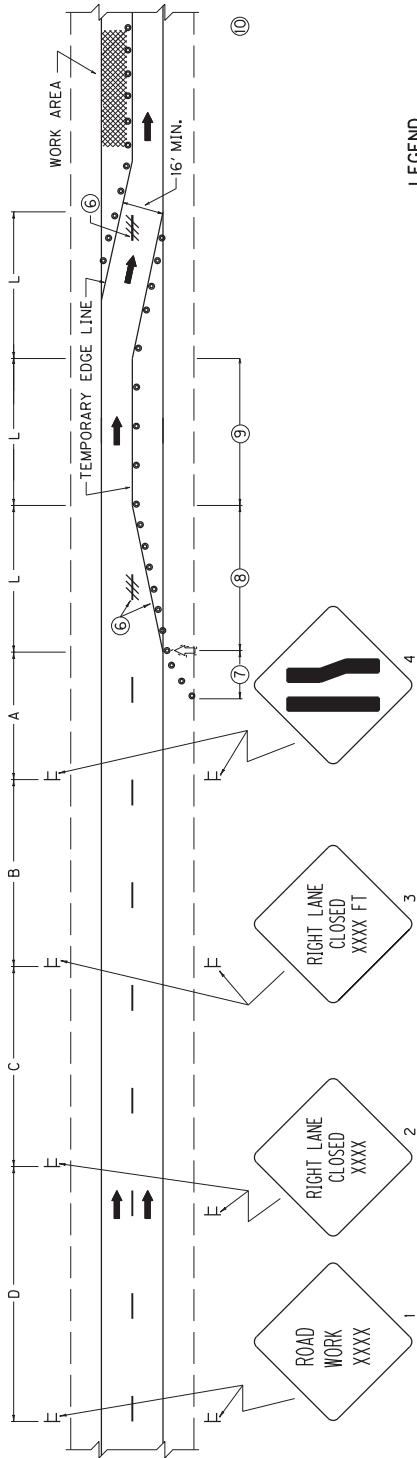
EASTBOUND LANES

WESTBOUND LANES

PHASE IV

ASPHALT SEAL
2 APPLICATIONS:
ASPHALT SEAL AGGREGATE - 20 LB/SY
ASPHALT SEAL COAT - 2.4 LB/SY

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LEGEND

- ARROW PANEL
- SIGN
- CHANNELIZING DEVICES
 - CONES
 - DRUMS
 - TYPE II BARRICADES
 - TUBULAR MARKERS

- IF MEDIAN IS NOT WIDE ENOUGH TO MAINTAIN LATERAL CLEARANCES SHOWN ON TSC-260, SIGNS 1 THRU 5 SHALL NOT BE DUAL MOUNTED. THE SIZE OF SIGNS 1 THRU 4 SHALL BE 48" X 48" IF THE NORMAL POSTED SPEED LIMIT IS 45 MPH OR GREATER. OTHERWISE, 36" X 36" SIGNS MAY BE USED.
- AN ARROW PANEL SHALL BE REQUIRED WHEN THE NORMAL POSTED SPEED LIMIT IS 45 MPH OR GREATER. IT SHALL BE LOCATED ON THE SHOULDER AT THE BEGINNING OF THE MERGING TAPER OR LOCATED IN THE CLOSED LANE WHEN THE SHOULDER IS NARROW.
- ALL VEHICLES, EQUIPMENT, WORKERS, AND THEIR ACTIVITIES SHALL BE RESTRICTED TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
- IF DURATION OF LANE CLOSURE EXCEEDS THREE DAYS, TEMPORARY EDGE LINE SHALL BE REQUIRED AND SKIP LINES SHALL BE OBLITERATED BY APPROVED METHOD THRU LENGTH OF TAPER. OTHER CONFLICTING PAVEMENT MARKINGS SHALL BE OBLITERATED.
- IF AN ARROW PANEL IS INSTALLED ON THE SHOULDER, A SHOULDER TAPER SHALL BE REQUIRED. TAPER LENGTH SHALL BE 0.33 L. SPACING OF CHANNELIZING DEVICES THRU THE SHOULDER TAPER SHALL NOT EXCEED A DISTANCE EQUAL TO THE NORMAL POSTED SPEED LIMIT.
- DRUMS OR TYPE II BARRICADES SHALL BE USED TO CHANNELIZE THE MERGING TAPER IF DURATION OF CLOSURE EXCEEDS 12 HOURS. OTHERWISE, CONES OR TUBULAR MARKERS MAY BE USED. SPACING OF CHANNELIZING DEVICES THRU THE MERGING TAPER SHALL NOT EXCEED A DISTANCE EQUAL TO THE NORMAL POSTED SPEED LIMIT.
- DRUMS OR TYPE II BARRICADES SHALL BE USED TO CHANNELIZE THE BUFFER SPACE AND WORK AREA IF DURATION OF CLOSURE EXCEEDS THREE DAYS. OTHERWISE, CONES OR TUBULAR MARKERS MAY BE USED. SPACING OF CHANNELIZING DEVICES THRU THE BUFFER SPACE AND WORK AREA SHALL NOT EXCEED A DISTANCE EQUAL TO TWO TIMES THE NORMAL POSTED SPEED LIMIT.
- SEE STANDARD DRAWING TTC-115 FOR DOWNSTREAM TAPER LENGTH AND "END ROAD WORK" SIGN LOCATIONS.

USE WITH CURRENT STD. DWGS.
TTD-100, TTD-105, TTD-110,
TTD-115, AND TTC-160

DRAWING NOT TO SCALE

SIGNING AND SPACING TABLE					
ROAD TYPE	A	B	C	D	L
EXPRESSWAY/ FREEWAY	1000'	500'	1100'	2600'	900'
RURAL/URBAN SP. L.T. \geq 45 MPH*	500'	500'	500'	1100'	660'
URBAN SP. L.T. \leq 40 MPH*	250'	250'	500'	500'	440'

*NOTE: USE NORMAL POSTED SPEED LIMIT

APPLICATION
THIS DRAWING APPLIES TO LANE CLOSURES OF THE LEFT LANE.
IF TEMPORARY TRAFFIC BARRIER IS USED, REFER TO CURRENT
STANDARD DRAWING TTC-120.

REFERENCES

- 1. Kentucky Transportation Cabinet, Department of Highways, Standard Specifications for Road and Bridge Construction, Edition of 2012.
- 2. FHWA Manual on Uniform Traffic Control Devices (MUTCD), latest edition.
- 3. Active Sepia List

<u>Drawing No.</u>	<u>Drawing Name</u>
001	Delineators at Narrow Shoulder Bridges
002	Delineators for Guardrail
003	Treatment of Open Sinkholes
004	Delineators for Concrete Barriers
007	Guardrail End Treatment Type 2A
008	Guardrail Components

- 4. Kentucky Department of Highways Standard Drawings, current editions, as applicable:

RBC-001	Guardrail Connector to Bridge End Type A and A1
RBC-002	Guardrail Connector to Bridge End Type A and A1 Components
RBC-003	Guardrail Connector to Bridge End Type A and A1 Components
RBE-205	Crash Cushion Type IX-A
RBI-001	Typical Guardrail Installations
RBI-002	Typical Guardrail Installations
RBI-003	Installation of Guardrail End Treatment Type 2A
RBI-004	Installation of Guardrail End Treatment Type 1
RBI-006	Guardrail Installation at Sign Supports
RBR-001	Steel Beam Guardrail ("W" Beam)
RBR-010	Guardrail Terminal Sections
RBR-015	Guardrail Posts
RBR-016	Guardrail Posts
RBR-020	Guardrail End Treatment Type 1
RBR-035	Guardrail End treatment Type 4A
RDB-001	Drop Box Inlet Type 1
RDB-005	Drop Box Inlet Type 5A-5B-5C-5D-5E & 5F
RDB-100	Sloped Box Outlet Type 1
RDB-101	Grates for Sloped Box Outlet Type 1
RDB-105	Sloped and Flared Box Inlet-Outlet
RDB-106	Grates for Sloped and Flared Box Inlet-Outlet
RDD-040	Channel Lining Class II and III
RDI-020	Pipe Bedding for Culverts, Entrance and Storm Sewer Pipe
RDI-021	Pipe Bedding for Culverts, Entrance and Storm Sewer Reinforced Concrete Pipe
RDI-026	Pipe Bedding Trench Condition Reinforced Conc. Pipe
RDX-160	Security Devices for Frames, Grates and Lids
RDX-060	Intermediate and End Anchors for Circular Pipe
RDX-210	Temporary Silt Fence
RDX-220	Silt Trap Type A
RDX-225	Silt Trap Type B

RDX-230	Silt Trap Type C
RGS-002	Superelevation for Multilane Pavement
RGX-001	Miscellaneous Standards Part I
RGX-200	One Point Proctor Family of Curves
RPM-100	Curb and Gutter, Curbs, and Valley Gutter
TPM-105	Pavement Marker Arrangements Multi-Lane Roadways
TPM-125	Pavement Marker Arrangement Exit Gore and Off-Ramp
TPM-130	Pavement Marker Arrangement On-Ramp with Tapered Acceleration Lane
TPM-135	Pavement Marker Arrangement On-Ramp with Parallel Acceleration Lane
TTC-115	Lane Closure Multi-Lane Highway Case I
TTC-120	Lane Closure Multi-Lane Highway Case II
TTC-135	Shoulder Closure
TTC-155	Temporary Pavement Marker Arrangements for Construction Zones
TTC-160	Temporary Pavement Marker Arrangements for Lane Closures
TTD-110	Post Splicing Detail
TTD-120	Work Zone Speed Limit and Double Fine Signs
TTD-125	Pavement Condition Warning Signs
TTS-110	Mobile Operation for Paint Striping Case III
TTS-115	Mobile Operation for Paint Striping Case IV

5. Kentucky Transportation Cabinet, Department of Highways, Standard Specifications for Road and Bridge Construction, Edition of 2012 - Supplemental Specifications, as applicable:

Special Note	Portable Changeable Message Signs (6/15/2012)
Special Note	Typical Section Dimensions <i>attached</i>
Special Note	Before You Dig <i>attached</i>
Special Note	Guardrail Delivery Verification Sheet <i>attached</i>
Special Note	Fixed Completion Date and Liquidated Damages <i>attached</i>
Special Note	Shoulder Preparation and Restoration <i>attached</i> (See MOT Notes)
Special Note	Erosion Prevention and Sediment Control <i>attached</i>
Special Note	Milling and Texturing <i>attached</i>
Special Note	Longitudinal Pavement Joint Adhesive <i>attached</i>
Special Note	Pavement Subsurface Drainage Outlet <i>attached</i>
General Provision 444	Asphalt Pavement Ride Quality (Category A)
General Provision 447	Compaction of Asphalt Mixtures

SPECIAL NOTE FOR TYPICAL SECTION DIMENSIONS
I-24

The dimensions shown on the typical sections for pavement and shoulder widths are nominal or typical dimensions. The actual dimensions to be constructed may be varied to fit existing conditions as directed or approved by the Engineer. It is not intended that existing pavement or shoulders be widened unless specified elsewhere in the Proposal.

SPECIAL NOTE FOR BEFORE YOU DIG

Call 1-800-752-6007 toll free a minimum of two and no more than ten business days prior to excavation for information on the location of existing under-ground utilities which subscribe to the before-u-dig (BUD) service. Coordinate excavation with all utility owners, including those who do not subscribe to bud.

Guardrail Delivery Verification Sheet
Item No. 1-2027.00

<u>Guardrail, End Treatment, Terminal Section or Post Type</u>	<u>Unit</u>	<u>Field Verified Amount</u>	<u>Delivered Amount</u>
Guardrail-Steel W Beam	LF		
Temporary Guardrail	LF		
Guardrail Terminal Section	Each		
Guardrail Terminal Section	Each		
Crash Cushion Type Ix-A	Each		
Guardrail End Treatment Type 1	Each		
Guardrail End Treatment Type 2a	Each		
Guardrail End Treatment Type 3	Each		
Guardrail End Treatment Type 4a	Each		
Guardrail End Treatment Type 7	Each		
Guardrail Connector To Bridge End	Each		
Guardrail Connector To Conc Med Barr	Each		
Guardrail Connect-Shld Bridge Pier	Each		
Steel Guardrail Post	Each		
Steel Blockouts	Each		

Removed guardrail, end treatments, terminal sections, and posts shall be delivered to the Central Sign Shop and Recycle Center in Frankfort, KY (502-564-8187) between the hours of 8:00 AM and 3:00 PM Monday through Friday and shall be neatly stacked in accordance with section 719.03.07 of the standard specifications. Contractor, engineer, and Central Sign Shop and Recycle Center representative must all sign off on this sheet before payment may be made.

	Printed Name	Signature	Date
Resident Engineer (or Representative)			
Contractor (or Representative)			
Central Sign Shop & Recycle Center Representative			

**Special Note for Fixed Completion Date and
Liquidated Damages
Livingston County
Item No. 1-2027.00**

Contrary to Section 108.09, Liquidated Damages of \$5,000 per calendar day will be assessed for each day or fraction thereof work remains uncompleted beyond the Specified Completion Date. This project has a Fixed Completion Date of June 30, 2013.

In addition to the Liquidated Damages specified above, Liquidated Damages in the following amounts will be charged when a lane closure remains in place during the prohibited period outlined in the Traffic Control Plan:

Mainline: \$1,000 for the first hour or fraction thereof
 \$2,500 for the second hour or fraction thereof
 \$5,000 for any additional hour or fraction thereof

These hourly disincentives will still be in effect after the Fixed Completion Date and will be charged in addition to the \$5,000 per calendar day if warranted. The contractor is expected to make every effort to complete the work in order to open the ramp or mainline lane closure within a specified timeframe.

Contrary to Section 108.09 of the Standard Specifications, **the disincentive fee will be charged during those periods when seasonal limitations of the Contract prohibit the Contractor from working on a controlling item or operation. This includes the months from December through March.**

All liquidated damages will be applied cumulatively.

All other applicable portions of Section 108 apply.

**Special Note For:
Erosion Prevention and Sediment Control
Livingston Co. Item No. 1-2027.00**

The Contractor shall be responsible for filing the Kentucky Pollution Discharge Elimination System (KPDES) KYR10 permit Notice of Intent (NOI) with the Kentucky Division of Water (DOW) and any KPDES local Municipal Separate Storm Sewer System (MS4) program that has jurisdiction. The NOI shall name the contractor as the Facility Operator and include the KYTC Contract ID Number (CID) for reference.

The Contractor shall perform all temporary erosion/sediment control functions including: providing a Best Management Practice (BMP) Plan, conducting required inspections, modifying the BMP plan documents as construction progresses and documenting the installation and maintenance of BMPs in conformance with the KPDES KYR10 permit effective on August 1, 2009 or a permit re-issued to replace that KYR10 permit. This work shall be conducted in conformance with the requirements of Section 213 of KYTC 2012 Department of Highways, Standard Specifications for Road and Bridge Construction.

Contrary to Section 213.03.03, paragraph 2, the Engineer shall conduct inspections as needed to verify compliance with Section 213 of KYTC 2012 Department of Highways, Standard Specifications for Road and Bridge Construction. The Engineer's inspections shall be performed a minimum of once per month and within seven days after a storm of ½ inch or greater. Copies of the Engineer's inspections shall not be provided to the contractor unless improvements to the BMP's are required. The contractor shall initiate corrective action within 24 hours of any reported deficiency and complete the work within 5 days. The Engineer shall use Form TC 63-61 A for this report. Inspections performed by the Engineer do not relieve the Contractor of any responsibility for compliance with the KPDES permit.

Contrary to Section 213.05, bid items for temporary BMPs will not be listed and will be replaced with one lump sum item for the services. Payment will be pro-rated based on the Project Schedule as submitted by the Contractor and as agreed to by the Engineer.

The contractor shall be responsible for applying "good engineering practices" as required by the KPDES permit. The contractor may use any temporary BMPs with the approval of the KYTC Engineer.

The contractor shall provide the Engineer copies of all documents required by the KPDES permit at the time they are prepared.

The contractor shall be responsible for the examination of the soils to be encountered and make his own independent determination of the temporary BMPs that will be required to accomplish effective erosion prevention and sediment control.

The Contractor shall be responsible for filing the KPDES permit Notice of Termination (NOT) with the Kentucky DOW and any local MS4 program that has jurisdiction. The NOT shall be filed after the Engineer agrees that the project is stabilized or the project has been formally accepted.

Payment: Payment will be by lump sum under the bid item "K.P.D.E.S. Permit & Temporary Erosion Control".

**Special Note For:
Milling and Texturing
Livingston Co. Item No. 1-2027.00**

Contrary to Section 408, the Department will retain possession of the material obtained from the milling operations. Deliver this material to the State Maintenance facility in Livingston County. The contractor, at his option, may elect to keep this material at an agreed cost of \$7.50 per ton. If the contractor elects this option, the Department will deduct the cost for this material from the money due on the contract.

NOTICE TO CONTRACTOR: The Department considers transfer of millings to the state maintenance facility to be part of the construction project, therefore truck operators are subject to receiving prevailing wages.

SPECIAL NOTE FOR LONGITUDINAL PAVEMENT JOINT ADHESIVE

- 1. DESCRIPTION. This specification covers the requirements and practices for applying an asphalt adhesive material to the longitudinal joint of the surface course of an asphalt pavement. Apply the adhesive to the face of longitudinal joint between driving lanes for the first lane paved. Then, place and compact the adjacent lane against the treated face to produce a strong, durable, waterproof longitudinal joint.
- 2. MATERIALS, EQUIPMENT, AND PERSONNEL.

2.1 Joint Adhesive. Provide material conforming to Subsection 2.1.1 or 2.1.2.

2.1.1 Provide an adhesive conforming to the following requirements:

Property	Specification	Test Procedure
Viscosity, 400 ° F (Pa·s)	4.0 – 10.0	ASTM D 3236
Cone Penetration, 77 ° F	60 – 100	ASTM D 5329
Flow, 140 ° F (mm)	5.0 max.	ASTM D 5329
Resilience, 77 ° F (%)	30 min.	ASTM D 5329
Ductility, 77 ° F (cm)	30.0 min.	ASTM D 113
Ductility, 39 ° F (cm)	30.0 min.	ASTM D 113
Tensile Adhesion, 77 ° F (%)	500 min.	ASTM D 5329
Softening Point, ° F	171 min.	AASHTO T 53
Asphalt Compatibility	Pass	ASTM D 5329

Ensure the temperature of the pavement joint adhesive is between 380 and 410 °F when the material is extruded in a 0.125-inch-thick band over the entire face of the longitudinal joint.

