



CALL NO. 315

CONTRACT ID. 121046

ROCKCASTLE COUNTY

FED/STATE PROJECT NUMBER FD04 SPP 102 0075 055-059

DESCRIPTION TENNESSEE STATE LINE-LEXINGTON ROAD(I-75)

WORK TYPE ASPHALT REHAB INTERSTATE/PARKWAY

PRIMARY COMPLETION DATE 5/31/2013

LETTING DATE: October 19,2012

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME October 19,2012. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

NO PLANS ASSOCIATED WITH THIS PROJECT.

REQUIRED BID PROPOSAL GUARANTY: Not less than 5% of the total bid.

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PART III	EMPLOYMENT, WAGE AND RECORD REQUIREMENTS <ul style="list-style-type: none">• LABOR AND WAGE REQUIREMENTS• EXECUTIVE BRANCH CODE OF ETHICS• KENTUCKY EQUAL EMPLOYMENT OPPORTUNITY ACT OF 1978 LOCALITY 1,2,3,4 / STATE (OVER 250,000)• PROJECT WAGE RATES LOCALITY 2 / STATE (OVER 250,000)
PART IV	INSURANCE
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PART I

SCOPE OF WORK

ADMINISTRATIVE DISTRICT - 08

CONTRACT ID - 121046

COUNTY -

PCN - DE10200751246

FD04 SPP 102 0075 055-059

TENNESSEE STATE LINE-LEXINGTON ROAD (I-75) MILL AND INTERMEDIATE OVERLAY ON I-75 SOUTHBOUND
FROM MP 55.744 TO MP 58.954.ASPHALT REHAB INTERSTATE/PARKWAY SYP NO. 08-02012.00.
GEOGRAPHIC COORDINATES LATITUDE 37:19:05.00 LONGITUDE 84:17:36.00

COMPLETION DATE(S):

COMPLETED BY 05/31/2013
APPLIES TO ENTIRE CONTRACT

CONTRACT NOTES

PROPOSAL ADDENDA

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

BID SUBMITTAL

Bidder must use the Department's Expedite Bidding Program available on the Internet web site of the Department of Highways, Division of Construction Procurement. (www.transportation.ky.gov/contract)

The Bidder must download the bid file located on the Bid Express website (www.bidx.com) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

JOINT VENTURE BIDDING

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

UNDERGROUND FACILITY DAMAGE PROTECTION

The contractor is advised that the Underground Facility Damage Protection Act of 1994, became law January 1, 1995. It is the contractor's responsibility to determine the impact of the act regarding this project, and take all steps necessary to be in compliance with the provision of the act.

SPECIAL NOTE FOR PIPE INSPECTION

Contrary to Section 701.03.08 of the 2012 Standard Specifications for Road and Bridge Construction and Kentucky Method 64-114, certification by the Kentucky Transportation Center for prequalified Contractors to perform laser/video inspection is not required on this contract. It will continue to be a requirement for the Contractor performing any laser/video pipe inspection to be prequalified for this specialized item with the Kentucky Transportation Cabinet-Division of Construction Procurement.

REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth (“certificate”) from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity’s solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by fax (502) 564-7299 or email to kytc.projectquestions@ky.gov. The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading “Questions & Answers” on the Construction Procurement website (www.transportation.ky.gov/contract). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

INSTRUCTIONS FOR EXCESS MATERIAL SITES AND BORROW SITES

Identification of excess material sites and borrow sites shall be the responsibility of the Contractor. The Contractor shall be responsible for compliance with all applicable state and federal laws and may wish to consult with the US Fish and Wildlife Service to seek protection under Section 10 of the Endangered Species Act for these activities.

ACCESS TO RECORDS

The contractor, as defined in KRS 45A.030 (9) agrees that the contracting agency, the Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this contract for the purpose of financial audit or program review. Records and other prequalification information confidentially disclosed as part of the bid process shall not be deemed as directly pertinent to the contract and shall be exempt from disclosure as provided in KRS 61.878(1)(c). The contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884.

In the event of a dispute between the contractor and the contracting agency, Attorney General, or the Auditor of Public Accounts over documents that are eligible for production and review, the Finance and Administration Cabinet shall review the dispute and issue a determination, in accordance with Secretary's Order 11-004. (See attachment)

09/26/2012



Steven L. Beshear
Governor

Commonwealth of Kentucky
Finance and Administration Cabinet
OFFICE OF THE SECRETARY
Room 383, Capitol Annex
702 Capital Avenue
Frankfort, KY 40601-3462
(502) 564-4240
Fax (502) 564-6785

Lori H. Flanery
Secretary

SECRETARY'S ORDER 11-004

FINANCE AND ADMINISTRATION CABINET

Vendor Document Disclosure

WHEREAS, in order to promote accountability and transparency in governmental operations, the Finance and Administration Cabinet believes that a mechanism should be created which would provide for review and assistance to an Executive Branch agency if said agency cannot obtain access to documents that it deems necessary to conduct a review of the records of a private vendor that holds a contract to provide goods and/or services to the Commonwealth; and

WHEREAS, in order to promote accountability and transparency in governmental operations, the Finance and Administration Cabinet believes that a mechanism should be created which would provide for review and assistance to an Executive Branch agency if said agency cannot obtain access to documents that it deems necessary during the course of an audit, investigation or any other inquiry by an Executive Branch agency that involves the review of documents; and

WHEREAS, KRS 42.014 and KRS 12.270 authorizes the Secretary of the Finance and Administration Cabinet to establish the internal organization and assignment of functions which are not established by statute relating to the Finance and Administration Cabinet; further, KRS Chapter 45A.050 and 45A.230 authorizes the Secretary of the Finance and Administration Cabinet to procure, manage and control all supplies and services that are procured by the Commonwealth and to intervene in controversies among vendors and state agencies; and

NOW, THEREFORE, pursuant to the authority vested in me by KRS 42.014, KRS 12.270, KRS 45A.050, and 45A.230, I, Lori H. Flanery, Secretary of the Finance and Administration Cabinet, do hereby order and direct the following:

- I. Upon the request of an Executive Branch agency, the Finance and Administration Cabinet ("FAC") shall formally review any dispute arising where the agency has requested documents from a private vendor that holds a state contract and the vendor has refused access to said documents under a claim that said documents are not directly pertinent or relevant to the agency's inquiry upon which the document request was predicated.
- II. Upon the request of an Executive Branch agency, the FAC shall formally review any situation where the agency has requested documents that the agency deems necessary to

conduct audits, investigations or any other formal inquiry where a dispute has arisen as to what documents are necessary to conclude the inquiry.

- III. Upon receipt of a request by a state agency pursuant to Sections I & II, the FAC shall consider the request from the Executive Branch agency and the position of the vendor or party opposing the disclosure of the documents, applying any and all relevant law to the facts and circumstances of the matter in controversy. After FAC's review is complete, FAC shall issue a Determination which sets out FAC's position as to what documents and/or records, if any, should be disclosed to the requesting agency. The Determination shall be issued within 30 days of receipt of the request from the agency. This time period may be extended for good cause.
- IV. If the Determination concludes that documents are being wrongfully withheld by the private vendor or other party opposing the disclosure from the state agency, the private vendor shall immediately comply with the FAC's Determination. Should the private vendor or other party refuse to comply with FAC's Determination, then the FAC, in concert with the requesting agency, shall effectuate any and all options that it possesses to obtain the documents in question, including, but not limited to, jointly initiating an action in the appropriate court for relief.
- V. Any provisions of any prior Order that conflicts with the provisions of this Order shall be deemed null and void.

SPECIAL NOTE FOR RECIPROCAL PREFERENCE

Reciprocal preference to be given by public agencies to resident bidders

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the Expedite Bidding Program. Submittal of the Affidavit should be done along with the bid in Bid Express.

03/01/2011

ASPHALT MIXTURE

Unless otherwise noted, the Department estimates the rate of application for all asphalt mixtures to be 110 lbs/sy per inch of depth.

DGA BASE

Unless otherwise noted, the Department estimates the rate of application for DGA Base to be 115 lbs/sy per inch of depth.

DGA BASE FOR SHOULDERS

Unless otherwise noted, the Department estimates the rate of application for DGA Base for Shoulders to be 115 lbs/sy per inch of depth. The Department will not measure necessary grading and/or shaping of existing shoulders prior to placing of DGA Base, but shall be incidental to the Contract unit price per ton for DGA Base.

Accept payment at the Contract unit price per ton as full compensation for all labor, materials, equipment, and incidentals for grading and/or shaping of existing shoulders and furnishing, placing, and compacting the DGA Base.

INCIDENTAL SURFACING

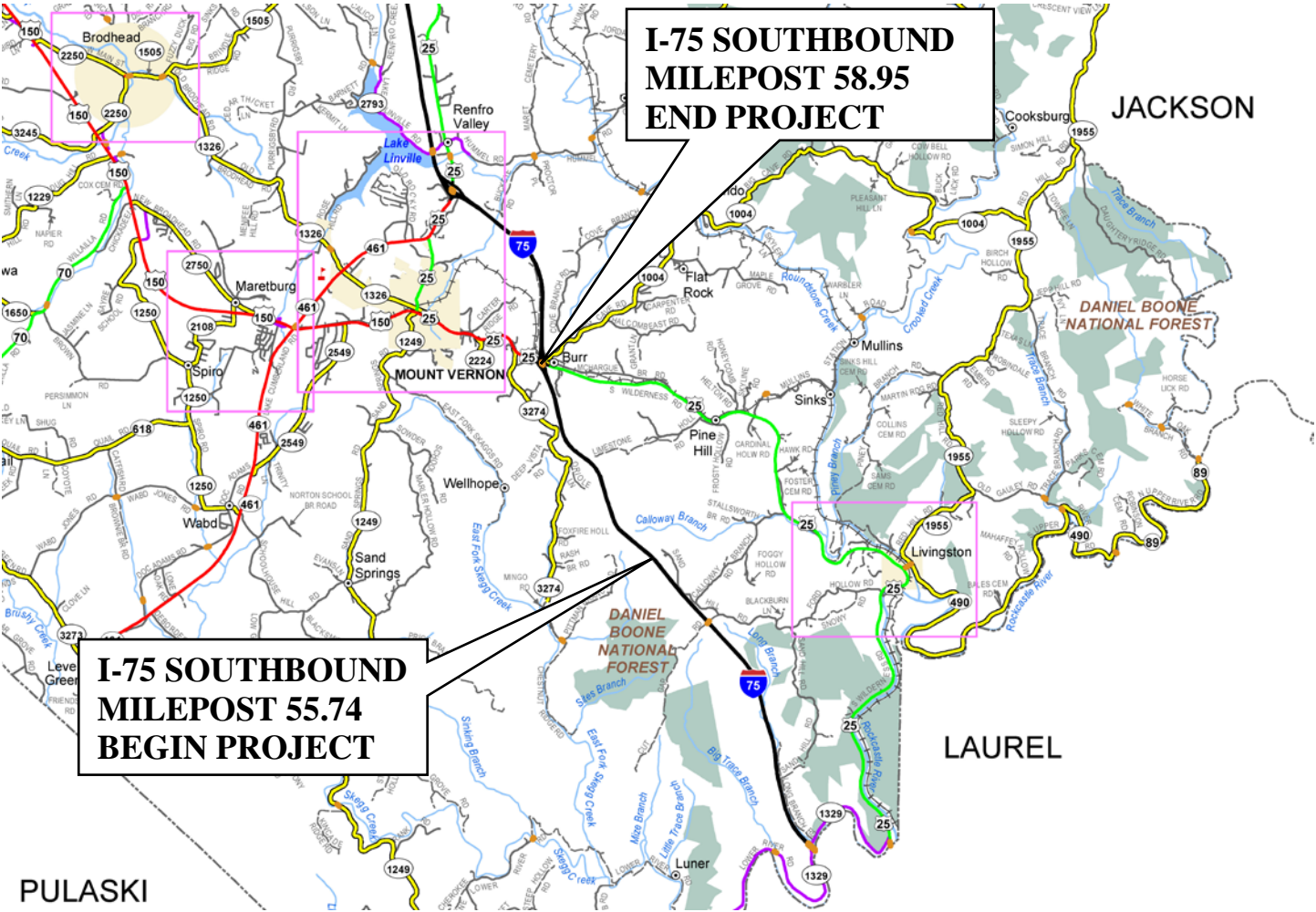
The Department has included in the quantities of asphalt mixtures established in the proposal estimated quantities required for resurfacing or surfacing mailbox turnouts, farm field entrances, residential and commercial entrances, curve widening, ramp gores and tapers, and road and street approaches, as applicable. Pave these areas to the limits as shown on Standard Drawing RPM-110-06 or as directed by the Engineer. In the event signal detectors are present in the intersecting streets or roads, pave the crossroads to the right of way limit or back of the signal detector, whichever is the farthest back of the mainline. Surface or resurface these areas as directed by the Engineer. The Department will not measure placing and compacting for separate payment but shall be incidental to the Contract unit price for the asphalt mixtures.

FUEL AND ASPHALT PAY ADJUSTMENT

The Department has included the Contract items Asphalt Adjustment and Fuel Adjustment for possible future payments at an established Contract unit price of \$1.00. The Department will calculate actual adjustment quantities after work is completed. If existing Contract amount is insufficient to pay all items on the contract with the adjustments, the Department will establish additional monies with a change order.

OPTION A

Be advised that the Department will accept compaction of asphalt mixtures furnished for driving lanes and ramps, at 1 inch (25mm) or greater, on this project according to OPTION A in accordance with Section 402 and Section 403 of the current Standard Specifications. The Department will require joint cores as described in Section 402.03.02 for surface mixtures only. The Department will accept compaction of all other asphalt mixtures according to OPTION B.



ITEM NUMBERS: 8-2012.00

PROJECT NUMBER: FD04 102 0075 055-059

CONSTRUCTION NUMBER:

LETTING DATE:

RECOMMENDED BY: Project Manager DATE:

PLAN APPROVED BY: State Highway Engineer DATE:

FHWA APPROVED BY: DATE:

MATCHLINE (SEE SHEET 2)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00

2785+00

PT 2784+74.90

2780+00

BEGIN PROJECT
M.P. 55.744

PI 2778+39.78

2775+00

PC 2771+99.37

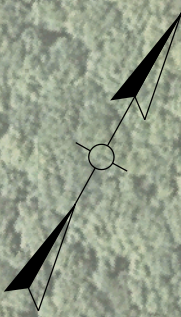
2770+00

SCALE: 1"=200'

I-75 - SOUTHBOUND
STATIONING INFORMATION SHEETS
(1 OF 9)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00

MATCHLINE (SEE SHEET 3)



MATCHLINE (SEE SHEET 1)

SCALE: 1"=200'

**I-75 - SOUTHBOUND
STATIONING INFORMATION SHEETS
(2 OF 9)**

MATCHLINE (SEE SHEET 4) 2830+00

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00

2825+00

2820+00

SB I-75

NB I-75

ST 2821+16.25

CS 2817+16.25

2815+00

PI 2813+94.21

SC 2810+40.96

SCALE: 1"=200'

MATCHLINE (SEE SHEET 2)

I-75 - SOUTHBOUND
STATIONING INFORMATION SHEETS
(3 OF 9)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00



MATCHLINE (SEE SHEET 3)

SCALE: 1"=200'
**I-75 - SOUTHBOUND
STATIONING INFORMATION SHEETS
(4 OF 9)**

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00



MATCHLINE (SEE SHEET 7)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00

2895+00

SB I-75

NB I-75

PI 2890+61.75
2890+00

2885+00

2880+00

SC 2881+12.53

TS 2878+62.53

SCALE: 1"=200'

MATCHLINE (SEE SHEET 5)

I-75 - SOUTHBOUND
STATIONING INFORMATION SHEETS
(6 OF 9)

MATCHLINE (SEE SHEET 8)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00



SCALE: 1"=200'

**I-75 - SOUTHBOUND
STATIONING INFORMATION SHEETS
(7 OF 9)**

MATCHLINE (SEE SHEET 9)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00



2935+00 PT 2935+19.61

2930+00

SB I-75
NB I-75

PI 2928+98.68

2925+00

PC 2922+72.80

2920+00

SCALE: 1"=200'

I-75 - SOUTHBOUND
STATIONING INFORMATION SHEETS
(8 OF 9)

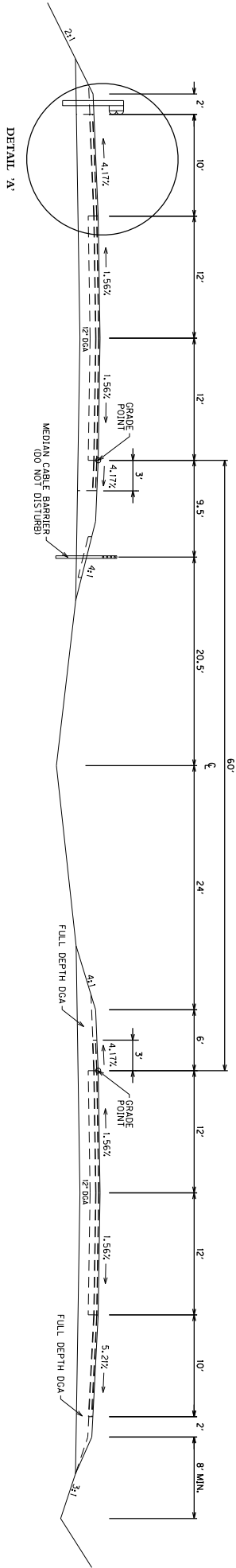
MATCHLINE (SEE SHEET 7)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00



