



CALL NO. 304

CONTRACT ID. 121028

ROCKCASTLE COUNTY

FED/STATE PROJECT NUMBER FD04 SPP 102 0075 055-059

DESCRIPTION TENNESSEE STATE LINE-LEXINGTON ROAD (I-75)

WORK TYPE ASPHALT REHAB INTERSTATE/PARKWAY

PRIMARY COMPLETION DATE 11/30/2012

LETTING DATE: August 17, 2012

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME August 17, 2012. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

REQUIRED BID PROPOSAL GUARANTY: Not less than 5% of the total bid.

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PART I

SCOPE OF WORK

CONTRACT ID - 121028

ADMINISTRATIVE DISTRICT - 08

PROJECT(S) IDENTIFICATION AND DESCRIPTION:

COUNTY - ROCKCASTLE

PCN - DE10200751228

FD04 SPP 102 0075 055-059

TENNESSEE STATE LINE-LEXINGTON ROAD (I-75) MILL AND INTERMEDIATE ASPHALT OVERLAY ON I-75
NORTHBOUND FROM MP 55.744 TO MP 58.954. ASPHALT REHAB INTERSTATE/PARKWAY. SYP NO.
08-02009.00.

GEOGRAPHIC COORDINATES LATITUDE 37^19'05" LONGITUDE 84^17'36"

COMPLETION DATE(S):

COMPLETION DATE - November 30, 2012

APPLIES TO ENTIRE CONTRACT

CONTRACT NOTES

PROPOSAL ADDENDA

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

BID SUBMITTAL

Bidder must use the Department's Expedite Bidding Program available on the Internet web site of the Department of Highways, Division of Construction Procurement. (www.transportation.ky.gov/contract)

The Bidder must download the bid file located on the Bid Express website (www.bidx.com) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

JOINT VENTURE BIDDING

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

UNDERGROUND FACILITY DAMAGE PROTECTION

The contractor is advised that the Underground Facility Damage Protection Act of 1994, became law January 1, 1995. It is the contractor's responsibility to determine the impact of the act regarding this project, and take all steps necessary to be in compliance with the provision of the act.

SPECIAL NOTE FOR PIPE INSPECTION

Contrary to Section 701.03.08 of the 2012 Standard Specifications for Road and Bridge Construction and Kentucky Method 64-114, certification by the Kentucky Transportation Center for prequalified Contractors to perform laser/video inspection is not required on this contract. It will continue to be a requirement for the Contractor performing any laser/video pipe inspection to be prequalified for this specialized item with the Kentucky Transportation Cabinet-Division of Construction Procurement.

REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth (“certificate”) from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity’s solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by fax (502) 564-7299 or email to kytc.projectquestions@ky.gov. The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading “Questions & Answers” on the Construction Procurement website (www.transportation.ky.gov/contract). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

ACCESS TO RECORDS

The contractor, as defined in KRS 45A.030 (9) agrees that the contracting agency, the Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this

contract for the purpose of financial audit or program review. Records and other prequalification information confidentially disclosed as part of the bid process shall not be deemed as directly pertinent to the contract and shall be exempt from disclosure as provided in KRS 61.878(1)(c). The contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884.

In the event of a dispute between the contractor and the contracting agency, Attorney General, or the Auditor of Public Accounts over documents that are eligible for production and review, the Finance and Administration Cabinet shall review the dispute and issue a determination, in accordance with Secretary's Order 11-004. (See attachment)

10/18/2011

SPECIAL NOTE FOR RECIPROCAL PREFERENCE

Reciprocal preference to be given by public agencies to resident bidders

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the Expedite Bidding Program. Submittal of the Affidavit should be done along with the bid in Bid Express.

03/01/2011

ASPHALT MIXTURE

Unless otherwise noted, the Department estimates the rate of application for all asphalt mixtures to be 110 lbs/sy per inch of depth.

DGA BASE

Unless otherwise noted, the Department estimates the rate of application for DGA Base to be 115 lbs/sy per inch of depth.

DGA BASE FOR SHOULDERS

Unless otherwise noted, the Department estimates the rate of application for DGA Base for Shoulders to be 115 lbs/sy per inch of depth. The Department will not measure necessary grading and/or shaping of existing shoulders prior to placing of DGA Base, but shall be incidental to the Contract unit price per ton for DGA Base.

Accept payment at the Contract unit price per ton as full compensation for all labor, materials, equipment, and incidentals for grading and/or shaping of existing shoulders and furnishing, placing, and compacting the DGA Base.

INCIDENTAL SURFACING

The Department has included in the quantities of asphalt mixtures established in the proposal estimated quantities required for resurfacing or surfacing mailbox turnouts, farm field entrances, residential and commercial entrances, curve widening, ramp gores and tapers, and road and street approaches, as applicable. Pave these areas to the limits as shown on Standard Drawing RPM-110-06 or as directed by the Engineer. In the event signal detectors are present in the intersecting streets or roads, pave the crossroads to the right of way limit or back of the signal detector, whichever is the farthest back of the mainline. Surface or resurface these areas as directed by the Engineer. The Department will not measure placing and compacting for separate payment but shall be incidental to the Contract unit price for the asphalt mixtures.

FUEL AND ASPHALT PAY ADJUSTMENT

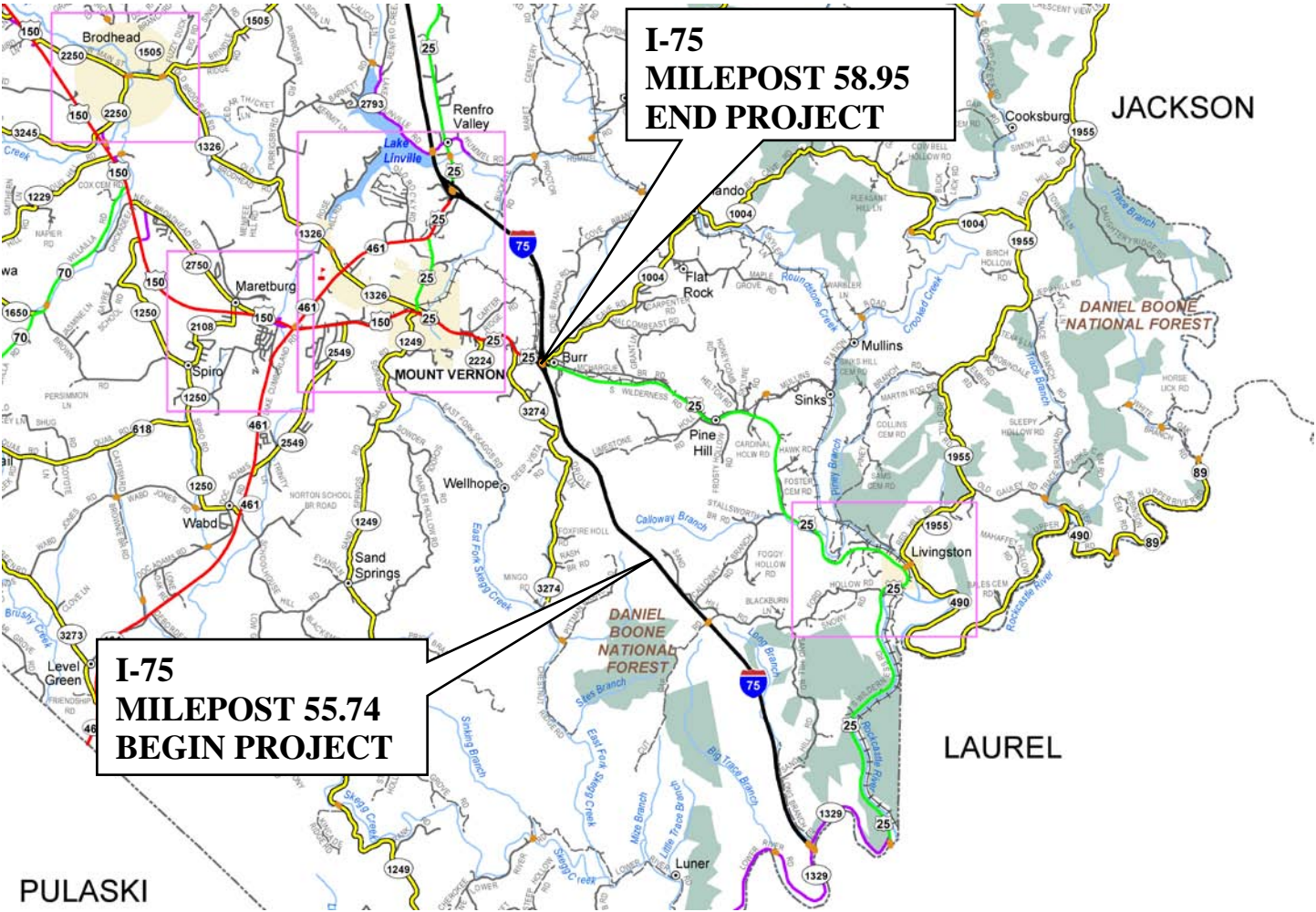
The Department has included the Contract items Asphalt Adjustment and Fuel Adjustment for possible future payments at an established Contract unit price of \$1.00. The Department will calculate actual adjustment quantities after work is completed. If existing Contract amount is insufficient to pay all items on the contract with the adjustments, the Department will establish additional monies with a change order.

ASPHALT PAVEMENT RIDE QUALITY CATEGORY A

The Department will apply Pavement Rideability Requirements on this project in accordance with Section 410, Category A.

OPTION A

Be advised that the Department will accept compaction of asphalt mixtures furnished for driving lanes and ramps, at 1 inch (25mm) or greater, on this project according to OPTION A in accordance with Section 402 and Section 403 of the current Standard Specifications. The Department will require joint cores as described in Section 402.03.02 for surface mixtures only. The Department will accept compaction of all other asphalt mixtures according to OPTION B.



ITEM NUMBERS: 8-2009.00

PROJECT NUMBER: FD52 102 0075 055-059

CONSTRUCTION NUMBER:

LETTING DATE: August 17, 2012

RECOMMENDED BY: _____ DATE: _____
Project Manager

PLAN APPROVED BY: _____ DATE: _____
State Highway Engineer

FHWA APPROVED BY: _____ DATE: _____

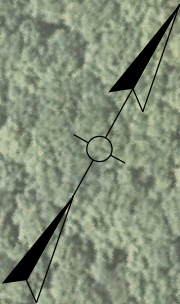
MATCHLINE (SEE SHEET 2)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.00



COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.00

MATCHLINE (SEE SHEET 3)



2805+00

2800+00

2795+00

2790+00

TS 2806+40.96

NB I-75
SB I-75

M.P. 56

NB I-75
SB I-75

SCALE: 1"=200'

MATCHLINE (SEE SHEET 1)

I-75
STATIONING INFORMATION SHEETS
(2 OF 9)

MATCHLINE (SEE SHEET 4) 2830+00

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.00

2825+00

2820+00

2815+00

PI 2813+94.21

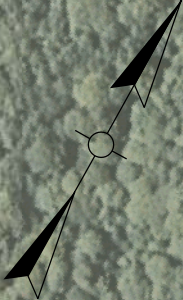
SB I-75

NB I-75

ST 2821+16.25

CS 2817+16.25

SC 2810+40.96



SCALE: 1"=200'

MATCHLINE (SEE SHEET 2)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.00



COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.00



MATCHLINE (SEE SHEET 7)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.00

2895+00

SB I-75
NB I-75

PI 2890+61.75
2890+00

2885+00

2880+00

SC 2881+12.53

TS 2878+62.53

SCALE: 1"=200'

MATCHLINE (SEE SHEET 5)

MATCHLINE (SEE SHEET 8)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.00



MATCHLINE (SEE SHEET 9)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.00



MATCHLINE (SEE SHEET 7)

SCALE: 1"=200'

I-75
STATIONING INFORMATION SHEETS
(8 OF 9)

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.00



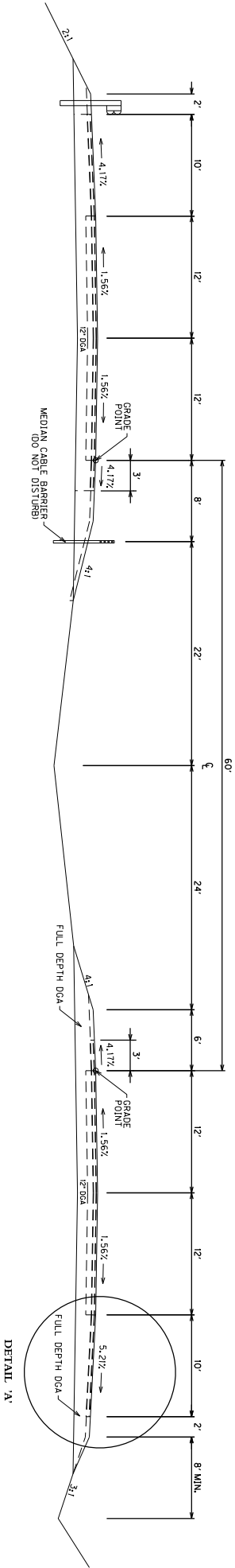
SCALE: 1"=200'

I-75 STATIONING INFORMATION SHEETS (9 OF 9)