2.1.2 Provide an adhesive conforming to the following requirements:

Property	Specification	Test Procedure
Softening Point ¹ , ° F	176 min.	AASHTO T 53
Cone Penetration ² , 77 ° F	20-60	ASTM D 5329
Flow ¹ , 140 ° F (mm)	5.0 max.	ASTM D 5329
Tensile Adhesion, 77 ° F (%)	500 min.	ASTM D 5329
Asphalt Compatibility	Pass	ASTM D 5329
Resilience ² , 77 ° F (%)	30 min.	ASTM D 5329
Slump Test ¹ , 300 ° F (mm)	2.0 max.	ASTM D 2202

¹Cold sample forced into molds at 325 ° F.

²Field sample extruded into mold at application temperature.

Ensure the temperature of the pavement joint adhesive is between 300 and 350 °F when the material is extruded in a 0.20 to 0.40-inch-thick band over the entire face of the longitudinal joint.

2.2. Equipment.

2.2.1 Melter Kettle. Provide an oil-jacketed, double-boiler, melter kettle equipped with any needed agitation and recirculating systems.

2.2.2 Applicator System. Provide a pressure-feed-wand applicator system with an applicator shoe attached.

2.3 Personnel. Ensure a technical representative from the manufacturer of the pavement joint adhesive is present during the initial construction activities and available upon the request of the Engineer.

3. CONSTRUCTION.

3.1 Surface Preparation. Prior to the application of the pavement joint adhesive, ensure the face of the longitudinal joint is thoroughly dry and free from dust or any other debris that would inhibit adhesion. Clean the joint face by the use of compressed air. Ensure this preparation process occurs shortly before application to prevent the return of debris on the joint face.

3.2 Pavement Joint Adhesive Application. Ensure the ambient temperature is a minimum of 40 ° F during the application of the pavement joint adhesive. Prior to applying the adhesive, demonstrate competence in applying the adhesive according to this note to the satisfaction of the Engineer. Heat the adhesive in the melter kettle to the specified temperature range. Pump the adhesive from the melter kettle through the wand onto the vertical face of the cold joint. Apply the adhesive in a continuous band over the entire face of the longitudinal joint. Do not use excessive material in either thickness or location. Ensure the edge of the extruded adhesive material is flush with the surface of the pavement. Then, place and compact the adjacent lane against the joint face. Remove any excessive material extruded from the joint after compaction (a small line of material may remain).

3.3 Pavement Joint Adhesive Certification. Furnish the joint adhesive's certification to the Engineer stating the material conforms to all requirements herein prior to use.

3.4 Sampling and Testing. The Department will require a random sample of pavement joint adhesive from each manufacturer's lot of material. Extrude two 5 lb. samples of the heated material and forward the sample to the Division of Materials for testing. Reynolds oven bags, turkey size, placed inside small cardboard boxes or cement cylinder molds have been found suitable. Ensure the product temperature is 400°F or below at the time of sampling.

4. MEASUREMENT. The Department will measure the quantity of Pavement Joint Adhesive in linear feet. The Department will not measure for payment any extra

materials, labor, methods, equipment, or construction techniques used to satisfy the requirements of this note. The Department will not measure for payment any trial applications of Pavement Joint Adhesive, the cleaning of the joint face, or furnishing and placing the adhesive. The Department will consider all such items incidental to the Pavement Joint Adhesive.

5. **PAYMENT.** The Department will pay for the Pavement Joint Adhesive at the Contract unit bid price and apply an adjustment for each manufacturer’s lot of material based on the degree of compliance as defined in the following schedule. When a sample fails on two or more tests, the Department may add the deductions, but the total deduction will not exceed 100 percent.

Pavement Joint Adhesive Price Adjustment Schedule						
Test	Specification	100% Pay	90% Pay	80% Pay	50% Pay	0% Pay
Joint Adhesive Referenced in Subsection 2.1.1						
Viscosity, 400 ° F (Pa•s) ASTM D 3236	4.0-10.0	3.5-10.5	3.0-3.4 10.6-11.0	2.5-2.9 11.1-11.5	2.0-2.4 11.6-12.0	≤1.9 ≥ 12.1
Cone Penetration, 77 ° F ASTM D 5329	60-100	57-103	54-56 104-106	51-53 107-109	48-50 110-112	≤ 47 ≥ 113
Flow, 140 ° F (mm) ASTM D 5329	≤ 5.0	≤ 5.5	5.6-6.0	6.1-6.5	6.6-7.0	≥ 7.1
Resilience, 77 ° F (%) ASTM D 5329	≥ 30	≥ 28	26-27	24-25	22-23	≤ 21
Tensile Adhesion, 77 ° F (%) ASTM D 5329	≥ 500	≥ 490	480-489	470-479	460-469	≤ 459
Softening Point, ° F AASHTO T 53	≥ 171	≥ 169	166-168	163-165	160-162	≤ 159
Ductility, 77 ° F (cm) ASTM D 113	≥ 30.0	≥ 29.0	28.0-28.9	27.0-27.9	26.0-26.9	≤ 25.9
Ductility, 39 ° F (cm) ASTM D 113	≥ 30.0	≥ 29.0	28.0-28.9	27.0-27.9	26.0-26.9	≤ 25.9
Joint Adhesive Referenced in Subsection 2.1.2						
Flow, 140 ° F (mm) ASTM D 5329	≤ 5	5.1-5.2	5.3-5.4	5.5-5.6	5.7-5.8	≥5.9
Resilience, 77 ° F (%) ASTM D 5329	≥ 30	29	28-27	26-25	24-23	≤ 22
Softening Point, ° F AASHTO T 53	≥ 176	≥ 174	171-173	168-170	165-167	≤ 164
Cone Penetration, 77 ° F ASTM D 5329	20-60	18-62	16-17 63-64	14-15 65-66	12-13 67-68	≤ 11 ≥ 69
Tensile Adhesion, 77 ° F (%) ASTM D 5329	≥ 500	≥ 490	480-489	470-479	460-469	≤ 459
Slump Test, 300 ° F (mm) ASTM D 2202	≤ 2.0	≤ 2.5	2.6-3.0	3.1-3.5	3.6-4.0	≥ 4.1
Asphalt Compatibility, ASTM D 5329	Pass					

Code
20071EC

Pay Item
Joint Adhesive

Pay Unit
Linear Foot

June 8, 2004

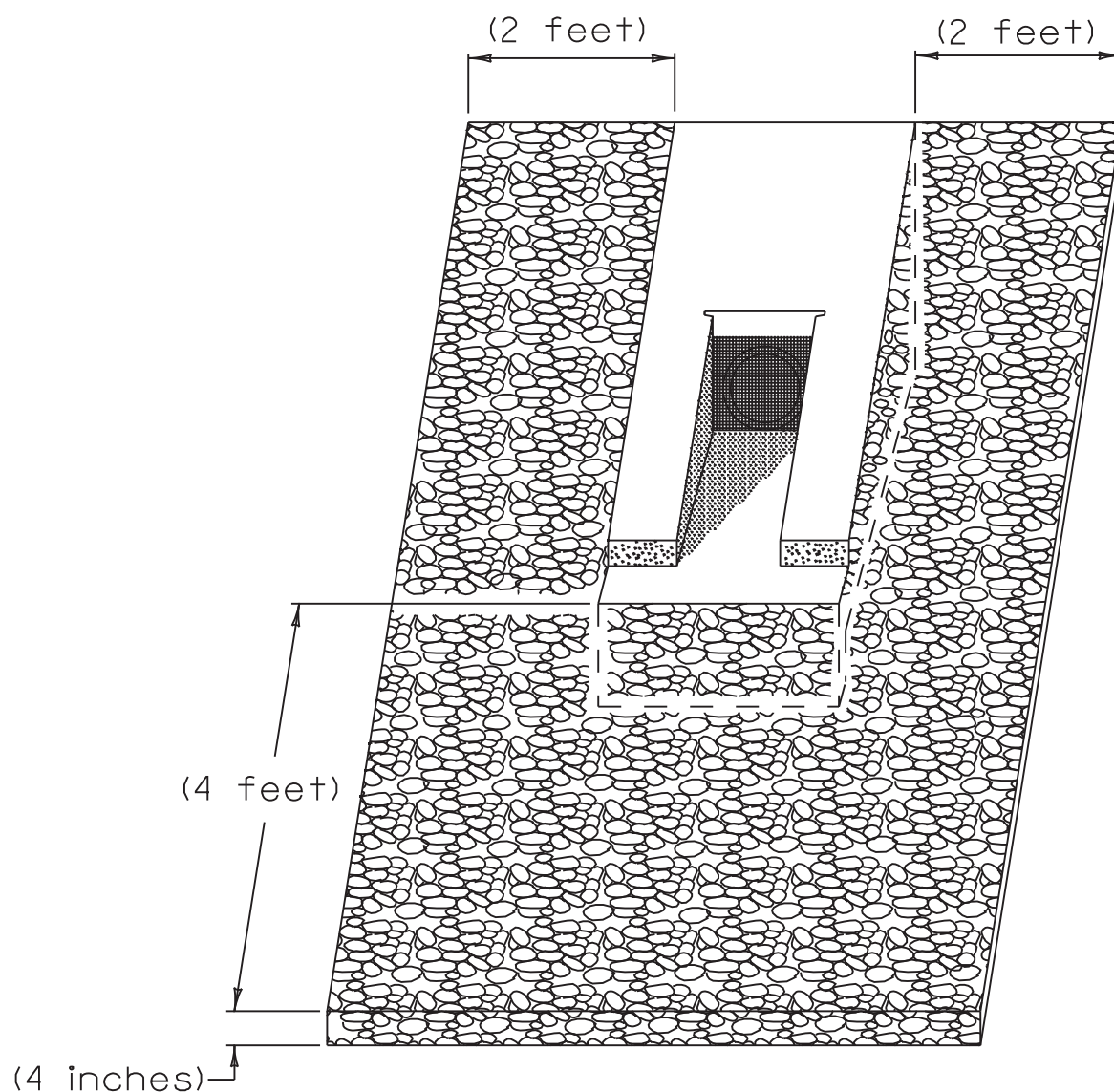
SPECIAL NOTE FOR PAVEMENT SUBSURFACE DRAINAGE OUTLET

Use approximately one ton of Crushed Aggregate Size No. 2 at all Perforated Pipe Headwall Outlets as illustrated in the detail below. Place Crushed Aggregate Size No. 2 to a minimum depth of 4" as detailed below.

Use Dense Graded Aggregate (DGA) removed during placement of the Crushed Aggregate Size No. 2 to dress existing shoulders where DGA is exposed. Waste other materials removed during placement of the Crushed Aggregate Size No. 2 as directed by the Engineer. The Department will make no direct payment for disposal of wasted material.

The Department will consider payment for Crushed Aggregate Size No. 2 as full compensation for all materials, labor, and other incidentals necessary to place Crushed Aggregate Size No. 2 for vegetation control and/or erosion control at pavement edge drain outlets.

See current Standard Drawing RDP-010 for dimensions and other details.



LIVINGSTON COUNTY, I-24

ITEM NO. 1-2027.00

**BRIDGE REHABILITATION
(1 LOCATION)**

MILE POINT 30.454 TO 33.66

INDEX

- SPECIAL NOTE FOR CONCRETE PATCHING REPAIR
- SPECIAL NOTE FOR REPLACING EXPANSION DAMS AND/OR INSTALLING ARMORED EDGES FOR CONCRETE BRIDGES
- DOVER ROAD OVER I-24 (070B00064L&R)

SPECIAL NOTE FOR CONCRETE PATCHING REPAIR

I. DESCRIPTION

Perform all work in accordance with the Kentucky Transportation Cabinet, Department of Highway's 2012 Standard Specifications for Road and Bridge Construction and applicable Supplemental Specifications, the Standard Drawings, and this Note. Section references are to the Standard Specifications.

This work consists of the following:

- (1) Furnish all labor, materials, tools, and equipment
- (2) Provide safe access to the bridge substructure, in accordance with Section 107.01.01, for the Engineer to sound possible repair areas and for workers to complete the construction
- (3) Remove the deteriorated concrete
- (4) Blast clean and prepare the surfaces for patching
- (5) Prime the areas immediately prior to patching
- (6) Apply the Vertical and Overhead Patch or Class M Concrete
- (7) Finish the patched surface
- (8) Maintain and control traffic
- (9) Any other work specified as part of this contract

II. MATERIALS

- A. Vertical and Overhead Patching Material.** Conform to Manufacturer's Technical Guidance.
- B. Class "M" Concrete.** Use either "M1" or "M2". See Section 601.

III. CONSTRUCTION

- A. Remove Deteriorated Concrete.** Prior to beginning the concrete repairs, provide safe access to the substructure, in accordance with Section 107.01.01, for the Engineer to sound possible repair areas. The Engineer will sound the concrete with a hammer and mark the areas of concrete to be removed and patched. All areas of deteriorated concrete found should be repaired as part of this work. Final payment for "Concrete Patching Repair" will be the field measured quantity of patching completed in accordance with this Note and as designated by the Engineer.

Remove specified areas of deteriorated concrete as directed by the Engineer. The removal of unsound material shall be accomplished with hand tools or pneumatic hammers that do not exceed twenty (20) pounds. Precautions shall be exercised to protect the underlying sound material. Saw, route, or otherwise manipulate the sides of the patch so that the interface between the old concrete and patch area are perpendicular. Remove all deteriorated loose concrete to a minimum depth of 2" for repairs using vertical and overhead patching material and 4" for repairs using Class M Concrete. Also ensure concrete removal in the patch area extends at least three-quarters (3/4) inch

beyond any steel reinforcement more than 50 percent exposed. Dispose of all removed material entirely away from the job site or as directed by the Engineer.

Extreme care shall be taken when removing the existing spalled or delaminated concrete so as not to damage the existing reinforcing steel. Completely clean all existing steel reinforcement encountered free of rust and leave in place. Wire brushing may be required to thoroughly clean exposed steel reinforcement. Repair or replace any damaged steel reinforcement as directed by the Engineer at no additional cost to the Department. Ensure that all exposed steel reinforcement is tied in accordance with Section 602.03.04. The Contractor must consult the Engineer before removing any concrete that is directly below the beam bearings.

B. Prepare Concrete Surfaces for Patching. Prepare concrete surfaces to be patched in accordance with Section 510.03.01. Final blast cleaning shall be completed within twelve (12) hours prior to placement of the epoxy mortar patch. Concrete must be sound, dry, and clean prior to placement of epoxy resin prime coat.

C. Apply Vertical and Overhead Patching Material or Class M Concrete. The Engineer shall have the option of designating a spalled or delaminated area to be repaired using Class M high early strength concrete or a Vertical and Overhead Patching Material. Any material used must be approved by the Engineer. Refer to the Transportation Cabinet, Division of Materials' List of Approved Materials for currently approved materials for vertical and overhead patching. Place either the class M Concrete or Vertical and Overhead Patching Material as approved by the Engineer. Place the epoxy resin primer in accordance with the standard specifications and manufacturer's recommendations. Place the Vertical and Overhead Patching Material in accordance with the manufacturer's specifications to restore the deteriorated areas to their original dimensions as directed by the Engineer. Place Class M Concrete according to the Standard Specifications.

IV. MEASUREMENT

A. Concrete Repair. The Department will measure the quantity in square feet.

V. PAYMENT

A. Concrete Repair. Payment at the contract unit price per square foot is full compensation for removal of deteriorated concrete, preparation of the concrete surface, application of the Vertical and Overhead Patching Material or Class M Concrete, application of the epoxy resin seal coat, and all incidental items necessary to complete the work in accordance with this Note.

The Department will consider payment as full compensation for all work required by this note.

SPECIAL NOTE FOR REPLACING EXPANSION DAMS AND/OR INSTALLING ARMORED EDGES FOR CONCRETE BRIDGES

I. DESCRIPTION

Perform all work in accordance with the Kentucky Transportation Cabinet, Department of Highway's 2012 Standard Specifications for Road and Bridge Construction and applicable Supplemental Specifications, the Standard Drawings, this Note, and the attached detail drawings. Section references are to the Standard Specifications.

This work consists of the following:

- (1) Furnish all labor, materials, tools, and equipment
- (2) Remove existing concrete and expansion devices and/or bridge ends
- (3) Install armored edges and new concrete as specified and in accordance with the attached detail drawings
- (4) Install new joint seals (where required)
- (5) Maintain and control traffic
- (6) Any other work specified as part of this contract.

II. MATERIALS

- A. Class "M" Concrete.** Use either "M1" or "M2". See Section 601.
- B. Structural Steel.** Use new, commercial grade steel suitable for welding. The Engineer will base acceptance on visual inspection. See Standard Drawing BJE-001, current edition, for Armored Edges. See manufacturer's specifications for Armored Edges on Strip Seal Expansion Dams.
- C. Stud Anchors.** The armored edge stud anchors are $\frac{3}{4}$ " x 6" embedded stud shear connectors conforming to ASTM A108, Grade 1015 (Nelson Studs or equal).
- D. Steel Reinforcement - Epoxy Coated.** Use Grade 60. See Section 602.
- E. Epoxy Bond Coat.** See Section 511.
- F. Preformed Expansion Joint Strip Seals.** Provide 4" Seals, See Section 807.03.03.

III. CONSTRUCTION

- A. Remove Existing Materials.** Remove the existing expansion dam/bridge end and specified areas of concrete as shown on the attached sketches. Remove debris and/or expansion joint filler as directed by the Engineer. When deteriorated concrete adjacent to the limits of removal is encountered, extend the removal area as directed by the Engineer. Dispose of all removed material entirely away from the job site. This work is incidental to the contract unit price for "Expansion Joint Replacement" or "Armored Edge for Concrete". Clean and leave all existing steel reinforcement encountered in place. Damaged steel reinforcement will be repaired/replaced as directed by the Engineer at no additional cost to the department.

- B. Place New Concrete and Armored Edges.** After all specified existing materials have been removed; place new armored edges to match the grade of the proposed overlay or to match the original grade (See attached detail drawings). Place the new Class “M” concrete to the scarified grade and finish to receive the new overlay or place the new Class “M” concrete to the original grade and finish with broom strokes drawn transversely from curb to curb (See attached detail drawings).

Blast clean all areas of existing concrete and structural steel to come in contact with new concrete until free of all laitance and deleterious substances immediately prior to the placement of the Class “M” Concrete. The surface areas of existing concrete to come in contact with the new Class “M” Concrete are to be coated with an epoxy bond coat immediately prior to placing new concrete in accordance with Section 511. The interfaces of the new and old concrete shall be as nearly vertical and horizontal as possible.