COUNTY OF	ITEM NO.	SHEET NO.
ROCKCASTLE	8-2012.00	

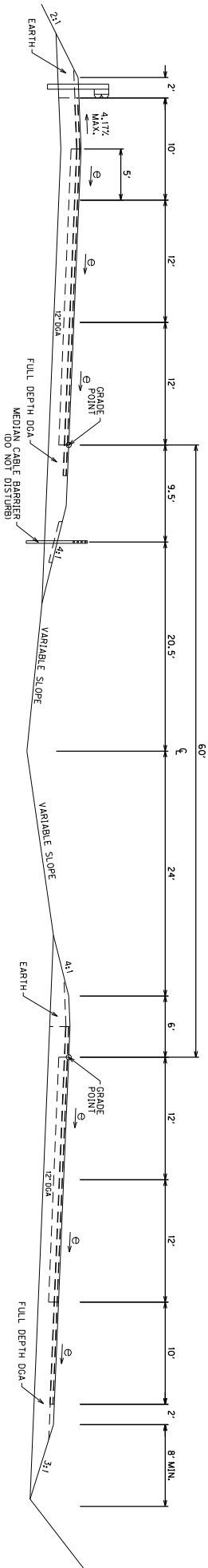
I-75 EXISTING TYPICAL SECTIONS



SOUTHBOUND LANES

NORMAL SECTION

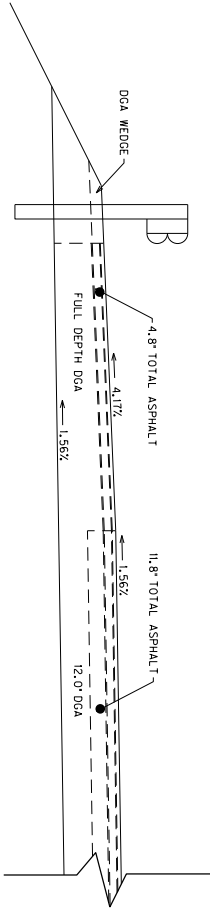
NORTHBOUND LANES



SOUTHBOUND LANES

SUPERELEVATED SECTION

NORTHBOUND LANES

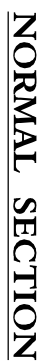


DETAIL 'A'

TYPICAL SECTIONS

I-75

COUNTY OF	ITEM NO.	SHEET NO.
ROCKCASTLE	8-2012.00	



PAVEMENT REHABILITATION (NORTHBOUND ONLY)

DRIVING LANES & INSIDE SHOULDERS

1.5" ASPHALT MILLING AND TEXTURING
VARIABLE DEPTH DGA WEDGE INSIDE SHOULDER
BASE ----- 3.5" CL4 ASPHALT BASE 1.00 PG66-22
SURFACE -- 1.25" CL4 ASPHALT SURFACE 0.384 PG66-22

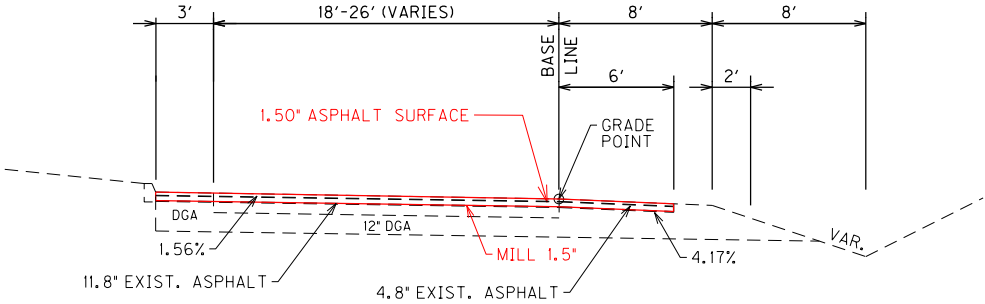
OUTSIDE SHOULDER

1.5" ASPHALT MILLING AND TEXTURING
VARIABLE DEPTH DGA WEDGE OUTSIDE SHOULDER
BASE ----- 3.5" CL3 ASPHALT BASE 1.00 PG64-22
SURFACE -- 1.25" CL3 ASPHALT SURFACE 0.380 PG64-22

*CROSS SLOPE CORRECTION IS TO BE CONSTRUCTED IN BASE COURSE WITH LEVELING AND WEDGING.

COUNTY OF	ITEM NO.
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TYPICAL SECTIONS RAMPS



RAMP NORMAL SECTION

DRIVING LANES AND SHOULDERS
1.50" ASPHALT MILLING AND TEXTURING
SURFACE -- 1.50" CL3 ASPHALT SURFACE 0.38D PG64-22

Diagram illustrating the cross-section of a 2:1 fill slope with guardrail, showing existing and proposed layers, slopes, and dimensions.

EXISTING 2:1 SLOPE

PROPOSED DGA WEDGE

EXISTING DGA WEDGE

EXISTING FULL DEPTH DGA

APPROX. 1:8.1

MINIMUM WIDTH

4.8' EXIST' ASPHALT PMWT

LEVELING AND WEDGING

1.25' ASPHALT SURFACE

3.50' ASPHALT BASE

LEVELING AND WEDGING

2.00% (NC)

4.00%

5.21%

MILL 1.5'

MILL 1.5'

11.8' EXIST' ASPHALT PMWT

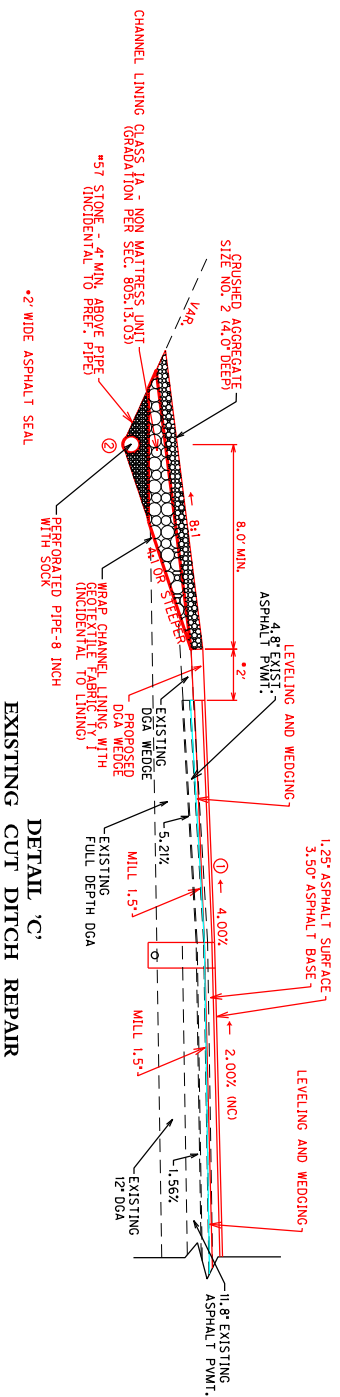
11.56'

EXISTING 12' DGA

DETAIL 'B'

2:1 FILL SLOPES WITH GUARDRAIL

DETAIL 'B'
2:1 FILL SLOPES WITH GUARDRAIL

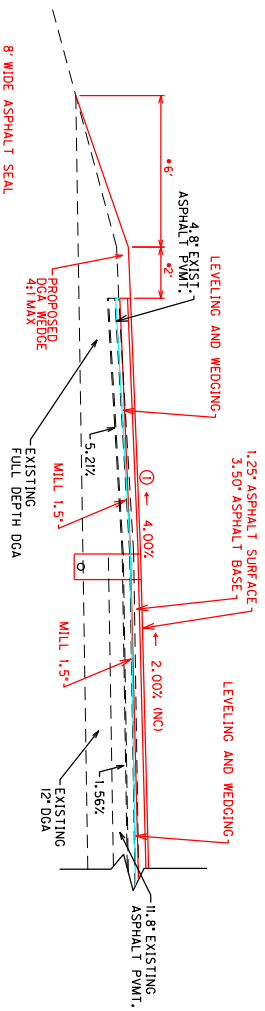


DETAIL 'C'
EXISTING CUT DITCH REPAIR

ASPHALT SEAL
2 APPLICATIONS:
ASPHALT SEAL AGGREGATE - 20 LB/SY
ASPHALT SEAL COAT - 2.4 LB/SY

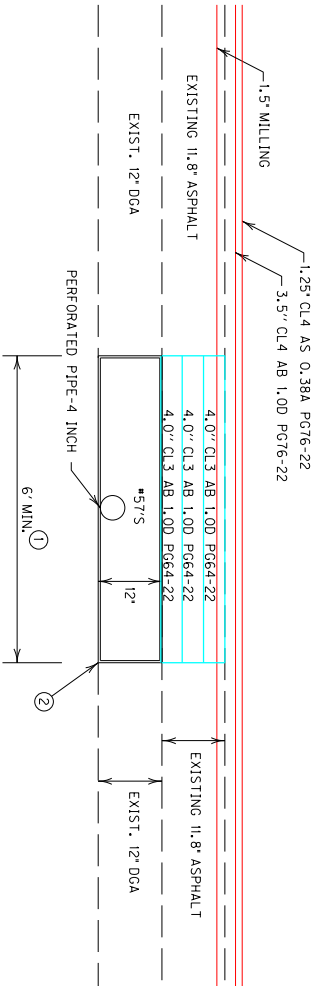
- ① 4.0% MAX. SHOULDER CROSS SLOPE ON SUPERELEVATION (6.00% MAX. ROLL-OVER).
- ② PRIOR TO PLACING THE PERFORATED PIPE, PREPARE THE CHANNEL, INCLUDING REMOVAL OF VEGETATION, LOOSE ROCK, AND OTIE DEBRIS. THE DEARTHMENT WILL NOT MEASURE. PREPARATION OF CHANNEL AND IS CONSIDERED INCIDENTAL TO DITCH REPAIR.

DETAIL 'D'
EXISTING LOCATIONS NOT OTHERWISE SPECIFIED



- ① 4.0% MAX. SHOULDER CROSS SLOPE ON SUPERELEVATION (6.00% MAX. ROLL-OVER).
- ② PRIOR TO PLACING THE PERFORATED PIPE, PREPARE THE CHANNEL, INCLUDING REMOVAL OF VEGETATION, LOOSE ROCK, AND OTIE DEBRIS. THE DEARTHMENT WILL NOT MEASURE. PREPARATION OF CHANNEL AND IS CONSIDERED INCIDENTAL TO DITCH REPAIR.

COUNTY OF	ITEM NO.	SHEET NO.
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① APPROXIMATE LOCATIONS ARE NOTED IN THE PROPOSAL. SPECIFIC LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. BEFORE REMOVAL OF THE EXISTING MATERIAL, SAW CUTS SHALL BE MADE ON EACH SIDE OF THE AREA TO BE REMOVED. THIS OPERATION SHALL BE PERFORMED ON THE OUTSIDE SHOULDER AND DRIVING LANES WHERE DIRECTED.

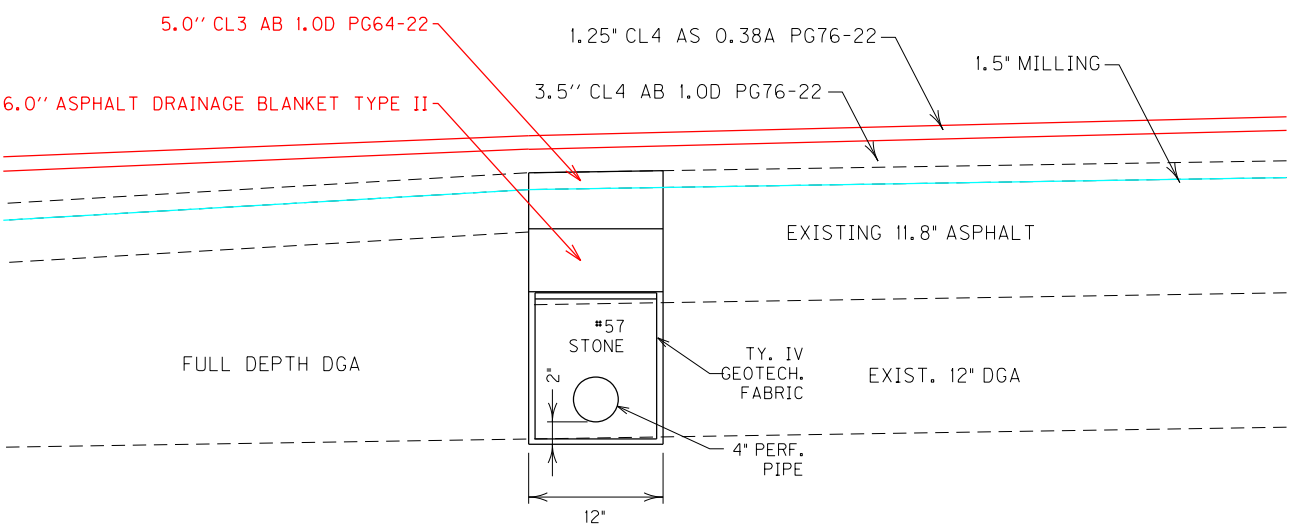
AFTER ALL EXISTING MATERIAL HAS BEEN REMOVED THEN EACH COURSE OF BACKFILL MATERIAL SHALL BE COMPACTED TO THE PROPER DENSITY FOR THE MATERIAL BEING PLACED AS REQUIRED IN THE STANDARD SPECIFICATIONS. THE 4" PERFORATED PIPE SHALL NOT BE WRAPPED AND ONLY COARSE AGGREGATE SHALL BE USED.

THE CONTRACT UNIT BID PRICE PER SQ YD FOR PAVEMENT REPAIR SHALL INCLUDE ROADWAY EXCAVATION, REMOVING PAVEMENT, SAW CUTTING, DCA, PERFORATED PIPE-4 INCH, CRUSHED AGGREGATE SIZE NO. 57, ASPHALT BASE, FABRIC-GEOTEXTILE TYPE IV AND ALL INCIDENTALS NECESSARY TO COMPLETE THE INSTALLATION AS DETAILED.

PLACE THE PERFORATED PIPE AT THE LOW POINT OF THE DISCUT.

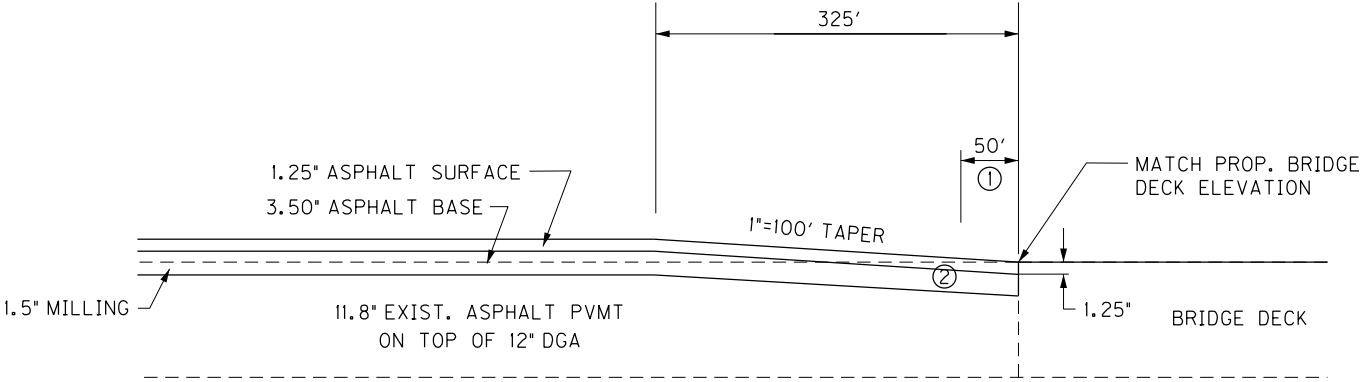
THE NON-PERFORATED OUTLET PIPE AND HEADWALL WILL BE PAID FOR SEPARATELY. ANY EXCAVATION OR BACKFILL REQUIRED WILL BE INCIDENTAL TO THE LENGTH OF NON-PERFORATED PIPE.

② FABRIC-GEOTEXTILE TYPE IV REQUIRED AROUND COARSE AGGREGATE. OVERLAP FABRIC ONE FOOT.



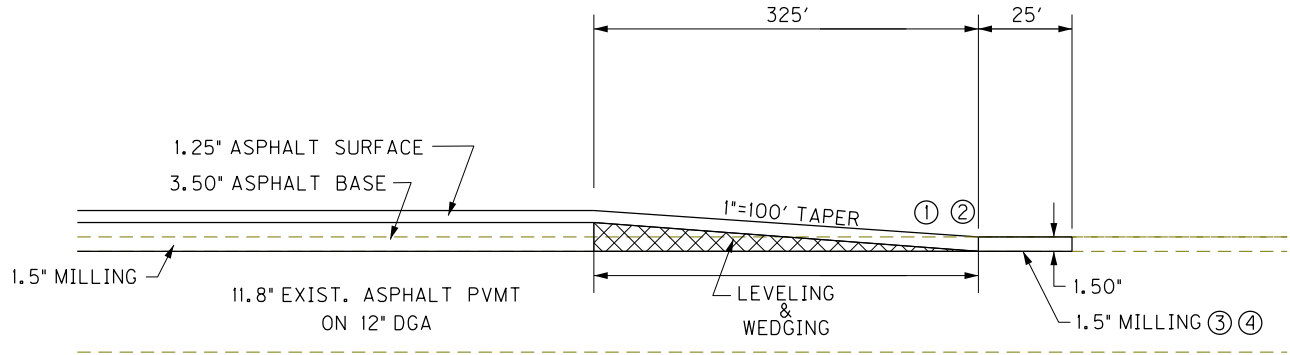
① **PERFORATED PIPE EDGE DRAIN DETAIL**

① THE FOLLOWING ITEMS ARE INCIDENTAL TO EDGE DRAIN PIPE INSTALLATION (SEE KYTC SPECS-SECT. 704); TRENCH EXCAVATION, MATERIALS FOR BEDDING AND BACKFILL, AND GEOTEXTILE FABRIC FOR WRAPPING PERFORATED PIPE OR FOR WRAPPING COARSE AGGREGATE BACKFILL.