COUNTY OF	ITEM NO.	SHEET NO.
ROCKCASTLE	8-2009.0	

I-75

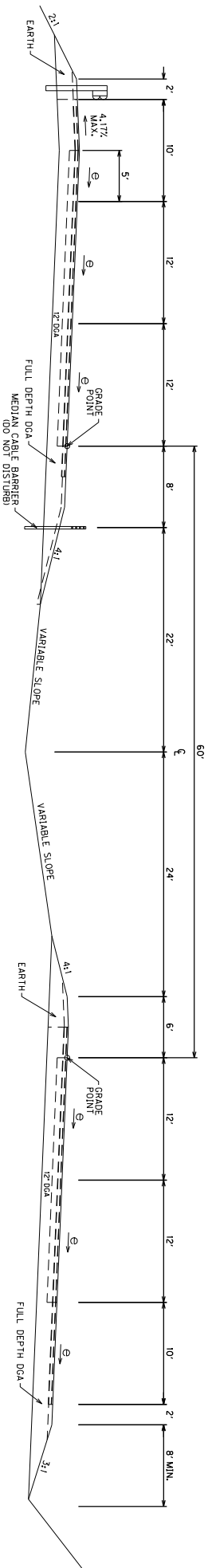
EXISTING TYPICAL SECTIONS



SOUTHBOUND LANES

NORMAL SECTION

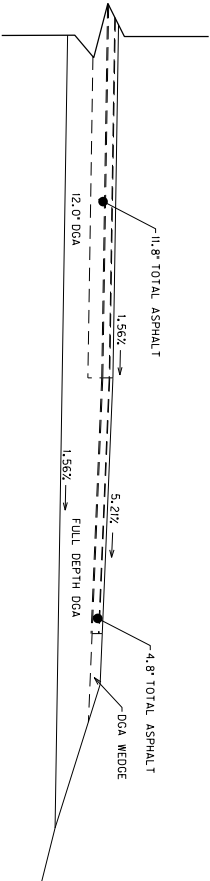
NORTHBOUND LANES



SOUTHBOUND LANES

SUPERELEVATED SECTION

NORTHBOUND LANES



DETAIL 'A'

TYPICAL SECTIONS

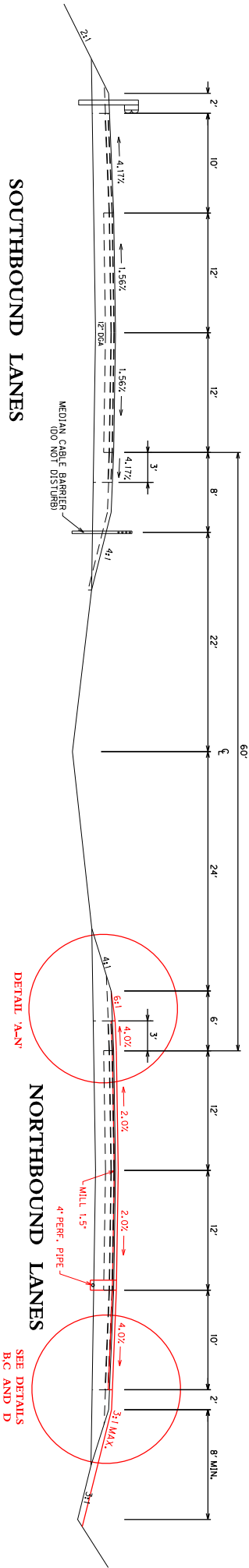
I-75

I-75

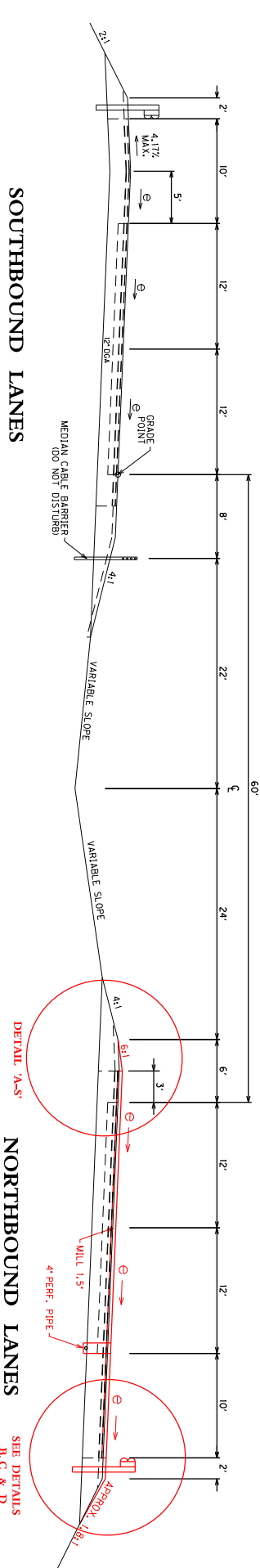
PROPOSED TYPICAL SECTIONS

M.P. 55.744 TO M.P. 58.954

COUNTY OF	ITEM NO.	SHEET NO.
ROCKCASTLE	8-2009.0	



NORMAL SECTION



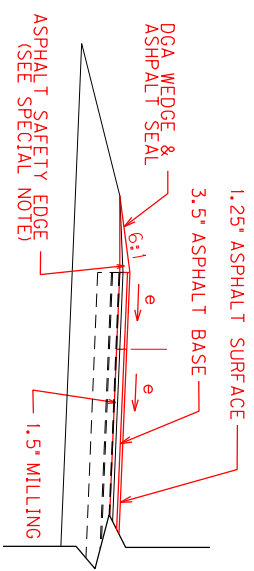
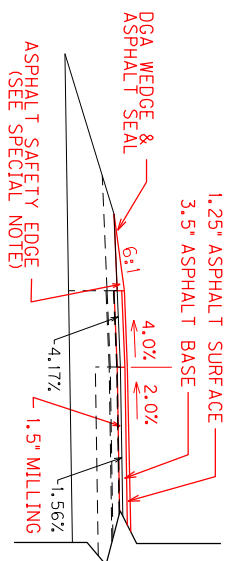
SUPERELEVATED SECTION

ASPHALT SEAL
2 APPLICATIONS:
ASPHALT SEAL AGGREGATE - 20 LB/SY
EMULSIFIED ASPHALT RS-2 - 2.4 LB/SY

DETAIL 'A-S'
SUPERELEVATED SECTION

- PAVEMENT REHABILITATION (NORTHBOUND ONLY)
- DRIVING LANES & INSIDE SHOULDERS
- 1.5" ASPHALT MILLING AND TEXTURING
- VARIABLE DEPTH DGA WEDGE INSIDE SHOULDER
- BASE ----- 3.5" CL4 ASPHALT BASE 1.0D PG76-22
- SURFACE -- 1.25" CL4 ASPHALT SURFACE 0.38A PG76-22
- OUTSIDE SHOULDER
- 1.5" ASPHALT MILLING AND TEXTURING
- VARIABLE DEPTH DGA WEDGE OUTSIDE SHOULDER
- BASE ----- 3.5" CL3 ASPHALT BASE 1.0D PG64-22
- SURFACE -- 1.25" CL3 ASPHALT SURFACE 0.38D PG64-22
- *CROSS SLOPE CORRECTION IS TO BE CONSTRUCTED IN BASE COURSE WITH LEVELING AND WEDGING.

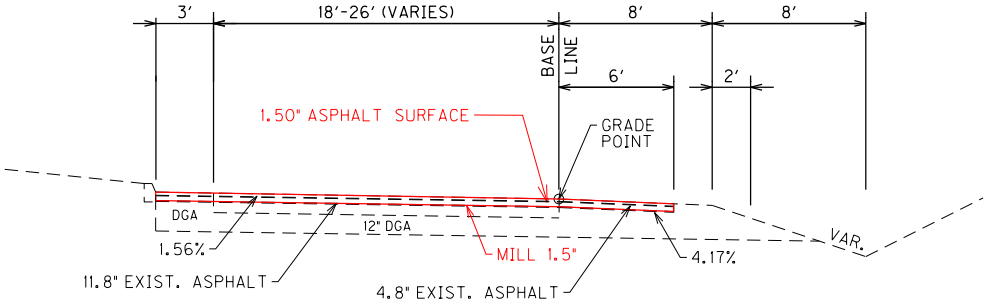
I-75
TYPICAL SECTIONS



COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.0

TYPICAL SECTIONS

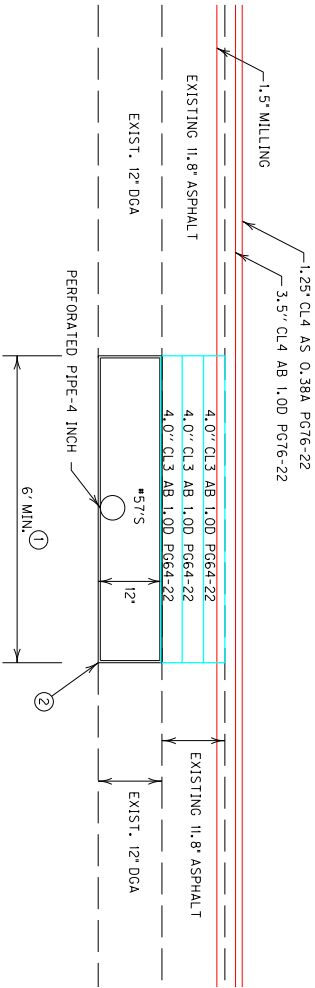
RAMPS



RAMP NORMAL SECTION

DRIVING LANES AND SHOULDERS
1.50" ASPHALT MILLING AND TEXTURING
SURFACE -- 1.50" CL3 ASPHALT SURFACE 0.38D PG64-22

COUNTY OF	ITEM NO.	SHEET NO.
ROCKCASTLE	8-2009.0	



PAVEMENT REPAIR DETAIL

- 1 APPROXIMATE LOCATIONS ARE NOTED IN THE PROPOSAL. SPECIFIC LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD. BEFORE REMOVAL OF THE EXISTING MATERIAL, SAW CUTS SHALL BE MADE ON EACH SIDE OF THE AREA TO BE REMOVED. THIS OPERATION SHALL BE PERFORMED ON THE OUTSIDE SHOULDER AND DRIVING LANES WHERE DIRECTED.

AFTER ALL EXISTING MATERIAL HAS BEEN REMOVED THEN EACH COURSE OF BACKFILL MATERIAL SHALL BE COMPACTED TO THE PROPER DENSITY FOR THE MATERIAL BEING PLACED AS REQUIRED IN THE STANDARD SPECIFICATIONS. THE 4" PERFORATED PIPE SHALL NOT BE WRAPPED AND ONLY COARSE AGGREGATE SHALL BE USED.

THE CONTRACT UNIT BID PRICE PER SQ YD FOR PAVEMENT REPAIR SHALL INCLUDE ROADWAY EXCAVATION, REMOVING PAVEMENT, SAW CUTTING, DGA, PERFORATED PIPE-4 INCH, CRUSHED AGGREGATE SIZE NO. 57, ASPHALT BASE, FABRIC-GEOTEXTILE TYPE IV AND ALL INCIDENTALS NECESSARY TO COMPLETE THE INSTALLATION AS DETAILED.

PLACE THE PERFORATED PIPE AT THE LOW POINT OF THE DISCUT.

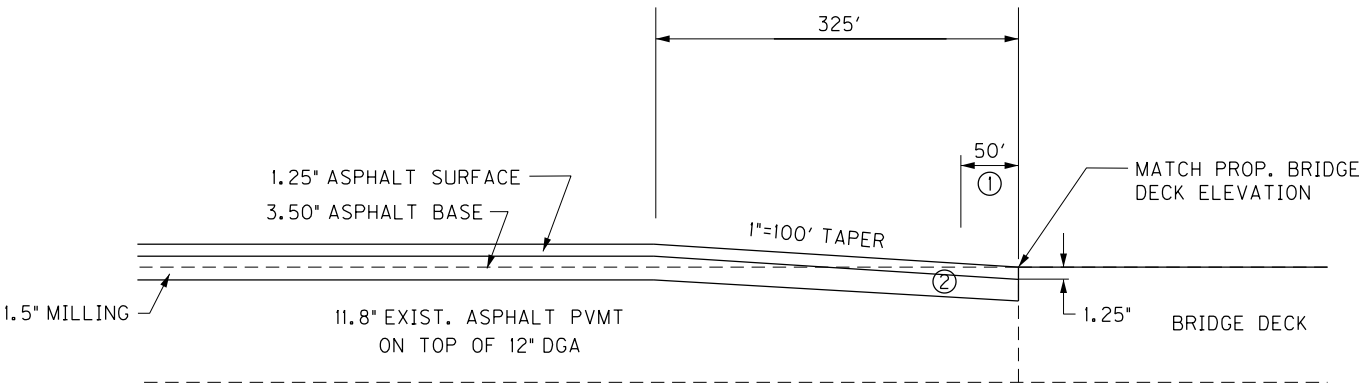
THE NON-PERFORATED OUTLET PIPE AND HEADWALL WILL BE PAID FOR SEPARATELY. ANY EXCAVATION OR BACKFILL REQUIRED WILL BE INCIDENTAL TO THE LENGTH OF NON-PERFORATED PIPE.
- 2 FABRIC-GEOTEXTILE TYPE IV REQUIRED AROUND COARSE AGGREGATE. OVERLAP FABRIC ONE FOOT.