- C. Additional Steel Reinforcement.** Furnish for this work, as directed by the Engineer, steel reinforcement as shown in the attached detail drawings. Splice these bars to the existing reinforcement in the deck in the areas of removed concrete as shown in the attached detail drawings or as directed by the Engineer. Ensure that all exposed steel reinforcement is tied in accordance with Section 602.03.04 prior to pouring the new Class “M” concrete. Field cutting and bending is permitted. Do not place any additional steel reinforcement above the height of the top row of Nelson studs on the armored edges.

Reinforcement, bar splices and mechanical connectors are incidental to the contract unit price for “Expansion Joint Replacement” or “Replace Armored Edge”.

- D. Stage Construction.** Install concrete and armored edges in two (or more if specified) stages as necessary. Join the armored edges at or near the centerline of the roadway or lane line, field weld, and grind smooth.
- E. Preformed Expansion Joint Strip Seals.** Place the strip seal in one continuous, unbroken length. Place joints as recommended by the manufacturer and in accordance with Section 609.03.04 (E).
- F. Shop Plans.** Shop Plans will not be required. The contractor is responsible for obtaining field measurements and supplying properly sized materials to complete the work.

IV. MEASUREMENT

- A. **Expansion Joint Replacement – 4”.** The Department will measure the quantity in linear feet from gutterline to gutterline along the centerline of the joint.
- B. **Armored Edge for Concrete.** The Department will measure the quantity in linear feet from gutterline to gutterline along the face of the bridge end.

V. PAYMENT

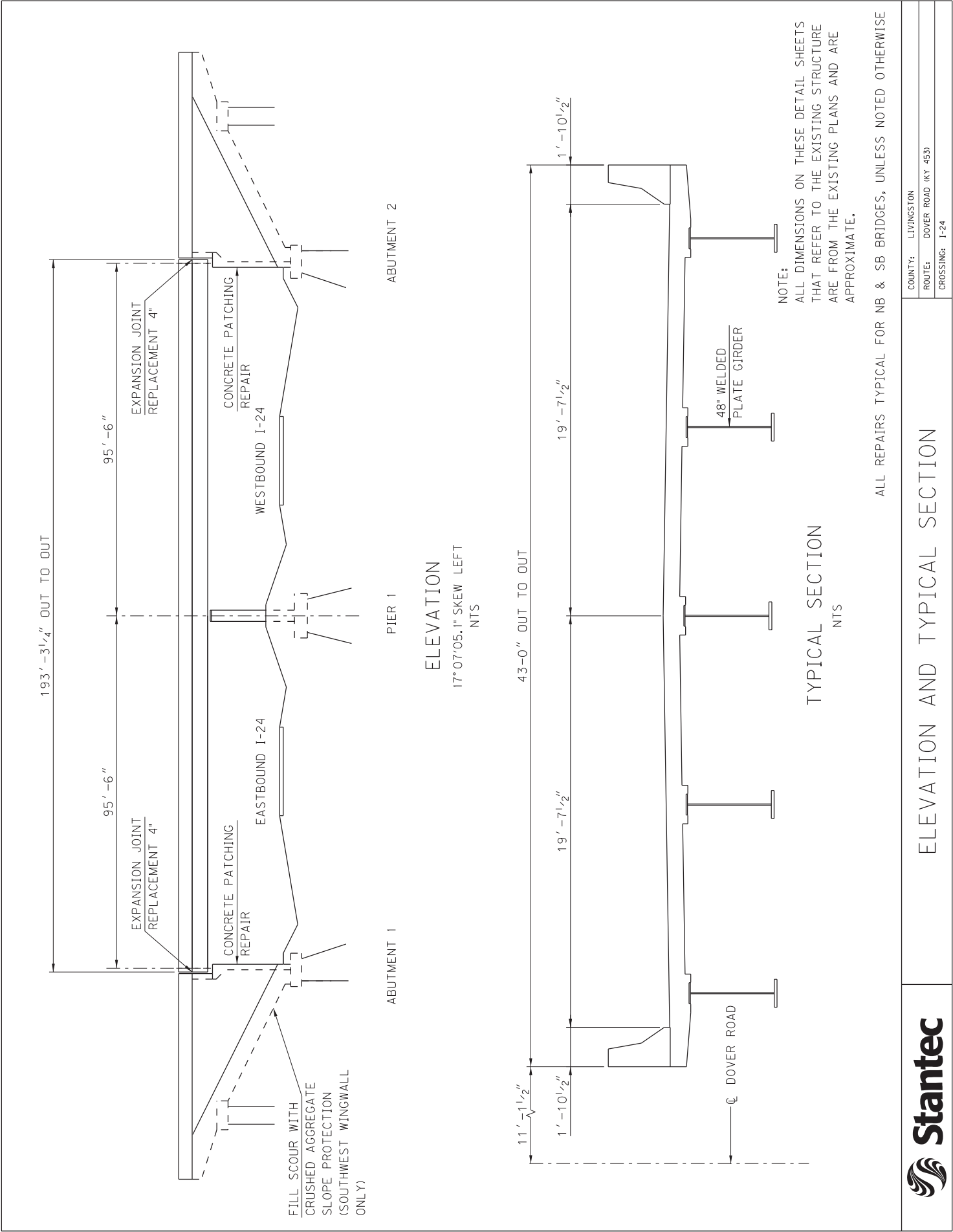
- A. **Expansion Joint Replacement – 4”.** Payment at the contract unit price per linear foot is full compensation for removing specified existing materials, furnishing and installing the new armored edges, concrete, reinforcement, preformed expansion joint strip seal, and all incidental items necessary to complete the work as specified by this note and as shown on the attached detail drawings.
- B. **Armored Edge for Concrete.** Payment at the contract unit price per linear foot is full compensation for removing specified existing materials, furnishing and installing the new armored edges, concrete, reinforcement, and all incidental items necessary to complete work as specified by this note and as shown on the attached detail drawings.

The Department will consider payment as full compensation for all work required by this note and the attached detail drawings.

DOVER ROAD(KY 453) OVER I-24 (070B00064L&R)
(MP 30.7)



<u>SUMMARY OF QUANTITIES</u>			
ITEM CODE	DESCRIPTION	QUANTITY	UNIT
3298	EXPANSION JOINT REPLACEMENT 4 IN	172	LF
8020	CRUSHED AGGREGATE SLOPE PROTECTION	20	TON
22146EN	CONCRETE PATCHING REPAIR	440	SQFT



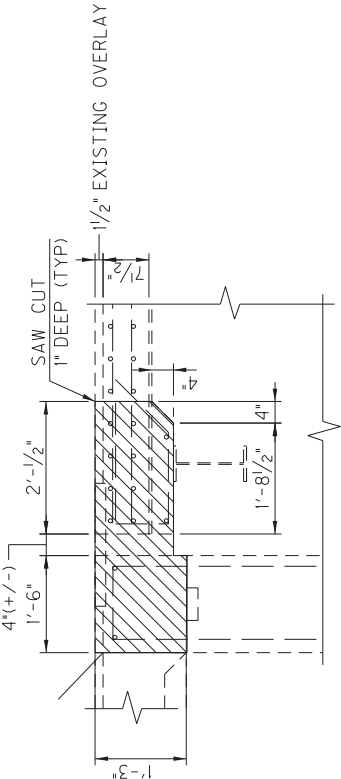
NOTES:

REMOVE HATCHED AREAS OF CONCRETE, EXPANSION DEVICE AND ARMORED EDGE. CLEAN, STRAIGHTEN & REUSE EXISTING REINFORCEMENT. THE CONTRACTOR HAS THE OPTION TO REPLACE EXISTING TRANSVERSE REINFORCEMENT.

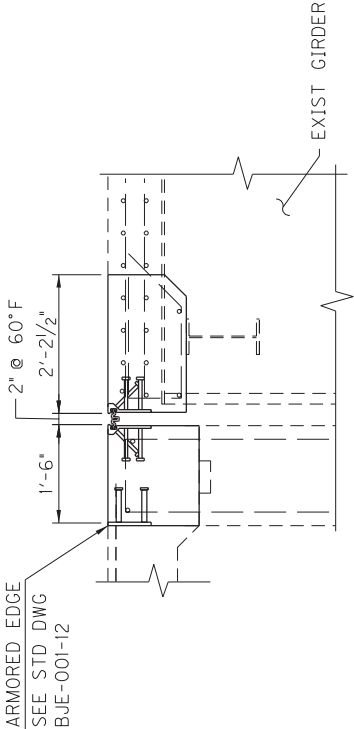
FOR TEMPERATURE CHANGE INCREMENTS, SEE STD DWG BJE-001-12 PREFORMED EXPANSION JOINT STRIP SEAL SHALL BE EITHER ONE OF THE FOLLOWING OR AN APPROVED EQUIVALENT:

D.S. BROWN COMPANY
L2-400 WITH SSPA STEELFLEX RAIL

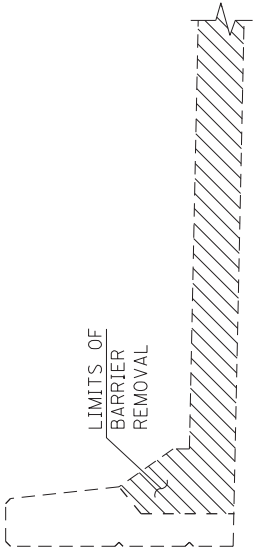
WATSON BOWMAN ACME
SE-400 WITH TYPE "P" RAIL



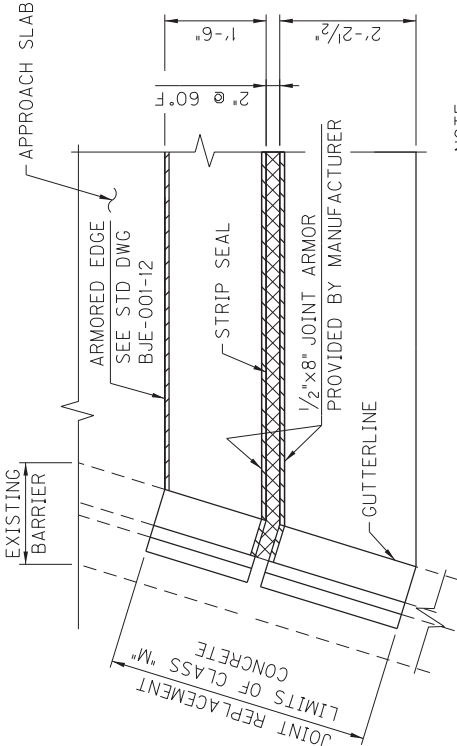
EXISTING SECTION
(SHOWING REMOVAL)



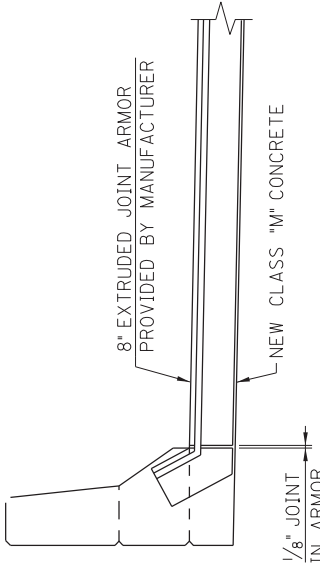
PROPOSED SECTION



SECTION AT BARRIER
(SHOWING REMOVAL)



PLAN AT ABUTMENT



SECTION AT BARRIER
(OPTIONAL)

NOTE:
STUDS ARE NOT SHOWN FOR CLARITY



EXPANSION JOINT REPLACEMENT 4 INCH

COUNTY: LIVINGSTON
ROUTE: DOVER ROAD
CROSSING: I-24



Interstate 24 Asphalt Paving

Livingston Co. I-24 Item Number 1-2027

MP 30.454 to 33.66

PUBLIC INFORMATION PLAN

The primary goal of the Public Information Plan (PIP) is to inform the motoring public and area stakeholders of project information including Maintenance of Traffic (MOT) which includes lane restrictions, reducing lane width, and/or ramp closures that may be required. The KYTC District 2 Public Information Officer (PIO) will coordinate and disseminate appropriate information regarding construction plans to stakeholders, the public, and the news media.

STAKEHOLDERS

- Legislators
 - State Representative Will Coursey
 - State Senator Bob Leeper
 - State Representative Mike Cherry
 - State Representative Brent Houseman
 - State Senator Dorsey Ridley
- State & Federal Agencies
 - KSP Mayfield Post 1- Lt. Brent White
 - KSP Madisonville Post 2-Trooper Stu Recke
 - KSP Division of Vehicle Enforcement- Lt. Kevin Rogers
 - KY Tourism Gil Lawson- Frankfort
 - KY Area 1 Emergency Management- Mark Garland
 - State Emergency Management-Buddy Rogers
 - KYTC Transportation Operations Center- Jeff Bibb
 - Kentucky Department of Vehicle Regulation Rick Taylor
 - KY Over Dimensional Permits Virgie Long
- McCracken County Agencies & Officials
 - McCracken Co. Judge-Executive Van Newberry
 - McCracken Co. Sheriff Jon Hayden
 - McCracken Co. 911 Center
 - McCracken County Schools Transportation
 - Western Baptist Hospital
 - Lourdes Hospital
 - McCracken Co. Ambulance Service
 - McCracken Co. Emergency Management- Paul Carter
 - Paducah Mayor/City Manager/City Government/Fire Department
 - Paducah-McCracken County Chamber of Commerce

- Marshall County Agencies & Officials
 - Marshall Co. Judge-Executive Mike Miller
 - Marshall Co. Sheriff Kevin Byars
 - Marshall Co. 911 Center
 - Marshall County Schools Transportation
 - Marshall Co. Hospital/Medical Center
 - Marshall Co. Ambulance Service
 - Marshall Co. Emergency Management- Melissa Combs
 - Calvert City Industrial Community- C-MAP Group
 - Calvert City Mayor/City Administrator/City Government/Fire Department
 - Marshall County Chamber of Commerce
- Livingston County Agencies & Officials
 - Livingston Co. Judge-Executive Chris Lasher
 - Livingston Co. Sheriff Bobby Davidson
 - Livingston Co. 911 Center
 - Livingston County Schools Transportation
 - Livingston Co. Hospital/Medical Center
 - Livingston Co. Ambulance Service
 - Livingston Co. Emergency Management- Brent Stringer
 - Grand Rivers Mayor/City Government
 - Grand Lakes Fire Department
 - Grand Lakes Chamber of Commerce
- Lyon County Agencies & Officials
 - Lyon Co. Judge-Executive Wade White
 - Lyon Co. Sheriff Kent Murphy
 - Lyon Co. 911 Center
 - Lyon County Schools Transportation
 - Lyon Co. Tourism
 - Lyon County Emergency Mgt Richard Martin
 - Eddyville Mayor Nancy Slaton
 - Kuttawa Mayor Lee McCollum
- Regional/State Agencies
 - Tourism KY Western Waterland- Marinel Larkin
 - Purchase Area Development District
 - Pennyrile Area Development District
 - C-LINK Regional Chamber of Commerce Group
 - West KY Regional Chamber Alliance
 - Tennessee Department of Transportation
 - Federal Highway Administration-Kentucky Office

TRUCKING FIRMS AND OUT OF STATE STAKEHOLDERS

- Trucking Trade Publications/Websites
 - E-Trucker Magazine- Max Heine, Ed.
 - The Trucker Magazine Lyndon Finney, Ed.
 - The Truckers Report
 - TruckingInfo.com- Deborah Lockridge, Ed
 - Truck Landline Magazine Clarissa Kell-Holland
 - Trucking XM Radio-Dave Nemo Show
- Area Trucking Companies/Shippers
 - UPS- Louisville-Bill Sprigg
 - Henry & Henry Trucking-Marion Billy Fox
 - PTL Truck Lines- Murray
 - Martin Tire Recycling
 - New Page Paper Mill- Wickliffe
 - Calvert City Industrial Community
 - Federal Express-Heather Montgomery
 - Information posted on the 511 website (www.511.ky.gov) and on the 511 telephone information system.

PRESENTATIONS

A project description including anticipated schedule will be provided to local and regional news media, stakeholders and other emergency service agencies via e-mail prior to construction. Information will be provided to these groups via traffic advisories, and/or press releases approximately one week prior to the start of work with additional advisories as appropriate for traffic shifts, etc.

MEDIA RELATIONS

The Public Information Officer will prepare an initial news release regarding the contract award for the project for distribution to both local and regional news media outlets. The PIO will conduct interviews with the media throughout the project duration to keep the public informed of construction progress. Traffic advisories will be submitted to the media when a change in the MOT occurs. The contractor must provide to the PIO via the Section Engineer notification of any change in the MOT at least three (3) days prior to the change.

SPECIAL PROVISION FOR WASTE AND BORROW SITES

Obtain U.S. Army Corps of Engineer's approval before utilizing a waste or borrow site that involves "Waters of the United States". The Corps of Engineers defines "Waters of the United States" as perennial or intermittent streams, ponds or wetlands. The Corps of Engineers also considers ephemeral streams, typically dry except during rainfall but having a defined drainage channel, to be jurisdictional waters. Direct questions concerning any potential impacts to "Waters of the United States" to the attention of the appropriate District Office for the Corps of Engineers for a determination prior to disturbance. Be responsible for any fees associated with obtaining approval for waste and borrow sites from the U.S. Army Corps of Engineer or other appropriate regulatory agencies.

1-296 Waste & Borrow Sites
01/02/2012

Right-of-Way Certification Form

Revised 2/22/11

☒ Federal Funded

☒ Original

☐ State Funded

☐ Re-Certification

This form must be completed and submitted to FHWA with the PS&E package for federal-aid funded Interstate, Appalachia, and Major projects. This form shall also be submitted to FHWA for all federal-aid projects that fall under Conditions No. 2 or 3 outlined elsewhere in this form. When Condition No. 2 or 3 apply, KYTC shall resubmit this ROW Certification prior to construction contract Award. For all other federal-aid projects, this form shall be completed and retained in the KYTC project file.

Date: August 30, 2012

Project Name: Asphalt Rehab on I-24

Letting Date: September 2012

Project #: MP 30.454 to MP 33.660

County: Livingston

Item #: 1-2027.00

Federal #: CID 12-1345

Description of Project: Asphalt Rehab on I-24.

Projects that require **NO** new or additional right-of-way acquisitions and/or relocations

- ☒ The proposed transportation improvement will be built within the existing rights-of-way and there are no properties to be acquired, individuals, families, and businesses ("relocatees") to be relocated, or improvements to be removed as a part of this project.