TAPERING OF OVERLAYS AT BRIDGE END (NORTH TERMINUS)

- ① TRANSITION ROADWAY CROSS SLOPE TO MATCH EXIST. BRIDGE DECK CROSS SLOPE
- ② MILL EXISTING PAVEMENT TO RECEIVE PROPOSED OVERLAY



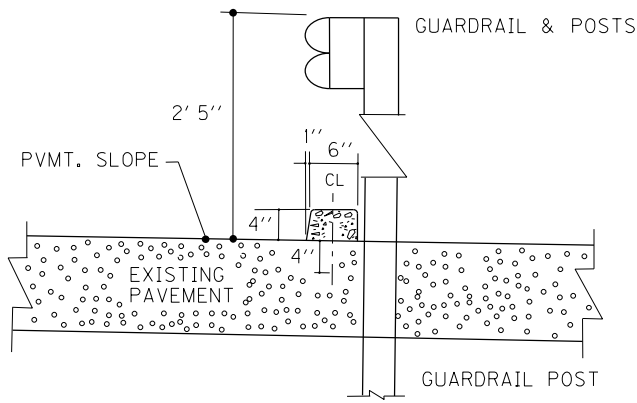
TAPERING OF OVERLAYS AT SOUTH TERMINUS AND RAMP TIE-IN

- ① TRANSITION ROADWAY CROSS SLOPE TO MATCH EXIST. CROSS SLOPE
- ② MILL EXISTING PAVEMENT TO RECEIVE PROPOSED OVERLAY
- ③ U.S. 25 ENTRANCE RAMP TO BE MILLED 1.5" AND OVERLAID WITH 1.5" ASPHALT SURFACE
- ④ I-75 SOUTH TERMINUS TO BE MILLED 1.5" FOR A LENGTH OF 25'

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2012.00

CROSS SECTION VIEW

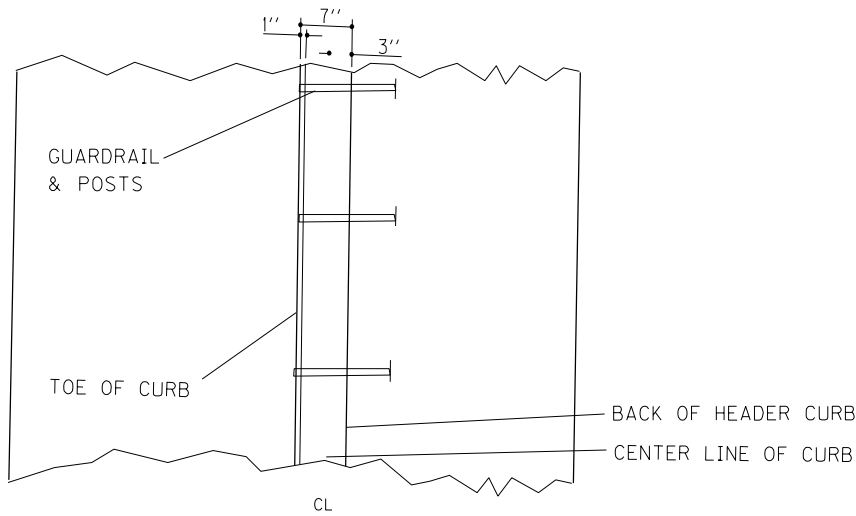
SPECIAL CONCRETE
HEADER CURB



NOT TO SCALE

PLAN VIEW

SPECIAL CONCRETE
HEADER CURB



NOT TO SCALE

I-75 SOUTHBOUND ROCKCASTLE COUNTY PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95 ITEM NO. 8-2012.00 GENERAL SUMMARY			
ITEM NUMBER	ITEM	QUANTITY	UNIT
1	DGA BASE	2806	TON
18	DRAINAGE BLANKET-TYPE II-ASPH (9)	573	TON
78	CRUSHED AGGREGATE SIZE NO. 2 (1)	2753	TON
100	ASPHALT SEAL AGGREGATE	356	TON
103	ASPHALT SEAL COAT	43	TON
194	LEVELING AND WEDGING PG76-22	195	TON
214	CL3 ASPH BASE 1.0D PG64-22	4518	TON
214	CL3 ASPH BASE 1.0D PG64-22 (9)	526	TON
219	CL4 ASPH BASE 1.0D PG76-22	11269	TON
339	CL3 ASPH SURF 0.38D PG64-22	1666	TON
342	CL4 ASPH SURF 0.38A PG76-22	3744	TON
1000	PERFORATED PIPE - 4 INCH (9)	7865	LIN FT
1001	PERFORATED PIPE - 6 INCH (9)	8535	LIN FT
1002	PERFORATED PIPE - 8 INCH (11)	9635	LIN FT
1010	NON-PERFORATED PIPE - 4 INCH	140	LIN FT
1011	NON-PERFORATED PIPE - 6 INCH	112	LIN FT
1012	NON-PERFORATED PIPE - 8 INCH	120	LIN FT
1015	INSPECT & CERTIFY EDGE DRAIN SYSTEM	1	LUMP SUM
1020	PERF PIPE HEADWALL TY 1-4 IN	15	EACH
1021	PERF PIPE HEADWALL TY 1-6 IN	7	EACH
1022	PERF PIPE HEADWALL TY 1-8 IN	10	EACH
1029	PERF PIPE HEADWALL TY 3-6 IN	1	EACH
1484	CURB BOX INLET TYPE B - T (2)	1	EACH
1719	ADJUST INLET	2	EACH
1890	ISLAND HEADER CURB TYPE 1 (3)	20	LIN FT
1904	REMOVE CURB (3)	20	LIN FT
1982	DELINEATOR FOR GUARDRAIL MONO DIRECTIONAL - WHITE	84	EACH
2014	BARRICADE TYPE III (4)	6	EACH
2230	EMBANKMENT IN PLACE (12)	500	CU YD
2237	DITCHING (5)	11000	LIN FT
2367	GUARDRAIL END TREATMENT TYPE 1	4	EACH
2369	GUARDRAIL END TREATMENT TYPE 2A	6	EACH
2373	GUARDRAIL END TREATMENT TYPE 3	1	EACH
2387	GUARDRAIL CONNECTOR TO BRIDGE END TYPE A-1	1	EACH
2381	REMOVE GUARDRAIL	7962.5	LIN FT
2482	CHANNEL LINING CLASS IA (6) (8)	3800	TON
2484	CHANNEL LINING CLASS III (8) (13)	100	TON
2562	SIGNS (7)	500	SQ FT
2568	MOBILIZATION	1	LUMP SUM
2569	DEMOBILIZATION	1	LUMP SUM
2599	FABRIC - GEOTEXTILE TYPE IV (14)	500	SQ YD
2650	MAINTAIN AND CONTROL TRAFFIC	1	LUMP SUM
2671	PORTABLE CHANGEABLE MESSAGE SIGN (4)	2	EACH
2676	MOBILIZATION FOR MILLING & TEXTURING	1	LUMP SUM
2677	ASPHALT PAVE MILLING & TEXTURING	7238	TON
2696	SHOULDER RUMBLE STRIPS-SAWED	34400	LIN FT
2775	ARROW PANEL (4)	2	EACH
3262	CLEAN PIPE STRUCTURE	1	EACH
5950	EROSION CONTROL BLANKET (12)	1000	SQ YD
6412	STEEL POST MILE MARKERS (10)	3	EACH
6417	FLEXIBLE DELINEATOR POST-W	106	EACH
6418	FLEXIBLE DELINEATOR POST-Y	12	EACH
6511	PAVEMENT STRIPING-TEMP PAINT -6 INCH	43000	LIN FT
6592	PAVEMENT MARKER TYPE V- B W/R	233	EACH

I-75 SOUTHBOUND ROCKCASTLE COUNTY PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95 ITEM NO. 8-2012.00 GENERAL SUMMARY			
ITEM NUMBER	ITEM	QUANTITY	UNIT
6593	PAVEMENT MARKER TYPE V- B Y/R	42	EACH
6600	REMOVE PAVEMENT MARKER TYPE V	233	EACH
10020NS	FUEL ADJUSTMENT	30031	DOLLAR
10030NS	ASPHALT ADJUSTMENT	52921	DOLLAR
20757ED	PAVEMENT REPAIR	955	SQ YD
21802EN	GUARDRAIL-STEEL W BEAM-S FACE (7 FT POST)	7962.5	LIN FT
23143ED	K.P.D.E.S. PERMIT AND TEMPORARY EROSION CONTROL	1	LUMP SUM
23237EN10W	WATERBLAST STRIPE REMOVAL	17200	LIN FT
24189ER	DURABLE WATERBORNE MARKING-6 IN W	21318	LIN FT
24190ER	DURABLE WATERBORNE MARKING-6 IN Y	18870	LIN FT
24191ER	DURABLE WATERBORNE MARKING-12 IN W	364	LIN FT

- (1) To be used to repair washouts behind guardrail, around perforated pipe headwalls and for ditch slope repairs.
(Includes 20 tons for washout repair behind guardrail and 2700 tons for ditch slope repair)
- (2) Includes the removal and replacement of the top phase of curb box inlets only
- (3) To be used at curb box inlet on downstream side of bridge
- (4) The quantity for these items includes the maximum in place at any one time. Any relocation required will not be paid for directly but will be considered incidental.
- (5) Ditching is intended for repair of the eroded areas in the median, pipe outlet ditches, roadway ditches and cleaning of drainage structures. Any embankment required is incidental to ditching.
- (6) Used for slope correction on 3:1 ditch slopes. Mattress units not required, aggregate only with gradation per 805.013.03 (see Detail 'C' on typical sections).
- (7) Intended for temporary traffic control signs and includes initial placement only. Any relocation of these temporary signs required will not be paid for directly but will be considered incidental.
- (8) Geotextile Fabric Type 1 to be placed under Aggregate Channel Lining is considered incidental to the lining. Any excavation required to place the channel lining is incidental to the lining.
- (9) Included for edge drain pipe system as shown in the typical sections or as directed by the Engineer.
- (10) Replace mile post markers at M.P. 56, 57, and 58.
- (11) Included for ditch drain pipe system as shown in the typical sections or as directed by the Engineer.
- (12) To be used for slopes (see Detail 'D' on typical sections) and for other areas as directed by the Engineer. Seeding and fertilizing are considered incidental
- (13) Included to be applied to eroded areas around drainage outlets and for some of the areas that are to be ditched. The actual limits of
- (14) To be used as directed by the Engineer.

NOTE: Quantities from all summaries have been carried over and included in this General Summary

I-75 SOUTHBOUND ROCKCASTLE COUNTY PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95 ITEM NO. 8-2012.00 GUARDRAIL SUMMARY									
LOCATION	SIDE	*FROM MILEPOST	*TO MILEPOST	GUARDRAIL- STEEL W BEAM- S FACE (7 FT POST) (LF)	GUARDRAIL END TREATMENT TYPE 1 (EACH)	GUARDRAIL END TREATMENT TYPE 2A (EACH)	GUARDRAIL END TREATMENT TYPE 3 (EACH)	GUARDRAIL CONNECTOR TO BRIDGE END TYPE A-1 (EACH)	REMOVE GUARDRAIL (LF)
				21802EN	2367	2369	2373	2387	2381
SOUTHBOUND	LT	55.99	56.04	262.5	1	1			300.0
SOUTHBOUND	LT	56.79	56.90	600.0		1	1		512.5
SOUTHBOUND	LT	57.12	57.90	4100.0	1	1			4062.5
SOUTHBOUND	LT	58.21	58.62	2150.0	1	1			2125.0
SOUTHBOUND	LT	58.89	58.97	425.0		1		1	425.0
RAMP A	RT	58.90	59.00	425.0	1	1			537.5
PROJECT TOTALS				7962.5	4	6	1	1	7962.5

*The Milepost references listed above are given as distances measured from the nearest milepost located south of the guardrail location.

I-75 SOUTHBOUND ROCKCASTLE COUNTY PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95 Item No. 8-2012.00 PIPE AND DRAINAGE SUMMARY									
Location	*Milepost	Curb Box Inlet Type B (T)	Adjust Inlet	Island Header Curb Type 1	Remove Curb	Clean Pipe Structure	COMMENTS		
Units		Each	Each	LF	LF	Each			
Item Number		1484	1719	1890	1904	3262			
LT	58.74		1				ADJUST DITCH INLET TO GRADE (G)		
LT	56.99		1				ADJUST DITCH INLET TO GRADE (G)		
LT	56.60						ADJUST DITCH INLET TO GRADE (G)		
	57.80					1	CLEAN 42" PIPE		
	58.96	1		20	20		CONST. CURB AT BRIDGE APPROACH AND CONST. TOP PHASE ONLY OF CBI		
PROJECT TOTAL		1	2	20	20	1			

*The Milepost references listed above are given as distances measured from the nearest mile post sign

I-75 SOUTHBOUND ROCKCASTLE COUNTY PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95 Item No. 8-2012.00 PERFORATED PIPE SUMMARY																									
Location	*Begin Milepost		*End Milepost		Perforated Pipe 4 inch	LF	Perforated Pipe 6 inch	Perforated Pipe 8 inch	Non-Perforated Pipe 4 inch	Non-Perforated Pipe 6 inch	Non-Perforated Pipe 8 inch	Perforated Pipe Type 1 4 inch	Each	Perforated Pipe Type 1 6 inch	Each	Perforated Pipe Type 1 8 inch	Each	Perforated Pipe Type 3 6 inch	Crushed Aggregate Size No. 2	COMMENTS					
	Units																								
	Item Number																								
EDGE DRAIN																									
75' LT	55.89	55.74								10				1						1	EDGE DRAIN OUTLET - MP 55.74				
75' LT	56.05	55.89								10				1						1	EDGE DRAIN OUTLET - MP 55.89				
75' LT	56.20	56.05								10				1						1	EDGE DRAIN OUTLET - MP 56.05				
	56.20																				HIGH POINT OF ROADWAY - MP 56.20				
75' LT	56.20	56.42								12											EDGE DRAIN OUTLET CONNECT TO 8" DITCH DRAIN - MP 56.42				
75' LT	56.39	56.60								10											EDGE DRAIN OUTLET TO DBI - MP 56.60				
	56.60																				LOW POINT OF ROADWAY - MP 56.60				
75' LT	56.66	56.60	300																		EDGE DRAIN OUTLET TO DBI - MP 56.60				
	56.66																				HIGH POINT OF ROADWAY - MP 56.66				
75' LT	56.66	56.79								10								1		1	EDGE DRAIN OUTLET - MP 56.79				
75' LT	56.79	56.85	350						10				1							1	EDGE DRAIN OUTLET - MP 56.85				
75' LT	56.85	56.99																			EDGE DRAIN OUTLET TO DBI - MP 56.99				
75' LT	56.99	57.12								10				1						1	EDGE DRAIN OUTLET - MP 57.12				
75' LT	57.12	57.21	500						10				1							1	EDGE DRAIN OUTLET - MP 57.21				
75' LT	57.21	57.31	500						10				1							1	EDGE DRAIN OUTLET - MP 57.31				
75' LT	57.31	57.40	500						10				1							1	EDGE DRAIN OUTLET - MP 57.40				
75' LT	57.40	57.45	250						10				1							1	EDGE DRAIN OUTLET - MP 57.45				
	57.45																				LOW POINT OF ROADWAY - MP 57.45				
75' LT	57.50	57.45	250										1							1	EDGE DRAIN OUTLET - MP 57.45				
75' LT	57.60	57.50	500						10				1							1	EDGE DRAIN OUTLET - MP 57.50				
75' LT	57.69	57.59	500							10				1						1	EDGE DRAIN OUTLET - MP 57.59				
75' LT	57.79	57.69	500						10				1							1	EDGE DRAIN OUTLET - MP 57.69				
75' LT	57.88	57.78	500						10				1							1	EDGE DRAIN OUTLET - MP 57.78				
75' LT	57.98	57.88	520						10				1							1	EDGE DRAIN OUTLET - MP 57.88				
	57.98																				HIGH POINT OF ROADWAY - MP 57.98				
75' LT	57.98	58.15								10				1						1	EDGE DRAIN OUTLET - MP 58.15				
75' LT	58.15	58.25	500						10				1							1	EDGE DRAIN OUTLET - MP 58.25				

PERFORATED PIPE SUMMARY

*The Milepost references listed above are given as distances measured from the nearest mile post sign

I-75 SOUTHBOUND					
ROCKCASTLE COUNTY					
PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95					
ITEM NO. 8-2012.00					
PAVEMENT REPAIR LOCATIONS					
*BEGIN MILEPOST	*END MILEPOST	WIDTH OF REPLACEMENT (FT)	LENGTH OF REPLACEMENT (FT)	AREA OF REPLACEMENT (SQ YD)	DESCRIPTION
57.90	57.90	12	6	8	INSIDE LANE
57.14	57.14	12	40	53	OUTSIDE LANE
57.09	57.12	12	140	187	OUTSIDE LANE
56.79	56.80	24	50	133	BOTH LANES
56.64	56.64	12	40	53	INSIDE LANE
56.56	56.57	24	40	107	BOTH LANES
56.40	56.42	24	75	200	BOTH LANES
55.82	55.83	24	40	107	BOTH LANES
55.81	55.81	24	40	107	BOTH LANES
PROJECT TOTALS				955	