DETAIL 'B'
2:1 FILL SLOPES WITH GUARDRAIL



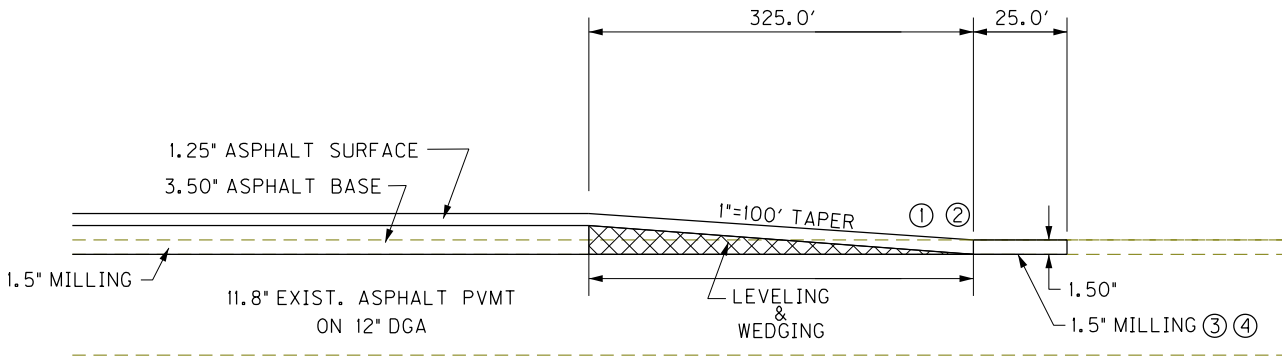
- ① 4.0% MAX. SHOULDER CROSS SLOPE ON SUPERELEVATION (6.00% MAX. ROLL-OVER).
- ② PRIOR TO PLACING THE PERFORATED PIPE, PREPARE THE CHANNEL, INCLUDING REMOVAL OF VEGETATION, LOOSE ROCK, AND OTIE DEBRIS. THE DEARTHMENT WILL NOT MEASURE PREPARATION OF CHANNEL AND IS CONSIDERED INCIDENTAL TO DITCH REPAIR.

DETAIL 'D'
EXISTING LOCATIONS NOT OTHERWISE SPECIFIED



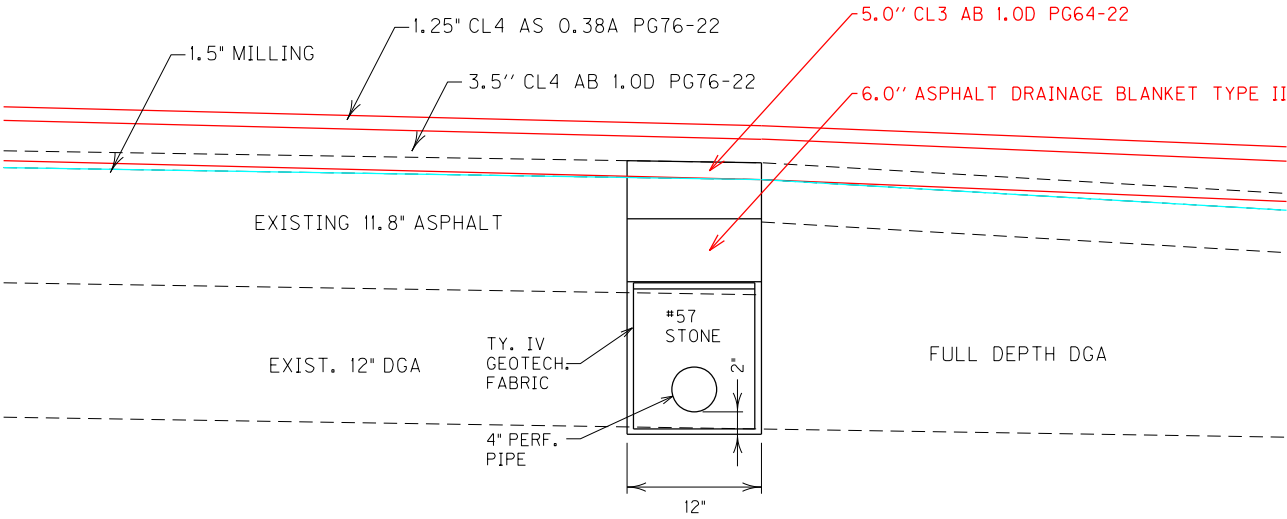
TAPERING OF OVERLAYS AT BRIDGE END (NORTH TERMINUS)

- ① TRANSITION ROADWAY CROSS SLOPE TO MATCH EXIST. BRIDGE DECK CROSS SLOPE
- ② MILL EXISTING PAVEMENT TO RECEIVE PROPOSED OVERLAY



TAPERING OF OVERLAYS AT SOUTH TERMINUS AND RAMP TIE-IN

- ① TRANSITION ROADWAY CROSS SLOPE TO MATCH EXIST. CROSS SLOPE
- ② MILL EXISTING PAVEMENT TO RECEIVE PROPOSED OVERLAY
- ③ U.S. 25 EXIT RAMP TO BE MILLED 1.5" AND OVERLAID WITH 1.5" ASPHALT SURFACE
- ④ I-75 SOUTH TERMINUS TO BE MILLED 1.5" FOR A LENGTH OF 25'



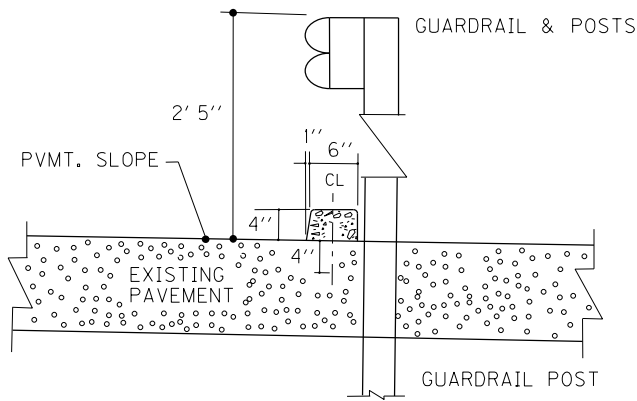
① **PERFORATED PIPE EDGE DRAIN DETAIL**

① THE FOLLOWING ITEMS ARE INCIDENTAL TO EDGE DRAIN PIPE INSTALLATION (SEE KYTC SPECS-SECT. 704); TRENCH EXCAVATION, MATERIALS FOR BEDDING AND BACKFILL, AND GEOTEXTILE FABRIC FOR WRAPPING PERFORATED PIPE OR FOR WRAPPING COARSE AGGREGATE BACKFILL.

COUNTY OF	ITEM NO.
ROCKCASTLE	8-2009.0

CROSS SECTION VIEW

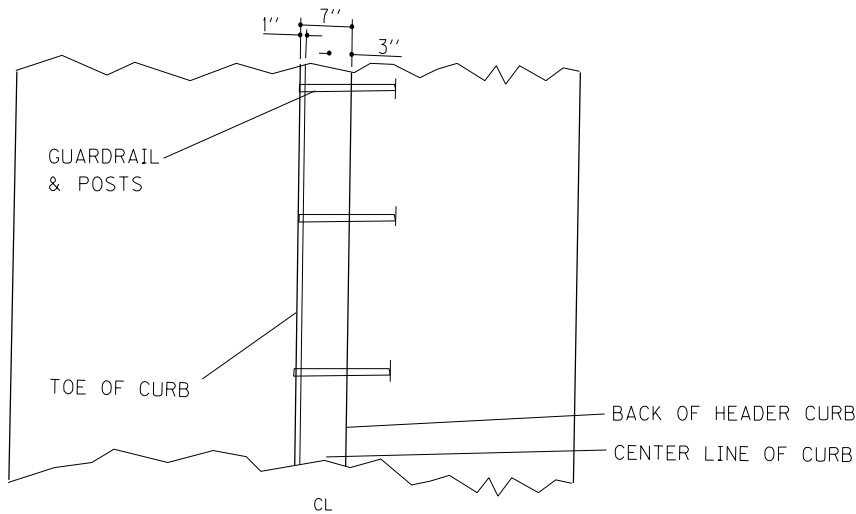
SPECIAL CONCRETE
HEADER CURB



NOT TO SCALE

PLAN VIEW

SPECIAL CONCRETE
HEADER CURB



NOT TO SCALE

I-75
ROCKCASTLE COUNTY
PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95
ITEM NO. 8-2009.00
GENERAL SUMMARY

ITEM NUMBER	ITEM	QUANTITY	UNIT
1	DGA BASE	617	TON
18	DRAINAGE BLANKET-TYPE II-ASPH (10)	573	TON
78	CRUSHED AGGREGATE SIZE NO. 2 (1)	2448	TON
100	ASPHALT SEAL AGGREGATE	349	TON
194	LEVELING AND WEDGING PG76-22	85	TON
214	CL3 ASPH BASE 1.0D PG64-22	6489	TON
214	CL3 ASPH BASE 1.0D PG64-22 (10)	526	TON
219	CL4 ASPH BASE 1.0D PG76-22	10690	TON
291	EMULSIFIED ASPHALT RS-2	42	TON
339	CL3 ASPH SURF 0.38D PG64-22	1389	TON
342	CL4 ASPH SURF 0.38A PG76-22	3548	TON
522	STORM SEWER PIPE - 18 IN	912	LIN FT
1000	PERFORATED PIPE - 4 INCH (10)	12330	LIN FT
1001	PERFORATED PIPE - 6 INCH (10)	4995	LIN FT
1002	PERFORATED PIPE - 8 INCH (15)	8485	LIN FT
1010	NON-PERFORATED PIPE - 4 INCH	152	LIN FT
1011	NON-PERFORATED PIPE - 6 INCH	92	LIN FT
1012	NON-PERFORATED PIPE - 8 INCH	100	LIN FT
1015	INSPECT & CERTIFY EDGE DRAIN SYSTEM	1	LUMP SUM
1020	PERF PIPE HEADWALL TY 1-4 IN	11	EACH
1021	PERF PIPE HEADWALL TY 1-6 IN	17	EACH
1022	PERF PIPE HEADWALL TY 1-8 IN	8	EACH
1029	PERF PIPE HEADWALL TY 3-6 IN	1	EACH
1480	CURB BOX INLET TYPE B	3	EACH
1484	CURB BOX INLET TYPE B - T (2)	1	EACH
1660	SPRING BOX INLET TYPE A	1	EACH
1691	FLUME INLET TYPE 2	2	EACH
1719	ADJUST INLET	2	EACH
1756	MANHOLE TYPE A	1	EACH
1890	ISLAND HEADER CURB TYPE 1 (3)	320	LIN FT
1904	REMOVE CURB	20	LIN FT
1982	DELINEATOR FOR GUARDRAIL - WHITE	91	EACH
1983	DELINEATOR FOR GUARDRAIL - YELLOW	4	EACH
2014	BARRICADE TYPE III (4)	6	EACH
2220	FLOWABLE FILL	4	CU YD
2237	DITCHING (5)	12000	LIN FT
2352	GUARDRAIL-STEEL W BEAM-D FACE (6)	275.0	LIN FT
2360	GUARDRAIL TERMINAL SECTION NO. 1	2	EACH
2363	GUARDRAIL CONNECTOR TO BRIDGE END TYPE A	2	EACH
2365	CRASH CUSHION TYPE IX-A	2	EACH
2367	GUARDRAIL END TREATMENT TYPE 1	3	EACH
2369	GUARDRAIL END TREATMENT TYPE 2A	6	EACH
2373	GUARDRAIL END TREATMENT TYPE 3	1	EACH
2381	REMOVE GUARDRAIL	8712.5	LIN FT
2482	CHANNEL LINING CLASS IA (7) (9)	3400	TON
2483	CHANNEL LINING CLASS II (9)	76	TON
2484	CHANNEL LINING CLASS III (9)	267	TON
2562	SIGNS (8)	500	SQ FT
2565	OBJECT MARKER TYPE 2	2	EACH
2568	MOBILIZATION	1	LUMP SUM
2569	DEMOBILIZATION	1	LUMP SUM
2650	MAINTAIN AND CONTROL TRAFFIC	1	LUMP SUM
2671	PORTABLE CHANGEABLE MESSAGE SIGN (4)	2	EACH
2676	MOBILIZATION FOR MILLING & TEXTURING	1	LUMP SUM
2677	ASPHALT PAVE MILLING & TEXTURING	7122	TON
2696	SHOULDER RUMBLE STRIPS-SAWED	34400	LF
2775	ARROW PANEL (4)	2	EACH
3262	CLEAN PIPE STRUCTURE	1	EACH
6412	STEEL POST MILE MARKERS (12)	3	EACH
6417	FLEXIBLE DELINEATOR POST-W	172	EACH
6418	FLEXIBLE DELINEATOR POST-Y	12	EACH
6511	PAVEMENT STRIPING-TEMP PAINT -6 INCH	43000	LIN FT
6568	PAVE MARK-THERMO STOP BAR-24 IN (13)	52	LIN FT
6592	PAVEMENT MARKER TYPE V- B W/R	257	EACH

I-75 ROCKCASTLE COUNTY PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95 ITEM NO. 8-2009.00 GENERAL SUMMARY			
ITEM NUMBER	ITEM	QUANTITY	UNIT
6593	PAVEMENT MARKER TYPE V- B Y/R	28	EACH
6600	REMOVE PAVEMENT MARKER TYPE V	257	EACH
8100	CONCRETE CLASS A	1	CU YD
8150	STEEL REINFORCEMENT	8	LB
10020NS	FUEL ADJUSTMENT	31135	DOLLAR
10030NS	ASPHALT ADJUSTMENT	54866	DOLLAR
20757ED	PAVEMENT REPAIR	2291	SQ YD
21802EN	GUARDRAIL-STEEL W BEAM-S FACE (7 FT POST) (14)	8687.5	LIN FT
23143ED	K.P.D.E.S. PERMIT AND TEMPORARY EROSION CONTROL	1	LUMP SUM
23237EN10W	WATERBLAST STRIPE REMOVAL	17200	LIN FT
24189ER	DURABLE WATERBORNE MARKING-6 IN W	21080	LIN FT
24190ER	DURABLE WATERBORNE MARKING-6 IN Y	18303	LIN FT
24191ER	DURABLE WATERBORNE MARKING-12 IN W	840	LIN FT
	SPECIAL CONCRETE HEADER CURB	1181	LIN FT