Projects that require new or additional right-of-way acquisitions and/or relocations

- ☐ Per 23 CFR 635.309, the KYTC hereby certify that all relocatees have been relocated to decent, safe, and sanitary housing or that KYTC has made available to relocatees adequate replacement housing in accordance with the provisions of the current FHWA directive(s) covering the administration of the Highway Relocation Assistance Program and that at least one of the following three conditions has been met. (Check those that apply.)

- ☐ **Condition 1.** All necessary rights-of-way, including control of access rights when applicable, have been acquired including legal and physical possession. Trial or appeal of cases may be pending in court but legal possession has been obtained. There may be some improvements remaining on the right-of-way, but all occupants have vacated the lands and improvements, and KYTC has physical possession and the rights to remove, salvage, or demolish all improvements and enter on all land. Fair market value has been paid or deposited with the court.

- ☐ **Condition 2.** Although all necessary rights-of-way have not been fully acquired, the right to occupy and to use all rights-of-way required for the proper execution of the project has been acquired. Trial or appeal of some parcels may be pending in court and on other parcels full legal possession has not been obtained, but right of entry has been obtained, the occupants of all lands and improvements have vacated, and KYTC has physical possession and right to remove, salvage, or demolish all improvements. Fair market value has been paid or deposited with the court for most parcels. Fair market value for all pending parcels will be paid or deposited with the court prior to AWARD of construction contract. (See note 1 below.)

Note 1: The KYTC shall re-submit a right-of-way certification form for this project prior to AWARD of all Federal-Aid construction contracts. Award must not to be made until after KYTC has obtained full legal possession and fair market value for all parcels has been paid or deposited with the court and FHWA has concurred in the re-submitted right-of-way certification.

Right-of-Way Certification Form

Revised 2/22/11

- ☐ **Condition 3.** The acquisition or right of occupancy and use of a few remaining parcels are not complete and/or some parcels still have occupants. However, all remaining occupants have had replacement housing made available to them in accordance with 49 CFR 24.204. The KYTC is hereby requesting authorization to advertise this project for bids and to proceed with bid letting even though the necessary rights-of-way will not be fully acquired, and/or some occupants will not be relocated, and/or the fair market value will not be paid or deposited with the court for some parcels until after bid letting. KYTC will fully meet all the requirements outlined in 23 CFR 635.309(c)(3) and 49 CFR 24.102(j) and will expedite completion of all acquisitions, relocations, and full payments after bid letting and prior to AWARD of the construction contract or force account construction. A full explanation and reason for this request, including identification of each such parcel and dates on which acquisitions, payments, and relocations will be completed, is attached to this certification form for FHWA concurrence. (See note 2.)

Note 2: The KYTC may request authorization on this basis only in unique and unusual circumstances. Proceeding to bid letting shall be the exception and never become the rule. In all cases, the KYTC shall make extraordinary efforts to expedite completion of the acquisition, payment for all affected parcels, and the relocation of all relocatees prior to AWARD of all Federal-Aid construction contracts or force account construction.

Approved:

Greg L. Morgan
Printed Name

[Signature]
Signature

8/30/12
Right-of-Way Supervisor

Approved:

DAVID L. ORR
Printed Name

[Signature]
Signature

9/4/12
KYTC, Director of ROW & Utilities

Approved:

DAVID WHITWORTH
Printed Name

[Signature]
Signature

9/4/12
FHWA, ROW Officer (when applicable)

Right-of-Way Certification Form

Revised 2/22/11

Date: August 30, 2012

Project Name: Asphalt Rehab on I-24
Project #: MP 30.454 to MP 33.660
Item #: 1-2027.00
Letting Date: September 2012

County: Livingston
Federal #: CID 12-1345

This project has 0 total number of parcels to be acquired, and 0 total number of individuals or families to be relocated, as well as 0 total number of businesses to be relocated.

- 0 Parcels where acquired by a signed fee simple deed and fair market value has been paid
- 0 Parcels have been acquired by IOJ through condemnation and fair market value has been deposited with the court
- 0 Parcels have not been acquired at this time (*explain below for each parcel*)
- 0 Parcels have been acquired or have a "right of entry" but fair market value has not been paid or has not been deposited with the court (*explain below for each parcel*)
- 0 Relocatees have not been relocated from parcels , , , , , and (*explain below for each parcel*)

Parcel #	Name/Station	Explanation for delayed acquisition, delayed relocation, or delayed payment of fair market value	Proposed date of payment or of relocation

There are 0 billboards and/or 0 cemeteries involved on this project.

There are 0 water or monitoring wells on parcels , , , , and . All have been acquired and are the responsibility of the project contractor to close/cap.

Form Effective Date: April 1, 2006
Last Revised: February 22, 2011

SPECIAL NOTES FOR UTILITY CLEARANCE
IMPACT ON CONSTRUCTION

LYON COUNTY
Mill and Thin Overlay on I-24
MP 30.454 to MP 33.660
Item No. 1-2027.00

There are no known utilities within the limits of this project.

There are no railroad facilities located on this project.

If Excavation Is Required

The contractor is instructed to call 1-800-752-6007 to reach KY 811, the one-call system for information on the location of existing underground utilities. The call is to be placed a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor should be aware that owners of underground facilities are not required to be members of the KY 811 one-call Before-U-Dig (BUD) service. The contractor must coordinate excavation with the utility owners, including those whom do not subscribe to KY 811. It may be necessary for the contractor to contact the County Court Clerk to determine what utility companies have facilities in the area.

PART II

SPECIFICATIONS AND STANDARD DRAWINGS

SPECIFICATIONS REFERENCE

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2012* and *Standard Drawings, Edition of 2012 with the 2012 Revision*.

**Supplemental Specifications to the Standard Specifications for Road and
Bridge Construction, 2012 Edition**

(Effective with the August 17, 2012 Letting)

Subsection:	402.03.02 Contractor Quality Control and Department Acceptance.
Part:	D) Testing Responsibilities.
Number:	4) Density.
Revision:	Replace the second sentence of the Option A paragraph with the following: Perform coring by the end of the following work day.
Subsection:	606.03.17 Special Requirements for Latex Concrete Overlays.
Part:	A) Existing Bridges and New Structures.
Number:	1) Prewetting and Grout-Bond Coat.
Revision:	Add the following sentence to the last paragraph: Do not apply a grout-bond coat on bridge decks prepared by hydrodemolition.
Subsection:	609.03 Construction.
Revision:	Replace Subsection 609.03.01 with the following: 609.03.01 A) Swinging the Spans. Before placing concrete slabs on steel spans or precast concrete release the temporary erection supports under the bridge and swing the span free on its supports. 609.03.01 B) Lift Loops. Cut all lift loops flush with the top of the precast beam once the beam is placed in the final location and prior to placing steel reinforcement. At locations where lift loops are cut, paint the top of the beam with galvanized or epoxy paint.

SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS

This Special Note will apply when indicated on the plans or in the proposal.

1.0 DESCRIPTION. Furnish, install, operate, and maintain variable message signs at the locations shown on the plans or designated by the Engineer. Remove and retain possession of variable message signs when they are no longer needed on the project.

2.0 MATERIALS.

2.1 General. Use LED Variable Message Signs Class I, II, or III, as appropriate, from the Department's List of Approved Materials.

Unclassified signs may be submitted for approval by the Engineer. The Engineer may require a daytime and nighttime demonstration. The Engineer will make a final decision within 30 days after all required information is received.

2.2 Sign and Controls. All signs must:

- 1) Provide 3-line messages with each line being 8 characters long and at least 18 inches tall. Each character comprises 35 pixels.
- 2) Provide at least 40 preprogrammed messages available for use at any time. Provide for quick and easy change of the displayed message; editing of the message; and additions of new messages.
- 3) Provide a controller consisting of:
 - a) Keyboard or keypad.
 - b) Readout that mimics the actual sign display. (When LCD or LCD type readout is used, include backlighting and heating or otherwise arrange for viewing in cold temperatures.)
 - c) Non-volatile memory or suitable memory with battery backup for storing pre-programmed messages.
 - d) Logic circuitry to control the sequence of messages and flash rate.
- 4) Provide a serial interface that is capable of supporting complete remote control ability through land line and cellular telephone operation. Include communication software capable of immediately updating the message, providing complete sign status, and allowing message library queries and updates.
- 5) Allow a single person easily to raise the sign to a satisfactory height above the pavement during use, and lower the sign during travel.
- 6) Be Highway Orange on all exterior surfaces of the trailer, supports, and controller cabinet.
- 7) Provide operation in ambient temperatures from -30 to + 120 degrees Fahrenheit during snow, rain and other inclement weather.
- 8) Provide the driver board as part of a module. All modules are interchangeable, and have plug and socket arrangements for disconnection and reconnection. Printed circuit boards associated with driver boards have a conformable coating to protect against moisture.
- 9) Provide a sign case sealed against rain, snow, dust, insects, etc. The lens is UV stabilized clear plastic (polycarbonate, acrylic, or other approved material) angled to prevent glare.
- 10) Provide a flat black UV protected coating on the sign hardware, character PCB, and appropriate lens areas.
- 11) Provide a photocell control to provide automatic dimming.

11

- 12) Allow an on-off flashing sequence at an adjustable rate.
- 13) Provide a sight to aim the message.
- 14) Provide a LED display color of approximately 590 nm amber.
- 15) Provide a controller that is password protected.
- 16) Provide a security device that prevents unauthorized individuals from accessing the controller.
- 17) Provide the following 3-line messages preprogrammed and available for use when the sign unit begins operation:

/KEEP/RIGHT/⇒⇒⇒/	/MIN/SPEED/**MPH/
/KEEP/LEFT/⇐⇐⇐/	/ICY/BRIDGE/AHEAD/ /ONE
/LOOSE/GRAVEL/AHEAD/	LANE/BRIDGE/AHEAD/
/RD WORK/NEXT/**MILES/	/ROUGH/ROAD/AHEAD/
/TWO WAY/TRAFFIC/AHEAD/	/MERGING/TRAFFIC/AHEAD/
/PAINT/CREW/AHEAD/	/NEXT/**/MILES/
/REDUCE/SPEED/**MPH/	/HEAVY/TRAFFIC/AHEAD/
/BRIDGE/WORK/**0 FT/	/SPEED/LIMIT/**MPH/
/MAX/SPEED/**MPH/	/BUMP/AHEAD/
/SURVEY/PARTY/AHEAD/	/TWO/WAY/TRAFFIC/

*Insert numerals as directed by the Engineer.

Add other messages during the project when required by the Engineer.

2.3 Power.

- 1) Design solar panels to yield 10 percent or greater additional charge than sign consumption. Provide direct wiring for operation of the sign or arrow board from an external power source to provide energy backup for 21 days without sunlight and an on-board system charger with the ability to recharge completely discharged batteries in 24 hours.

3.0 CONSTRUCTION. Furnish and operate the variable message signs as designated on the plans or by the Engineer. Ensure the bottom of the message panel is a minimum of 7 feet above the roadway in urban areas and 5 feet above in rural areas when operating. Use Class I, II, or III signs on roads with a speed limit less than 55 mph. Use Class I or II signs on roads with speed limits 55 mph or greater.

Maintain the sign in proper working order, including repair of any damage done by others, until completion of the project. When the sign becomes inoperative, immediately repair or replace the sign. Repetitive problems with the same unit will be cause for rejection and replacement.

Use only project related messages and messages directed by the Engineer, unnecessary messages lessen the impact of the sign. Ensure the message is displayed in either one or 2 phases with each phase having no more than 3 lines of text. When no message is needed, but it is necessary to know if the sign is operable, flash only a pixel.

When the sign is not needed, move it outside the clear zone or where the Engineer directs. Variable Message Signs are the property of the Contractor and shall be removed from the project when no longer needed. The Department will not assume ownership of these signs.

4.0 MEASUREMENT. The final quantity of Variable Message Sign will be

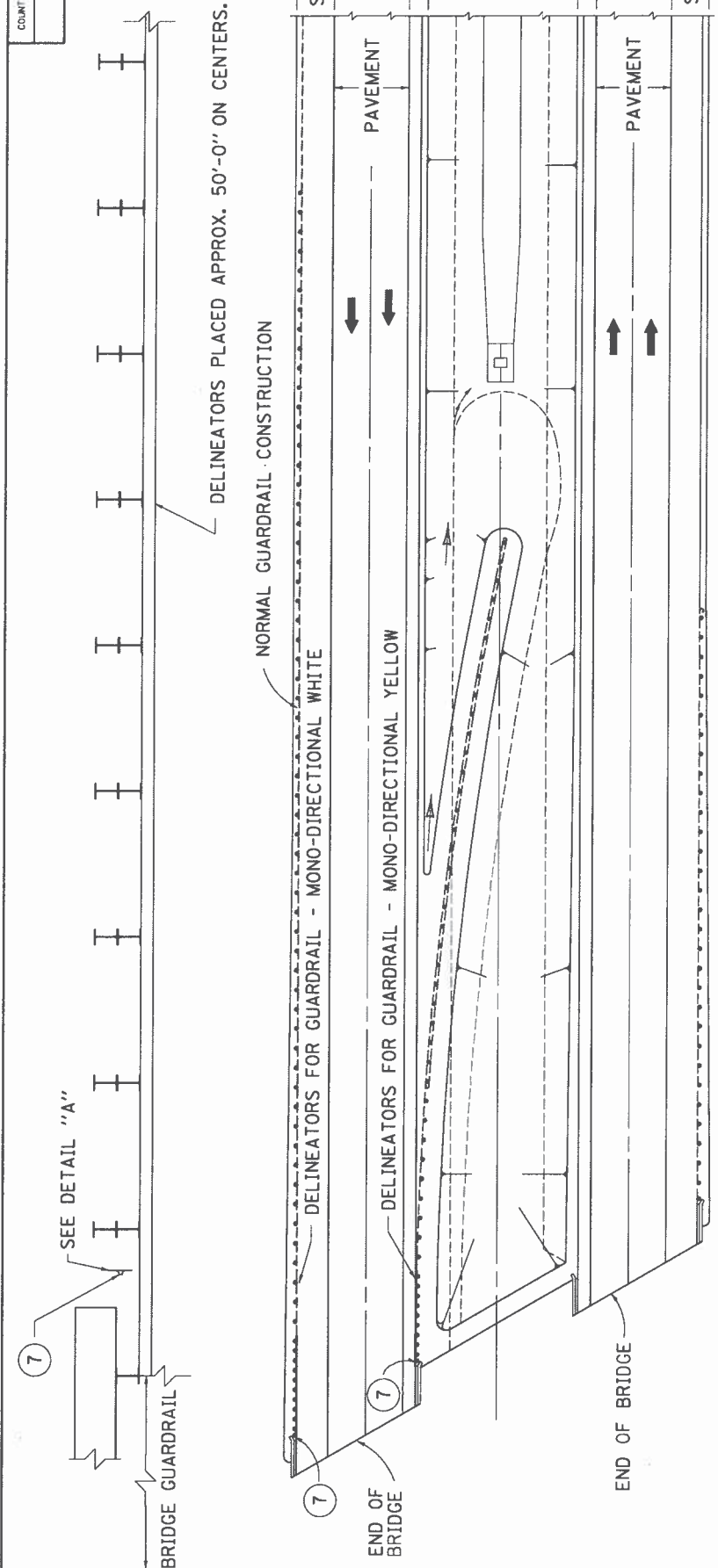
11
the actual number of individual signs acceptably furnished and operated during the project. The Department will not measure signs replaced due to damage or rejection.

5.0 PAYMENT. The Department will pay for the Variable Message Signs at the unit price each. The Department will not pay for signs replaced due to damage or rejection. Payment is full compensation for furnishing all materials, labor, equipment, and service necessary to, operate, move, repair, and maintain or replace the variable message signs. The Department will make payment for the completed and accepted quantities under the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
02671	Portable Changeable Message Sign	Each

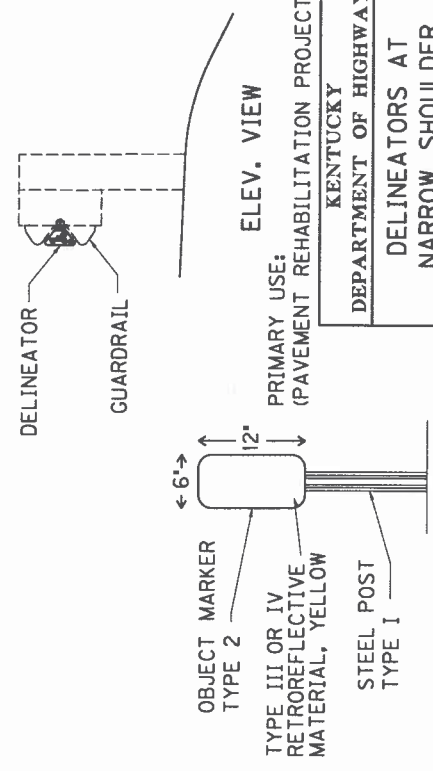
Effective June 15, 2012

COUNTY OF	ITEM NO.	SHEET



NOTES:

1. DELINEATOR SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH, AND SHALL INCLUDE ALL MATERIALS AND LABOR NECESSARY FOR ONE COMPLETE INSTALLATION.
2. CODE PAY ITEM PAY UNIT
- 1984 DELINEATOR FOR CONCRETE BARRIERS EACH
- 1985 DELINEATOR FOR BARRIER WALL - MONO-DIRECTIONAL WHITE EACH
- 1985 DELINEATOR FOR BARRIER WALL - MONO-DIRECTIONAL YELLOW EACH
- 1982 DELINEATORS ON GUARDRAIL EACH
- 1983 DELINEATOR FOR GUARDRAIL - MONO-DIRECTIONAL WHITE EACH
- 1983 DELINEATOR FOR GUARDRAIL - MONO-DIRECTIONAL YELLOW EACH
3. THE DELINEATORS SHALL BE YELLOW IN COLOR WHEN THE BARRIER IS PLACED IN THE MEDIAN AND/OR ON THE LEFT SIDE OF THE DRIVING LANE. THE DELINEATORS SHALL BE WHITE IN COLOR WHEN THE BARRIER IS PLACED ON THE RIGHT SIDE OF THE DRIVING LANE.
4. DELINEATORS SHALL BE APPLIED 300 FEET IN ADVANCE OF AND THROUGHOUT THE LENGTH OF ALL BRIDGES THAT DO NOT HAVE FULL WIDTH SHOULDERS. SPACING ON BRIDGES AND 300 FEET IN ADVANCE OF BRIDGES SHALL BE 50 FEET ON CENTERS. THE FIRST DELINEATOR ON THE GUARDRAIL SHALL BE PLACED 50 FEET FROM THE DELINEATOR AT THE END OF THE BRIDGE. DELINEATORS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
5. WHEN CONCRETE BARRIERS EXTEND ACROSS NARROW SHOULDER WIDTH STRUCTURES IN LIEU OF STEEL BEAM GUARDRAIL, DELINEATORS SHALL BE INSTALLED AT SAME VERTICAL ALIGNMENT AS ON THE GUARDRAIL AND DELINEATORS SHALL COMPLY WITH CURRENT SEPJA DRAWING 004.
6. GUARDRAIL DELINEATORS SHALL COMPLY WITH CURRENT SEPJA DRAWING 002.
7. SEE SECTION 718 OF THE CURRENT STANDARD SPECIFICATIONS FOR "OBJECT MARKER TYPE 2".