*The Milepost references listed above are given as distances measured from the nearest mile post sign

I-75 SOUTHBOUND ROCKCASTLE COUNTY PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95 ITEM NO. 8-2012.00 PAVING AREAS			
PAVING AREAS MAINLINE		PAVING AREAS U.S. 25 EXIT RAMP	
ITEM	TOTAL	ITEM	TOTAL
INSIDE SHOULDER AND DRIVING LANES 1.25" CL4 ASPH SURF 0.38A PG76-22 3.50" CL4 ASPH BASE 1.0D PG76-22 OUTSIDE SHOULDER 1.25" CL3 ASPH SURF 0.38D PG64-22 3.50" CL3 ASPH BASE 1.0D PG64-22 LEVELING AND WEDGING - DRIVING LANES (CROSS SLOPE CORRECTION, INCLUDED WITH BASE LIFT) CL4 ASPH BASE 1.0D PG76-22 (0.3" AVG. DEPTH) LEVELING AND WEDGING - OUTSIDE SHOULDER (CROSS SLOPE CORRECTION, INCLUDED WITH BASE LIFT) CL3 ASPH BASE 1.0D PG64-22 (0.75" AVG. DEPTH) ASPHALT PAVE MILLING & TEXTURING PROJECT LIMITS (1.5") ADDITIONAL MILLING TO MATCH BRIDGE DECK ELEVATION (3.25" AVG. DEPTH) LEVELING AND WEDGING - TAPERING OF OVERLAY LEVELING & WEDGING PG76-22 (1.75" AVG. DEPTH)	S.Y. 53333 53333 19111 19111 47600 19111 70711 1336 1336	DRIVING LANES UP TO GORE (M.P. 58.81) 1.25" CL4 ASPH SURF 0.38A PG76-22 3.50" CL4 ASPH BASE 1.0D PG76-22 ASPHALT PAVE MILLING & TEXTURING (4.75" DEPTH) RAMP TO U.S. 25 1.50" CL3 ASPH SURF 0.38D PG64-22 ASPHALT PAVE MILLING & TEXTURING (4.75" DEPTH) LEVELING AND WEDGING - TAPERING OF OVERLAY LEVELING & WEDGING PG76-22 (1.75" AVG. DEPTH) PAVING AREAS - BRIDGE APPROACH REPAIRS INSIDE SHOULDERS AT APPROACH BRIDGE END (4' WIDE BY 300' LONG) 1.25" CL3 ASPH SURF 0.38D PG64-22 3.50" CL3 ASPH BASE 1.0D PG64-22 (TWO LIFTS) MILLING INSIDE SHOULDER (4.75" DEPTH) EDGE DRAIN PAVEMENT 5.00" CL3 ASPH BASE 1.0D PG64-22 6.00" DRAINAGE BLANKET - TYPE II ASPHALT	S.Y. 1129 1129 1129 3200 3200 686 133 133 133 S.Y. 1911 1911
UNPAVED SHOULDER AREAS - MAINLINE AND RAMPS			
DGA WEDGE (C.Y.)	C.Y. 1355		
ASPHALT SEAL COAT	S.Y. 17802		
ASPHALT SEAL AGGREGATE	17802		

I-75 SOUTHBOUND ROCKCASTLE COUNTY PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95 ITEM NO. 8-2012.00 PAVING SUMMARY			
CODE	ITEM	UNITS	TOTAL
1	DGA BASE	TON	2806
18	DRAINAGE BLANKET - TYPE II ASPHALT	(3) TON	573
100	ASPHALT SEAL AGGREGATE	(2) TON	356
194	LEVELING & WEDGING PG76-22	TON	195
214	CL3 ASPH BASE 1.0D PG64-22	TON	4518
214	CL3 ASPH BASE 1.0D PG64-22	(3) TON	526
219	CL4 ASPH BASE 1.0D PG76-22	(4) TON	11269
103	ASPHALT SEAL COAT	(1) TON	43
339	CL3 ASPH SURF 0.38D PG64-22	TON	1666
342	CL4 ASPH SURF 0.38A PG76-22	(4) TON	3744
2677	ASPHALT PAVE MILLING & TEXTURING	TON	7238
2696	SHOULDER RUMBLE STRIPS-SAWED	LIN. FT.	34400
(1) TWO APPLICATIONS AT 2.4 LB/SY (2) TWO APPLICATIONS AT 20 LB/SY (3) FOR EDGE DRAIN CAP (SEE EDGE DRAIN DETAIL) (4) TO BE PLACED WITH MATERIAL TRANSFER VEHICLE			

QUANTITIES HAVE BEEN CARRIED OVER AND INCLUDED IN THE GENERAL SUMMARY

**ROCKCASTLE COUNTY
I-75 SOUTHBOUND
MP 55.744 TO MP 58.954
FD04 102 0075 055-059
Item No. 8-2012.00**

**THIS PROJECT IS A FULLY
CONTROLLED ACCESS HIGHWAY**

I. DESCRIPTION

Perform all work in accordance with the Department's 2012 Standard Specifications, Supplemental Specifications, any applicable Special Provisions, and applicable Standard and Sepia Drawings, except as hereafter specified. Article references are to the Standard Specifications. Furnish all materials, labor, equipment, and incidentals for the following work:

(1) Maintain and Control Traffic; (2) Remove and replace Guardrail and Guardrail End treatments at the locations listed and/or as directed by the Engineer; (3) Type V pavement markers; (4) Asphalt Pavement Milling and Texturing; (5) Asphalt Surface and Asphalt Base at locations listed and/or as directed by the Engineer; and (6) All other work specified as part of this contract.

II. MATERIALS

Except as specified in these notes or on the drawings, all materials will be according to the Standard Specifications and applicable Special Provisions and Special Notes. The Department will sample and test all materials according to Department's Sampling Manual and the Contractor will have the materials available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing, unless otherwise specified in these notes.

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Dense Graded Aggregate.** Crushed Stone Base may not be furnished in lieu of DGA.
- C. **Pavement Markings -6 inch Paint.** Use Durable Waterborne Marking 6-inch for permanent striping (12 inch at entrance and exit ramp tapers).
- D. **Crushed Aggregate Size No. 2.** Crushed Aggregate Size No. 2 will be limestone.

- E. **Channel Lining Class III.** Channel lining will be limestone and is to be placed at pipe outlets with significant erosion and in ditch repair locations as directed by the Engineer.
- F. **Erosion Control Blanket.** Erosion control blanket is to be placed in all ditching areas when ditching is complete, on slope stabilization areas, or as directed by the Engineer. Use Seed Mixture No. 1

III. CONSTRUCTION METHODS

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Site Preparation.** Be responsible for all site preparation. Do not disturb existing signs. This item will include, but is not limited to, incidental excavation and backfilling; removal of all obstructions or any other items; disposal of materials; sweeping and removal of debris; shoulder preparation and restoration, temporary and permanent erosion and pollution control; and all incidentals. Site preparation will be only as approved or directed by the Engineer. Other than the bid items listed, no direct payment will be made for site preparation, but will be incidental to the other items of work.
- C. **Disposal of Waste.** Dispose of all cuttings, debris, and other waste off the right-of-way at approved sites obtained by the Contractor at no additional cost to the Department. The contractor will be responsible for obtaining any necessary permits for this work. Temporary openings in the right of way fence for direct access to waste sites off the right of way or for access to other public roads will not be allowed. No separate payment will be made for the disposal of waste and debris from the project or obtaining the necessary permits, but will be incidental to the other items of the work.
- D. **Final Dressing, Clean Up, and Seeding and Protection.** After all work is completed, completely remove all debris from the job site. Perform Class A Final Dressing on all disturbed areas. Sow disturbed earthen areas with Seed Mixture No. I. These items are incidental to other items in the contract.
- E. **Guardrail.** Remove and replace guardrail and guardrail End Treatments listed in the Guardrail Summary or as directed by the Engineer. Guardrail, End Treatments and Terminal Sections are listed by mile points and quantities are approximate only. Actual locations will be determined by the Engineer at the time of construction. Grade and reshape shoulders to proper template for new End Treatment. Utilize DGA for embankment when required for new end treatments. Remove any existing guardrail with a lane closure in place.

Do not leave the area unprotected. After the guardrail is removed, a shoulder closure shall remain in place until the guardrail is replaced in that area. To minimize safety hazards, guardrail removal is to be performed at the latest practical time prior to initiating the paving operation in an area and re-installation is to begin within 5 calendar days from the time that the final base course is completed and shall be

pursued until completion. If guardrail installation is not started within 5 calendar days after paving operations ends, Liquidated Damages will be charged as outlined in Section 108 of the 2012 Standard Specifications.

The Contractor shall deliver existing salvaged guardrail system materials to the Guardrail and Sign Center on Wilkinson Blvd in Frankfort, KY and shall be neatly stacked in accordance with section 719.03.07 of the Standard Specifications. Contact Section Supervisor at (502) 564-8187 to schedule the delivery of material. Deliver the material between the hours of 8:00AM and 3:00PM, Monday through Friday. There is a guardrail delivery verification sheet which must be completed. The Contractor, engineer, and Central Sign/Guardrail Center representative must all sign off on this sheet before payment may be made.

- F. **Pavement Striping and Pavement Markers.** Permanent striping will be in accordance with Section 112, except that:
- (1). Striping will be 6" in width, except 12" in gore area;
 - (2). Permanent striping will be in place before a lane is opened to traffic; and
 - (3). Permanent striping will be 6" Durable Waterborne Marking.
- G. **On-Site Inspection.** Each Contractor submitting a bid for this work will make a thorough inspection of the site prior to submitting a bid and will thoroughly familiarize himself with existing conditions so that the work can be expeditiously performed after a contract is awarded. Submission of a bid will be considered evidence of this inspection having been made. Any claims resulting from site conditions will not be honored by the Department.
- H. **Caution:** Information shown on the drawings and in this proposal and the types and quantities of work listed are not to be taken as an accurate or complete evaluation of the material and conditions to be encountered during construction. The bidder must draw his own conclusions as to the conditions encountered. The Department does not give any guarantee as to the accuracy of the data and no claim will be considered for additional compensation if the conditions encountered are not in accordance with the information above.
- I. **Utility Clearance.** It is not anticipated that utility facilities will need to be relocated and/or adjusted; however, in the event that it is discovered that the work does require that utilities be relocated and/or adjusted, the utility companies will work concurrently with the Contractor while relocating their facilities.

IV. METHOD OF MEASUREMENT

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Site Preparation.** Other than the bid items listed, site preparation will not be measured for payment, but will be incidental to the other items of work.

- C. **Crushed Aggregate Size No. 2.** Payment will be based on the tons used around the perforated pipe outlet headwalls.
- D. **Dense Graded Aggregate.** DGA will be used for shoulder slope washouts.
- E. **Raised Pavement Markers and Permanent Striping.** Permanent striping Durable Waterborne Marking (6" and 12") is measured per linear foot. See Traffic Control Plan. Type V Pavement Markers are measured as each.
- F. **Erosion Control.** Erosion control items not listed as bid items will not be measured for payment, but will be considered incidental to the "lump sum" price for the bid item "Erosion Control".
- G. **Erosion Control Blanket.** Erosion Control Blanket is measured by square yard and is to be used in ditching areas and slope stabilization areas as directed by the Engineer.
- H. **Embankment.** Embankment is measured by cubic yard and is to be placed in pipe repair/extension locations, slope stabilization areas and as directed by the Engineer. Contrary to the Standard Specifications, payment will be based on measured quantity **NOT** plan quantity.

V. BASIS OF PAYMENT

No direct payment will be made other than for the bid items listed. All other items required to complete the construction will be incidental to the bid items listed. Existing signs damaged by the Contractor will be replaced by the Contractor at his expense.

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Site Preparation.** Other than the bid items listed, no direct payment will be allowed for site preparation, but will be incidental to the other items of work.
- C. **Dense Grade Aggregate.** See Section 302 of the Standard Specifications.
- D. **Raised Pavement Markers and Permanent Striping.** See Traffic Control Plan.

**NOTES APPLICABLE TO PROJECT
PAVEMENT REHABILITATION
I ROCKCASTLE COUNTY
I-75 SOUTHBOUND
Item No. 8-2012.0**

1. There is a summary pavement repair locations. The Engineer will determine the ultimate locations that will be repaired based upon the condition of the pavement at the time the repairs are accomplished. The repair locations listed may be lengthened, shortened, or eliminated completely if the conditions are such that modification of the locations would be deemed desirable by the Department.
2. The dimensions shown on the typical section for pavement and shoulder widths and thickness are nominal or typical dimensions. The actual dimensions to be constructed may be varied to fit existing conditions as directed or approved by the Engineer. It is not intended that existing pavement or shoulders be widened unless specified in the Proposal.
3. The contractor is to be advised locations of low wires may exist. The following location is approximate:

I-75 M.P. 58.84 (Sta. 2945+00)

CAUTION: Other Locations may exist. These and all other utilities should be avoided on this project. If any utility is impacted, it will be the contractor's responsibility to contact the affected utility and cover any costs associated with the impact.

4. Guardrail, End Treatments, and Terminal Sections to be replaced are listed by mileposts. Exact placement to be approved by the Engineer on construction.
5. The existing cable median barrier is not to be disturbed with this project. In accordance with Section 107.12 of the Standard Specifications for Road and Bridge Construction, Current Edition, the Contractor will be responsible for the cost to repair any cable rail that is damaged due to the operations of the Contractor. The Department will make any necessary repairs at the Contractor's expense.
6. Remove any existing guardrail with a lane closure in place. Do not leave the area unprotected. After the guardrail is removed, a shoulder closure shall remain in place until the guardrail is replaced in that area. The Contractor shall deliver existing salvaged guardrail system materials to the Central Sign Shop and Recycle Center in Frankfort, KY (502-564-8187) between the hours of 8:00AM and 3:00PM, Monday through Friday and shall be neatly stacked in accordance with section 719.03.07 of the standard specifications. Contractor, engineer, and Central Sign Shop and Recycle Center representative must all sign off on this sheet before payment may be made.
7. All pipe connections in the edge drain system will be rigid.
8. A quantity of Channel Lining Class III has been included to be applied to eroded areas around drainage outlets and for some of the areas that are to be ditched. The actual limits of ditching

and/or channel lining shall be as directed and/or approved by the Engineer. Geotextile Fabric Type I will not be measured for payment and will be considered incidental to channel lining.

9. Any roadway signs that are damaged during construction are to be replaced at the contractor's expense.
10. Any light poles that are damaged during construction are to be replaced at the contractor's expense.
11. The cleaning of existing pipe culvert inlets and outlets 36 inches or less in diameter are incidental to the bid item for "Ditching" in accordance with Section 209.03.01 of the 2012 Edition of the Standard Specifications for Road and Bridge Construction.
12. Overlay pavement shall be tapered to create a smooth transition to the existing pavement at the existing median cross-overs.
13. The existing roll curb island on the US 25 interchange (Exit 59) ramp is not to be disturbed. If the curb is damaged during construction, the Contractor will be responsible for replacing any damaged curb at no cost to the Department.
14. Safety Pavement Edge shall be incorporated on the inside 3' shoulder (see Special Note). Prior to placing asphalt pavement, prepare the shoulder material, including removal of vegetation/sod, where the pavement wedge will be placed to provide a foundation that will support the placement of the pavement wedge according to Section 403.03.02 of the Standard Specifications. The Department will not measure preparation or construction of pavement wedge for separate payment, but make payment for the completed and accepted quantities of Asphalt Surface Mixtures according to Section 402 of the Standard Specifications.
15. New grates shall be installed on adjusted drop box inlets in the roadside ditch. The existing grates shall be delivered to the Rockcastle County Maintenance Garage, US 25 South, Mount Vernon, KY 40456 (606-256-2116). No additional payment will be considered for the new grates or delivery of the existing grates as this will be considered incidental to the bid item "Adjust Inlet."

TRAFFIC CONTROL PLAN

**ROCKCASTLE COUNTY
I-75 SOUTHBOUND
MP 55.744 TO MP 58.954
FD04 102 0075 055-059
Item No. 8-2012.00**

<p>THIS PROJECT IS A FULLY CONTROLLED ACCESS HIGHWAY</p>

TRAFFIC CONTROL GENERAL

Except as provided herein, maintain and control traffic in accordance with the 2012 Standard Specifications and the Standard Drawings, current editions. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic". All lane closures used on the Project will be in compliance with the appropriate Standard Drawings. Do NOT use Cones for lane closures or shoulder closures.

Contrary to Section 106.01, traffic control devices used on this project may be new, or used in like new condition at the beginning of the work and maintained in like new condition until completion of the work. Traffic Control Devices will conform to current MUTCD.

Reduce the speed limit in work areas to 55 miles per hour and establish double fines for work zone speeding violations. The extent of these areas within the project limits will be restricted to the proximity of actual work areas as determined by the Engineer. Notify the Engineer a minimum of 12 hours prior to using the double fine signs. At the beginning of the work zone, the "WARNING FINE DOUBLED IN WORK ZONE" signs will be dual mounted. At the end of the work zone, the "END DOUBLE FINE" signs will be dual mounted as well. Remove or cover the signs when the highway work zone does not have workers present for more than a two-hour period of time. Payment for the signs will be at the unit bid price for signs erected. Any relocation or covering of the signs will be incidental to Maintain and Control Traffic.