- (1) To be used to repair washouts behind guardrail, around perforated pipe headwalls and for ditch slope repairs.
(Includes 20 tons for washout repair behind guardrail and 2400 tons for ditch slope repair)
- (2) Includes the removal and replacement of the top phase of curb box inlets only
- (3) Includes 20 ft at curb box inlets on downstream side of bridges and 300 to reconstruct curb on U.S. 25 Exit ramp
- (4) The quantity for these items includes the maximum in place at any one time. Any relocation required will not be paid for directly but will be considered incidental.
- (5) Ditching is intended for repair of the eroded areas in the median, pipe outlet ditches, and cleaning of drainage structures. Any embankment required is incidental to ditching.
- (6) Guardrail must utilize 7 foot long posts
- (7) Used for slope correction on 3:1 ditch slopes. Mattress units not required, aggregate only with gradation per 805.013.03 (see Detail 'C' on typical sections).
- (8) Intended for temporary traffic control signs and includes initial placement only. Any relocation of these temporary signs required will not be paid for directly but will be considered incidental.
- (9) Geotextile Fabric Type 1 to be placed under Aggregate Channel Lining is considered incidental to the lining. Any excavation required to place the Channel Lining Class II or Class III is incidental to the lining.
- (10) Included for edge drain pipe system as shown in the typical sections or as directed by the Engineer.
- (11) To be used for slope correction on 3:1 ditch slopes (see Detail 'C' on typical sections) and for other areas as directed by the Engineer. Seeding and fertilizing are considered incidental to the Erosion Control Blanket
- (12) Replace mile post markers at M.P. 56, 57, and 58.
- (13) To be placed at end of exit ramp to U.S. 25
- (14) A length of 25 feet of double ply guardrail is to be installed at every proposed flume on the project. The double ply rail is considered incidental to Guardrail-Steel W Beam-S Face.
- (15) Included for ditch drain pipe system as shown in the typical sections or as directed by the Engineer.

NOTE: Quantities from all summaries have been carried over and included in this General Summary

**I-75
ROCKCASTLE COUNTY**

PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95

ITEM NO. 8-2009

PAVING AREAS MAINLINE		PAVING AREAS U.S. 25 EXIT RAMP	
ITEM	TOTAL	ITEM	TOTAL
INSIDE SHOULDER AND DRIVING LANES		DRIVING LANES UP TO GORE (M.P. 58.81)	
1.25" CL4 ASPH SURF 0.38A PG76-22	S.Y. 51600	1.25" CL4 ASPH SURF 0.38A PG76-22	S.Y. 1861
3.50" CL4 ASPH BASE 1.0D PG76-22	51600	3.5" CL ASPH BASE 1.0D PG76-22	1861
		ASPHALT PAVE MILLING & TEXTURING	1861
OUTSIDE SHOULDER		RAMP TO U.S. 25	
1.25" CL3 ASPH SURF 0.38D PG64-22	19111	1.50" CL3 ASPH SURF 0.38D PG64-23	2178
3.50" CL3 ASPH BASE 1.0D PG64-22	19111	ASPHALT PAVE MILLING & TEXTURING	2178
LEVELING AND WEDGING - DRIVING LANES		LEVELING AND WEDGING - TAPERING OF OVERLAY	
(GROSS SLOPE CORRECTION, INCLUDED WITH BASE LIFT)		LEVELING & WEDGING PG76-22	667
CL4 ASPH BASE 1.0D PG76-22 (0.3" AVG. DEPTH)	45867		
LEVELING AND WEDGING - OUTSIDE SHOULDER		PAVING AREAS - BRIDGE APPROACH REPAIRS	
(GROSS SLOPE CORRECTION, INCLUDED WITH BASE LIFT)		INSIDE SHOULDERS AT APPROACH BRIDGE END	
CL3 ASPH BASE 1.0D PG64-22 (0.75" AVG. DEPTH)	19111	(4' WIDE BY 300' LONG)	
ASPHALT PAVE MILLING & TEXTURING		1.25" CL3 ASPH SURF 0.38D PG64-22	133
PROJECT LIMITS (1.5")	70711	3.50" CL3 ASPH BASE 1.0D PG64-22 (TWO LIFTS)	133
ADDITIONAL MILLING TO MATCH BRIDGE DECK ELEVATION		MILLING INSIDE SHOULDER (4.75" DEPTH)	133
(1.25" AVG. DEPTH)	2878		
LEVELING AND WEDGING - TAPERING OF OVERLAY		EDGE DRAIN PAVEMENT	
LEVELING & WEDGING PG76-22	1233	5.00" CL4 ASPH BASE 1.0D PG64-22	S.Y. 1911
		6.00" DRAINAGE BLANKET - TYPE II ASPHALT	1911
UNPAVED SHOULDER AREAS - MAINLINE AND RAMPS			
DGA WEDGE (C.Y.)	C.Y. 1295		
EMULSIFIED ASPHALT RS-2	S.Y. 17461		
ASPHALT SEAL AGGREGATE	17461		

**I-75
ROCKCASTLE COUNTY
PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95
ITEM NO. 8-2009**

PAVING SUMMARY

CODE	ITEM	UNITS	TOTAL
1	DGA BASE	TON	617
18	DRAINAGE BLANKET - TYPE II ASPHALT	(3) TON	573
100	ASPHALT SEAL AGGREGATE	(2) TON	349
194	LEVELING & WEDGING PG76-22	TON	85
214	CL3 ASPH BASE 1.0D PG64-22	TON	6489
214	CL3 ASPH BASE 1.0D PG64-22	(3) TON	526
219	CL4 ASPH BASE 1.0D PG76-22	(4) TON	10690
291	EMULSIFIED ASPHALT RS-2	(1) TON	42
339	CL3 ASPH SURF 0.38D PG64-22	TON	1389
342	CL4 ASPH SURF 0.38A PG76-22	(4) TON	3548
2677	ASPHALT PAVE MILLING & TEXTURING	TON	7122
2696	SHOULDER RUMBLE STRIPS-SAWED	LIN. FT.	34400

(1) TWO APPLICATIONS AT 2.4 LB/SY
(2) TWO APPLICATIONS AT 20 LB/SY
(3) FOR EDGE DRAIN CAP (SEE EDGE DRAIN DETAIL)
(4) TO BE PLACED WITH MATERIAL TRANSFER VEHICLE

QUANTITIES HAVE BEEN CARRIED OVER AND INCLUDED IN THE GENERAL SUMMARY

I-75					
ROCKCASTLE COUNTY					
PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95					
ITEM NO. 8-2009					
PAVEMENT REPAIR LOCATIONS					
BEGIN MILEPOST	END MILEPOST	WIDTH OF REPLACEMENT (FT)	LENGTH OF REPLACEMENT (FT)	AREA OF REPLACEMENT (SQ YD)	DESCRIPTION
NORTHBOUND LANES					
56.25	56.26	12	15	20	OUTSIDE LANE
56.30	56.30	12	40	53	OUTSIDE LANE
56.36	56.37	12	45	60	OUTSIDE LANE
56.38	56.40	12	60	80	OUTSIDE LANE
56.42	56.44	12	100	133	OUTSIDE LANE
56.46	56.46	6	6	4	OUTSIDE LANE
56.75	56.77	12	100	133	OUTSIDE LANE
56.78	56.79	6	6	4	INSIDE LANE
56.79	56.80	6	6	4	OUTSIDE LANE
56.85	56.85	6	6	4	OUTSIDE LANE
56.88	56.88	6	6	4	OUTSIDE LANE
57.06	57.06	6	6	4	OUTSIDE LANE
57.58	57.59	12	35	47	OUTSIDE LANE
57.60	57.60	6	6	4	OUTSIDE LANE
57.62	57.63	12	65	87	OUTSIDE LANE
57.66	57.67	12	30	40	OUTSIDE LANE
57.68	57.68	6	6	4	OUTSIDE LANE
57.70	57.71	12	65	87	OUTSIDE LANE
57.72	57.72	6	6	4	OUTSIDE LANE
57.73	57.73	6	6	4	OUTSIDE LANE
57.74	57.76	12	125	167	OUTSIDE LANE
57.78	57.79	12	40	53	OUTSIDE LANE
57.80	57.80	12	35	47	OUTSIDE LANE
57.81	57.82	12	60	80	OUTSIDE LANE
57.83	57.83	6	6	4	OUTSIDE LANE
57.83	57.84	24	15	40	BOTH LANES
57.84	57.91	12	385	513	OUTSIDE LANE
57.92	57.93	12	40	53	OUTSIDE LANE
57.94	57.96	12	115	153	OUTSIDE LANE
57.99	57.99	12	30	40	OUTSIDE LANE
57.99	58.00	12	40	53	OUTSIDE LANE
58.38	58.38	12	30	40	OUTSIDE LANE
58.70	58.70	6	6	4	INSIDE LANE
58.71	58.71	6	6	4	OUTSIDE LANE
58.73	58.74	12	65	87	OUTSIDE LANE
58.75	58.76	12	40	53	OUTSIDE LANE
58.77	58.77	12	40	53	OUTSIDE LANE
58.83	58.83	6	6	4	OUTSIDE LANE
58.85	58.85	6	6	4	OUTSIDE LANE
58.85	58.85	6	6	4	OUTSIDE LANE
58.89	58.90	12	40	53	OUTSIDE LANE
PROJECT TOTALS				2291	

The Milepost references listed above are given as distances measured from the nearest mile post sign

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ROCKCASTLE COUNTY
PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95
ITEM NO. 8-2009

C

NORTHBOUND GUARDRAIL SUMMARY

LOCATION	SIDE	FROM MILEPOST	TO MILEPOST	GUARDRAIL- STEEL W BEAM- S FACE (7 FT POST) (LF)	GUARDRAIL- STEEL W BEAM- D FACE (LF)	GUARDRAIL TERMINAL SECTION NO 1 (EACH)	GUARDRAIL END TREATMENT TYPE 1 (EACH)	GUARDRAIL END TREATMENT TYPE 2A (EACH)	GUARDRAIL END TREATMENT TYPE 3 (EACH)	GUARDRAIL CONNECTOR TO BRIDGE END TYPE A (EACH)	CRASH CUSHION TYPE IX-A (EACH)	REMOVE GUARDRAIL (LF)	DESCRIPTION
				21802EN	2352	2360	2367	2369	2373	2363	2365	2381	
NORTHBOUND	RT	55.93	56.00	362.5				1	1			275.0	
NORTHBOUND	RT	56.47	56.56	500.0			1	1				500.0	
NORTHBOUND	RT	56.99	57.62	3300.0			1	1				3212.5	
NORTHBOUND	RT	57.75	57.86	600.0				2				550.0	
NORTHBOUND	RT	58.18	58.79	3200.0			1	1				3100.0	
NORTHBOUND	RT	58.81	58.95	625.0		1				1	1	800.0	
NORTHBOUND	LT	58.91	58.96	100.0		1				1	1	275.0	
PROJECT TOTALS				8687.5	275.0	2	3	6	1	2	2	8712.5	

h) A length of 25 feet of double ply guardrail is to be installed at every proposed flume on the project. The double ply rail is considered incidental to Guardrail-Steel W Beam-S Face.
The Milepost references listed above are given as distances measured from the nearest milepost located south of the guardrail location.