OBJECT MARKER TYPE 2
DETAIL "A" 7

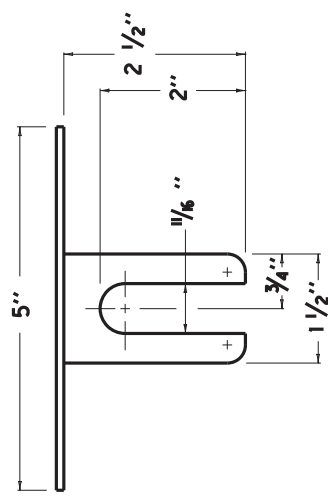
KENTUCKY
DEPARTMENT OF HIGHWAYS
DELINATORS AT
NARROW SHOULDER
BRIDGES

SUBMITTED: 4-1-2008
DATE: 06/08/2008

COUNTY OF	ITEM NO.	SHEET

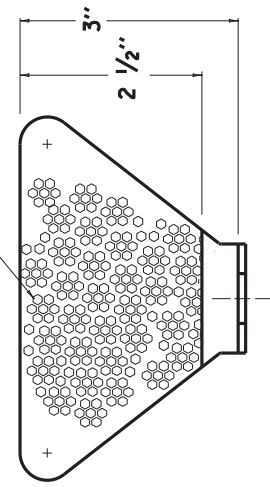
NOTES

1. THE DELINEATOR'S SHAPE AND DIMENSIONS ARE SHOWN FOR ILLUSTRATION PURPOSES ONLY. TYPES OF DELINEATORS PERMITTED SHALL BE FROM THE LIST OF APPROVED MATERIALS.
2. DELINEATOR SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH AND SHALL INCLUDE ALL MATERIALS AND LABOR NECESSARY FOR ONE COMPLETE INSTALLATION.
3. CODE PAY ITEM
1982 DELINEATOR FOR GUARDRAIL - MONO DIRECTIONAL WHITE EACH
1983 DELINEATOR FOR GUARDRAIL - MONO DIRECTIONAL YELLOW EACH
1987 DELINEATOR FOR GUARDRAIL - BI-DIRECTIONAL WHITE EACH
4. GUARDRAIL DELINEATORS SHALL BE REQUIRED ON ALL GUARDRAIL.
5. DELINEATORS SHALL BE MANUFACTURED FROM 12 GA. GALVANIZED STEEL.
6. DIMENSIONS SHOWN ARE APPROXIMATE AND ARE SUBJECT TO MANUFACTURER'S TOLERANCES.
7. WHEN CONCRETE BARRIERS EXTEND ACROSS BRIDGE STRUCTURES IN LIEU OF STEEL BEAM GUARDRAIL, DELINEATORS SHALL BE INSTALLED AT SAME VERTICAL ALIGNMENT AS ON THE GUARDRAIL, AND DELINEATORS SHALL COMPLY WITH CURRENT SEPIA DRAWING 004.
8. DELINEATORS SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

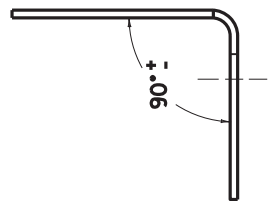


PLAN VIEW

TYPE XI SHEETING,
YELLOW OR WHITE

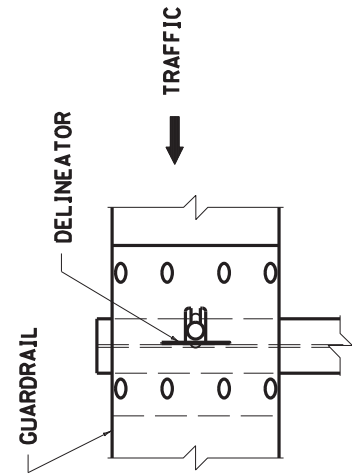


FRONT VIEW

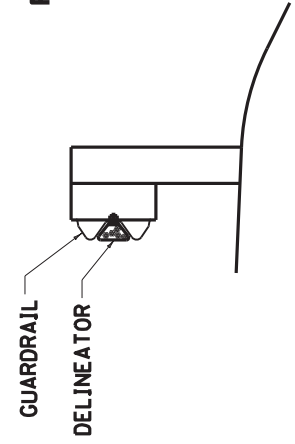


SIDE VIEW

NOTE: DIMENSIONS SHOWN ARE FOR ONE VERSION OF A WEB-MOUNTED GUARDRAIL DELINEATOR. DELINEATORS WITH ALTERNATE DIMENSIONS MAY BE CONSIDERED FOR INCLUSION ON THE APPROVED PRODUCTS LIST.

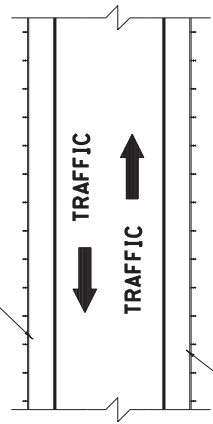


FRONT VIEW



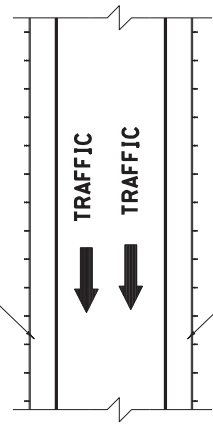
SIDE VIEW

BI-DIRECTIONAL WHITE DELINEATOR
FOR GUARDRAIL FACING TRAFFIC



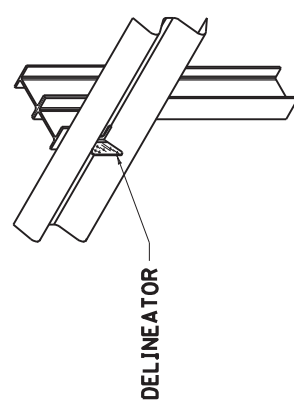
BI-DIRECTIONAL WHITE DELINEATOR
FOR GUARDRAIL FACING TRAFFIC

MONO-DIRECTIONAL WHITE DELINEATOR
FOR GUARDRAIL FACING TRAFFIC



MONO-DIRECTIONAL YELLOW DELINEATOR
FOR GUARDRAIL FACING TRAFFIC

PLACEMENT OF DELINEATORS
FOR GUARDRAIL



ISOMETRIC VIEW

APPROXIMATE DELINEATOR SPACING	
TANGENT	100'
CURVE	50'

SPACING SHOULD BE ADJUSTED IN CURVES SO THAT SEVERAL DELINEATORS ARE ALWAYS SIMULTANEOUSLY VISIBLE TO THE ROAD USER.

KENTUCKY
DEPARTMENT OF HIGHWAYS

DELINEATORS
FOR GUARDRAIL

Submitted: *[Signature]* 6-15-2012
DATE OF DESIGN: 0621
Contract No. 21345
Page 119

CONDITION NO. 1: SOIL EMBANKMENT OVER DEEP OVERBURDEN WITH OPEN SINKHOLES
MAINTAIN POSITIVE DRAINAGE

ORIGINAL GROUND LINE
TYPE IV GEOTEXTILE FABRIC
SOIL OVERBURDEN $\geq 15'$
SOIL EMBANKMENT
EXIST. SOIL
CLAY SOIL CAP (2' MIN.)
TYPE IV GEOTEXTILE FABRIC
GRANULAR EMB.
DETAIL "1 A"

PROCEDURE:
a. REMOVE DEBRIS. DO NOT EXCAVATE SOIL OVERBURDEN.
b. LINE OPENING WITH TYPE IV GEOTEXTILE FABRIC.
c. REFILL WITH GRANULAR EMBANKMENT.
d. PLACE TYPE IV GEOTEXTILE FABRIC ON TOP OF GRANULAR EMBANKMENT.
e. REFILL WITH (2' MINIMUM) CLAY SOIL CAP.

CONDITION NO. 2: SOIL EMBANKMENT OVER SHALLOW OVERBURDEN WITH SINKHOLE OPENING IN ROCK
MAINTAIN POSITIVE DRAINAGE

ORIGINAL GROUND LINE
SOIL OVERBURDEN $< 15'$
TYPE IV GEOTEXTILE FABRIC
SOIL EMB.
CLAY SOIL CAP (2' MIN.)
GRANULAR EMB.
EXIST. SOIL
ROCK LINE
1' REINFORCED CONCRETE CAP
GRANULAR EMB.
(CONC. CAP)
ALTERNATE NO. 2B

PROCEDURE FOR ALTERNATE NO. 2A
a. REMOVE DEBRIS AND SOIL OVERBURDEN.
b. REFILL OPENING WITH GRANULAR EMBANKMENT TO 2' MIN. BELOW ROCK LINE.
c. PLACE TYPE IV GEOTEXTILE FABRIC ON TOP OF GRANULAR EMB. OVERLAPPING ORIG. GROUND LINE.
d. REFILL WITH (2' MIN.) CLAY SOIL CAP.
PROCEDURE FOR ALTERNATE NO. 2B
a. REMOVE DEBRIS AND SOIL OVERBURDEN.
b. REFILL OPENING WITH GRANULAR EMBANKMENT TO 1' MIN. BELOW ROCK LINE.
c. CONST. 1' REINFORCED CONC. CAP. CAP SHOULD BE INTERLOCKED WITH ROCK FOR SUPPORT.

CONDIT. NO. 3: ROCK EMB. OVER DEEP OVERBURDEN WITH OPEN SINKHOLES
ORIGINAL GROUND LINE

TYPE IV GEOTEXTILE FABRIC
SOIL OVERBURDEN $\geq 15'$
ROCK EMBANKMENT
ROCK LINE
GRANULAR EMB.

PROCEDURE:
a. REMOVE DEBRIS. DO NOT EXCAVATE SOIL OVERBURDEN.
b. LINE OPENING WITH TYPE IV GEOTEXTILE FABRIC.
c. REFILL OPENING WITH GRANULAR EMBANKMENT. TO TOP OF DEPRESSION.

CONDITION NO. 6: CUT SECTIONS WITH SINKHOLE OPENINGS IN SOIL

ORIGINAL GROUND LINE
TYPE IV GEOTEXTILE FABRIC
SOIL OVERBURDEN $< 15'$
SEE ALTN. 6A
SEE ALTN. 6B
BOULDERS
ROCK LINE
SOIL OVERBURDEN $< 15'$
SOIL SUBGRADE
SINKHOLE EXCAVATION SLOPE
GRANULAR EMB.
CLAY SOIL CAP (2' MIN.)
ALTN. 6A
ALTN. 6B

ALTERNATE NO. 6A
a. REMOVE DEBRIS. DO NOT EXCAVATE SOIL OVERBURDEN.
b. LINE OPENING WITH TYPE IV GEOTEXTILE FABRIC.
c. REFILL WITH GRANULAR EMB.
d. PLACE TYPE IV GEOTEXTILE FABRIC OVER GRANULAR EMBANKMENT OVERLAPPING ORIG. GROUND LINE.
e. REFILL WITH (2' MIN.) CLAY SOIL CAP. IF ROCK SUBGRADE IS USED OMIT SOIL CAP AND FABRIC UNDERLYING SOIL CAP.
ALTERNATE NO. 6B
a. REMOVE DEBRIS AND SOIL OVERBURDEN.
b. REFILL OPENING WITH GRANULAR EMBANKMENT TO 2' MIN. BELOW ROCK LINE.
c. PLACE TYPE IV GEOTEXTILE FABRIC OVER GRANULAR EMBANKMENT OVERLAPPING ORIG. GROUND LINE.
d. REFILL WITH (2' MIN.) CLAY SOIL CAP. IF CONCRETE CAP IS USED THE FABRIC SHALL BE OMITTED AND CAP SHALL BE INTERLOCKED WITH THE BEDROCK FOR SUPPORT AS DETAILED IN COND. NO. 2
ALTERNATE NO. 2B.

CONDITION NO. 5: CUT SECTIONS WITH SINKHOLE OPENINGS IN ROCK

EXIST. SOIL
ROCK LINE
SOIL OVERBURDEN $< 15'$
SEE ALTN. 5A OR
SEE ALTN. 5B
ROADBED
TYPE IV GEOTEXTILE FABRIC
GRANULAR EMB.
CLAY SOIL CAP (2' MIN.)
SUBGRADE
ALTERNATE NO. 5A
ALTERNATE NO. 5B

PROCEDURE FOR ALTERNATE NO. 5A
a. REFILL OPENING WITH GRANULAR EMBANKMENT. IF CONCRETE CAP IS USED IT SHALL BE INTERLOCKED WITH THE BEDROCK FOR SUPPORT AS DETAILED IN CONDITION NO. 2 ALTERNATE NO. 2B.
PROCEDURE FOR ALTERNATE NO. 5B
a. REFILL OPENING WITH GRANULAR EMBANKMENT. TO 2' MINIMUM BELOW SOIL SUBGRADE.
b. PLACE TYPE IV GEOTEXTILE FABRIC OVER GRANULAR EMBANKMENT.
c. REFILL WITH (2' MIN.) CLAY SOIL CAP. IF CONCRETE CAP IS USED THE FABRIC SHALL BE OMITTED AND CAP SHALL BE INTERLOCKED WITH THE BEDROCK FOR SUPPORT AS DETAILED IN COND. NO. 2 ALTERNATE NO. 2B.

CONDITION NO. 4: ROCK EMBANKMENT OVER SHALLOW OVERBURDEN WITH SINKHOLE OPENINGS IN ROCK

ORIGINAL GROUND LINE
SEE ALTN. 4B
SEE ALTN. 4A
MAINTAIN POSITIVE DRAINAGE
ROCK LINE
SOIL OVERBURDEN $< 15'$
EXIST. SOIL
ROCK EMB.
1' REINFORCED CONCRETE CAP
EXIST. SOIL
GRANULAR EMBANKMENT
ROCK LINE
ALTERNATE NO. 4A
ALTERNATE NO. 4B

PROCEDURE FOR ALTERNATE NO. 4A
a. REMOVE DEBRIS AND SOIL OVERBURDEN.
b. REFILL OPENING TO ROCK LINE WITH GRANULAR EMBANKMENT.
PROCEDURE FOR ALTERNATE NO. 4B
a. REMOVE DEBRIS AND SOIL OVERBURDEN.
b. REFILL OPENING WITH GRANULAR EMBANKMENT TO 1' MIN. BELOW ROCK LINE.
c. CONST. 1' REINFORCED CONC. CAP. CAP SHOULD BE INTERLOCKED WITH ROCK FOR SUPPORT.

DEPARTMENT OF HIGHWAYS

KENTUCKY

TREATMENT OF OPEN SINKHOLES

SUBMITTED: 12-1-98

DATE: 12-1-98

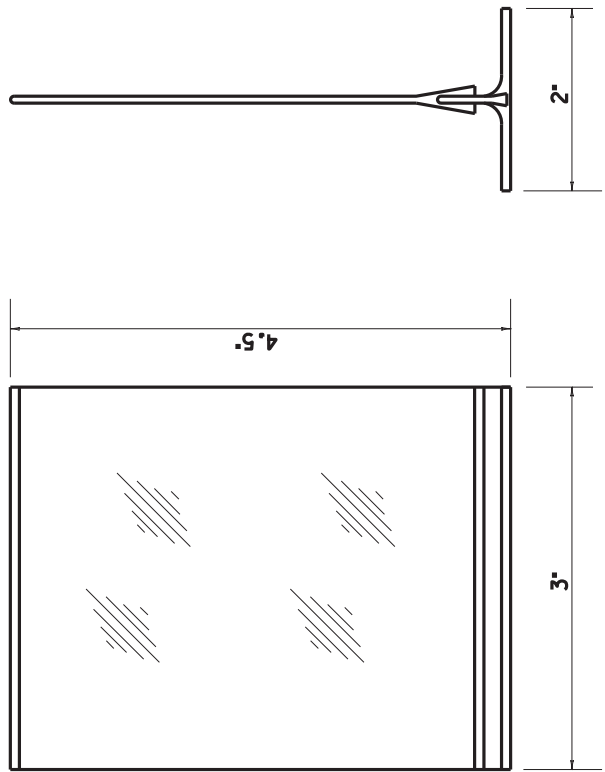
TERMINATION OF DESIGN

COUNTY OF	ITEM NO.	SHEET NO.

NOTES

1. BARRIER WALL DELINEATORS SHALL BE REQUIRED ON ALL BARRIER WALL.
2. DELINEATORS SHALL BE MEASURED AND PAID FOR AT THE CONTRACT UNIT PRICE EACH, AND SHALL INCLUDE ALL MATERIALS AND LABOR NECESSARY FOR ONE COMPLETE INSTALLATION.
3.