Night work is required on this project. Obtain approval from the Engineer for the method of lighting prior to its use.

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Rockcastle County
I 75
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PROJECT PHASING & CONSTRUCTION PROCEDURES

No lane closures will be allowed during the following days and hours:

November 21-25, 2012	Thanksgiving Weekend
December 21, 2012-January 1, 2013	Christmas/New Year's Holiday
March 29-April 1, 2013	Easter Weekend
May 24-27, 2013	Memorial Day Weekend

No lane closures will be allowed during the following days and hours unless otherwise stated in this proposal or approved by the Engineer:

12:00 p.m. to 6:00 p.m.	Monday – Thursday
12:00 p.m. Friday to 6:00 p.m. Sunday	

Traffic may be reduced to one lane in each direction during the following days and hours unless otherwise stated in this proposal or approved by the Engineer:

6:00 p.m. to 12:00 p.m.	Sunday – Thursday
-------------------------	-------------------

The Department will allow for six (6) extended lanes closures from 6:00 p.m. Sunday through 12:00 p.m. (noon) Wednesday to complete pavement repairs and edge drain construction along with any other roadway milling and paving operations and roadside operations feasible to complete during this time.

Shoulders used as temporary roadways will be inspected by the Engineer and if deemed necessary by the Engineer, repaired with Asphalt Mixture for Level & Wedging as directed prior to opening to traffic. Perform any maintenance of the shoulder as deemed necessary by the Engineer in order to maintain traffic. Remove existing striping by water blasting. Remove both edge lines throughout the project. Paint temporary edge lines through the lane closure.

All pavement edge transitions must be smooth and level before opening both lanes up to traffic. A lane closure must be in place during all times that pavement edge drop-offs are present (see Pavement Edge Dropoff note).

Approximate full depth pavement repair locations are listed in the proposal. The Engineer will determine the exact location at the time of construction. Once removal of pavement at a particular repair location has begun, work continuously within the parameters outlined above to complete the work and eliminate the "hole". Place Type III Barricades immediately in front of pavement removal areas.

Once pavement removal at a site has begun, full depth replacement must be completed within the time a lane closure is allowed.

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Access to all ramps at all interchanges on the project shall be maintained at all times unless otherwise directed by the Engineer. All diversions to access ramps in areas of lane closures shall be approved by the Engineer prior to implementing the particular lane closure

Note that Lane shifts are required throughout the project. See the Exhibits for lane locations and widths. Stripe according to the MUTCD.

During the days and hours when a lane closure is allowed, implement the following procedures: Maintain traffic as specified in the phasing notes. Any other work not requiring traffic lane widths to be restricted due to barrels or equipment encroaching into the interior lanes can be done during the remaining hours when two lanes of traffic must be maintained. Please refer to the "Special Note for Fixed Completion Date and Liquidated Damages" for damage rates per hour associated with failure to maintain the required number of lanes during the specified time period.

Once pavement removal at a site has begun, full depth replacement must be completed within the time a lane closure is allowed. Liquidated Damages, at the rate specified per hour in the "Special Note for Fixed Completion Date and Liquidated Damages", will be assessed for each hour two lanes of traffic is not maintained.

The contractor must notify the Engineer at least fourteen (14) days prior to beginning Phase I construction in either direction.

SHOULDER PREPARATION AND RESTORATION

Prior to placing any lane closures that require shifting traffic onto existing shoulders, patch the shoulders as directed by the Engineer. Remove failed materials and perform additional patching as directed by the Engineer during the time the shoulder is used as a travel lane. All work required for shoulder preparation and restoration is incidental to Maintenance of Traffic, with the exception of the asphalt patching, which will be paid at the contract unit bid price for "LEVELING AND WEDGING PG76-22".

Prior to shifting traffic onto any existing shoulders at bridge ends, the inside and/or outside shoulders shall be reconstructed to existing grade a minimum of four (4) feet wide and 300 feet in length at the end of the bridge or as directed by the engineer. This construction shall consist of 2 – 3.5" lifts of CL3 ASPH BASE 1.0D PG64-22 and 1.25" CL3 ASPH SURF 0.38D PG64-22.

PHASE I

Close the inside lanes to traffic. Reconstruct the inside shoulders at bridge ends as stated above and shown in the proposal details.

PHASE II

Shift traffic to the inside lanes and close the outside lanes to traffic. Repair pavement

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failures in the outside lanes and shoulder. Once the pavement has been removed, the Contractor must work continuously until the pavement has been replaced. The Engineer may restrict the number of locations being repaired at the same time. Install the perforated pipe edge drain. Complete drainage structure repair and installation. Mill roadway 1.5 inches and place the 3.5 inch base lift and leveling & wedging where required on the outside lanes and shoulder. Mill roadway 1.5 inches and place the 3.5 inch base lift and leveling and wedging where required on the entrance ramp to US 25 utilizing part width construction phasing. The entrance ramp must remain open during the lane closure. Complete any roadside work including guardrail installation.

PHASE III

Shift traffic to the outside lanes. Repair pavement failures in the inside lanes and shoulder. Once the pavement has been removed, the Contractor must work continuously until the pavement has been replaced. The Engineer may restrict the number of locations being repaired at the same time. Mill roadway 1.5 inches and place the 3.5 inch base lift and leveling & wedging where required on the inside lanes and shoulder. Complete any roadside work including guardrail installation.

PHASE IV

Utilizing temporary lane closures place the final surface course on all lanes, ramps and shoulders and complete any outstanding roadside work.

PHASE V – PERMANENT STRIPING

After all other work is completed, place permanent striping. Mobile operations may be utilized. In addition to newly paved areas, place permanent striping on bridge decks within the project limits.

LANE CLOSURES

Limit the lengths of lane closures to only that needed for actual operations in accordance with the phasing specified herein, or as directed by the Engineer. Limit lane closures to one lane closure per direction at any given time. Contrary to section 112, lane closures will **NOT** be measured for payment, but are considered incidental to Maintain and Control Traffic.

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SIGNS

Additional traffic control signs in addition to normal lane closure signing detailed on the Standard Drawings may be required by the Engineer. Additional signs needed for lane closures may include, but are not limited to, dual mounted TRUCKS USE LEFT/RIGHT LANE, LEFT/RIGHT LANE CLOSED 1 MILE, LEFT/RIGHT LANE CLOSED 2 MILE, LEFT/RIGHT LANE CLOSED 3 MILE, SLOWED/STOPPED TRAFFIC AHEAD. Signage for reduced speed limits and double fine work zones will be furnished, relocated, and maintained by the Contractor.

Contrary to section 112, Individual signs will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged signs or signs directed to be replaced by the Engineer due to poor legibility or reflectivity will not be measured for payment.

A quantity of signs has been included for lane shifts, "Roadwork Ahead" signs on entrance ramps, and extra Double Fine signs and Speed Limit signs between interchanges to be paid only once no matter how many times they are moved or relocated.

FLASHING ARROWS

Flashing arrows will be paid for once, no matter how many times they are moved or relocated.

PORTABLE CHANGEABLE MESSAGE SIGNS

Provide portable changeable message signs in advance of and within the project at locations to be determined by the Engineer. If work is in progress concurrently in both directions, or if more than one lane closure is in place in the same direction of travel, provide additional portable changeable message signs. Place portable changeable message signs one mile in advance of the anticipated queue at each lane closure. As the actual queue lengthens and/or shortens relocate or provide additional portable changeable message signs so that traffic has warning of slowed or stopped traffic at least one mile but not more than two miles before reaching the end of the actual queue. The locations designated may vary as the work progresses. The messages required to be provided will be designated by the Engineer. The portable changeable message signs will be in operation at all times. In the event of damage or mechanical/electrical failure, the Contractor will repair or replace the Portable Changeable Message Sign immediately. Portable Changeable Message Boards will be paid for once, no matter how many times they are moved or relocated. The Department **WILL NOT** take possession of the signs upon completion of the work.

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TRUCK MOUNTED ATTENUATORS

Furnish and install MUTCD approved Truck Mounted Attenuators in advance of work areas when workers are present less than 12 feet from traffic. If there is less than 500 feet between work sites, only a single TMA will be required at a location directed by the Engineer. Locate the TMAs at the individual work sites and move them as the work zone moves within the project limits. All details of the TMA installations shall be approved by the Engineer. Truck Mounted Attenuators will not be measured for payment, but are incidental to Maintain and Control Traffic. The Department **WILL NOT** take possession of the TMAs upon completion of the work.

PAVEMENT MARKINGS

If lane closures are in place during nighttime hours, remove or cover the lenses of raised pavement markers that do not conform to the traffic control scheme in use, or as directed by the Engineer. Replace or uncover lenses before a closed lane is reopened to traffic. No direct payment will be made for removing and replacing or covering and uncovering the lenses, but will be incidental to "Maintain and Control Traffic".

Place temporary and permanent striping in accordance with Section 112, except that:

1. Temporary and permanent striping will be 6" in width; and
2. If the contractor's operations or phasing requires temporary markings which must be subsequently removed from the ultimate pavement, an approved removable lane tape will be used; however removable tape will be measured and paid as Pavement Striping-Temporary Paint 6"; and
3. Edge lines will be required for temporary striping; and
4. Existing, temporary, or permanent striping will be in place before a lane is opened to traffic.
5. Place permanent striping on pavement within the project limits.
6. Permanent striping will be Durable Waterborne Marking.

Should the Contractor change the existing striping pattern, the Contractor is to restripe the roadway back to its original configuration after a certain period of time especially if no work is anticipated for a period of time (i.e. Winter shutdown).

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PAVEMENT EDGE DROP-OFFS

Pavement edge drop-offs will be protected by a lane or shoulder closure. Lane closures will be protected with plastic drums, vertical panels, or barricades as shown on the Standard Drawings.

A pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation shall not have an elevation difference greater than 1 ½". Place warning signs (MUTCD W8-11 or W8-9A) in advance of and at 1500' intervals throughout the drop-off area. Dual posting on both sides of the traveled way shall be required. Wedge all transverse transitions between resurfaced and unresurfaced areas which traffic may cross with asphalt mixture for leveling and wedging. Remove the wedges prior to placement of the final surface course. Pavement edges that traffic is not expected to cross, except accidentally, shall be treated as follows:

Less than 2" – Protect with a lane closure.

2" to 4" – Protect with a lane closure. Place plastic drums, vertical panels, or barricades every 50 feet. Cones may not be used in place of plastic drums, panels, and barricades at any time. Construct a wedge with compacted cuttings from milling, trenching, or asphalt mixtures with a 3:1 or flatter slope, when work is not active in the drop-off area. Place Type III Barricades at the beginning of the lane closures, and place additional Type III Barricades spaced at 2,500 feet during the time the lane closure is in place.

Greater than 4" – Pavement Repair areas – In areas where pavement is to be removed, work should proceed continuously so that traffic is exposed to a drop-off for the minimum amount of time necessary to bring the pavement back up to existing grade. Barrel spacing should be 20 feet and appropriate lighting should be utilized to illuminate the area during nighttime operations.

Guardrail Installation – Guardrail will be removed at the last practical moment and replaced as soon as the placement of all base courses in an area requiring guardrail is complete. All areas from which guardrail is removed shall be protected by a shoulder closure or other method approved by the Engineer until the new guardrail is installed.

TRAFFIC COORDINATOR

Designate an employee to be traffic coordinator. The designated Traffic Coordinator must be certified by an agency qualified for training in this area. The Traffic Coordinator will inspect the project maintenance of traffic once every two hours during the Contractor's operations and at any time a lane closure is in place. The Traffic Coordinator will report all incidents throughout the work zone to the Engineer on the project. The Contractor will furnish the name and telephone number where the Traffic Coordinator can be contacted at all times.

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During any period when a lane closure is in place, the Traffic Coordinator will arrange for personnel to be present on the project at all times to inspect the traffic control, maintain the signing and devices, and relocate portable changeable message boards as queue lengths change. The personnel will have access on the project to a radio or telephone to be used in case of emergencies or accidents.

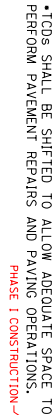
COORDINATION OF WORK

The Contractor is advised that other projects may be in progress within or in the near vicinity of this project. The traffic control of those projects may affect this project and the traffic control of this project may affect those projects. The Contractor will coordinate the work on this project with the work of the other contractors. In case of conflict, the Engineer will determine the relative priority to give to work phasing on the various projects.

CONTRACTOR'S AND CONTRACTOR'S EMPLOYEES' VEHICLES

Do not use or allow employees to use median crossovers at any time except when inside lanes are closed for construction. In all other phases of construction, change vehicular direction of travel only at interchanges.

SOUTHBOUND LANES



NORTHBOUND LANES

OF THE OUTSIDE LANE.

NORTHBOUND LANES

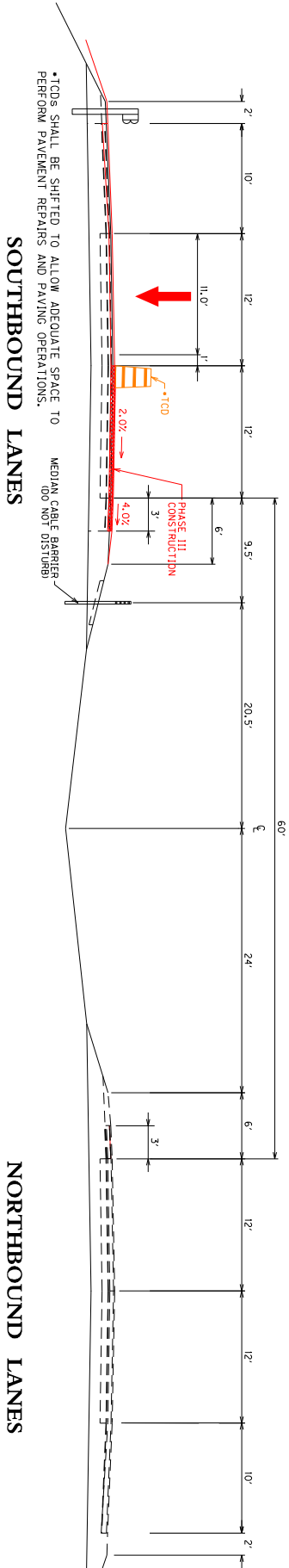


NORTHBOUND LANES

- PERFORM PAVEMENT REPAIRS WHEREVER NECESSARY
- INSTALL PERFORATED PIPE EDGE DRAIN
- COMPLETE DRAINAGE REPAIRS AND INSTALLATION
- CONSTRUCT MILLING LEVELING & ASPHALT BASE
- CONSTRUCT MILLING LEVELING & ASPHALT BASE ON
- COMPLETE ANY ROADSIDE WORK INCLUDING GUARDRAIL

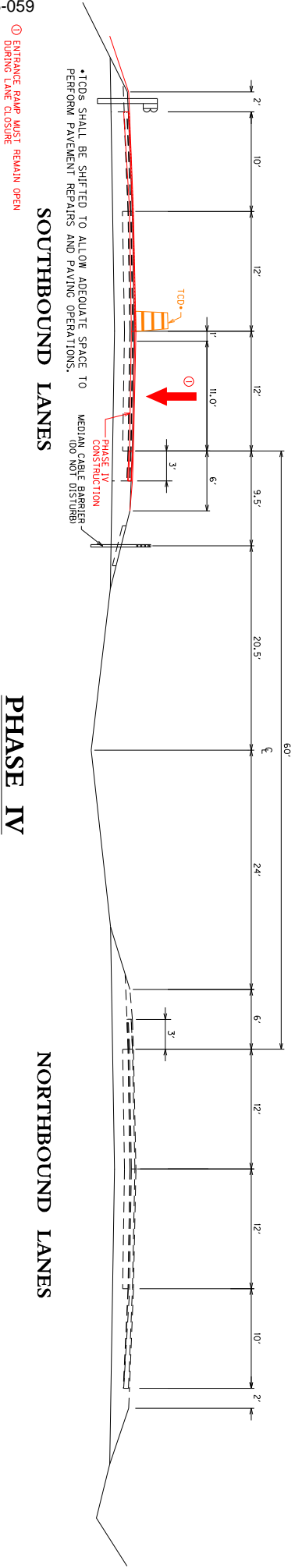
COUNTY OF	ITEM NO.	SHEET NO.
ROCKCASTLE	8-2012.00	

I-75
MAINTENANCE OF TRAFFIC
TYPICAL SECTIONS



PHASE III

- PERFORM PAVEMENT REPAIRS WHEREVER NECESSARY
- CONSTRUCT MILLING, LEVELING & ASPHALT BASE
- COMPLETE ANY ROADSIDE WORK INCLUDING MEDIAN GRADING AND DRAINAGE REPAIRS



PHASE IV

- PLACE ASPHALT SURFACE
- COMPLETE ANY REMAINING ROADSIDE WORK



- ARROW PANEL

CHANNELIZING DEVICES

CONES
DRUMS
TYPE II BARRICADES
TUBULAR MARKERS

DRAWING NOT TO SCALE

USE WITH CURRENT STD. DWGS.
TTD-100, TTD-105, TTD-110,
TTD-115, AND TTC-160

SIGNING AND SPACING TABLE					
ROAD TYPE	A	B	C	D	L
EXPRESSWAY/ FREEWAY	1000'	500'	1100'	2600'	900'
RURAL/URBAN SP. LT. \geq 45 MPH	500'	500'	500'	1100'	660'
SP. LT. \leq 40 MPH	250'	250'	500'	500'	440'

*NOTE: USE NORMAL POSTED SPEED LIMIT

APPLICATION

THIS DRAWING APPLIES TO LANE CLOSURES OF THE LEFT LANE.
IF TEMPORARY TRAFFIC BARRIER IS USED, REFER TO CURRENT
STANDARD DRAWING TTC-120.