56.30	56.30	12	40	53	OUTSIDE LANE
56.36	56.37	12	45	60	OUTSIDE LANE
56.38	56.40	12	60	80	OUTSIDE LANE
56.42	56.44	12	100	133	OUTSIDE LANE
56.46	56.46	6	6	4	OUTSIDE LANE
56.75	56.77	12	100	133	OUTSIDE LANE
56.78	56.79	6	6	4	INSIDE LANE
56.79	56.80	6	6	4	OUTSIDE LANE
56.85	56.85	6	6	4	OUTSIDE LANE
56.88	56.88	6	6	4	OUTSIDE LANE
57.06	57.06	6	6	4	OUTSIDE LANE
57.58	57.59	12	35	47	OUTSIDE LANE
57.60	57.60	6	6	4	OUTSIDE LANE
57.62	57.63	12	65	87	OUTSIDE LANE
57.66	57.67	12	30	40	OUTSIDE LANE
57.68	57.68	6	6	4	OUTSIDE LANE
57.70	57.71	12	65	87	OUTSIDE LANE
57.72	57.72	6	6	4	OUTSIDE LANE
57.73	57.73	6	6	4	OUTSIDE LANE
57.74	57.76	12	125	167	OUTSIDE LANE
57.78	57.79	12	40	53	OUTSIDE LANE
57.80	57.80	12	35	47	OUTSIDE LANE
57.81	57.82	12	60	80	OUTSIDE LANE
57.83	57.83	6	6	4	OUTSIDE LANE
57.83	57.84	24	15	40	BOTH LANES
57.84	57.91	12	385	513	OUTSIDE LANE
57.92	57.93	12	40	53	OUTSIDE LANE
57.94	57.96	12	115	153	OUTSIDE LANE
57.99	57.99	12	30	40	OUTSIDE LANE
57.99	58.00	12	40	53	OUTSIDE LANE
58.38	58.38	12	30	40	OUTSIDE LANE
58.70	58.70	6	6	4	INSIDE LANE
58.71	58.71	6	6	4	OUTSIDE LANE
58.73	58.74	12	65	87	OUTSIDE LANE
58.75	58.76	12	40	53	OUTSIDE LANE
58.77	58.77	12	40	53	OUTSIDE LANE
58.83	58.83	6	6	4	OUTSIDE LANE
58.85	58.85	6	6	4	OUTSIDE LANE
58.85	58.85	6	6	4	OUTSIDE LANE
58.89	58.90	12	40	53	OUTSIDE LANE
PROJECT TOTALS				2291	

The Milepost references listed above are given as distances measured from the nearest mile post sign

I-75 ROCKCASTLE COUNTY PAVEMENT REHABILITATION, MILEPOST 55.74 TO 58.95 Item No. 8-2009.00 NORTHBOUND I-75 - PERFORATED PIPE SUMMARY													
Location	Milepost	Perforated Pipe 4 Inch	Perforated Pipe 6 Inch	Perforated Pipe 8 Inch	Non-Perforated Pipe 4 Inch	Non-Perforated Pipe 6 Inch	Non-Perforated Pipe 8 Inch	Perforated Pipe Headwall Type 1 4 Inch	Perforated Pipe Headwall Type 1 6 Inch	Perforated Pipe Headwall Type 1 8 Inch	Perforated Pipe Headwall Type 3 6 Inch	Crushed Aggregate Size No. 2	COMMENTS
Units		LF	LF	LF	LF	LF	LF	Each	Each	Each	Each	Tons	
Item Number		1000	1001	1002	1010	1011	1012	1020	1021	1022	1029	78	
EDGE DRAIN													
75' RT	55.74	500	700			12			1			1	END 4" PERFORATED PIPE EDGE DRAIN - OUTLET
75' RT	55.94	500	800			10			1			1	EDGE DRAIN OUTLET
	56.19												HIGH POINT OF ROADWAY PROFILE
75' RT	56.48	500	1050			10			1			1	EDGE DRAIN OUTLET
75' RT	56.61	1000	150		12						1	1	EDGE DRAIN OUTLET - LOW POINT OF ROADWAY PROFILE
	56.70												HIGH POINT OF ROADWAY PROFILE
75' RT	56.82	500	100			10							EDGE DRAIN OUTLET - TO DITCH INLET
75' RT	57.00	500	500			10			10			1	EDGE DRAIN OUTLET
75' RT	57.09	450			10			1				1	EDGE DRAIN OUTLET
75' RT	57.19	550			10			1				1	EDGE DRAIN OUTLET
75' RT	57.29	500			10			1				1	EDGE DRAIN OUTLET
75' RT	57.38	500			10			1				1	EDGE DRAIN OUTLET
75' RT	57.45	700			10			1				1	EDGE DRAIN OUTLET - LOW POINT OF ROADWAY PROFILE
75' RT	57.52	360			10			1				1	EDGE DRAIN OUTLET
75' RT	57.58	500	540			10			1			1	EDGE DRAIN OUTLET
75' RT	57.78	300			10			1				1	EDGE DRAIN OUTLET - PAVEMENT REPAIR AREA
75' RT	57.84	500	250			10			1			1	EDGE DRAIN OUTLET - PAVEMENT REPAIR AREA
	57.98												HIGH POINT OF ROADWAY
75' RT	58.22	500	750			10			1			1	EDGE DRAIN OUTLET
75' RT	58.31	500			10			1				1	EDGE DRAIN OUTLET
75' RT	58.40	450			10			1				1	EDGE DRAIN OUTLET
75' RT	58.48	450			10			1				1	EDGE DRAIN OUTLET
75' RT	58.58	1000	155			10			1			1	EDGE DRAIN OUTLET - LOW POINT OF ROADWAY PROFILE
82' RT	58.70	465			10			1				1	EDGE DRAIN OUTLET - PAVEMENT REPAIR AREA
	58.79	380			10								EDGE DRAIN OUTLET TO PROPOSED MANHOLE
	58.86	500			10								EDGE DRAIN OUTLET TO PROPOSED CBI
	58.95												BEGIN 4" PERFORATED PIPE EDGE DRAIN
DITCH DRAIN													
80' RT	55.74			1075			10			1		1	END 8" PERFORATED PIPE DITCH DRAIN - OUTLET
80' RT	55.98			1210			10			1		1	END 8" PERFORATED PIPE DITCH DRAIN - OUTLET
80' RT	56.46			1400			10			1		1	END 8" PERFORATED PIPE DITCH DRAIN - OUTLET
75' RT	56.61			500			10			1		1	END 8" PERFORATED PIPE DITCH DRAIN - OUTLET
75' RT	56.82			600			10						END 8" PERFORATED PIPE DITCH DRAIN - OUTLET TO DITCH INLET
80' RT	56.99			900			10			1		1	END 8" PERFORATED PIPE DITCH DRAIN - OUTLET
80' RT	57.61			200			10			1		1	END 8" PERFORATED PIPE DITCH DRAIN - OUTLET
75' RT	57.65			700			10						END 8" PERFORATED PIPE DITCH DRAIN - OUTLET TO DITCH INLET
80' RT	57.86			650			10			1		1	END 8" PERFORATED PIPE DITCH DRAIN - OUTLET
80' RT	58.22			1250			10			1		1	END 8" PERFORATED PIPE DITCH DRAIN - OUTLET
MEDIAN DRAIN													
MEDIAN	57.00	225			10								MEDIAN DRAIN BETWEEN PROP. SPRING BOX AND EXIST. MEDIAN INLET
SHEET TOTAL													
		12,330	4,995	8,485	152	92	100	11	17	8	1	28	

The Milepost references listed above are given as distances measured from the nearest mile post sign

**I-75 ROCKCASTLE COUNTY
MP 55.744 TO MP 58.954
FD52 102 0075 055-059
Item No. 8-2009.00**

**THIS PROJECT IS A FULLY
CONTROLLED ACCESS HIGHWAY**

I. DESCRIPTION

Perform all work in accordance with the Department's 2012 Standard Specifications, Supplemental Specifications, any applicable Special Provisions, and applicable Standard and Sepia Drawings, except as hereafter specified. Article references are to the Standard Specifications. Furnish all materials, labor, equipment, and incidentals for the following work:

(1) Maintain and Control Traffic; (2) Remove and replace Guardrail and Guardrail End treatments at the locations listed and/or as directed by the Engineer; (3) Type V pavement markers; (4) Asphalt Pavement Milling and Texturing; (5) Asphalt Surface and Asphalt Base at locations listed and/or as directed by the Engineer; and (6) All other work specified as part of this contract.

II. MATERIALS

Except as specified in these notes or on the drawings, all materials will be according to the Standard Specifications and applicable Special Provisions and Special Notes. The Department will sample and test all materials according to Department's Sampling Manual and the Contractor will have the materials available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing, unless otherwise specified in these notes.

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Dense Graded Aggregate.** Crushed Stone Base may not be furnished in lieu of DGA.
- C. **Pavement Markings -6 inch Paint.** Use Durable Waterborne Marking 6-inch for permanent striping (12 inch at entrance and exit ramp tapers).
- D. **Crushed Aggregate Size No. 2.** Crushed Aggregate Size No. 2 will be limestone.

- E. **Channel Lining Class II & III.** Channel lining will be limestone and is to be placed at pipe outlets with significant erosion and in ditch repair locations as directed by the Engineer.
- F. **Erosion Control Blanket.** Erosion control blanket is to be placed in all ditching areas when ditching is complete, on slope stabilization areas, or as directed by the Engineer. Use Seed Mixture No. 1

III. CONSTRUCTION METHODS

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Site Preparation.** Be responsible for all site preparation. Do not disturb existing signs. This item will include, but is not limited to, incidental excavation and backfilling; removal of all obstructions or any other items; disposal of materials; sweeping and removal of debris; shoulder preparation and restoration, temporary and permanent erosion and pollution control; and all incidentals. Site preparation will be only as approved or directed by the Engineer. Other than the bid items listed, no direct payment will be made for site preparation, but will be incidental to the other items of work.
- C. **Disposal of Waste.** Dispose of all cuttings, debris, and other waste off the right-of-way at approved sites obtained by the Contractor at no additional cost to the Department. The contractor will be responsible for obtaining any necessary permits for this work. Temporary openings in the right of way fence for direct access to waste sites off the right of way or for access to other public roads will not be allowed. No separate payment will be made for the disposal of waste and debris from the project or obtaining the necessary permits, but will be incidental to the other items of the work.
- D. **Final Dressing, Clean Up, and Seeding and Protection.** After all work is completed, completely remove all debris from the job site. Perform Class A Final Dressing on all disturbed areas. Sow disturbed earthen areas with Seed Mixture No. I. These items are incidental to other items in the contract.
- E. **Guardrail.** Remove and replace guardrail and guardrail End Treatments listed in the Guardrail Summary or as directed by the Engineer. Guardrail, End Treatments and Terminal Sections are listed by mile points and quantities are approximate only. Actual locations will be determined by the Engineer at the time of construction. Grade and reshape shoulders to proper template for new End Treatment. Utilize DGA for embankment when required for new end treatments. Remove any existing guardrail with a lane closure in place.

Do not leave the area unprotected. After the guardrail is removed, a shoulder closure shall remain in place until the guardrail is replaced in that area. To minimize safety hazards, guardrail removal is to be performed at the latest practical time prior to initiating the paving operation in an area and re-installation is to begin within 5 calendar days from the time that the final base course is completed and shall be

pursued until completion. If guardrail installation is not started within 5 calendar days after paving operations ends, Liquidated Damages will be charged as outlined in Section 108 of the 2012 Standard Specifications.

The Contractor shall deliver existing salvaged guardrail system materials to the Guardrail and Sign Center on Wilkinson Blvd in Frankfort, KY and shall be neatly stacked in accordance with section 719.03.07 of the Standard Specifications. Contact Section Supervisor at (502) 564-8187 to schedule the delivery of material. Deliver the material between the hours of 8:00AM and 3:00PM, Monday through Friday. There is a guardrail delivery verification sheet which must be completed. The Contractor, engineer, and Central Sign/Guardrail Center representative must all sign off on this sheet before payment may be made.

- F. **Pavement Striping and Pavement Markers.** Permanent striping will be in accordance with Section 112, except that:
- (1). Striping will be 6" in width, except 12" in gore area;
 - (2). Permanent striping will be in place before a lane is opened to traffic; and
 - (3). Permanent striping will be 6" Durable Waterborne Marking.
- G. **On-Site Inspection.** Each Contractor submitting a bid for this work will make a thorough inspection of the site prior to submitting a bid and will thoroughly familiarize himself with existing conditions so that the work can be expeditiously performed after a contract is awarded. Submission of a bid will be considered evidence of this inspection having been made. Any claims resulting from site conditions will not be honored by the Department.
- H. **Caution:** Information shown on the drawings and in this proposal and the types and quantities of work listed are not to be taken as an accurate or complete evaluation of the material and conditions to be encountered during construction. The bidder must draw his own conclusions as to the conditions encountered. The Department does not give any guarantee as to the accuracy of the data and no claim will be considered for additional compensation if the conditions encountered are not in accordance with the information above.
- I. **Utility Clearance.** It is not anticipated that utility facilities will need to be relocated and/or adjusted; however, in the event that it is discovered that the work does require that utilities be relocated and/or adjusted, the utility companies will work concurrently with the Contractor while relocating their facilities.

IV. METHOD OF MEASUREMENT

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Site Preparation.** Other than the bid items listed, site preparation will not be measured for payment, but will be incidental to the other items of work.

- C. **Crushed Aggregate Size No. 2.** Payment will be based on the tons used around the perforated pipe outlet headwalls.
- D. **Dense Graded Aggregate.** DGA will be used for shoulder slope washouts.
- E. **Raised Pavement Markers and Permanent Striping.** Permanent striping Durable Waterborne Marking (6" and 12") is measured per linear foot. See Traffic Control Plan. Type V Pavement Markers are measured as each.
- F. **Erosion Control.** Erosion control items not listed as bid items will not be measured for payment, but will be considered incidental to the "lump sum" price for the bid item "Erosion Control".
- G. **Erosion Control Blanket.** Erosion Control Blanket is measured by square yard and is to be used in ditching areas and slope stabilization areas as directed by the Engineer.
- H. **Embankment.** Embankment is measured by cubic yard and is to be placed in pipe repair/extension locations, slope stabilization areas and as directed by the Engineer. Contrary to the Standard Specifications, payment will be based on measured quantity **NOT** plan quantity.

V. BASIS OF PAYMENT

No direct payment will be made other than for the bid items listed. All other items required to complete the construction will be incidental to the bid items listed. Existing signs damaged by the Contractor will be replaced by the Contractor at his expense.

- A. **Maintain and Control Traffic.** See Traffic Control Plan.
- B. **Site Preparation.** Other than the bid items listed, no direct payment will be allowed for site preparation, but will be incidental to the other items of work.
- C. **Dense Grade Aggregate.** See Section 302 of the Standard Specifications.
- D. **Raised Pavement Markers and Permanent Striping.** See Traffic Control Plan.