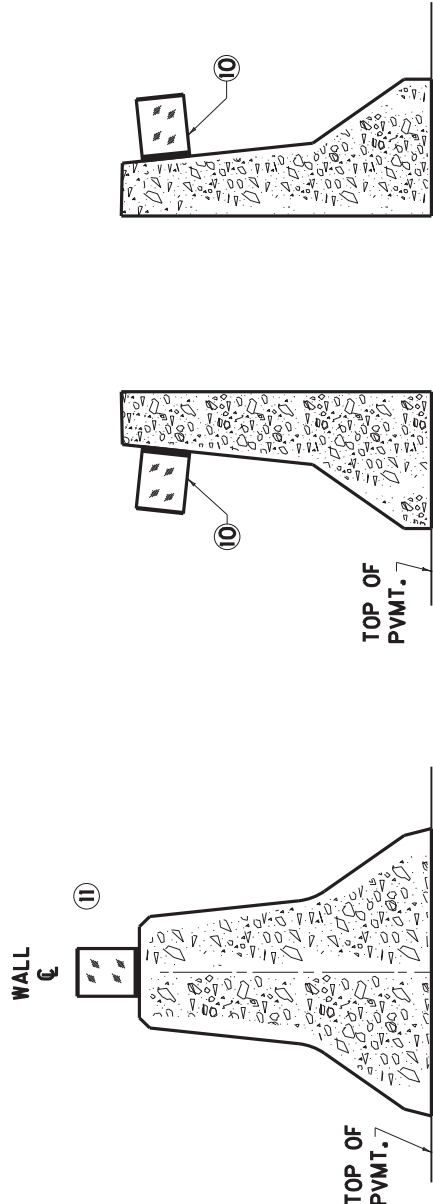
CODE	PAY ITEM	PAY UNIT
1984	DELINEATOR FOR BARRIER WALL - MONO DIRECTIONAL WHITE	EACH
1985	DELINEATOR FOR BARRIER WALL - MONO DIRECTIONAL YELLOW	EACH
1986	DELINEATOR FOR BARRIER WALL - BI-DIRECTIONAL YELLOW	EACH
1990	DELINEATOR FOR BARRIER WALL - BI-DIRECTIONAL WHITE	EACH
4. IN ACCORDANCE WITH THE MUTCD (CURRENT EDITION), THE COLOR OF DELINEATORS SHALL MATCH THE COLOR OF THE EDGE LINE THAT THEY SUPPLEMENT. IN GENERAL, DELINEATORS ON BARRIER WALL ALONG THE LEFT SIDE OF DRIVING LANES SHALL BE YELLOW, AND DELINEATORS ON BARRIER WALL ALONG THE RIGHT SIDE OF DRIVING LANES SHALL BE WHITE. DELINEATORS IN BOTH DIRECTIONS ON A TWO-LANE, TWO-WAY ROADWAY SHALL BE BI-DIRECTIONAL WHITE.
5. TYPES OF DELINEATORS PERMITTED SHALL BE FROM THE LIST OF APPROVED MATERIALS. THE DELINEATOR'S SHAPE AND DIMENSIONS ARE FOR ILLUSTRATION PURPOSES ONLY.
6. THE DELINEATOR UNIT SHALL HAVE THE REFLECTIVE SURFACE INSTALLED FACING TRAFFIC.
7. DELINEATORS SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATION.
8. DELINEATORS SHALL BE ATTACHED TO CONCRETE MEDIAN BARRIER WITH AN APPROVED ADHESIVE.
9. DELINEATOR SHEETING SHALL BE TYPE XI, YELLOW OR WHITE.
10. DELINEATORS SHOULD BE MOUNTED AT A HEIGHT OF APPROXIMATELY 4' ABOVE PAVEMENT. WHEN CONCRETE BARRIERS EXTEND ACROSS BRIDGE STRUCTURES IN LIEU OF STEEL BEAM GUARDRAIL, DELINEATORS SHALL BE INSTALLED AT THE SAME VERTICAL ALIGNMENT AS ON THE GUARDRAIL.
11. FOR BARRIER WALLS 50" OR LESS IN HEIGHT, DELINEATORS MAY BE INSTALLED ON TOP OF THE BARRIER WALL. FOR MEDIAN BARRIER WALLS 50" OR LESS IN HEIGHT THAT SEPARATE TWO-WAY TRAFFIC, BI-DIRECTIONAL YELLOW DELINEATORS MAY BE INSTALLED ON THE TOP OF THE BARRIER WALL IN LIEU OF SIDE-MOUNTED MONO-DIRECTIONAL YELLOW DELINEATORS.



FRONT ELEVATION
SIDE ELEVATION

APPROXIMATE DELINEATOR SPACING	
TANGENT	100'
CURVE	50'

SPACING SHOULD BE ADJUSTED IN CURVES SO THAT SEVERAL DELINEATORS ARE ALWAYS SIMULTANEOUSLY VISIBLE TO THE ROAD USER.



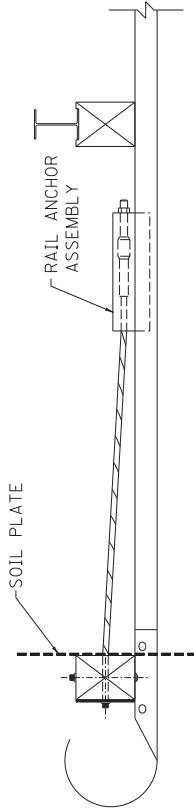
NORMAL (SOLID) WALL SECTION
(SEPARATE SEGMENT) WALL SECTION

KENTUCKY
DEPARTMENT OF HIGHWAYS

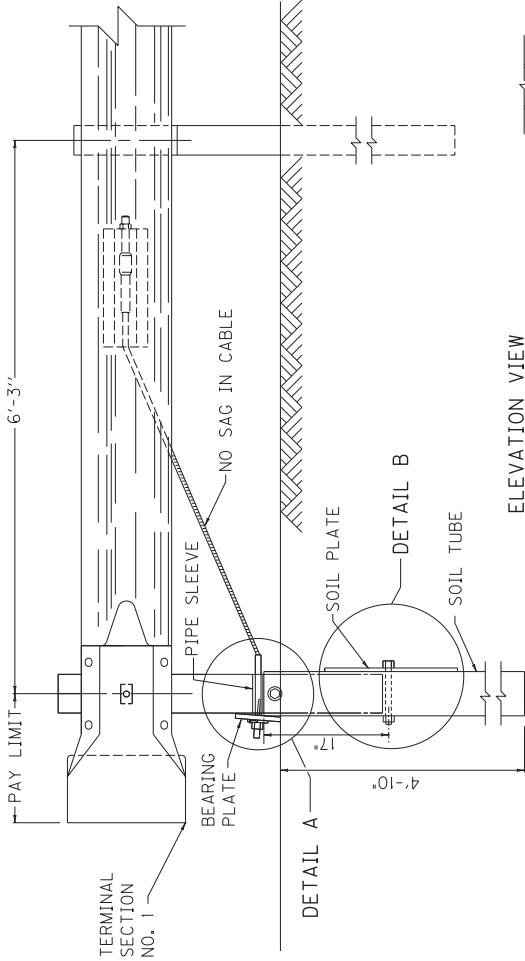
DELINEATORS FOR
CONCRETE BARRIERS

SUBMITTED: 7-13-2012
DATE: 7-13-2012
DESIGN: 119

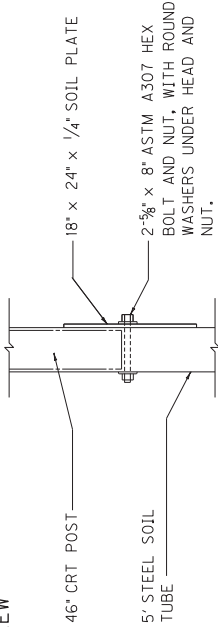
COUNTY OF	ITEM NO.	SHEET NO.



PLAN VIEW



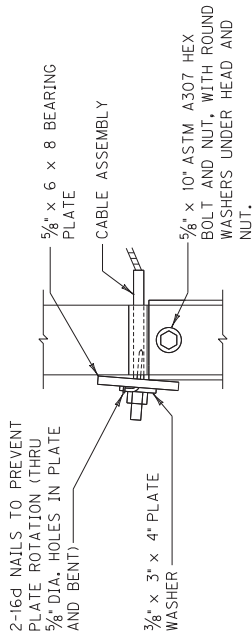
ELEVATION VIEW



DETAIL B

NOTES

1. GUARDRAIL END TREATMENT TYPE 2A SHALL BE TO THE PAY LIMITS AS DETAILED. THE CONTRACT UNIT BID IS EACH AND SHALL INCLUDE A TERMINAL SECTION NO. 1, RAIL ANCHOR ASSEMBLY, CABLE ANCHOR ASSEMBLY AND ALL OTHER INCIDENTALS NECESSARY FOR A COMPLETE INSTALLATION AS DETAILED.
2. IN THE EVENT SOLID ROCK IS ENCOUNTERED, THE SOIL TUBE MAY BE SHORTENED, PROVIDED IT EXTENDS INTO THE SOLID GROUND A MINIMUM OF 3 FEET.
3. INSTALL BEARING PLATE SO THAT THE "V" OPENING IS AT THE TOP.



DETAIL A

USE WITH CUR. STD. DWGS.
RBR-010, RBI-001,
RBI-002, RBI-003

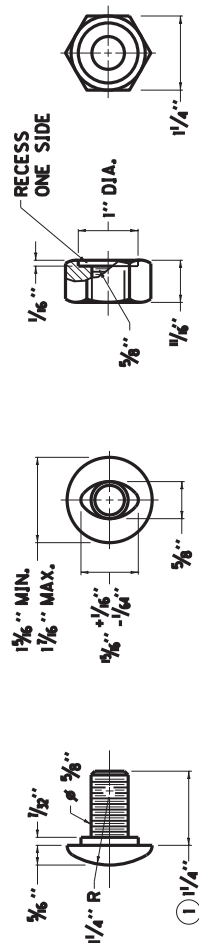
KENTUCKY DEPARTMENT OF HIGHWAYS
GUARDRAIL END TREATMENT TYPE 2A

SUBMITTED DATE 6-15-2012
0070

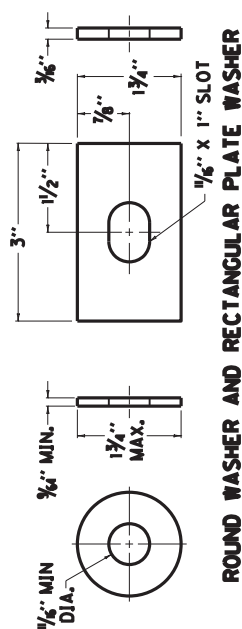
COUNTY OF	ITEM NO.	SHEET NO.
		070

NOTES

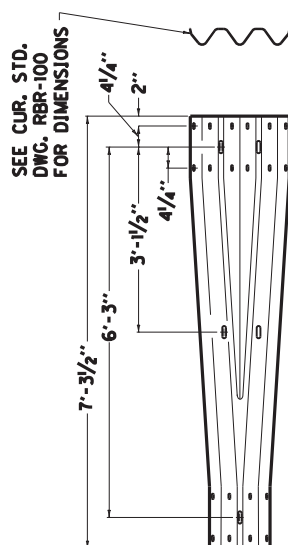
- ① RAIL BOLT SIMILAR EXCEPT LENGTH.
② THE THREE BEAM TO "W" BEAM CONNECTOR SHALL COMPLY WITH AASHTO M-180 CLASS A, TYPE 2 EXCEPT WHERE IN CONFLICT WITH THIS DETAIL.



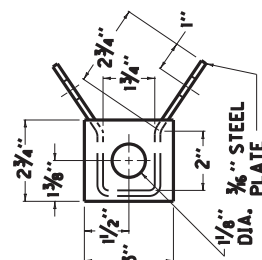
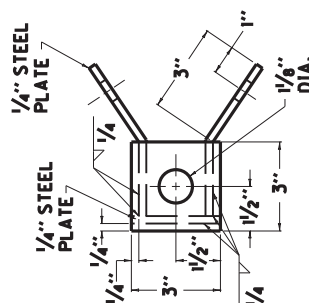
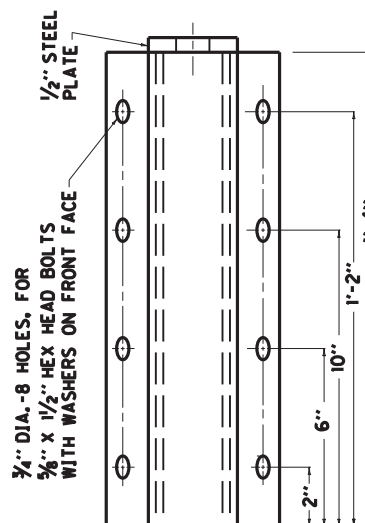
5/8" BUTTON HEAD BOLT AND RECESSED NUT



ROUND WASHER AND RECTANGULAR PLATE WASHER



THREE BEAM TO "W" BEAM CONNECTOR ②



ALTERNATE NO. 1

ALTERNATE NO. 2

RAIL ANCHOR ASSEMBLY

KENTUCKY
 DEPARTMENT OF HIGHWAYS

GUARDRAIL COMPONENTS

SUBMITTED: *[Signature]* 6-15-2012
 TEAM DIVISION OF DESIGN DATE

800

PART III

EMPLOYMENT, WAGE AND RECORD REQUIREMENTS

TRANSPORTATION CABINET DEPARTMENT OF HIGHWAYS

LABOR AND WAGE REQUIREMENTS APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS

- I. Application
- II. Nondiscrimination of Employees (KRS 344)
- III. Payment of Predetermined Minimum Wages
- IV. Statements and Payrolls

I. APPLICATION

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

II. NONDISCRIMINATION OF EMPLOYEES

AN ACT OF THE KENTUCKY GENERAL ASSEMBLY TO PREVENT DISCRIMINATION IN EMPLOYMENT KRS CHAPTER 344 EFFECTIVE JUNE 16, 1972

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (between forty and seventy); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age (between forty and seventy). The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, disability or age (between forty and seventy), except that such notice or advertisement may indicate a preference, limitation, or specification based on religion, or national origin when religion, or national origin is a bona fide occupational qualification for employment.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual

because of his race, color, religion, national origin, sex, disability or age (between forty and seventy), in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

III. PAYMENT OF PREDETERMINED MINIMUM WAGES

1. These special provisions are supplemented elsewhere in the contract by special provisions which set forth certain predetermined minimum wage rates. The contractor shall pay not less than those rates.

2. The minimum wage determination schedule shall be posted by the contractor, in a manner prescribed by the Department of Highways, at the site of the work in prominent places where it can be easily seen by the workers.

IV. STATEMENTS AND PAYROLLS

1. All contractors and subcontractors affected by the terms of KRS 337.505 to 337.550 shall keep full and accurate payroll records covering all disbursements of wages to their employees to whom they are required to pay not less than the prevailing rate of wages. Payrolls and basic records relating thereto will be maintained during the course of the work and preserved for a period of one (1) year from the date of completion of this contract.

2. The payroll records shall contain the name, address and social security number of each employee, his correct classification, rate of pay, daily and weekly number of hours worked, itemized deductions made and actual wages paid.

3. The contractor shall make his daily records available at the project site for inspection by the State Department of Highways contracting office or his authorized representative.

Periodic investigations shall be conducted as required to assure compliance with the labor provisions of the contract. Interrogation of employees and officials of the contractor shall be permitted during working hours.

Aggrieved workers, Highway Managers, Assistant District Engineers, Resident Engineers and Project Engineers shall report all complaints and violations to the Division of Contract Procurement.

The contractor shall be notified in writing of apparent violations. The contractor may correct the reported violations and notify the Department of Highways of the action taken or may request an informal hearing. The request for hearing shall be in writing within ten (10) days after receipt of the notice of the reported violation. The contractor may submit

records and information which will aid in determining the true facts relating to the reported violations.

Any person or organization aggrieved by the action taken or the findings established as a result of an informal hearing by the Division of Contract Procurement may request a formal hearing.

4. The wages of labor shall be paid in legal tender of the United States, except that this condition will be considered satisfied if payment is made by a negotiable check, on a solvent bank, which may be cashed readily by the employee in the local community for the full amount, without discount or collection charges of any kind. Where checks are used for payments, the contractor shall make all necessary arrangements for them to be cashed and shall give information regarding such arrangements.

5. No fee of any kind shall be asked or accepted by the contractor or any of his agents from any person as a condition of employment on the project.

6. No laborers shall be charged for any tools used in performing their respective duties except for reasonably avoidable loss or damage thereto.

7. Every employee on the work covered by this contract shall be permitted to lodge, board, and trade where and with whom he elects and neither the contractor nor his agents, nor his employees shall directly or indirectly require as a condition of employment that an employee shall lodge, board or trade at a particular place or with a particular person.

8. Every employee on the project covered by this contract shall be an employee of either the prime contractor or an approved subcontractor.

9. No charge shall be made for any transportation furnished by the contractor or his agents to any person employed on the work.

10. No individual shall be employed as a laborer or mechanic on this contract except on a wage basis, but this shall not be construed to prohibit the rental of teams, trucks or other equipment from individuals.

No Covered employee may be employed on the work except in accordance with the classification set forth in the schedule mentioned above; provided, however, that in the event additional classifications are required, application shall be made by the contractor to the Department of Highways and (1) the Department shall request appropriate classifications and rates from the proper agency, or (2) if there is urgent need for additional classification to avoid undue delay in the work, the contractor may employ such workmen at rates deemed comparable to rates established for similar classifications provided he has made written application through the Department of Highways, addressed to the proper agency, for the supplemental rates. The contractor shall retroactively adjust, upon receipt of the supplemental rates schedule, the wages of any employee paid less than the established rate and may adjust the wages of any employee overpaid.

11. No contractor or subcontractor contracting for any part of the contract work which may require or involve the employment of laborers or mechanics shall require or permit any laborer or mechanic in any work-week in which he is employed on such work, to work in excess of eight hours in any calendar day or in excess of forty hours in such work-week unless such laborer or mechanic receives compensation at a rate not less than one and one half times his basic rate of pay for all hours worked in excess of eight hours in any calendar day or in excess of forty hours in such work-week. A laborer, workman or mechanic and an employer may enter into a written agreement or a collective bargaining agreement to work more than eight (8) hours a calendar day but not more than ten (10) hours a calendar day for the straight time hourly rate. This agreement shall be in writing and shall be executed prior to the employee working in excess of eight (8) hours, but not more than ten (10) hours, in any one (1) calendar day.

12. Payments to the contractor may be suspended or withheld due to failure of the contractor to pay any laborer or

mechanic employed or working on the site of the work, all or part of the wages required under the terms of the contract. The Department may suspend or withhold payments only after the contractor has been given written notice of the alleged violation and the contractor has failed to comply with the wage determination of the Department of Highways.

13. Contractors and subcontractors shall comply with the sections of Kentucky Revised Statutes, Chapter 337 relating to contracts for Public Works.

Revised 2-16-95

EXECUTIVE BRANCH CODE OF ETHICS

In the 1992 regular legislative session, the General Assembly passed and Governor Brereton Jones signed Senate Bill 63 (codified as KRS 11A), the Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (6) provides:

No present or former public servant shall, within six (6) months of following termination of his office or employment, accept employment, compensation or other economic benefit from any person or business that contracts or does business with the state in matters in which he was directly involved during his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, provided that, for a period of six (6) months, he personally refrains from working on any matter in which he was directly involved in state government. This subsection shall not prohibit the performance of ministerial functions, including, but not limited to, filing tax returns, filing applications for permits or licenses, or filing incorporation papers.

KRS 11A.040 (8) states:

A former public servant shall not represent a person in a matter before a state agency in which the former public servant was directly involved, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not as a way to obtain private benefits.

If you have worked for the executive branch of state government within the past six months, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, Room 136, Capitol Building, 700 Capitol Avenue, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Kentucky Equal Employment Opportunity Act of 1978

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:
<https://www.eProcurement.ky.gov>.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **finance.contractcompliance@ky.gov** or by phone at 502-564-2874.