REFERENCES

- 1. Kentucky Transportation Cabinet, Department of Highways, Standard Specifications for Road and Bridge Construction, Edition of 2012.
- 2. FHWA Manual on Uniform Traffic Control Devices.
- 3. Active Sepia List

<u>Drawing No.</u>	<u>Drawing Name</u>
002	Delineators for Guardrail
007	Guardrail End Treatment Type 2A
008	Guardrail Components

- 4. Kentucky Department of Highways Standard Drawings, current editions, as applicable:

RBB-002	Guardrail and Bridge End Drainage for Twin Structures
RBB-003	Layout of Guardrail at Twin Structures
RBC-001	Guardrail Connector to Bridge End Type A and A1
RBC-002	Guardrail Connector to Bridge End Type A and A1 Components
RBC-003	Guardrail Connector to Bridge End Type A and A1 Components
RBI-001	Typical Guardrail Installations
RBI-002	Typical Guardrail Installations
RBI-003	Installation of Guardrail End Treatment Type 2A
RBI-004	Installation of Guardrail End Treatment Type 1
RBR-001	Steel Beam Guardrail ("W" Beam)
RBR-005	Guardrail Components
RBR-010	Guardrail Terminal Sections
RBR-015	Guardrail Posts
RBR-016	Guardrail Posts
RBR-020	Guardrail End Treatment Type 1
RBR-025	Guardrail End treatment Type 2A
RBR-030	Guardrail End Treatment Type 3
RDB-280	Curb Box Inlet Type B
RDB-281	Curb Box Inlet Type B – Steel Details
RDB-282	Curb Box Inlet Type B – Top Phase Tables
RDB-283	Curb Box Inlet Type B - Details
RDB-400	Box Inlet Riser
RDD-040	Channel Lining Class II and III
RDP-001	Perforated Pipe Types and Cover Heights
RDP-005	Perforated Pipe for Subgrade Drainage on Two-Lane (class 2) and Multi-Lane Roads
RDP-010	Perforated Pipe Headwalls

RDX-160	Security Devices for Frames, Grates and Lids
RDX-210	Temporary Silt Fence
RDX-220	Silt Trap Type A
RDX-225	Silt Trap Type B
RDX-230	Silt Trap Type C
RGS-002	Superelevation for Multilane Pavement
RGX-001	Miscellaneous Standards Part I
RGX-200	One Point Proctor Family of Curves
RPM-100	Curb and Gutter, Curbs, and Valley Gutter
TPM-105	Pavement Marker Arrangements Multi-Lane Roadways
TPM-135	Pavement Marker Arrangement On-Ramp with Parallel Acceleration Ramp
TTC-115	Lane Closure Multi-Lane Highway Case I
TTC-120	Lane Closure Multi-Lane Highway Case II
TTC-135	Shoulder Closure
TTC-155	Temporary Pavement Marker Arrangements for Construction Zones
TTC-160	Temporary Pavement Marker Arrangements for Lane Closures
TTD-110	Post Splicing Detail
TTD-120	Work Zone Speed Limit and Double Fine Signs
TTD-125	Pavement Condition Warning Signs
TTS-110	Mobile Operation for Paint Striping Case III
TTS-115	Mobile Operation for Paint Striping Case IV
TTS-120	Mobile Operation for Durable Striping Case I

5. Kentucky Transportation Cabinet, Department of Highways, Standard Specifications for Road and Bridge Construction, Edition of 2012, Appendix B - Supplemental Specifications, as applicable:

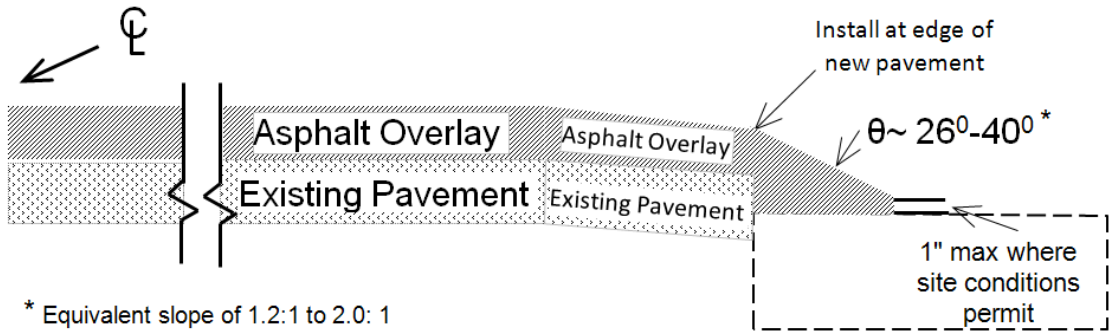
Special Note II	Portable Changeable Message Signs (6/15/2012)
Special Note	Typical Section Dimensions <i>attached</i>
Special Note	Before You Dig <i>attached</i>
Special Note	Guardrail Delivery Verification Sheet <i>attached</i>
Special Note	Fixed Completion Date and Liquidated Damages <i>attached</i>
Special Note	Shoulder Preparation and Restoration <i>attached</i> (See MOT Notes)
Special Note	Erosion Control and Sediment Control <i>attached</i>
Special Note	Safety Pavement Edge (Experimental) <i>attached</i>

**Special Note For:
Safety Pavement Edge
Rockcastle Co. Item No. 8-2012
(Experimental)**

CONSTRUCTION Attach a device to the screed of the paver that confines the material at the end gate and extrudes the asphalt material in such a way that results in a compacted wedge shape pavement edge of approximately 30 degrees (not steeper than 40 degrees). The device shall maintain contact to the road shoulder surface. It shall also allow for automatic transition to cross roads, driveways and obstructions. The device shall constrain the asphalt head reducing the area by 10 to 15% increasing the density of the extruded profile. The use of a single plate strike off is not allowed.

The contractor may use the TransTech Shoulder Wedge Maker, the Advant-Edger, Carlson Safety Edge End Gate, Troxler SafeTSlope Edge Smoother, or a similar device that produces the same wedge consolidation results. If the contractor uses a similar device, they must provide proof that the device has been used on previous projects with acceptable results or the contractor shall construct a test section prior to the beginning of work and demonstrate wedge compaction to the satisfaction of the Engineer. Short sections of handwork will be allowed when necessary for transitions and turnouts or otherwise authorized by the Engineer.

The following sketch is primarily for the computation of quantities; however, the wedge will result in a similar cross-section. Do not construct a shoulder for placing the wedge unless specified elsewhere in the Contract.



UNPAVED SHOULDER PREPARATION Prior to placing asphalt pavement, prepare the shoulder material, including removal of vegetation/sod, where the pavement wedge will be placed to provide a foundation that will support the placement of the pavement wedge according to Section 403.03.02 of the Standard Specifications.

COORDINATION A Preconstruction Conference will be required for this project. A representative from the Central Office Division of Traffic Operations – Traffic Engineering Safety Section should be in attendance. At this meeting, the Contractor is to provide the details of the device that will be used to place the wedge.

PAYMENT The Department will not measure preparation or construction of pavement wedge for separate payment, but make payment for the completed and accepted quantities of Asphalt Surface Mixtures according to Section 402 of the Standard Specifications.

SPECIAL NOTE FOR TYPICAL SECTION DIMENSIONS
I-75

The dimensions shown on the typical sections for pavement and shoulder widths are nominal or typical dimensions. The actual dimensions to be constructed may be varied to fit existing conditions as directed or approved by the Engineer. It is not intended that existing pavement or shoulders be widened unless specified elsewhere in the Proposal.

SPECIAL NOTE FOR BEFORE YOU DIG

The contractor is instructed to call 1-800-752-6007 to reach KY 811, the one-call system for information on the location of existing underground utilities. The call is to be placed a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor should be aware that owners of underground facilities are not required to be members of the KY 811 one-call Before –U-Dig (BUD) service. The contractor must coordinate excavation with the utility owners, including those whom do not subscribe to KY 811. It may be necessary for the contractor to contact the County Court Clerk to determine what utility companies have facilities in the area.

Guardrail Delivery Verification Sheet
Item No. 8-2012.00

<u>Guardrail, End Treatment, Terminal Section or Post Type</u>	<u>Unit</u>	<u>Field Verified Amount</u>	<u>Delivered Amount</u>
Guardrail-Steel W Beam	LF		
Temporary Guardrail	LF		
Guardrail Terminal Section	Each		
Crash Cushion Type IX-A	Each		
Guardrail End Treatment Type 1	Each		
Guardrail End Treatment Type 2A	Each		
Guardrail End Treatment Type 3	Each		
Guardrail End Treatment Type 4A	Each		
Guardrail End Treatment Type 7	Each		
Guardrail Connector To Bridge End	Each		
Guardrail Connector To Conc Med Barr	Each		
Guardrail Connect-Shld Bridge Pier	Each		
Steel Guardrail Post	Each		
Steel Blockouts	Each		

Removed guardrail, end treatments, terminal sections, and posts shall be delivered to the Central Sign Shop and Recycle Center in Frankfort, KY (502-564-8187) between the hours of 8:00 AM and 3:00 PM Monday through Friday and shall be neatly stacked in accordance with section 719.03.07 of the standard specifications. Contractor, engineer, and Central Sign Shop and Recycle Center representative must all sign off on this sheet before payment may be made.

	Printed Name	Signature	Date
Resident Engineer (or Representative)			
Contractor (or Representative)			
Central Sign Shop & Recycle Center Representative			

**Special Note for Fixed Completion Date and
Liquidated Damages
Rockcastle County
Item No. 8-2012**

Contrary to Section 108.09, Liquidated Damages of \$5,000 per calendar day will be assessed for each day or fraction thereof work remains uncompleted beyond the Specified Completion Date. This project has a Fixed Completion Date of May 24, 2013.

In addition to the Liquidated Damages specified above, Liquidated Damages in the following amounts will be charged when a lane closure remains in place during the prohibited period outlined in the Traffic Control Plan:

Mainline:	\$5,000 for the first hour or fraction thereof
	\$10,000 for the second hour or fraction thereof
	\$20,000 for any additional hour or fraction thereof
Ramps:	\$500 for the first hour or fraction thereof
	\$1,500 any additional hour or fraction thereof

These hourly disincentives will still be in effect after the Fixed Completion Date and will be charged in addition to the \$5,000 per calendar day if warranted. The contractor is expected to make every effort to complete the work in order to open the ramp or mainline lane closure within a specified timeframe.

Contrary to Section 108.09 of the Standard Specifications, **the disincentive fee will be charged during those periods when seasonal limitations of the Contract prohibit the Contractor from working on a controlling item or operation. This includes the months from December through March.**

All liquidated damages will be applied cumulatively.

All other applicable portions of Section 108 apply.

Special Note For: Erosion Prevention and Sediment Control Rockcastle Co. Item No. 8-2012

The Contractor shall be responsible for filing the Kentucky Pollution Discharge Elimination System (KPDES) KYR10 permit Notice of Intent (NOI) with the Kentucky Division of Water (DOW) and any KPDES local Municipal Separate Storm Sewer System (MS4) program that has jurisdiction. The NOI shall name the contractor as the Facility Operator and include the KYTC Contract ID Number (CID) for reference.

The Contractor shall perform all temporary erosion/sediment control functions including: providing a Best Management Practice (BMP) Plan, conducting required inspections, modifying the BMP plan documents as construction progresses and documenting the installation and maintenance of BMPs in conformance with the KPDES KYR10 permit effective on August 1, 2009 or a permit re-issued to replace that KYR10 permit. This work shall be conducted in conformance with the requirements of Section 213 of KYTC 2012 Department of Highways, Standard Specifications for Road and Bridge Construction.

Contrary to Section 213.03.03, paragraph 2, the Engineer shall conduct inspections as needed to verify compliance with Section 213 of KYTC 2012 Department of Highways, Standard Specifications for Road and Bridge Construction. The Engineer's inspections shall be performed a minimum of once per month and within seven days after a storm of ½ inch or greater. Copies of the Engineer's inspections shall not be provided to the contractor unless improvements to the BMP's are required. The contractor shall initiate corrective action within 24 hours of any reported deficiency and complete the work within 5 days. The Engineer shall use Form TC 63-61 A for this report. Inspections performed by the Engineer do not relieve the Contractor of any responsibility for compliance with the KPDES permit.

Contrary to Section 213.05, bid items for temporary BMPs will not be listed and will be replaced with one lump sum item for the services. Payment will be pro-rated based on the Project Schedule as submitted by the Contractor and as agreed to by the Engineer.

The contractor shall be responsible for applying "good engineering practices" as required by the KPDES permit. The contractor may use any temporary BMPs with the approval of the KYTC Engineer.

The contractor shall provide the Engineer copies of all documents required by the KPDES permit at the time they are prepared.

The contractor shall be responsible for the examination of the soils to be encountered and make his own independent determination of the temporary BMPs that will be required to accomplish effective erosion prevention and sediment control.

The Contractor shall be responsible for filing the KPDES permit Notice of Termination (NOT) with the Kentucky DOW and any local MS4 program that has jurisdiction. The NOT shall be filed after the Engineer agrees that the project is stabilized or the project has been formally accepted.

Payment: Payment will be by lump sum under the bid item "K.P.D.E.S. Permit & Temporary Erosion Control".

Note 1: The KYTC shall re-submit a right-of-way certification form for this project prior to AWARD of all Federal-Aid construction contracts. Award must not be made until after KYTC has obtained full legal possession and fair market value for all parcels has been paid or deposited with the court and FHWA has concurred in the re-submitted right-of-way certification.

☐ **Condition 2.** Although all necessary rights-of-way have not been fully acquired, the right to occupy and use all rights-of-way required for the proper execution of the project has been acquired. Trial or appeal of some parcels may be pending in court and on other parcels full legal possession has not been obtained, but right of entry has been obtained, the occupants of all lands and improvements have vacated, and KYTC has physical possession and right to remove, salvage, or demolish all improvements. Fair market value has been paid or deposited with the court for most parcels. Fair market value for all pending parcels will be paid or deposited with the court prior to AWARD of construction contract. (See note 1 below.)

☐ **Condition 1.** All necessary rights-of-way, including control of access rights when applicable, have been acquired including legal and physical possession. Trial or appeal of cases may be pending in court but legal possession has been obtained. There may be some improvements remaining on the right-of-way, but all occupants have vacated the lands and improvements, and KYTC has physical possession and the rights to remove, salvage, or demolish all improvements and enter on all land. Fair market value has been paid or deposited with the court.

☐ Per 23 CFR 635.309, the KYTC hereby certify that all relocatees have been relocated to decent, safe, and sanitary housing or that KYTC has made available to relocatees adequate replacement housing in accordance with the provisions of the current FHWA directive(s) covering the administration of the Highway Relocation Assistance Program and that at least one of the following three conditions has been met. (Check those that apply.)

Projects that require new or additional right-of-way acquisitions and/or relocations

☐ The proposed transportation improvement will be built within the existing rights-of-way and there are no properties to be acquired, individuals, families, and businesses ("relocatees") to be relocated, or improvements to be removed as a part of this project.

Projects that require NO new or additional right-of-way acquisitions and/or relocations

Project Name: Pavement Rehab INT-(P)		Letting Date: October 19, 2012
Project #:	FD04 SPP 102 0075 055-059	County: Rockcastle
Item #:	08-2012.00	Federal #: N/A
Description of Project: Mill and Thin Asphalt Overlay on I-75 Southbound from MP 55.744 to MP 58.954.		

Date: October 1, 2012

This form must be completed and submitted to FHWA with the PS&E package for federal-aid funded Interstate, Appalachia, and Major projects. This form shall also be submitted to FHWA for all federal-aid projects that fall under Conditions No. 2 or 3 outlined elsewhere in this form. When Condition No. 2 or 3 apply, KYTC shall resubmit this ROW Certification prior to construction contract Award. For all other federal-aid projects, this form shall be completed and retained in the KYTC project file.

<input checked="" type="checkbox"/> Federal Funded	<input type="checkbox"/> State Funded	<input type="checkbox"/> Re-Certification
<input type="checkbox"/> Original		

Right-of-Way Certification Form

Page 2

Approved:	Printed Name	Signature	FHWA, ROW Officer (when applicable)
Approved:	Printed Name	Signature	KYTC, Director of ROW & Utilities
Approved:	Printed Name	Signature	Right-of-Way Supervisor

Charles Hale

10/1/12

Note 2: The KYTC may request authorization on this basis only in unique and unusual circumstances. Proceeding to bid letting shall be the exception and never become the rule. In all cases, the KYTC shall make extraordinary efforts to expedite completion of the acquisition, payment for all affected parcels, and the relocation of all relocatees prior to AWARD of all Federal-aid construction contracts or force account construction.