**NOTES APPLICABLE TO PROJECT
PAVEMENT REHABILITATION
I-75 ROCKCASTLE COUNTY
Item No. 8-2009.0**

-
1. There is a summary pavement repair locations. The Engineer will determine the ultimate locations that will be repaired based upon the condition of the pavement at the time the repairs are accomplished. The repair locations listed may be lengthened, shortened, or eliminated completely if the conditions are such that modification of the locations would be deemed desirable by the Department.
 2. The dimensions shown on the typical section for pavement and shoulder widths and thickness are nominal or typical dimensions. The actual dimensions to be constructed may be varied to fit existing conditions as directed or approved by the Engineer. It is not intended that existing pavement or shoulders be widened unless specified in the Proposal.
 3. The contractor is to be advised locations of low wires may exist. The following location is approximate:

I-75 M.P. 58.84 (Sta. 2945+00)

CAUTION: Other Locations may exist. These and all other utilities should be avoided on this project. If any utility is impacted, it will be the contractor's responsibility to contact the affected utility and cover any costs associated with the impact.

4. Guardrail, End Treatments, and Terminal Sections to be replaced are listed by mileposts. Exact placement to be approved by the Engineer on construction.
5. The existing cable median barrier is not to be disturbed with this project. In accordance with Section 107.12 of the Standard Specifications for Road and Bridge Construction, Current Edition, the Contractor will be responsible for the cost to repair any cable rail that is damaged due to the operations of the Contractor. The Department will make any necessary repairs at the Contractor's expense.
6. Remove any existing guardrail with a lane closure in place. Do not leave the area unprotected. After the guardrail is removed, a shoulder closure shall remain in place until the guardrail is replaced in that area. The Contractor shall deliver existing salvaged guardrail system materials to the Central Sign Shop and Recycle Center in in Frankfort, KY (502-564-8187) between the hours of 8:00AM and 3:00PM, Monday through Friday and shall be neatly stacked in accordance with section 719.03.07 of the standard specifications. Contractor, engineer, and Central Sign Shop and Recycle Center representative must all sign off on this sheet before payment may be made.
7. All pipe connections in the edge drain system will be rigid.
8. A quantity of Channel Lining Class III has been included to be applied to eroded areas around drainage outlets and for some of the areas that are to be ditched. The actual limits of ditching and/or channel lining shall be as directed and/or approved by the Engineer. Geotextile Fabric

Type I will not be measured for payment and will be considered incidental to channel lining.

9. Any roadway signs that are damaged during construction are to be replaced at the contractor's expense.
10. Any light poles that are damaged during construction are to be replaced at the contractor's expense.
11. The cleaning of existing pipe culvert inlets and outlets 36 inches or less in diameter are incidental to the bid item for "Ditching" in accordance with Section 209.03.01 of the 2012 Edition of the Standard Specifications for Road and Bridge Construction.
12. Overlay pavement shall be tapered to create a smooth transition to the existing pavement at the existing median cross-overs.
13. The existing roll curb island on the US 25 interchange (Exit 59) ramp is not to be disturbed. If the curb is damaged during construction, the Contractor will be responsible for replacing any damaged curb at no cost to the Department.
14. Safety Pavement Edge shall be incorporated on the inside 3' shoulder (see Special Note). Prior to placing asphalt pavement, prepare the shoulder material, including removal of vegetation/sod, where the pavement wedge will be placed to provide a foundation that will support the placement of the pavement wedge according to Section 403.03.02 of the Standard Specifications. The Department will not measure preparation or construction of pavement wedge for separate payment, but make payment for the completed and accepted quantities of Asphalt Surface Mixtures according to Section 402 of the Standard Specifications.
15. New grates shall be installed on adjusted drop box inlets in the roadside ditch. The existing grates shall be delivered to the Rockcastle County Maintenance Garage, US 25 South, Mount Vernon, KY 40456 (606-256-2116). No additional payment will be considered for the new grates or delivery of the existing grates as this will be considered incidental to the bid item "Adjust Inlet."
16. Approximately 300 tons of asphalt millings from the project are to become the property of the Department. Millings are to be delivered to the Rockcastle County Maintenance Garage, US 25 South, Mount Vernon, Kentucky 40456 (606-256-2116). No additional payment will be considered for the delivery of the asphalt millings. This delivery will be considered incidental to the bid item "Asphalt Pavement Milling and Texturing." The remaining asphalt pavement millings will be property of the Contractor.
17. A Spring Box shall be installed at approximate milepost 56.95 and connected to the next downstream median box inlet at approximate milepost 57.00 with 235' of 18" storm sewer pipe. The existing perforated pipe shall be connected to the upstream side of the proposed spring box. A 4" perforated pipe shall also be installed between milepost 56.95 and 57.00 and connected to the existing median box inlet at milepost 57.00. The median in this area will be regraded for positive drainage and seeded. No additional payment will be considered for the regarding and seeding and will be incidental to the installation of the 18" storm sewer and perforated pipe.

TRAFFIC CONTROL PLAN

**I-75 ROCKCASTLE COUNTY
MP 55.744 TO MP 58.954
FD52 102 0075 055-059
Item No. 8-2009.00**

<p>THIS PROJECT IS A FULLY CONTROLLED ACCESS HIGHWAY</p>

TRAFFIC CONTROL GENERAL

Except as provided herein, maintain and control traffic in accordance with the 2012 Standard Specifications and the Standard Drawings, current editions. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic". All lane closures used on the Project will be in compliance with the appropriate Standard Drawings. Do NOT use Cones for lane closures or shoulder closures.

Contrary to Section 106.01, traffic control devices used on this project may be new, or used in like new condition at the beginning of the work and maintained in like new condition until completion of the work. Traffic Control Devices will conform to current MUTCD.

Reduce the speed limit in work areas to 55 miles per hour and establish double fines for work zone speeding violations. The extent of these areas within the project limits will be restricted to the proximity of actual work areas as determined by the Engineer. Notify the Engineer a minimum of 12 hours prior to using the double fine signs. At the beginning of the work zone, the "WARNING FINE DOUBLED IN WORK ZONE" signs will be dual mounted. At the end of the work zone, the "END DOUBLE FINE" signs will be dual mounted as well. Remove or cover the signs when the highway work zone does not have workers present for more than a two-hour period of time. Payment for the signs will be at the unit bid price for signs erected. Any relocation or covering of the signs will be incidental to Maintain and Control Traffic.

Night work is required on this project. Obtain approval from the Engineer for the method of lighting prior to its use.

Traffic Control Plan
Rockcastle County
I 75
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PROJECT PHASING & CONSTRUCTION PROCEDURES

No lane closures will be allowed during the following days and hours:

August 31 - September 3, 2012
November 21-25, 2012

Labor Day Weekend
Thanksgiving Weekend

No lane closures will be allowed during the following days and hours unless otherwise stated in this proposal or approved by the Engineer:

12:00 p.m. to 6:00 p.m.
12:00 p.m. Friday to 6:00 p.m. Sunday

Monday – Thursday

Traffic may be reduced to one lane in each direction during the following days and hours unless otherwise stated in this proposal or approved by the Engineer:

6:00 p.m. to 12:00 p.m.

Sunday – Thursday

The Department will allow for six (6) extended lanes closures from 6:00 p.m. Sunday through 12:00 p.m. (noon) Wednesday to complete pavement repairs and edge drain construction along with any other roadway milling and paving operations and roadside operations feasible to complete during this time.

The clear lane width will be 11 feet. Use a lane closure all times when work is performed in the lane or adjacent shoulder. Shoulders used as temporary roadways will be inspected by the Engineer and if deemed necessary by the Engineer, repaired with Asphalt Mixture for Level & Wedging as directed prior to opening to traffic. Perform any maintenance of the shoulder as deemed necessary by the Engineer in order to maintain traffic. Remove existing striping by water blasting. Remove both edge lines throughout the project. Paint temporary edge lines through the lane closure.

All pavement edge transitions must be smooth and level before opening both lanes up to traffic. A lane closure must be in place during all times that pavement edge drop-offs are present (see Pavement Edge Dropoff note).

Approximate full depth pavement repair locations are listed in the proposal. The Engineer will determine the exact location at the time of construction. Once removal of pavement at a particular repair location has begun, work continuously within the parameters outlined above to complete the work and eliminate the “hole”. Place Type III Barricades immediately in front of pavement removal areas.

Once pavement removal at a site has begun, full depth replacement must be completed within the time a lane closure is allowed.

Access to all ramps at all interchanges on the project shall be maintained at all times unless otherwise directed by the Engineer. All diversions to access ramps in areas of lane closures shall

Traffic Control Plan
Rockcastle County
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be approved by the Engineer prior to implementing the particular lane closure

Note that Lane shifts are required throughout the project. See the Exhibits for lane locations and widths. Stripe according to the MUTCD.

During the days and hours when a lane closure is allowed, implement the following procedures: Maintain traffic as specified in the phasing notes. Any other work not requiring traffic lane widths to be restricted due to barrels or equipment encroaching into the interior lanes can be done during the remaining hours when two lanes of traffic must be maintained. Please refer to the “Special Note for Fixed Completion Date and Liquidated Damages” for damage rates per hour associated with failure to maintain the required number of lanes during the specified time period. Once pavement removal at a site has begun, full depth replacement must be completed within the time a lane closure is allowed. Liquidated Damages, at the rate specified per hour in the “Special Note for Fixed Completion Date and Liquidated Damages”, will be assessed for each hour two lanes of traffic is not maintained.

The contractor must notify the Engineer at least fourteen (14) days prior to beginning Phase I construction in either direction.

SHOULDER PREPARATION AND RESTORATION

Prior to placing any lane closures that require shifting traffic onto existing shoulders, patch the shoulders as directed by the Engineer. Remove failed materials and perform additional patching as directed by the Engineer during the time the shoulder is used as a travel lane. All work required for shoulder preparation and restoration is incidental to Maintenance of Traffic, with the exception of the asphalt patching, which will be paid at the contract unit bid price for “LEVELING AND WEDGING PG76-22”.

Prior to shifting traffic onto any existing shoulders at bridge ends, the inside and/or outside shoulders shall be reconstructed to existing grade a minimum of four (4) feet wide and 350 feet in length at the end of the bridge or as directed by the engineer. This construction shall consist of 2 – 3.5 inch lifts of CL3 ASPH BASE 1.0D PG64-22 and 1.0” CL3 ASPH SURF 0.38D PG64-22.

PHASE I

Close the inside lanes to traffic. Reconstruct the inside shoulders at bridge ends as stated above and shown in the proposal details.

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PHASE II

Shift traffic to the inside lanes and close the outside lanes to traffic. Repair pavement failures in the outside lanes and shoulder. Once the pavement has been removed, the Contractor must work continuously until the pavement has been replaced. The Engineer may restrict the number of locations being repaired at the same time. Install the perforated pipe edge drain. Complete drainage structure repair and installation. Mill roadway 1.5 inches and place the 3.5 inch base lift and leveling & wedging where required on the outside lanes and shoulder. Mill roadway 1.5 inches and place the 3.5 inch base lift and leveling and wedging where required on the exit ramp to US 25 utilizing part width construction phasing. The exit ramp must remain open during the lane closure. Complete any roadside work including guardrail installation.

PHASE III

Shift traffic to the outside lanes. Repair pavement failures in the inside lanes and shoulder. Once the pavement has been removed, the Contractor must work continuously until the pavement has been replaced. The Engineer may restrict the number of locations being repaired at the same time. Mill roadway 1.5 inches and place the 3.5 inch base lift and leveling & wedging where required on the inside lanes and shoulder. Complete any roadside work including guardrail installation. Place the 1.5 final surface course.

PHASE IV

Utilizing temporary lane closures place the final surface course on all lanes, ramps and shoulders and complete any outstanding roadside work.

PHASE V – PERMANENT STRIPING

After all other work is completed, place permanent striping. Mobile operations may be utilized. In addition to newly paved areas, place permanent striping on bridge decks within the project limits.

LANE CLOSURES

Limit the lengths of lane closures to only that needed for actual operations in accordance with the phasing specified herein, or as directed by the Engineer. Limit lane closures to one lane closure per direction at any given time. Contrary to section 112, lane closures will **NOT** be measured for payment, but are considered incidental to Maintain and Control Traffic.

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SIGNS

Additional traffic control signs in addition to normal lane closure signing detailed on the Standard Drawings may be required by the Engineer. Additional signs needed for lane closures may include, but are not limited to, dual mounted TRUCKS USE LEFT/RIGHT LANE, LEFT/RIGHT LANE CLOSED 1 MILE, LEFT/RIGHT LANE CLOSED 2 MILE, LEFT/RIGHT LANE CLOSED 3 MILE, SLOWED/STOPPED TRAFFIC AHEAD. Signage for reduced speed limits and double fine work zones will be furnished, relocated, and maintained by the Contractor.

Contrary to section 112, Individual signs will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged signs or signs directed to be replaced by the Engineer due to poor legibility or reflectivity will not be measured for payment.

A quantity of signs has been included for lane shifts, "Roadwork Ahead" signs on entrance ramps, and extra Double Fine signs and Speed Limit signs between interchanges to be paid only once no matter how many times they are moved or relocated.

FLASHING ARROWS

Flashing arrows will be paid for once, no matter how many times they are moved or relocated.

PORTABLE CHANGEABLE MESSAGE SIGNS

Provide portable changeable message signs in advance of and within the project at locations to be determined by the Engineer. If work is in progress concurrently in both directions, or if more than one lane closure is in place in the same direction of travel, provide additional portable changeable message signs. Place portable changeable message signs one mile in advance of the anticipated queue at each lane closure. As the actual queue lengthens and/or shortens relocate or provide additional portable changeable message signs so that traffic has warning of slowed or stopped traffic at least one mile but not more than two miles before reaching the end of the actual queue. The locations designated may vary as the work progresses. The messages required to be provided will be designated by the Engineer. The portable changeable message signs will be in operation at all times. In the event of damage or mechanical/electrical failure, the Contractor will repair or replace the Portable Changeable Message Sign immediately. Portable Changeable Message Boards will be paid for once, no matter how many times they are moved or relocated. The Department **WILL NOT** take possession of the signs upon completion of the work.