General Decision Number: KY120127 08/31/2012 KY127

Superseded General Decision Number: KY20100214

State: Kentucky

Construction Type: Highway

Counties: Allen, Ballard, Butler, Caldwell, Calloway, Carlisle, Christian, Crittenden, Daviess, Edmonson, Fulton, Graves, Hancock, Henderson, Hickman, Hopkins, Livingston, Logan, Lyon, Marshall, McCracken, McLean, Muhlenberg, Ohio, Simpson, Todd, Trigg, Union, Warren and Webster Counties in Kentucky.

HIGHWAY CONSTRUCTION PROJECTS (excluding tunnels, building structures in rest area projects & railroad construction; bascule, suspension & spandrel arch bridges designed for commercial navigation, bridges involving marine construction; and other major bridges).

Modification Number	Publication Date
0	01/06/2012
1	01/13/2012
2	02/10/2012
3	05/18/2012
4	05/25/2012
5	06/01/2012
6	06/15/2012
7	07/06/2012
8	07/13/2012
9	07/20/2012
10	08/03/2012
11	08/10/2012
12	08/24/2012
13	08/31/2012

BRIN0004-002 06/01/2011

BALLARD, BUTLER, CALDWELL, CARLISLE, CRITTENDEN, DAVIESS, EDMONSON, FULTON, GRAVES, HANCOCK, HENDERSON, HICKMAN, HOPKINS, LIVINGSTON, LYON, MARSHALL, MCCracken, MCLEAN, MUHLENBERG, OHIO, UNION, and WEBSTER COUNTIES

	Rates	Fringes
BRICKLAYER		
Ballard, Caldwell, Carlisle, Crittenden, Fulton, Graves, Hickman, Livingston, Lyon, Marshall, and McCracken Counties.....	\$ 24.11	10.30
Butler, Edmonson, Hopkins, Muhlenberg, and Ohio Counties.....	\$ 24.61	10.22
Daviess, Hancock,		

Cable spicers receive \$.25 per hour additional.

* ELEC1925-002 06/01/2012

FULTON COUNTY (Up to a 5 mile radius of City Hall in Fulton):

	Rates	Fringes
CABLE SPLICER.....	\$ 25.00	10.27
ELECTRICIAN.....	\$ 25.00	10.43

ENGI0181-017 07/01/2012

	Rates	Fringes
Operating Engineer:		
GROUP 1.....	\$ 27.35	13.40
GROUP 2.....	\$ 24.87	13.40
GROUP 3.....	\$ 25.26	13.40
GROUP 4.....	\$ 24.60	13.40

OPERATING ENGINEER CLASSIFICATIONS

GROUP 1 - A-Frame Winch Truck; Auto Patrol; Backfiller; Batcher Plant; Bituminous Paver; Bituminous Transfer Machine; Boom Cat; Bulldozer; Mechanic; Cableway; Carry-All Scoop; Carry Deck Crane; Central Compressor Plant; Cherry Picker; Clamshell; Concrete Mixer (21 cu. ft. or Over); Concrete Paver; Truck-Mounted Concrete Pump; Core Drill; Crane; Crusher Plant; Derrick; Derrick Boat; Ditching & Trenching Machine; Dragline; Dredge Operator; Dredge Engineer; Elevating Grader & Loaders; Grade-All; Gurries; Heavy Equipment Robotics Operator/Mechanic; High Lift; Hoe-Type Machine; Hoist (Two or More Drums); Hoisting Engine (Two or More Drums); Horizontal Directional Drill Operator; Hydrocrane; Hyster; KeCal Loader; LeTourneau; Locomotive; Mechanic; Mechanically Operated Laser Screed; Mechanic Welder; Mucking Machine; Motor Scraper; Orangepeel Bucket; Overhead Crane; Piledriver; Power Blade; Pumpcrete; Push Dozer; Rock Spreader, attached to equipment; Rotary Drill; Roller (Bituminous); Rough Terrain Crane; Scarifier; Scoopmobile; Shovel; Side Boom; Subgrader; Tailboom; Telescoping Type Forklift; Tow or Push Boat; Tower Crane (French, German & other types); Tractor Shovel; Truck Crane; Tunnel Mining Machines, including Moles, Shields or similar types of Tunnel Mining Equipment

GROUP 2 - Air Compressor (Over 900 cu. ft. per min.); Bituminous Mixer; Boom Type Tamping Machine; Bull Float; Concrete Mixer (Under 21 cu. ft.); Dredge Engineer; Electric Vibrator; Compactor/Self-Propelled Compactor; Elevator (One Drum or Buck Hoist); Elevator (When used to Hoist Building Material); Finish Machine; Firemen & Hoist (One Drum); Flexplane; Forklift (Regardless of Lift Height); Form Grader; Joint Sealing Machine; Outboard Motor Boat; Power Sweeper (Riding Type); Roller (Rock); Ross Carrier; Skid Mounted or Trailer Mounted Concrete Pump; Skid Steer Machine with all Attachments; Switchman or Brakeman; Throttle Valve Person; Tractair & Road Widening Trencher; Tractor (50 H.P. or Over); Truck Crane Oiler; Tugger;

Welding Machine; Well Points;& Whirley Oiler

GROUP 3 -All Off Road Material Handling Equipment, including
Articulating Dump Trucks; Greaser on Grease Facilities
servicing Heavy Equipment

GROUP 4 - Bituminous Distributor; Burlap & Curing Machine;
Cement Gun; Concrete Saw; Conveyor; Deckhand Oiler; Grout
Pump; Hydraulic Post Driver; Hydro Seeder; Mud Jack; Oiler;
Paving Joint Machine; Power Form Handling Equipment; Pump;
Roller (Earth); Steerman; Tamping Machine; Tractor (Under
50 H.P.); & Vibrator

CRANES - with booms 150 ft. & Over (Including JIB), and where
the length of the boom in combination with the length of
the piling equals or exceeds 150 ft. - \$1.00 above Group 1
rate

EMPLOYEES ASSIGNED TO WORK BELOW GROUND LEVEL ARE TO BE PAID
10% ABOVE BASIC WAGE RATE. THIS DOES NOT APPLY TO OPEN CUT
WORK.

IRON0070-005 06/01/2012

BUTLER COUNTY (Eastern eighth, including the Townships of
Decker, Lee & Tilford);
EDMONSON COUNTY (Northern three-fourths, including the
Townships of Asphalt, Bee Spring, Brownsville, Grassland, Huff,
Kyrock, Lindseyville, Mammoth Cave, Ollie, Prosperity, Rhoda,
Sunfish & Sweden)

	Rates	Fringes
Ironworkers:		
Structural; Ornamental;		
Reinforcing; Precast		
Concrete Erectors.....	\$ 26.34	18.58

IRON0103-004 04/01/2011

DAVISS, HANCOCK, HENDERSON, HOPKINS, MCLEAN, OHIO, UNION &
WEBSTER COUNTIES
BUTLER COUNTY (Townships of Aberdeen, Bancock, Casey,
Dexterville, Dunbar, Elfie, Gilstrap, Huntsville, Logansport,
Monford, Morgantown, Provo, Rochester, South Hill & Welchs
Creek);
CALDWELL COUNTY (Northeastern third, including the Township of
Creswell);
CHRISTIAN COUNTY (Northern third, including the Townships of
Apex, Crofton, Kelly, Mannington & Wynns);
CRITTENDEN COUNTY (Northeastern half, including the Townships
of Grove, Mattoon, Repton, Shady Grove & Tribune);
MUHLENBERG COUNTY (Townships of Bavier, Beech Creek Junction,
Benton, Brennen, Browder, Central City, Cleaton, Depoy,
Drakesboro, Eunis, Graham, Hillside, Luzerne, Lynn City,
Martwick, McNary, Millport, Moorman, Nelson, Paradise,
Powderly, South Carrollton, Tarina & Weir)

	Rates	Fringes
Ironworkers:.....	\$ 28.25	14.475

IRON0492-003 05/01/2012

ALLEN, LOGAN, SIMPSON, TODD & WARREN COUNTIES
BUTLER COUNTY (Southern third, including the Townships of Boston, Berrys Lick, Dimple, Jetson, Quality, Sharer, Sugar Grove & Woodbury);
CHRISTIAN COUNTY (Eastern two-thirds, including the Townships of Bennettstown, Casky, Herndon, Hopkinsville, Howell, Masonville, Pembroke & Thompsonville);
EDMONSON COUNTY (Southern fourth, including the Townships of Chalybeate & Rocky Hill);
MUHLENBERG COUNTY (Southern eighth, including the Townships of Dunnior, Penrod & Rosewood)

	Rates	Fringes
Ironworkers:.....	\$ 23.00	10.70

IRON0782-006 05/01/2012

BALLARD, CALLOWAY, CARLISLE, FULTON, GRAVES, HICKMAN, LIVINGSTON, LYON, MARSHALL, MCCracken & TRIGG COUNTIES
CALDWELL COUNTY (Southwestern two-thirds, including the Townships of Cedar Bluff, Cider, Claxton, Cobb, Crowtown, Dulaney, Farmersville, Fredonia, McGowan, Otter Pond & Princeton);
CHRISTIAN COUNTY (Western third, Excluding the Townships of Apex, Crofton, Kelly, Mannington, Wynns, Bennettstown, Casky, Herndon, Hopkinsville, Howell, Masonville, Pembroke & Thompsonville);
CRITTENDEN COUNTY (Southwestern half, including the Townships of Crayne, Dycusburg, Frances, Marion, Mexico, Midway, Sheridan & Told)

	Rates	Fringes
Ironworkers:		
Projects with a total contract cost of \$20,000,000.00 or above.....	\$ 26.00	18.91
All Other Work.....	\$ 24.66	17.65

LABO0189-005 07/01/2012

BALLARD, CALLOWAY, CARLISLE, FULTON, GRAVES, HICKMAN, LIVINGSTON, LYON, MARSHALL & MCCracken COUNTIES

	Rates	Fringes
Laborers:		
GROUP 1.....	\$ 20.75	11.81
GROUP 2.....	\$ 21.00	11.81
GROUP 3.....	\$ 21.05	11.81

GROUP 4.....\$ 21.65 11.81

LABORER CLASSIFICATIONS

GROUP 1 - Aging & Curing of Concrete; Asbestos Abatement Worker; Asphalt Plant; Asphalt; Batch Truck Dump; Carpenter Tender; Cement Mason Tender; Cleaning of Machines; Concrete; Demolition; Dredging; Environmental - Nuclear, Radiation, Toxic & Hazardous Waste - Level D; Flagperson; Grade Checker; Hand Digging & Hand Back Filling; Highway Marker Placer; Landscaping, Mesh Handler & Placer; Puddler; Railroad; Rip-rap & Grouter; Right-of-Way; Sign, Guard Rail & Fence Installer; Signal Person; Sound Barrier Installer; Storm & Sanitary Sewer; Swamper; Truck Spotter & Dumper; Wrecking of Concrete Forms; General Cleanup

GROUP 2 - Batter Board Man (Sanitary & Storm Sewer); Brickmason Tender; Mortar Mixer Operator; Scaffold Builder; Burner & Welder; Bushhammer; Chain Saw Operator; Concrete Saw Operator; Deckhand Scow Man; Dry Cement Handler; Environmental - Nuclear, Radiation, Toxic & Hazardous Waste - Level C; Forklift Operator for Masonary; Form Setter; Green Concrete Cutting; Hand Operated Grouter & Grinder Machine Operator; Jackhammer; Pavement Breaker; Paving Joint Machine; Pipelayer; Plastic Pipe Fusion; Power Driven Georgia Buggy & Wheel Barrow; Power Post Hole Digger; Precast Manhole Setter; Walk-Behind Tamper; Walk-Behind Trencher; Sand Blaster; Concrete Chipper; Surface Grinder; Vibrator Operator; Wagon Driller

GROUP 3 - Asphalt Luteman & Raker; Gunnite Nozzleman; Gunnite Operator & Mixer; Grout Pump Operator; Blaster; Side Rail Setter; Rail Paved Ditches; Screw Operator; Tunnel (Free Air); Water Blaster

GROUP 4 - Caisson Worker (Free Air); Cement Finisher; Environmental - Nuclear, Radiation, Toxic & Hazardous Waste - Levels A & B; Miner & Driller (Free Air); Tunnel Blaster; & Tunnel Mucker (Free Air); Directional & Horizontal Boring; Air Track Drillers (All Types); Powdermen & Blasters; Troxler & Concrete Tester if Laborer is Utilized

LABO0189-006 07/01/2012

ALLEN, BUTLER, CALDWELL, CHRISTIAN, DAVIESS, EDMONSON, HANCOCK, HOPKINS, LOGAN, MCLEAN, MUHLENBERG, OHIO, SIMPSON, TODD, TRIGG & WARREN COUNTIES

	Rates	Fringes
Laborers:		
GROUP 1.....	\$ 21.96	10.60
GROUP 2.....	\$ 22.21	10.60
GROUP 3.....	\$ 22.26	10.60
GROUP 4.....	\$ 22.86	10.60

LABORER CLASSIFICATIONS

GROUP 1 - Aging & Curing of Concrete; Asbestos Abatement

Worker; Asphalt Plant; Asphalt; Batch Truck Dump; Carpenter Tender; Cement Mason Tender; Cleaning of Machines; Concrete; Demolition; Dredging; Environmental - Nuclear, Radiation, Toxic & Hazardous Waste - Level D; Flagperson; Grade Checker; Hand Digging & Hand Back Filling; Highway Marker Placer; Landscaping, Mesh Handler & Placer; Puddler; Railroad; Rip-rap & Grouter; Right-of-Way; Sign, Guard Rail & Fence Installer; Signal Person; Sound Barrier Installer; Storm & Sanitary Sewer; Swamper; Truck Spotter & Dumper; Wrecking of Concrete Forms; General Cleanup

GROUP 2 - Batter Board Man (Sanitary & Storm Sewer); Brickmason Tender; Mortar Mixer Operator; Scaffold Builder; Burner & Welder; Bushhammer; Chain Saw Operator; Concrete Saw Operator; Deckhand Scow Man; Dry Cement Handler; Environmental - Nuclear, Radiation, Toxic & Hazardous Waste - Level C; Forklift Operator for Masonary; Form Setter; Green Concrete Cutting; Hand Operated Grouter & Grinder Machine Operator; Jackhammer; Pavement Breaker; Paving Joint Machine; Pipelayer; Plastic Pipe Fusion; Power Driven Georgia Buggy & Wheel Barrow; Power Post Hole Digger; Precast Manhole Setter; Walk-Behind Tamper; Walk-Behind Trencher; Sand Blaster; Concrete Chipper; Surface Grinder; Vibrator Operator; Wagon Driller

GROUP 3 - Asphalt Luteman & Raker; Gunnite Nozzleman; Gunnite Operator & Mixer; Grout Pump Operator; Blaster; Side Rail Setter; Rail Paved Ditches; Screw Operator; Tunnel (Free Air); Water Blaster

GROUP 4 - Caisson Worker (Free Air); Cement Finisher; Environmental - Nuclear, Radiation, Toxic & Hazardous Waste - Levels A & B; Miner & Driller (Free Air); Tunnel Blaster; & Tunnel Mucker (Free Air); Directional & Horizontal Boring; Air Track Drillers (All Types); Powdermen & Blasters; Troxler & Concrete Tester if Laborer is Utilized

LABO0561-001 07/01/2012

CRITTENDEN, HENDERSON, UNION & WEBSTER COUNTIES

	Rates	Fringes
Laborers:		
GROUP 1.....	\$ 20.86	11.70
GROUP 2.....	\$ 21.11	11.70
GROUP 3.....	\$ 21.16	11.70
GROUP 4.....	\$ 21.76	11.70

LABORER CLASSIFICATIONS

GROUP 1 - Aging & Curing of Concrete; Asbestos Abatement Worker; Asphalt Plant; Asphalt; Batch Truck Dump; Carpenter Tender; Cement Mason Tender; Cleaning of Machines; Concrete; Demolition; Dredging; Environmental - Nuclear, Radiation, Toxic & Hazardous Waste - Level D; Flagperson; Grade Checker; Hand Digging & Hand Back Filling; Highway Marker Placer; Landscaping, Mesh Handler & Placer; Puddler; Railroad; Rip-rap & Grouter; Right-of-Way; Sign, Guard Rail & Fence Installer; Signal Person; Sound Barrier Installer;

Storm & Sanitary Sewer; Swamper; Truck Spotter & Dumper;
Wrecking of Concrete Forms; General Cleanup

GROUP 2 - Batter Board Man (Sanitary & Storm Sewer);
Brickmason Tender; Mortar Mixer Operator; Scaffold Builder;
Burner & Welder; Bushhammer; Chain Saw Operator; Concrete
Saw Operator; Deckhand Scow Man; Dry Cement Handler;
Environmental - Nuclear, Radiation, Toxic & Hazardous Waste
- Level C; Forklift Operator for Masonary; Form Setter;
Green Concrete Cutting; Hand Operated Grouter & Grinder
Machine Operator; Jackhammer; Pavement Breaker; Paving
Joint Machine; Pipelayer; Plastic Pipe Fusion; Power Driven
Georgia Buggy & Wheel Barrow; Power Post Hole Digger;
Precast Manhole Setter; Walk-Behind Tamper; Walk-Behind
Trencher; Sand Blaster; Concrete Chipper; Surface
Grinder; Vibrator Operator; Wagon Driller

GROUP 3 - Asphalt Luteman & Raker; Gunnite Nozzleman; Gunnite
Operator & Mixer; Grout Pump Operator; Blaster; Side Rail
Setter; Rail Paved Ditches; Screw Operator; Tunnel (Free
Air); Water Blaster

GROUP 4 - Caisson Worker (Free Air); Cement Finisher;
Environmental - Nuclear, Radiation, Toxic & Hazardous Waste
- Levels A & B; Miner & Driller (Free Air); Tunnel Blaster;
& Tunnel Mucker (Free Air); Directional & Horizontal
Boring; Air Track Drillers (All Types); Powdermen &
Blasters; Troxler & Concrete Tester if Laborer is Utilized

PAIN0032-002 05/01/2012

BALLARD COUNTY

	Rates	Fringes
Painters:		
Bridges.....	\$ 30.56	14.20
All Other Work.....	\$ 28.26	14.20
Spray, Blast, Steam, High & Hazardous (Including Lead Abatement) and All Epoxy - \$1.00 Premium		

PAIN0118-003 05/01/2010

EDMONSON COUNTY:

	Rates	Fringes
Painters:		
Brush & Roller.....	\$ 18.50	10.30
Spray, Sandblast, Power Tools, Waterblast & Steam Cleaning.....	\$ 19.50	10.30