☐ **Condition 3.** The acquisition or right of occupancy and use of a few remaining parcels are not complete and/or some parcels still have occupants. However, all remaining occupants have had replacement housing made available to them in accordance with 49 CFR 24.204. The KYTC is hereby requesting authorization to advertise this project for bids and to proceed with bid letting even though the necessary rights-of-way will not be fully acquired, and/or some occupants will not be relocated, and/or the fair market value will not be paid or deposited with the court for some parcels until after bid letting. KYTC will fully meet all the requirements outlined in 23 CFR 635.309(c)(3) and 49 CFR 24.102(i) and will expedite completion of all acquisitions, relocations, and full payments after bid letting and prior to AWARD of the construction contract or force account construction. A full explanation and reason for this request, including identification of each such parcel and dates on which acquisitions, payments, and relocations will be completed, is attached to this certification form for FHWA concurrence. (See note 2.)

There are 0 billboards and/or 0 cemeteries involved on this project.

There are 0 water or monitoring wells on parcels , , and . All have been acquired and are the responsibility of the project contractor to close/cap.

[illegible][illegible]

This project has _____ total number of parcels to be acquired, and _____ total number of individuals or families to be relocated, as well as _____ total number of businesses to be relocated.

Project Name:	Pavement Rehab INT-(P)
Project #:	FD04 SPP 102 0075 055-059
Item #:	08-2012.00
Letting Date:	October 19, 2012
County:	Rockcastle
Federal #:	N/A

Date: October 1, 2012

Right-of-Way Certification Form

Revised 2/22/11

PART II

SPECIFICATIONS AND STANDARD DRAWINGS

SPECIFICATIONS REFERENCE

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2012* and *Standard Drawings, Edition of 2012 with the 2012 Revision*.

**Supplemental Specifications to the Standard Specifications for Road and
Bridge Construction, 2012 Edition**

(Effective with the August 17, 2012 Letting)

Subsection:	402.03.02 Contractor Quality Control and Department Acceptance.
Part:	D) Testing Responsibilities.
Number:	4) Density.
Revision:	Replace the second sentence of the Option A paragraph with the following: Perform coring by the end of the following work day.
Subsection:	606.03.17 Special Requirements for Latex Concrete Overlays.
Part:	A) Existing Bridges and New Structures.
Number:	1) Prewetting and Grout-Bond Coat.
Revision:	Add the following sentence to the last paragraph: Do not apply a grout-bond coat on bridge decks prepared by hydrodemolition.
Subsection:	609.03 Construction.
Revision:	Replace Subsection 609.03.01 with the following: 609.03.01 A) Swinging the Spans. Before placing concrete slabs on steel spans or precast concrete release the temporary erection supports under the bridge and swing the span free on its supports. 609.03.01 B) Lift Loops. Cut all lift loops flush with the top of the precast beam once the beam is placed in the final location and prior to placing steel reinforcement. At locations where lift loops are cut, paint the top of the beam with galvanized or epoxy paint.

SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS

This Special Note will apply when indicated on the plans or in the proposal.

1.0 DESCRIPTION. Furnish, install, operate, and maintain variable message signs at the locations shown on the plans or designated by the Engineer. Remove and retain possession of variable message signs when they are no longer needed on the project.

2.0 MATERIALS.

2.1 General. Use LED Variable Message Signs Class I, II, or III, as appropriate, from the Department's List of Approved Materials.

Unclassified signs may be submitted for approval by the Engineer. The Engineer may require a daytime and nighttime demonstration. The Engineer will make a final decision within 30 days after all required information is received.

2.2 Sign and Controls. All signs must:

- 1) Provide 3-line messages with each line being 8 characters long and at least 18 inches tall. Each character comprises 35 pixels.
- 2) Provide at least 40 preprogrammed messages available for use at any time. Provide for quick and easy change of the displayed message; editing of the message; and additions of new messages.
- 3) Provide a controller consisting of:
 - a) Keyboard or keypad.
 - b) Readout that mimics the actual sign display. (When LCD or LCD type readout is used, include backlighting and heating or otherwise arrange for viewing in cold temperatures.)
 - c) Non-volatile memory or suitable memory with battery backup for storing pre-programmed messages.
 - d) Logic circuitry to control the sequence of messages and flash rate.
- 4) Provide a serial interface that is capable of supporting complete remote control ability through land line and cellular telephone operation. Include communication software capable of immediately updating the message, providing complete sign status, and allowing message library queries and updates.
- 5) Allow a single person easily to raise the sign to a satisfactory height above the pavement during use, and lower the sign during travel.
- 6) Be Highway Orange on all exterior surfaces of the trailer, supports, and controller cabinet.
- 7) Provide operation in ambient temperatures from -30 to + 120 degrees Fahrenheit during snow, rain and other inclement weather.
- 8) Provide the driver board as part of a module. All modules are interchangeable, and have plug and socket arrangements for disconnection and reconnection. Printed circuit boards associated with driver boards have a conformable coating to protect against moisture.
- 9) Provide a sign case sealed against rain, snow, dust, insects, etc. The lens is UV stabilized clear plastic (polycarbonate, acrylic, or other approved material) angled to prevent glare.
- 10) Provide a flat black UV protected coating on the sign hardware, character PCB, and appropriate lens areas.
- 11) Provide a photocell control to provide automatic dimming.

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- 12) Allow an on-off flashing sequence at an adjustable rate.
- 13) Provide a sight to aim the message.
- 14) Provide a LED display color of approximately 590 nm amber.
- 15) Provide a controller that is password protected.
- 16) Provide a security device that prevents unauthorized individuals from accessing the controller.
- 17) Provide the following 3-line messages preprogrammed and available for use when the sign unit begins operation:

/KEEP/RIGHT/⇒⇒⇒/	/MIN/SPEED/**MPH/
/KEEP/LEFT/⇐⇐⇐/	/ICY/BRIDGE/AHEAD/ /ONE
/LOOSE/GRAVEL/AHEAD/	LANE/BRIDGE/AHEAD/
/RD WORK/NEXT/**MILES/	/ROUGH/ROAD/AHEAD/
/TWO WAY/TRAFFIC/AHEAD/	/MERGING/TRAFFIC/AHEAD/
/PAINT/CREW/AHEAD/	/NEXT/***/MILES/
/REDUCE/SPEED/**MPH/	/HEAVY/TRAFFIC/AHEAD/
/BRIDGE/WORK/***() FT/	/SPEED/LIMIT/**MPH/
/MAX/SPEED/**MPH/	/BUMP/AHEAD/
/SURVEY/PARTY/AHEAD/	/TWO/WAY/TRAFFIC/

*Insert numerals as directed by the Engineer.

Add other messages during the project when required by the Engineer.

2.3 Power.

- 1) Design solar panels to yield 10 percent or greater additional charge than sign consumption. Provide direct wiring for operation of the sign or arrow board from an external power source to provide energy backup for 21 days without sunlight and an on-board system charger with the ability to recharge completely discharged batteries in 24 hours.

3.0 CONSTRUCTION. Furnish and operate the variable message signs as designated on the plans or by the Engineer. Ensure the bottom of the message panel is a minimum of 7 feet above the roadway in urban areas and 5 feet above in rural areas when operating. Use Class I, II, or III signs on roads with a speed limit less than 55 mph. Use Class I or II signs on roads with speed limits 55 mph or greater.

Maintain the sign in proper working order, including repair of any damage done by others, until completion of the project. When the sign becomes inoperative, immediately repair or replace the sign. Repetitive problems with the same unit will be cause for rejection and replacement.

Use only project related messages and messages directed by the Engineer, unnecessary messages lessen the impact of the sign. Ensure the message is displayed in either one or 2 phases with each phase having no more than 3 lines of text. When no message is needed, but it is necessary to know if the sign is operable, flash only a pixel.

When the sign is not needed, move it outside the clear zone or where the Engineer directs. Variable Message Signs are the property of the Contractor and shall be removed from the project when no longer needed. The Department will not assume ownership of these signs.

4.0 MEASUREMENT. The final quantity of Variable Message Sign will be

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the actual number of individual signs acceptably furnished and operated during the project. The Department will not measure signs replaced due to damage or rejection.

5.0 PAYMENT. The Department will pay for the Variable Message Signs at the unit price each. The Department will not pay for signs replaced due to damage or rejection. Payment is full compensation for furnishing all materials, labor, equipment, and service necessary to, operate, move, repair, and maintain or replace the variable message signs. The Department will make payment for the completed and accepted quantities under the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
02671	Portable Changeable Message Sign	Each

Effective June 15, 2012

PART III

EMPLOYMENT, WAGE AND RECORD REQUIREMENTS

TRANSPORTATION CABINET DEPARTMENT OF HIGHWAYS

LABOR AND WAGE REQUIREMENTS APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS

- I. Application
- II. Nondiscrimination of Employees (KRS 344)
- III. Payment of Predetermined Minimum Wages
- IV. Statements and Payrolls

I. APPLICATION

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

II. NONDISCRIMINATION OF EMPLOYEES

AN ACT OF THE KENTUCKY GENERAL ASSEMBLY TO PREVENT DISCRIMINATION IN EMPLOYMENT KRS CHAPTER 344 EFFECTIVE JUNE 16, 1972

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (between forty and seventy); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age (between forty and seventy). The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, disability or age (between forty and seventy), except that such notice or advertisement may indicate a preference, limitation, or specification based on religion, or national origin when religion, or national origin is a bona fide occupational qualification for employment.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual

because of his race, color, religion, national origin, sex, disability or age (between forty and seventy), in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

III. PAYMENT OF PREDETERMINED MINIMUM WAGES

1. These special provisions are supplemented elsewhere in the contract by special provisions which set forth certain predetermined minimum wage rates. The contractor shall pay not less than those rates.

2. The minimum wage determination schedule shall be posted by the contractor, in a manner prescribed by the Department of Highways, at the site of the work in prominent places where it can be easily seen by the workers.

IV. STATEMENTS AND PAYROLLS

1. All contractors and subcontractors affected by the terms of KRS 337.505 to 337.550 shall keep full and accurate payroll records covering all disbursements of wages to their employees to whom they are required to pay not less than the prevailing rate of wages. Payrolls and basic records relating thereto will be maintained during the course of the work and preserved for a period of one (1) year from the date of completion of this contract.

2. The payroll records shall contain the name, address and social security number of each employee, his correct classification, rate of pay, daily and weekly number of hours worked, itemized deductions made and actual wages paid.

3. The contractor shall make his daily records available at the project site for inspection by the State Department of Highways contracting office or his authorized representative.

Periodic investigations shall be conducted as required to assure compliance with the labor provisions of the contract. Interrogation of employees and officials of the contractor shall be permitted during working hours.

Aggrieved workers, Highway Managers, Assistant District Engineers, Resident Engineers and Project Engineers shall report all complaints and violations to the Division of Contract Procurement.

The contractor shall be notified in writing of apparent violations. The contractor may correct the reported violations and notify the Department of Highways of the action taken or may request an informal hearing. The request for hearing shall be in writing within ten (10) days after receipt of the notice of the reported violation. The contractor may submit

records and information which will aid in determining the true facts relating to the reported violations.

Any person or organization aggrieved by the action taken or the findings established as a result of an informal hearing by the Division of Contract Procurement may request a formal hearing.

4. The wages of labor shall be paid in legal tender of the United States, except that this condition will be considered satisfied if payment is made by a negotiable check, on a solvent bank, which may be cashed readily by the employee in the local community for the full amount, without discount or collection charges of any kind. Where checks are used for payments, the contractor shall make all necessary arrangements for them to be cashed and shall give information regarding such arrangements.

5. No fee of any kind shall be asked or accepted by the contractor or any of his agents from any person as a condition of employment on the project.

6. No laborers shall be charged for any tools used in performing their respective duties except for reasonably avoidable loss or damage thereto.

7. Every employee on the work covered by this contract shall be permitted to lodge, board, and trade where and with whom he elects and neither the contractor nor his agents, nor his employees shall directly or indirectly require as a condition of employment that an employee shall lodge, board or trade at a particular place or with a particular person.

8. Every employee on the project covered by this contract shall be an employee of either the prime contractor or an approved subcontractor.

9. No charge shall be made for any transportation furnished by the contractor or his agents to any person employed on the work.

10. No individual shall be employed as a laborer or mechanic on this contract except on a wage basis, but this shall not be construed to prohibit the rental of teams, trucks or other equipment from individuals.

No Covered employee may be employed on the work except in accordance with the classification set forth in the schedule mentioned above; provided, however, that in the event additional classifications are required, application shall be made by the contractor to the Department of Highways and (1) the Department shall request appropriate classifications and rates from the proper agency, or (2) if there is urgent need for additional classification to avoid undue delay in the work, the contractor may employ such workmen at rates deemed comparable to rates established for similar classifications provided he has made written application through the Department of Highways, addressed to the proper agency, for the supplemental rates. The contractor shall retroactively adjust, upon receipt of the supplemental rates schedule, the wages of any employee paid less than the established rate and may adjust the wages of any employee overpaid.

11. No contractor or subcontractor contracting for any part of the contract work which may require or involve the employment of laborers or mechanics shall require or permit any laborer or mechanic in any work-week in which he is employed on such work, to work in excess of eight hours in any calendar day or in excess of forty hours in such work-week unless such laborer or mechanic receives compensation at a rate not less than one and one half times his basic rate of pay for all hours worked in excess of eight hours in any calendar day or in excess of forty hours in such work-week. A laborer, workman or mechanic and an employer may enter into a written agreement or a collective bargaining agreement to work more than eight (8) hours a calendar day but not more than ten (10) hours a calendar day for the straight time hourly rate. This agreement shall be in writing and shall be executed prior to the employee working in excess of eight (8) hours, but not more than ten (10) hours, in any one (1) calendar day.

12. Payments to the contractor may be suspended or withheld due to failure of the contractor to pay any laborer or

mechanic employed or working on the site of the work, all or part of the wages required under the terms of the contract. The Department may suspend or withhold payments only after the contractor has been given written notice of the alleged violation and the contractor has failed to comply with the wage determination of the Department of Highways.

13. Contractors and subcontractors shall comply with the sections of Kentucky Revised Statutes, Chapter 337 relating to contracts for Public Works.

Revised 2-16-95

EXECUTIVE BRANCH CODE OF ETHICS

In the 1992 regular legislative session, the General Assembly passed and Governor Brereton Jones signed Senate Bill 63 (codified as KRS 11A), the Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (6) provides:

No present or former public servant shall, within six (6) months of following termination of his office or employment, accept employment, compensation or other economic benefit from any person or business that contracts or does business with the state in matters in which he was directly involved during his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, provided that, for a period of six (6) months, he personally refrains from working on any matter in which he was directly involved in state government. This subsection shall not prohibit the performance of ministerial functions, including, but not limited to, filing tax returns, filing applications for permits or licenses, or filing incorporation papers.

KRS 11A.040 (8) states:

A former public servant shall not represent a person in a matter before a state agency in which the former public servant was directly involved, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not as a way to obtain private benefits.

If you have worked for the executive branch of state government within the past six months, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, Room 136, Capitol Building, 700 Capitol Avenue, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Kentucky Equal Employment Opportunity Act of 1978

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:
<https://www.eProcurement.ky.gov>.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **finance.contractcompliance@ky.gov** or by phone at 502-564-2874.

KENTUCKY LABOR CABINET
PREVAILING WAGE DETERMINATION
CURRENT REVISION
HIGHWAY CONSTRUCTION LOCALITY NO. II

Determination No. CR-III-II-HWY

Project No.
Highway

Date of Determination: September 5, 2012

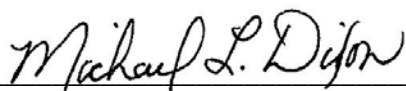
This schedule of the prevailing rate of wages for Locality No. II including the counties of ADAIR, BARREN, BELL, BREATHITT, CASEY, CLAY, CLINTON, CUMBERLAND, ESTILL, FLOYD, GARRARD, GREEN, HARLAN, HART, JACKSON, JOHNSON, KNOTT, KNOX, LAUREL, LAWRENCE, LEE, LESLIE, LETCHER, LINCOLN, MCCREARY, MAGOFFIN, MARTIN, MENIFEE, METCALFE, MONROE, MORGAN, OWSLEY, PERRY, PIKE, POWELL, PULASKI, ROCKCASTLE, RUSSELL, TAYLOR, WAYNE, WHITLEY, and WOLFE has been determined in accordance with the provisions of KRS 337.505 to 337.550. This determination shall be referred to as Prevailing Wage Determination No. CR-III-II-HWY.

The following schedule of rates is to be used for highway construction projects advertised or awarded by the Kentucky Transportation Cabinet. This includes any contracts for the relocation of any utilities or other incidental construction projects advertised or awarded by public authorities as a result of the highway construction project.

Apprentices or trainees shall be permitted to work in accordance with Administrative Regulations adopted by the Commissioner of the Department of Workplace Standards. Copies of these regulations will be furnished upon request to any interested person.

Overtime is to be computed at not less than one and one-half (1 1/2) times the indicated BASE RATE for all hours worked in excess of eight (8) hours per day, or in excess of forty (40) hours per week. However, KRS 337.540 permits an employee and employer to agree, in writing, that the employee will be compensated at a straight time base rate for hours worked in excess of eight (8) hours in any one calendar day, but not more than ten (10) hours worked in any one calendar day, if such written agreement is prior to the over eight (8) hours in a calendar day actually being worked, or where provided for in a collective bargaining agreement. The fringe benefit rate is to be paid for each hour worked at a straight time rate for all hours worked. Fringe benefit amounts are applicable for all hours worked except when otherwise noted. Welders will receive rate for craft in which welding is incidental.

No laborer, workman or mechanic shall be paid at a rate less than that of the General Laborer except those classified as bona fide apprentices registered with the Kentucky State Apprenticeship Supervisor unless otherwise specified in this schedule of wage rates.