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TRUCK MOUNTED ATTENUATORS

Furnish and install MUTCD approved Truck Mounted Attenuators in advance of work areas when workers are present less than 12 feet from traffic. If there is less than 500 feet between work sites, only a single TMA will be required at a location directed by the Engineer. Locate the TMAs at the individual work sites and move them as the work zone moves within the project limits. All details of the TMA installations shall be approved by the Engineer. Truck Mounted Attenuators will not be measured for payment, but are incidental to Maintain and Control Traffic. The Department **WILL NOT** take possession of the TMAs upon completion of the work.

PAVEMENT MARKINGS

If lane closures are in place during nighttime hours, remove or cover the lenses of raised pavement markers that do not conform to the traffic control scheme in use, or as directed by the Engineer. Replace or uncover lenses before a closed lane is reopened to traffic. No direct payment will be made for removing and replacing or covering and uncovering the lenses, but will be incidental to "Maintain and Control Traffic".

Place temporary and permanent striping in accordance with Section 112, except that:

1. Temporary and permanent striping will be 6" in width; and
2. If the contractor's operations or phasing requires temporary markings which must be subsequently removed from the ultimate pavement, an approved removable lane tape will be used; however removable tape will be measured and paid as Pavement Striping-Temporary Paint 6"; and
3. Edge lines will be required for temporary striping; and
4. Existing, temporary, or permanent striping will be in place before a lane is opened to traffic.
5. Place permanent striping on pavement within the project limits.
6. Permanent striping will be Durable Waterborne Marking.

Should the Contractor change the existing striping pattern, the Contractor is to restripe the roadway back to its original configuration after a certain period of time especially if no work is anticipated for a period of time (i.e. Winter shutdown).

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PAVEMENT EDGE DROP-OFFS

Pavement edge drop-offs will be protected by a lane or shoulder closure. Lane closures will be protected with plastic drums, vertical panels, or barricades as shown on the Standard Drawings.

A pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation shall not have an elevation difference greater than 1 1/2". Place warning signs (MUTCD W8-11 or W8-9A) in advance of and at 1500' intervals throughout the drop-off area. Dual posting on both sides of the traveled way shall be required. Wedge all transverse transitions between resurfaced and unresurfaced areas which traffic may cross with asphalt mixture for leveling and wedging. Remove the wedges prior to placement of the final surface course. Pavement edges that traffic is not expected to cross, except accidentally, shall be treated as follows:

Less than 2" – Protect with a lane closure.

2" to 4" – Protect with a lane closure. Place plastic drums, vertical panels, or barricades every 50 feet. Cones may not be used in place of plastic drums, panels, and barricades at any time. Construct a wedge with compacted cuttings from milling, trenching, or asphalt mixtures with a 3:1 or flatter slope, when work is not active in the drop-off area. Place Type III Barricades at the beginning of the lane closures, and place additional Type III Barricades spaced at 2,500 feet during the time the lane closure is in place.

Greater than 4" – Pavement Repair areas – In areas where pavement is to be removed, work should proceed continuously so that traffic is exposed to a drop-off for the minimum amount of time necessary to bring the pavement back up to existing grade. Barrel spacing should be 20 feet and appropriate lighting should be utilized to illuminate the area during nighttime operations.

Guardrail Installation – Guardrail will be removed at the last practical moment and replaced as soon as the placement of all base courses in an area requiring guardrail is complete. All areas from which guardrail is removed shall be protected by a shoulder closure or other method approved by the Engineer until the new guardrail is installed.

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TRAFFIC COORDINATOR

Designate an employee to be traffic coordinator. The designated Traffic Coordinator must be certified by an agency qualified for training in this area. The Traffic Coordinator will inspect the project maintenance of traffic once every two hours during the Contractor's operations and at any time a lane closure is in place. The Traffic Coordinator will report all incidents throughout the work zone to the Engineer on the project. The Contractor will furnish the name and telephone number where the Traffic Coordinator can be contacted at all times.

During any period when a lane closure is in place, the Traffic Coordinator will arrange for personnel to be present on the project at all times to inspect the traffic control, maintain the signing and devices, and relocate portable changeable message boards as queue lengths change. The personnel will have access on the project to a radio or telephone to be used in case of emergencies or accidents.

COORDINATION OF WORK

The Contractor is advised that other projects may be in progress within or in the near vicinity of this project. The traffic control of those projects may affect this project and the traffic control of this project may affect those projects. The Contractor will coordinate the work on this project with the work of the other contractors. In case of conflict, the Engineer will determine the relative priority to give to work phasing on the various projects.

CONTRACTOR'S AND CONTRACTOR'S EMPLOYEES' VEHICLES

Do not use or allow employees to use median crossovers at any time except when inside lanes are closed for construction. In all other phases of construction, change vehicular direction of travel only at interchanges.

COUNTY OF	ITEM NO.	SHEET NO.
ROCKCASTLE	8-2009.0	

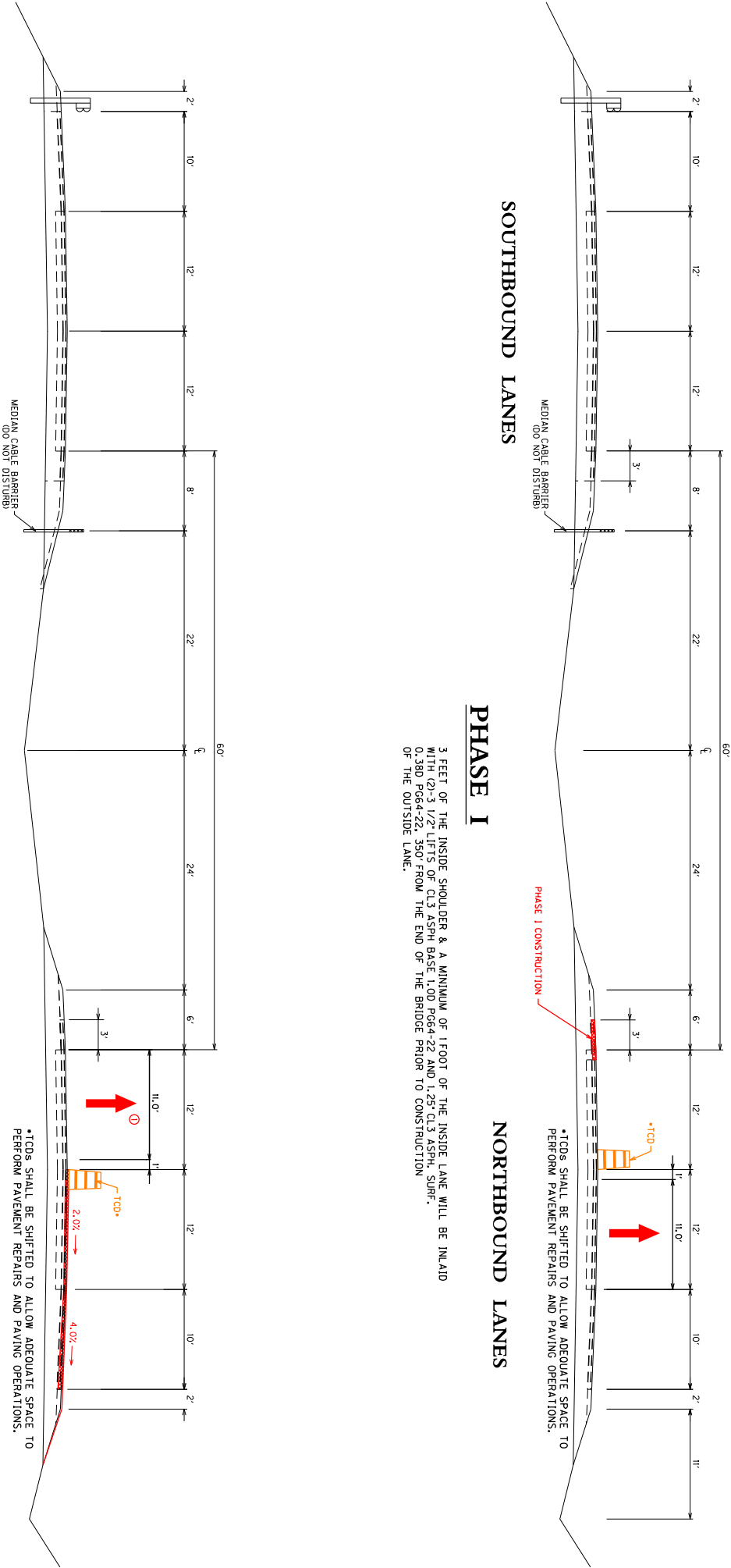
I-75 MAINTENANCE OF TRAFFIC TYPICAL SECTIONS

SOUTHBOUND LANES

PHASE I

3 FEET OF THE INSIDE SHOULDER & A MINIMUM OF 1 FOOT OF THE INSIDE LANE WILL BE IN L&D WITH (2)-3 1/2" LIFTS OF CL3 ASPH BASE 1.00 PG64-22 AND 1.25" CL3 ASPH. SURF.
0.380 PG64-22, 350' FROM THE END OF THE BRIDGE PRIOR TO CONSTRUCTION
OF THE OUTSIDE LANE.

NORTHBOUND LANES



SOUTHBOUND LANES

PHASE II

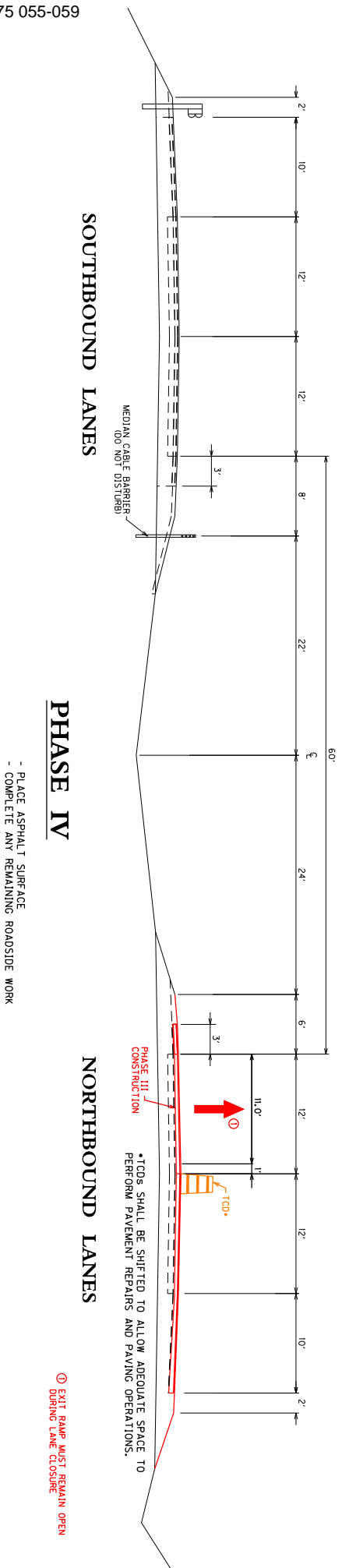
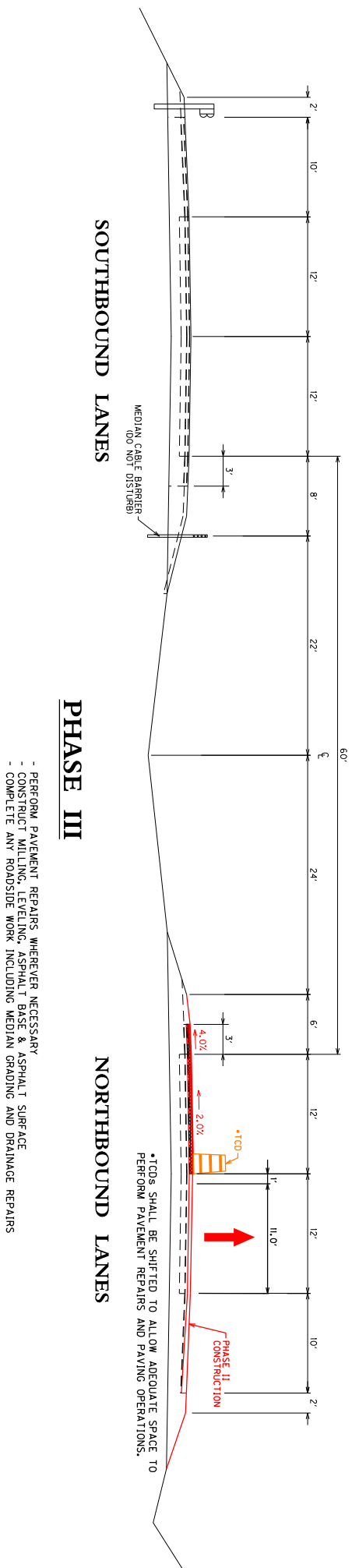
- PERFORM PAVEMENT REPAIRS WHEREVER NECESSARY
- INSTALL PERFORATED PIPE EDGE DRAIN
- COMPLETE DRAINAGE REPAIRS AND INSTALLATION
- CONSTRUCT MILLING LEVELING & ASPHALT BASE
- CONSTRUCT MILLING LEVELING & ASPHALT BASE ON U.S. 25 EXIT RAMP UTILIZING PART WIDTH CONSTRUCTION
- COMPLETE ANY ROADSIDE WORK INCLUDING GUARDRAIL INSTALLATION