PAIN0156-006 04/01/2010

DAVISS, HANCOCK, HENDERSON, MCLEAN, OHIO, UNION & WEBSTER
COUNTIES

	Rates	Fringes
Painters:		
BRIDGES		
GROUP 1.....	\$ 25.60	10.05
GROUP 2.....	\$ 25.85	10.05
GROUP 3.....	\$ 26.60	10.05
GROUP 4.....	\$ 27.60	10.05
ALL OTHER WORK:		
GROUP 1.....	\$ 25.60	11.30
GROUP 2.....	\$ 25.85	11.30
GROUP 3.....	\$ 26.60	11.30
GROUP 4.....	\$ 27.60	11.30

PAINTER CLASSIFICATIONS

GROUP 1 - Brush & Roller

GROUP 2 - Plasterers

GROUP 3 - Spray; Sandblast; Power Tools; Waterblast;
Steamcleaning; Brush & Roller of Mastics, Creosotes, Kwinch
Koate & Coal Tar Epoxy

GROUP 4 - Spray of Mastics, Creosotes, Kwinch Koate & Coal
Tar Epoxy

PAIN0456-003 07/01/2011

ALLEN, BUTLER, LOGAN, MUHLENBERG, SIMPSON, TODD & WARREN
COUNTIES:

	Rates	Fringes
Painters:		
BRIDGES		
Brush & Roller.....	\$ 22.55	9.65
Spray; Sandblast; Power Tools; Waterblast & Steam Cleaning.....	\$ 23.55	9.65
ALL OTHER WORK		
Brush & Roller.....	\$ 17.55	9.65
Spray; Sandblast; Power Tools; Waterblast & Steam Cleaning.....	\$ 18.55	9.65

ALL OTHER WORK - HIGH TIME PAY
Over 35 feet (up to 100 feet) - \$1.00 above base wage
100 feet and over - \$2.00 above base wage

DURING SPRAY PAINTING AND SANDBLASTING OPERATIONS, POT
TENDERS SHALL RECEIVE THE SAME WAGE RATES AS THE SPRAY
PAINTER OR NOZZLE OPERATOR

PAIN0500-002 07/01/2012

CALDWELL, CALLOWAY, CARLISLE, CHRISTIAN, CRITTENDEN, FULTON,
GRAVES, HICKMAN, HOPKINS, LIVINGSTON, LYON, MARSHALL, MCCracken
& TRIGG COUNTIES:

	Rates	Fringes
Painters:		
Bridges.....	\$ 25.25	11.90
All Other Work.....	\$ 19.00	11.90
Waterblasting units with 3500 PSI and above - \$.50 premium		
Spraypainting and all abrasive blasting - \$1.00 premium		
Work 40 ft. and above ground level - \$1.00 premium		

PLUM0184-002 07/01/2011		

BALLARD, CALDWELL, CALLOWAY, CARLISLE, CHRISTIAN, CRITTENDEN,
FULTON, GRAVES, HICKMAN, LIVINGSTON, LYON, MARSHALL, MCCracken
and TRIGG COUNTIES

	Rates	Fringes
Plumber; Steamfitter.....	\$ 31.45	13.99

PLUM0502-004 08/01/2011		

ALLEN, BUTLER, EDMONSON, SIMPSON & WARREN

	Rates	Fringes
Plumber; Steamfitter.....	\$ 31.00	16.13

PLUM0633-002 07/01/2011		

DAVIESS, HANCOCK, HENDERSON, HOPKINS, LOGAN, MCLEAN,
MUHLENBERG, OHIO, TODD, UNION & WEBSTER COUNTIES:

	Rates	Fringes
PLUMBER/PIPEFITTER.....	\$ 29.22	12.65

TEAM0089-003 04/01/2012		

Zone 1: ALLEN, BUTLER, EDMONSON, LOGAN, SIMPSON, & WARREN
COUNTIES
Zone 2: BALLARD, CALLOWAY, CALDWELL, CARLISLE, CHRISTIAN,
CRITTENDEN, FULTON, GRAVES, HICKMAN, LIVINGSTON, LYON,
MARSHALL, MCCracken, TODD, & TRIGG COUNTIES
Zone 3: DAVIESS, HANCOCK, HENDERSON, HOPKINS, MCLEAN,
MUHLENBERG, OHIO, & WEBSTER COUNTIES

	Rates	Fringes
Truck drivers:		
Zone 1:		
Group 1.....	\$ 19.38	16.15
Group 2.....	\$ 19.56	16.15
Group 3.....	\$ 19.64	16.15
Group 4.....	\$ 19.66	16.15

Zone 2:		
Group 1.....	\$ 19.38	16.15
Group 2.....	\$ 19.56	16.15
Group 3.....	\$ 19.56	16.15
Group 4.....	\$ 19.66	16.15
Group 5.....	\$ 19.64	16.15
Zone 3:		
Group 1.....	\$ 19.38	16.15
Group 2.....	\$ 19.56	16.15
Group 3.....	\$ 19.56	16.15
Group 4.....	\$ 19.66	16.15

TRUCK DRIVER CLASSIFICATIONS FOR ZONE 1:

- GROUP 1 - Greaser; Tire Changer
- GROUP 2 - Truck Mechanic; Single Axle Dump; Flat Bed; All Terrain Vehicles when used to haul materials; Semi Trailer or Pole Trailer when used to pull building materials and equipment; Tandem Axle Dump; Driver of Distributors
- GROUP 3 - Mixer All Types
- GROUP 4 - Winch and A-Frame when used in transporting materials; Ross Carrier; Fork Lift when used to transport building materials; Driver on Pavement Breaker; Euclid and Other Heavy Earth Moving Equipment; Low Boy; Articulator Cat; Five Axle Vehicle

TRUCK DRIVER CLASSIFICATIONS FOR ZONE 2:

- GROUP 1 - Greaser; Tire Changer
- GROUP 2 - Truck Mechanic
- GROUP 3 - Single Axle Dump; Flat Bed; all Terrain Vehicles when used to haul materials; Semi Trailer or Pole Trailer when used to pull building materials and equipment; Tandem Axle Dump; Driver of Distributors
- GROUP 4 - Euclid and Other Heavy Earth Moving Equipment; Low Boy; Articulator Cat; Five Axle Vehicle; Winch and A-Frame when used in transporting materials; Ross Carrier

GROUP 5 - Mixer All Types

TRUCK DRIVER CLASSIFICATIONS FOR ZONE 3:

- GROUP 1 - Greaser, Tire Changer
- GROUP 2 - Truck Mechanic
- GROUP 3 - Single Axle Dump; Flat Bed; all Terrain Vehicle when used to haul materials; Semi Trailer or Pole Trailer when used to pull building materials and equipment; Tandem Axle Dump; Driver of Distributors; Mixer All Types
- GROUP 4 - Euclid and Other Heavy Earth moving Equipment;

Lowboy; Articulator Cat; 5 Axle Vehicle; Winch and A-Frame
when used in transporting materials; Ross Carrier; Fork
Lift when used to transport building materials; Driver on
Pavement Breaker

WELDERS - Receive rate prescribed for craft performing
operation to which welding is incidental.

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Unlisted classifications needed for work not included within
the scope of the classifications listed may be added after
award only as provided in the labor standards contract clauses
(29CFR 5.5 (a) (1) (ii)).

The body of each wage determination lists the classification
and wage rates that have been found to be prevailing for the
cited type(s) of construction in the area covered by the wage
determination. The classifications are listed in alphabetical
order of "identifiers" that indicate whether the particular
rate is union or non-union.

Union Identifiers

An identifier enclosed in dotted lines beginning with
characters other than "SU" denotes that the union
classification and rate have found to be prevailing for that
classification. Example: PLUM0198-005 07/01/2011. The
first four letters , PLUM, indicate the international union and
the four-digit number, 0198, that follows indicates the local
union number or district council number where applicable ,
i.e., Plumbers Local 0198. The next number, 005 in the
example, is an internal number used in processing the wage
determination. The date, 07/01/2011, following these
characters is the effective date of the most current
negotiated rate/collective bargaining agreement which would be
July 1, 2011 in the above example.

Union prevailing wage rates will be updated to reflect any
changes in the collective bargaining agreements governing the
rate.

Non-Union Identifiers

Classifications listed under an "SU" identifier were derived
from survey data by computing average rates and are not union
rates; however, the data used in computing these rates may
include both union and non-union data. Example: SULA2004-007
5/13/2010. SU indicates the rates are not union rates, LA
indicates the State of Louisiana; 2004 is the year of the
survey; and 007 is an internal number used in producing the
wage determination. A 1993 or later date, 5/13/2010, indicates
the classifications and rates under that identifier were issued
as a General Wage Determination on that date.

Survey wage rates will remain in effect and will not change until a new survey is conducted.

WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- * an existing published wage determination
- * a survey underlying a wage determination
- * a Wage and Hour Division letter setting forth a position on a wage determination matter
- * a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations
Wage and Hour Division
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board
U.S. Department of Labor
200 Constitution Avenue, N.W.
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

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END OF GENERAL DECISION

Fringe benefit amounts are applicable for all hours worked except when otherwise noted.

These rates are listed pursuant to the Kentucky Determination No. CR-III-I-HWY dated September 5, 2012.

No laborer, workman or mechanic shall be paid at a rate less than that of a Journeyman except those classified as bona fide apprentices.

Apprentices or trainees shall be permitted to work as such subject to Administrative Regulations adopted by the Commissioner of Workplace Standards. Copies of these regulations will be furnished upon request from any interested person.

Before using apprentices on the job the contractor shall present to the Contracting Officer written evidence of registration of such employees in a program of a State apprenticeship and training agency approved and recognized by the U. S. Bureau of Apprenticeship and Training. In the absence of such a State agency, the contractor shall submit evidence of approval and registration by the U. S. Bureau of Apprenticeship and Training.

The contractor shall submit to the Contracting Officer, written evidence of the established apprenticeship-journeyman ratios and wage rates in the project area, which will be the basis for establishing such ratios and rates for the project under the applicable contract provisions.

TO: EMPLOYERS/EMPLOYEES

PREVAILING WAGE SCHEDULE:

The wages indicated on this wage schedule are the least permitted to be paid for the occupations indicated. When an employee works in more than one classification, the employer must record the number of hours worked in each classification at the prescribed hourly base rate.

OVERTIME:

Overtime is to be paid after an employee works eight (8) hours a day or forty (40) hours a week, whichever gives the employee the greater wages. At least time and one-half the base rate is required for all overtime. A laborer, workman or mechanic and an employer may enter into a written agreement or a collective bargaining agreement to work more than eight (8) hours a calendar day but not more than ten (10) hours a calendar day for the straight time hourly rate. Wage violations or questions should be directed to the designated Engineer or the undersigned.

Ryan Griffith, Director
Division of Construction Procurement
Frankfort, Kentucky 40622

PART IV

INSURANCE

INSURANCE

The Contractor shall procure and maintain the following insurance in addition to the insurance required by law:

- 1) Commercial General Liability-Occurrence form – not less than \$2,000,000 General aggregate, \$2,000,000 Products & Completed Aggregate, \$1,000,000 Personal & Advertising, \$1,000,000 each occurrence.
- 2) Automobile Liability- \$1,000,000 per accident
- 3) Employers Liability:
 - a) \$100,000 Each Accident Bodily Injury
 - b) \$500,000 Policy limit Bodily Injury by Disease
 - c) \$100,000 Each Employee Bodily Injury by Disease
- 4) The insurance required above must be evidenced by a Certificate of Insurance and this Certificate of Insurance must contain one of the following statements:
 - a) "policy contains no deductible clauses."
 - b) "policy contains _____ (amount) deductible property damage clause but company will pay claim and collect the deductible from the insured."
- 5) KENTUCKY WORKMEN'S COMPENSATION INSURANCE. The contractor shall furnish evidence of coverage of all his employees or give evidence of self-insurance by submitting a copy of a certificate issued by the Workmen's Compensation Board.

The cost of insurance is incidental to all contract items. All subcontractors must meet the same minimum insurance requirements.

PART V

BID ITEMS

KENTUCKY TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS
FRANKFORT, KY 40622

CONTRACT ID: 121345
COUNTY: LIVINGSTON
PROPOSAL: FD04 SPP 070 0024 030-034

PAGE: 1
LETTING: 09/14/12
CALL NO: 320

LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
SECTION 0001 ROADWAY					
0010	00001	DGA BASE	1,500.000 TON		
0020	00078	CRUSHED AGGREGATE SIZE NO 2	24.000 TON		
0030	00100	ASPHALT SEAL AGGREGATE	888.000 TON		
0040	00103	ASPHALT SEAL COAT	107.000 TON		
0050	00312	CL3 ASPH SURF 0.50D PG64-22	2,841.000 TON		
0060	00335	CL4 ASPH SURF 0.50A PG76-22	10,298.000 TON		
0070	00464	CULVERT PIPE-24 IN	24.000 LF		
0080	01010	NON-PERFORATED PIPE-4 IN	508.000 LF		
0090	01020	PERF PIPE HEADWALL TY 1-4 IN	9.000 EACH		
0100	01028	PERF PIPE HEADWALL TY 3-4 IN	7.000 EACH		
0110	01032	PERF PIPE HEADWALL TY 4-4 IN	8.000 EACH		
0120	01490	DROP BOX INLET TYPE 1	1.000 EACH		
0130	01505	DROP BOX INLET TYPE 5B	1.000 EACH		
0140	01877	SPECIAL HEADER CURB	7,376.000 LF		
0150	01890	ISLAND HEADER CURB TYPE 1	150.000 LF		
0160	01904	REMOVE CURB	7,526.000 LF		
0170	01982	DELINEATOR FOR GUARDRAIL MONO DIRECTIONAL WHITE	114.000 EACH		
0180	01983	DELINEATOR FOR GUARDRAIL MONO DIRECTIONAL YELLOW	40.000 EACH		
0190	01984	DELINEATOR FOR BARRIER - WHITE	4.000 EACH		
0200	01985	DELINEATOR FOR BARRIER - YELLOW	4.000 EACH		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
0210	02220	FLOWABLE FILL	17.000 CUYD		
0220	02237	DITCHING	33,681.000 LF		
0230	02352	GUARDRAIL-STEEL W BEAM-D FACE	625.000 LF		
0240	02363	GUARDRAIL CONNECTOR TO BRIDGE END TY A	10.000 EACH		
0250	02365	CRASH CUSHION TYPE IX-A	4.000 EACH		
0260	02367	GUARDRAIL END TREATMENT TYPE 1	1.000 EACH		
0270	02369	GUARDRAIL END TREATMENT TYPE 2A	9.000 EACH		
0280	02381	REMOVE GUARDRAIL	13,712.500 LF		
0290	02387	GUARDRAIL CONNECTOR TO BRIDGE END TY A-1	7.000 EACH		
0300	02391	GUARDRAIL END TREATMENT TYPE 4A	3.000 EACH		
0310	02483	CHANNEL LINING CLASS II	1,278.000 TON		
0320	02484	CHANNEL LINING CLASS III	388.000 TON		
0330	02562	SIGNS	500.000 SQFT		
0340	02599	FABRIC-GEOTEXTILE TYPE IV	100.000 SQYD		
0350	02650	MAINTAIN & CONTROL TRAFFIC	(1.00) LS		
0360	02671	PORTABLE CHANGEABLE MESSAGE SIGN	4.000 EACH		
0370	02676	MOBILIZATION FOR MILL & TEXT	(1.00) LS		
0380	02677	ASPHALT PAVE MILLING & TEXTURING	13,139.000 TON		
0390	02696	SHOULDER RUMBLE STRIPS-SAWED	82,937.000 LF		
0400	02714	SHOULDERING	15,000.000 LF		
0410	02775	ARROW PANEL	2.000 EACH		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
0420	03262	CLEAN PIPE STRUCTURE	1.000 EACH		
0430	05950	EROSION CONTROL BLANKET	54,090.000 SQYD		
0440	06407	SBM ALUM SHEET SIGNS .125 IN	100.000 SQFT		
0450	06412	STEEL POST MILE MARKERS	6.000 EACH		
0460	06417	FLEXIBLE DELINEATOR POST-W	172.000 EACH		
0470	06418	FLEXIBLE DELINEATOR POST-Y	7.000 EACH		
0480	06549	PAVE STRIPING-TEMP REM TAPE-B	7,200.000 LF		
0490	06550	PAVE STRIPING-TEMP REM TAPE-W	7,200.000 LF		
0500	06551	PAVE STRIPING-TEMP REM TAPE-Y	3,600.000 LF		
0510	06592	PAVEMENT MARKER TYPE V-B W/R	543.000 EACH		
0520	06593	PAVEMENT MARKER TYPE V-B Y/R	103.000 EACH		
0530	06600	REMOVE PAVEMENT MARKER TYPE V	491.000 EACH		
0540	10020NS	FUEL ADJUSTMENT	17,963.000 DOLL	1.00	17,963.00
0550	10030NS	ASPHALT ADJUSTMENT	31,654.000 DOLL	1.00	31,654.00
0560	20071EC	JOINT ADHESIVE	67,720.000 LF		
0570	20191ED	OBJECT MARKER TY 3	6.000 EACH		
0580	20366NN	REPLACE GRATE	8.000 EACH		
0590	20411ED	LAW ENFORCEMENT OFFICER	2,000.000 HOUR		
0600	20757ED	PAVEMENT REPAIR (MAJOR)	168.000 SQYD		
0610	20757ED	PAVEMENT REPAIR (MINOR)	3,108.000 SQYD		
0620	21533EN	EMBANKMENT	34.000 CUYD		

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LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
0630	21802EN	G/R STEEL W BEAM-S FACE (7 FT POST)	12,825.000 LF		
0640	23143ED	KPDES PERMIT AND TEMP EROSION CONTROL	(1.00) LS		
0650	24189ER	DURABLE WATERBORNE MARKING-6 IN W	47,737.000 LF		
0660	24190ER	DURABLE WATERBORNE MARKING-6 IN Y	38,111.000 LF		
0670	24191ER	DURABLE WATERBORNE MARKING-12 IN W	1,194.000 LF		
SECTION 0002 BRIDGE					
0680	03298	EXPAN JOINT REPLACE 4 IN	172.000 LF		
0690	08020	CRUSHED AGGREGATE SLOPE PROT	20.000 TON		
0700	22146EN	CONCRETE PATCHING REPAIR	440.000 SQFT		
SECTION 0003 MOBILIZATION / DEMOBILIZATION					
0710	02568	MOBILIZATION (NO MORE THAN 5%)	LUMP		
0720	02569	DEMOBILIZATION (AT LEAST 1.5%)	LUMP		
		TOTAL BID			