Michael Dixon, Commissioner
Department of Workplace Standards

CLASSIFICATIONS	RATE AND FRINGE BENEFITS	
BOILERMAKERS:	BASE RATE	\$24.65
	FRINGE BENEFIT	12.94

BRICKLAYERS:		
Bricklayers:	BASE RATE	\$22.90
	FRINGE BENEFITS	8.00
Stone Mason:	BASE RATE	\$21.50
	FRINGE BENEFITS	8.50

CARPENTERS:		
Carpenters:	BASE RATE	\$23.71
	FRINGE BENEFITS	13.50
Piledrivers:	BASE RATE	\$23.96
	FRINGE BENEFITS	13.50

CEMENT MASONS:	BASE RATE	\$21.25
	FRINGE BENEFITS	8.50

ELECTRICIANS:	*BASE RATE	\$29.36
	FRINGE BENEFITS	10.55
<p>*When workmen are required to work from bosum chairs, trusses, stacks, tanks, scaffolds, catwalks, radio and T.V. towers, structural steel (open, unprotected, unfloored raw steel), and bridges or similar hazardous locations where workmen are subject to a direct fall, except where using JLG's and bucket trucks up to 75 feet: Add 25% to workman's base rate for 50 to 75 feet, and add 50% to workman's base rate for over 75 feet.</p>		
LINEMAN:	*BASE RATE	\$30.09
	FRINGE BENEFITS	10.94
EQUIPMENT OPERATOR:	*BASE RATE	\$26.90
	FRINGE BENEFITS	10.31
GROUNDSMAN:	*BASE RATE	\$17.79
	FRINGE BENEFITS	8.51

IRONWORKERS:	BASE RATE	\$ 26.34
	FRINGE BENEFITS	18.54

CLASSIFICATIONS

RATE AND FRINGE BENEFITS

LABORERS:

GROUP 1: Aging and curing of concrete (any mode or method), asbestos abatement worker, asphalt plant laborers, asphalt laborers; batch truck dumpers; carpenter tenders, cement mason tenders, cleaning of machines, concrete laborers, demolition laborers, dredging laborers, drill helper, environmental laborer - nuclear, radiation, toxic and hazardous waste – Level D, flagmen, grade checkers, all hand digging and hand back filling, highway marker placers, landscaping laborers, mesh handlers and placers, puddler, railroad laborers, rip-rap and grouters, right of way laborers, sign, guard rail and fence installers (all types), signalmen, sound barrier installer, storm and sanitary sewer laborers, swampers, truck spotters and dumpers, wrecking of concrete forms, general cleanup:

HEAVY & HIGHWAY	BASE RATE	\$21.15
	FRINGE BENEFITS	11.41

GROUP 2: Batter board men (sanitary and storm sewer), brickmason tenders, mortar mixer operator, scaffold builders, burner and welder, bushammers, chain saw operator, concrete saw operators, deckhand scow man, dry cement handlers, environmental laborers – nuclear, radiation, toxic and hazardous waste – Level C, forklift operators for masonry, form setters, green concrete cutting, hand operated grouter and grinder machine operator, jack hammers, lead paint abatement, pavement breakers, paving joint machine, pipe layers – laser operators (non-metallic), plastic pipe fusion, power driven Georgia buggy and wheel barrow, power post hole diggers, precast manhole setters, walk-behind tampers, walk-behind trenchers, sand blasters, concrete chippers, surface grinders, vibrator operators, wagon drillers:

HEAVY & HIGHWAY	BASE RATE	\$21.40
	FRINGE BENEFITS	11.41

GROUP 3: Air track driller (all types), asphalt luteman and rakersm gunnite nozzleman, gunnite operators and mixers, grout pump operator, powderman and blaster, side rail setters, rail paved ditches, screw operators, tunnel laborers (free air), and water blasters:

HEAVY & HIGHWAY	BASE RATE	\$21.45
	FRINGE BENEFITS	11.41

GROUP 4: Caisson workers (free air), cement finishers, environmental laborer – nuclear, radiation, toxic and hazardous waste – Level A and B, miners and drillers (free air), tunnel blasters, and tunnel mockers (free air), directional and horizontal boring, air track drillers (all types), powder man and blasters, troxler and concrete tester if laborer is utilized:

HEAVY & HIGHWAY	BASE RATE	\$22.05
	FRINGE BENEFITS	11.41

OPERATING ENGINEERS:

Group A-1:
NCCCO or OECP Certified; Crane, dragline, hoist (1 drum when used for stack or chimney construction or repair), hoisting engineer (2 or more drums), orangepeel, overhead crane, piledriver, truck crane, tower crane, hydraulic crane:

BASE RATE	\$28.40
FRINGE BENEFITS	13.40

CLASSIFICATIONS

RATE AND FRINGE BENEFITS

Group A:
Auto patrol, batcher plant, bituminous paver, cable-way, clamshell, concrete mixer (21 cu. ft. or over), concrete pump, crane, crusher plant, derrick, derrick boat, ditching and trenching machine, dragline, dredge engineer, elevator (regardless of ownership when used for hoisting any building material), elevating grader and all types of loaders, hoe-type machine, hoisting engine, locomotive, LeTourneau or carry-all scoop, bulldozer, mechanic, orangepeel bucket, piledriver, power blade, roller (bituminous), roller (earth), roller (rock), scarifier, shovel, tractor shovel, truck crane, well points, winch truck, push dozer, grout pump, high lift, fork lift (regardless of lift height), all types of boom cats, multiple operator, core drill, tow or push boat, A-Frame winch truck, concrete paver, gradeall, hoist, hyster, material pump, pumpcrete, ross carrier, sheepfoot, sideboom, throttle-valve man, rotary drill, power generator, mucking machine, rock spreader attached to equipment, scoopmobile, KeCal loader, tower cranes (French, German and other types), hydrocrane, tugger, backfiller guries, self-propelled compactor, self-contained hydraulic percussion drill:

BASE RATE\$27.35

FRINGE BENEFITS13.40

Group B:
All air compressors (200 cu. ft. per min. or greater capacity), bituminous mixer, concrete mixer (under 21 cu. ft.), welding machine, form grader, tractor (50 H.P. and over), bull float, finish machine, outboard motor boat, brakeman, mechanic helper, whirly oiler, tractair and road widening trencher, articulating trucks:

BASE RATE\$24.87

FRINGE BENEFITS13.40

Group B2:
Greaser on grease facilities servicing heavy equipment:

BASE RATE\$25.26

FRINGE BENEFITS13.40

Group C:
Bituminous distributor, cement gun, conveyor, mud jack, paving joint machine, pump, tamping machine, tractors (under 50 H.P.), vibrator, oiler, air compressors (under 200 cu. ft. per min. capacity), concrete saw, burlap and curing machine, hydro seeder, power form handling equipment, deckhand oiler, hydraulic post driver:

BASE RATE\$24.60

FRINGE BENEFITS13.40

PAINTERS:

All Excluding Bridges:

BASE RATE\$19.92

FRINGE BENEFITS9.57

Bridges:

BASE RATE\$23.92

FRINGE BENEFITS10.07

CLASSIFICATIONS	RATE AND FRINGE BENEFITS	
PLUMBERS:	BASE RATE	\$22.52
	FRINGE BENEFITS	7.80

SHEET METAL:	BASE RATE	\$20.40
	FRINGE BENEFITS	7.80

TRUCK DRIVERS:		
Truck helper and warehouseman:	BASE RATE	\$22.82
	FRINGE BENEFITS	13.50
Driver, winch truck and A-Frame when used in transporting materials:	BASE RATE	\$23.21
	FRINGE BENEFITS	13.50
Driver, (semi-trailer or pole trailer), driver (dump truck, tandem axle), driver of distributor:	BASE RATE	\$23.11
	FRINGE BENEFITS	13.50
Driver on mixer trucks (all types):	BASE RATE	\$23.14
	FRINGE BENEFITS	13.50
Truck mechanic:	BASE RATE	\$23.11
	FRINGE BENEFITS	13.50
Driver (3 tons and under), tire changer and truck mechanic helper:	BASE RATE	\$22.93
	FRINGE BENEFITS	13.50
Driver on pavement breakers:	BASE RATE	\$23.21
	FRINGE BENEFITS	13.50
Driver (over 3 tons), driver (truck mounted rotary drill):	BASE RATE	\$23.11
	FRINGE BENEFITS	13.50
Driver, Euclid and other heavy earth moving equipment and Low Boy:	BASE RATE	\$23.21
	FRINGE BENEFITS	13.50
Greaser on greasing facilities:	BASE RATE	\$22.93
	FRINGE BENEFITS	13.50

Kentucky Determination No. CR-III-II-HWY dated September 5, 2012

Fringe benefit amounts are applicable for all hours worked except when otherwise noted.

No laborer, workman or mechanic shall be paid at a rate less than that of the General Laborer except those classified as bona fide apprentices registered with the Kentucky State Apprenticeship Supervisor unless otherwise specified in this schedule of wage rates.

These rates are listed pursuant to the Kentucky Determination No. CR-III-II-HWY dated September 5, 2012. Apprentices or trainees shall be permitted to work as such subject to Administrative Regulations adopted by the Commissioner of Workplace Standards. Copies of these regulations will be furnished upon request from any interested person.

Before using apprentices on the job the contractor shall present to the Contracting Officer written evidence of registration of such employees in a program of a State apprenticeship and training agency approved and recognized by the U. S. Bureau of Apprenticeship and Training. In the absence of such a State agency, the contractor shall submit evidence of approval and registration by the U. S. Bureau of Apprenticeship and Training.

The contract or shall submit to the Contracting Officer, written evidence of the established apprenticeship-journeyman ratios and wage rates in the project area, which will be the basis for establishing such ratios and rates for the project under the applicable contract provisions.

TO: EMPLOYERS/EMPLOYEES

PREVAILING WAGE SCHEDULE:

The wages indicated on this wage schedule are the least permitted to be paid for the occupations indicated. When an employee works in more than one classification, the employer must record the numbers of hours worked in each classification at the prescribed hourly base rate.

OVERTIME:

Overtime is to be paid after an employee works eight (8) hours a day or forty (40) hours a week, whichever gives the employee the greater wage. At least time and one-half the base rate is required for all overtime. A laborer, workman or mechanic and an employer may enter into a written agreement or a collective bargaining agreement to work more than eight (8) hours a calendar day but not more than ten (10) hours a calendar day for the straight time hourly rate. Wage violations or questions should be directed to the designated Engineer or to the undersigned.

Ryan Griffith, Director
Division of Construction Procurement
Frankfort, Kentucky 40622

PART IV

INSURANCE

INSURANCE

The Contractor shall procure and maintain the following insurance in addition to the insurance required by law:

- 1) Commercial General Liability-Occurrence form – not less than \$2,000,000 General aggregate, \$2,000,000 Products & Completed Aggregate, \$1,000,000 Personal & Advertising, \$1,000,000 each occurrence.
- 2) Automobile Liability- \$1,000,000 per accident
- 3) Employers Liability:
 - a) \$100,000 Each Accident Bodily Injury
 - b) \$500,000 Policy limit Bodily Injury by Disease
 - c) \$100,000 Each Employee Bodily Injury by Disease
- 4) The insurance required above must be evidenced by a Certificate of Insurance and this Certificate of Insurance must contain one of the following statements:
 - a) "policy contains no deductible clauses."
 - b) "policy contains _____ (amount) deductible property damage clause but company will pay claim and collect the deductible from the insured."
- 5) KENTUCKY WORKMEN'S COMPENSATION INSURANCE. The contractor shall furnish evidence of coverage of all his employees or give evidence of self-insurance by submitting a copy of a certificate issued by the Workmen's Compensation Board.

The cost of insurance is incidental to all contract items. All subcontractors must meet the same minimum insurance requirements.

PART V

BID ITEMS

PROPOSAL BID ITEMS

Report Date 10/11/12

Page 1 of 2

SECTION: 0001 - ROADWAY

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	EXTENDED PRICE
0010	00001		DGA BASE	2,829.00	TON		
0020	00018		DRAINAGE BLANKET-TYPE II-ASPH	573.00	TON		
0030	00078		CRUSHED AGGREGATE SIZE NO 2	2,753.00	TON		
0040	00100		ASPHALT SEAL AGGREGATE	360.00	TON		
0050	00103		ASPHALT SEAL COAT	43.00	TON		
0060	00194		LEVELING & WEDGING PG76-22	195.00	TON		
0070	00214		CL3 ASPH BASE 1.00D PG64-22	4,518.00	TON		
0080	00214		CL3 ASPH BASE 1.00D PG64-22 FOR PERF PIPE EDGE DRAIN CAP	526.00	TON		
0090	00219		CL4 ASPH BASE 1.00D PG76-22	11,269.00	TON		
0100	00339		CL3 ASPH SURF 0.38D PG64-22	1,666.00	TON		
0110	00342		CL4 ASPH SURF 0.38A PG76-22	3,744.00	TON		
0120	01000		PERFORATED PIPE-4 IN	7,865.00	LF		
0130	01001		PERFORATED PIPE-6 IN	8,535.00	LF		
0140	01002		PERFORATED PIPE-8 IN	9,635.00	LF		
0150	01010		NON-PERFORATED PIPE-4 IN	140.00	LF		
0160	01011		NON-PERFORATED PIPE-6 IN	112.00	LF		
0170	01012		NON-PERFORATED PIPE-8 IN	120.00	LF		
0180	01015		INSPECT & CERTIFY EDGE DRAIN SYSTEM	1.00	LS		
0190	01020		PERF PIPE HEADWALL TY 1-4 IN	15.00	EACH		
0200	01021		PERF PIPE HEADWALL TY 1-6 IN	7.00	EACH		
0210	01022		PERF PIPE HEADWALL TY 1-8 IN	10.00	EACH		
0220	01029		PERF PIPE HEADWALL TY 3-6 IN	1.00	EACH		
0230	01484		CURB BOX INLET TYPE B-T	1.00	EACH		
0240	01719		ADJUST INLET	2.00	EACH		
0250	01890		ISLAND HEADER CURB TYPE 1	20.00	LF		
0260	01904		REMOVE CURB	20.00	LF		
0270	01982		DELINEATOR FOR GUARDRAIL MONO DIRECTIONAL WHITE	86.00	EACH		
0280	02014		BARRICADE-TYPE III	6.00	EACH		
0290	02230		EMBANKMENT IN PLACE	500.00	CUYD		
0300	02237		DITCHING	11,000.00	LF		
0310	02367		GUARDRAIL END TREATMENT TYPE 1	4.00	EACH		
0320	02369		GUARDRAIL END TREATMENT TYPE 2A	6.00	EACH		
0330	02373		GUARDRAIL END TREATMENT TYPE 3	1.00	EACH		
0340	02381		REMOVE GUARDRAIL	7,962.50	LF		

PROPOSAL BID ITEMS

Report Date 10/11/12

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SECTION: 0001 - ROADWAY

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	EXTENDED PRICE
0350	02387		GUARDRAIL CONNECTOR TO BRIDGE END TY A-1	1.00	EACH		
0360	02482		CHANNEL LINING CLASS IA	3,800.00	TON		
0370	02484		CHANNEL LINING CLASS III	100.00	TON		
0380	02562		SIGNS	500.00	SQFT		
0390	02650		MAINTAIN & CONTROL TRAFFIC	1.00	LS		
0400	02671		PORTABLE CHANGEABLE MESSAGE SIGN	2.00	EACH		
0410	02676		MOBILIZATION FOR MILL & TEXT	1.00	LS		
0420	02677		ASPHALT PAVE MILLING & TEXTURING	7,238.00	TON		
0430	02696		SHOULDER RUMBLE STRIPS-SAWED	34,400.00	LF		
0440	02775		ARROW PANEL	2.00	EACH		
0450	03262		CLEAN PIPE STRUCTURE	1.00	EACH		
0460	05950		EROSION CONTROL BLANKET	1,000.00	SQYD		
0470	06412		STEEL POST MILE MARKERS	3.00	EACH		
0480	06417		FLEXIBLE DELINEATOR POST-W	106.00	EACH		
0490	06418		FLEXIBLE DELINEATOR POST-Y	12.00	EACH		
0500	06511		PAVE STRIPING-TEMP PAINT-6 IN	43,000.00	LF		
0510	06592		PAVEMENT MARKER TYPE V-B W/R	233.00	EACH		
0520	06593		PAVEMENT MARKER TYPE V-B Y/R	42.00	EACH		
0530	06600		REMOVE PAVEMENT MARKER TYPE V	233.00	EACH		
0540	10020NS		FUEL ADJUSTMENT	30,031.00	DOLL	\$1.00	\$30,031.00
0550	10030NS		ASPHALT ADJUSTMENT	52,921.00	DOLL	\$1.00	\$52,921.00
0560	20757ED		PAVEMENT REPAIR	955.00	SQYD		
0570	21802EN		G/R STEEL W BEAM-S FACE (7 FT POST)	8,162.50	LF		
0580	23143ED		KPDES PERMIT AND TEMP EROSION CONTROL	1.00	LS		
0590	23237EN10W		WATERBLAST STRIPE REMOVAL	17,200.00	LF		
0600	24189ER		DURABLE WATERBORNE MARKING-6 IN W	21,318.00	LF		
0610	24190ER		DURABLE WATERBORNE MARKING-6 IN Y	18,870.00	LF		
0620	24191ER		DURABLE WATERBORNE MARKING-12 IN W	364.00	LF		

SECTION: 0002 - MOB AND DEMOB

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	EXTENDED PRICE
0630	02568		MOBILIZATION	1.00	LS		
0640	02569		DEMOBILIZATION	1.00	LS		