NORTHBOUND LANES

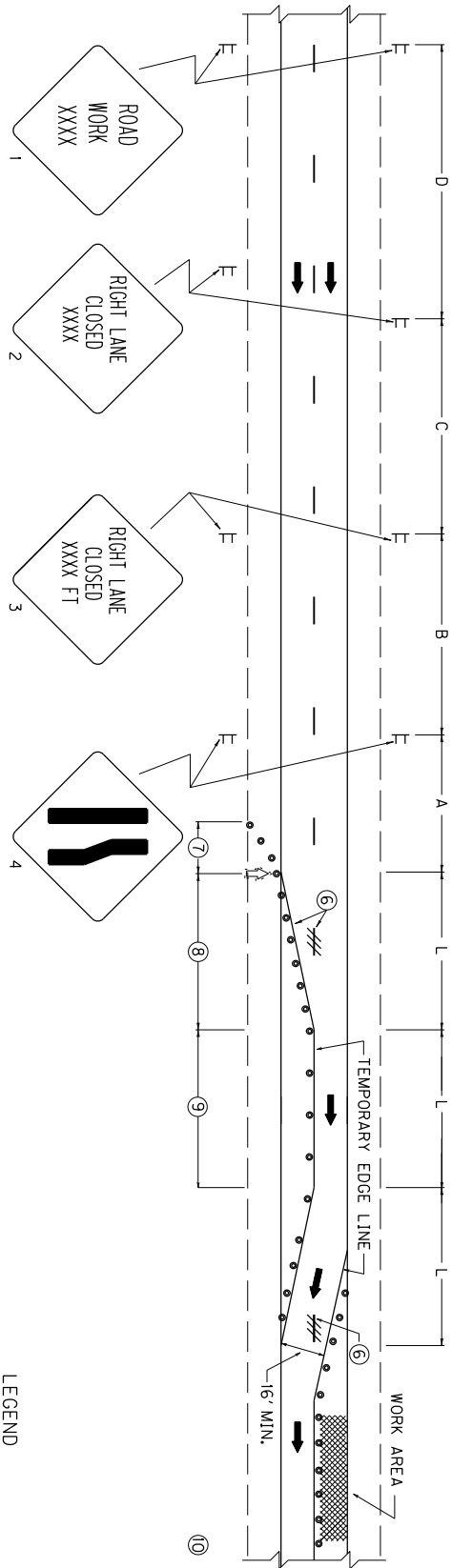
Ⓢ EXIT RAMP MUST REMAIN OPEN
DURING LANE CLOSURE

MAINTENANCE OF TRAFFIC TYPICAL SECTIONS

COUNTY OF	ITEM NO.	SHEET NO.
ROCKCASTLE	8-2009.0	



① EXIT RAMP MUST REMAIN OPEN DURING LANE CLOSURE



1. IF MEDIAN IS NOT WIDE ENOUGH TO MAINTAIN LATERAL CLEARANCES SHOWN ON TSC-260, SIGNS 1 THRU 5 SHALL NOT BE DUAL MOUNTED.
2. THE SIZE OF SIGNS 1 THRU 4 SHALL BE 48" X 48" IF THE NORMAL POSTED SPEED LIMIT IS 45 MPH OR GREATER. OTHERWISE, 36" X 36" SIGNS MAY BE USED.
3. AN ARROW PANEL SHALL BE REQUIRED WHEN THE NORMAL POSTED SPEED LIMIT IS 45 MPH OR GREATER. IT SHALL BE LOCATED ON THE SHOULDER AT THE BEGINNING OF THE MERGING TAPER OR LOCATED IN THE CLOSED LANE WHEN THE SHOULDER IS NARROW.
4. ALL VEHICLES, EQUIPMENT, WORKERS, AND THEIR ACTIVITIES SHALL BE RESTRICTED TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
6. IF DURATION OF LANE CLOSURE EXCEEDS THREE DAYS, TEMPORARY EDGE LINE SHALL BE REQUIRED AND SKIP LINES SHALL BE OBLITERATED BY APPROVED METHOD THRU LENGTH OF TAPER. OTHER CONFLICTING PAVEMENT MARKINGS SHALL BE OBLITERATED.
7. IF AN ARROW PANEL IS INSTALLED ON THE SHOULDER, A SHOULDER TAPER SHALL BE REQUIRED. TAPER LENGTH SHALL BE 0.33 L. SPACING OF CHANNELIZING DEVICES THRU THE SHOULDER TAPER SHALL NOT EXCEED A DISTANCE EQUAL TO THE NORMAL POSTED SPEED LIMIT.
8. DRUMS OR TYPE II BARRICADES SHALL BE USED TO CHANNELIZE THE MERGING TAPER IF DURATION OF CLOSURE EXCEEDS 12 HOURS. OTHERWISE, CONES OR TUBULAR MARKERS MAY BE USED. SPACING OF CHANNELIZING DEVICES THRU THE MERGING TAPER SHALL NOT EXCEED A DISTANCE EQUAL TO THE NORMAL POSTED SPEED LIMIT.
9. DRUMS OR TYPE II BARRICADES SHALL BE USED TO CHANNELIZE THE BUFFER SPACE AND WORK AREA IF DURATION OF CLOSURE EXCEEDS THREE DAYS. OTHERWISE, CONES OR TUBULAR MARKERS MAY BE USED. SPACING OF CHANNELIZING DEVICES THRU THE ACTIVITY AREA SHALL NOT EXCEED A DISTANCE EQUAL TO TWO TIMES THE NORMAL POSTED SPEED LIMIT.
10. SEE STANDARD DRAWING TTC-115 FOR DOWNSTREAM TAPER LENGTH AND 'END ROAD WORK' SIGN LOCATIONS.

DRAWING NOT TO SCALE

USE WITH CURRENT STD. DWGS.
TTD-100, TTD-105, TTD-110,
TTD-115, AND TTC-160

APPLICATION

THIS DRAWING APPLIES TO LANE CLOSURES OF THE LEFT LANE.
IF TEMPORARY TRAFFIC BARRIER IS USED, REFER TO CURRENT
STANDARD DRAWING TTC-120.

SIGNING AND SPACING TABLE					
ROAD TYPE	A	B	C	D	L
EXPRESSWAY/ FREEWAY	1000'	500'	1100'	2600'	900'
RURAL/URBAN SP. LT. \geq 45 MPH*	500'	500'	500'	1100'	660'
URBAN SP. LT. \leq 40 MPH*	250'	250'	500'	500'	440'

*NOTE: USE NORMAL POSTED SPEED LIMIT

REFERENCES

- 1. Kentucky Transportation Cabinet, Department of Highways, Standard Specifications for Road and Bridge Construction, Edition of 2012.
- 2. FHWA Manual on Uniform Traffic Control Devices.
- 3. Active Sepia List

<u>Drawing No.</u>	<u>Drawing Name</u>
001	Delineators at Narrow Shoulder Bridges
002	Delineators for Guardrail
004	Delineators for Concrete Barriers
007	Guardrail End Treatment Type 2A
008	Guardrail Components

- 4. Kentucky Department of Highways Standard Drawings, current editions, as applicable:

RBB-002	Guardrail and Bridge End Drainage for Twin Structures
RBB-003	Layout of Guardrail at Twin Structures
RBC-001	Guardrail Connector to Bridge End Type A and A1
RBC-002	Guardrail Connector to Bridge End Type A and A1 Components
RBC-003	Guardrail Connector to Bridge End Type A and A1 Components
RBE-205	Crash Cushion Type IX-A
RBI-001	Typical Guardrail Installations
RBI-002	Typical Guardrail Installations
RBI-003	Installation of Guardrail End Treatment Type 2A
RBI-004	Installation of Guardrail End Treatment Type 1
RBM-020	Delineators for Concrete Barriers
RBR-001	Steel Beam Guardrail ("W" Beam)
RBR-005	Guardrail Components
RBR-010	Guardrail Terminal Sections
RBR-015	Guardrail Posts
RBR-016	Guardrail Posts
RBR-020	Guardrail End Treatment Type 1
RBR-025	Guardrail End treatment Type 2A
RBR-030	Guardrail End Treatment Type 3
RDB-001	Drop Box Inlet Type 1
RDB-280	Curb Box Inlet Type B
RDB-281	Curb Box Inlet Type B – Steel Details
RDB-282	Curb Box Inlet Type B – Top Phase Tables
RDB-283	Curb Box Inlet Type B - Details
RDB-400	Box Inlet Riser

RDB-410	Box Inlet Pipe Chamber
RDB-410	Box Inlet Pipe Chamber – Additional Steel
RDB-430	Manhole Steps
RDD-021	Flume Inlet Type 2
RDD-040	Channel Lining Class II and III
RDH-005	Concrete Headwalls for 12”-27” Circular Pipe Culverts
RDI-001	Culvert, Entrance & Storm Sewer Pipe Types & Cover Heights
RDI-020	Pipe Bedding for Culverts, Entrance and Storm Sewer Pipe
RDI-021	Pipe Bedding for Culverts, Entrance and Storm Sewer Reinforced Concrete Pipe
RDI-025	Pipe Bedding Trench Condition
RDI-026	Pipe Bedding Trench Condition Reinforced Conc. Pipe
RDM-001	Manhole Type A
RDM-100	Frame and Lid Type 1
RDP-001	Perforated Pipe Types and Cover Heights
RDP-005	Perforated Pipe for Subgrade Drainage on Two-Lane (class 2) and Multi-Lane Roads
RDP-010	Perforated Pipe Headwalls
RDX-160	Security Devices for Frames, Grates and Lids
RDX-210	Temporary Silt Fence
RDX-220	Silt Trap Type A
RDX-225	Silt Trap Type B
RDX-230	Silt Trap Type C
RGS-002	Superelevation for Multilane Pavement
RGX-001	Miscellaneous Standards Part I
RGX-200	One Point Proctor Family of Curves
RPM-100	Curb and Gutter, Curbs, and Valley Gutter
TPM-105	Pavement Marker Arrangements Multi-Lane Roadways
TPM-125	Pavement Marker Arrangement Exit Gore and Off-Ramp
TTC-115	Lane Closure Multi-Lane Highway Case I
TTC-120	Lane Closure Multi-Lane Highway Case II
TTC-135	Shoulder Closure
TTC-155	Temporary Pavement Marker Arrangements for Construction Zones
TTC-160	Temporary Pavement Marker Arrangements for Lane Closures
TTD-110	Post Splicing Detail
TTD-120	Work Zone Speed Limit and Double Fine Signs
TTD-125	Pavement Condition Warning Signs
TTS-110	Mobile Operation for Paint Striping Case III
TTS-115	Mobile Operation for Paint Striping Case IV
TTS-120	Mobile Operation for Durable Striping Case I

5. Kentucky Transportation Cabinet, Department of Highways, Standard Specifications for Road and Bridge Construction, Edition of 2012, Appendix B - Supplemental Specifications, as applicable:

Special Note 1I	Portable Changeable Message Signs (6/15/2012)
Special Note 11F	Turf Reinforcing Mat (6/15/2012)
Special Note	Typical Section Dimensions <i>attached</i>
Special Note	Before You Dig <i>attached</i>
Special Note	Guardrail Delivery Verification Sheet <i>attached</i>
Special Note	Fixed Completion Date and Liquidated Damages <i>attached</i>
Special Note	Shoulder Preparation and Restoration <i>attached</i> (See MOT Notes)
Special Note	Erosion Control <i>attached</i>
Special Note	Safety Pavement Edge (Experimental) <i>attached</i>

SPECIAL NOTE FOR TYPICAL SECTION DIMENSIONS
I-75

The dimensions shown on the typical sections for pavement and shoulder widths are nominal or typical dimensions. The actual dimensions to be constructed may be varied to fit existing conditions as directed or approved by the Engineer. It is not intended that existing pavement or shoulders be widened unless specified elsewhere in the Proposal.

SPECIAL NOTE FOR BEFORE YOU DIG

Call 1-800-752-6007 toll free a minimum of two and no more than ten business days prior to excavation for information on the location of existing under-ground utilities which subscribe to the before-u-dig (BUD) service. Coordinate excavation with all utility owners, including those who do not subscribe to bud.

Guardrail Delivery Verification Sheet
Item No. 8-2009.00

<u>Guardrail, End Treatment, Terminal Section or Post Type</u>	<u>Unit</u>	<u>Field Verified Amount</u>	<u>Delivered Amount</u>
Guardrail-Steel W Beam	LF		
Temporary Guardrail	LF		
Guardrail Terminal Section	Each		
Guardrail Terminal Section	Each		
Crash Cushion Type Ix-A	Each		
Guardrail End Treatment Type 1	Each		
Guardrail End Treatment Type 2a	Each		
Guardrail End Treatment Type 3	Each		
Guardrail End Treatment Type 4a	Each		
Guardrail End Treatment Type 7	Each		
Guardrail Connector To Bridge End	Each		
Guardrail Connector To Conc Med Barr	Each		
Guardrail Connect-Shld Bridge Pier	Each		
Steel Guardrail Post	Each		
Steel Blockouts	Each		

Removed guardrail, end treatments, terminal sections, and posts shall be delivered to the Central Sign Shop and Recycle Center in Frankfort, KY (502-564-8187) between the hours of 8:00 AM and 3:00 PM Monday through Friday and shall be neatly stacked in accordance with section 719.03.07 of the standard specifications. Contractor, engineer, and Central Sign Shop and Recycle Center representative must all sign off on this sheet before payment may be made.

	Printed Name	Signature	Date
Resident Engineer (or Representative)			
Contractor (or Representative)			
Central Sign Shop & Recycle Center Representative			

**Special Note for Fixed Completion Date and
Liquidated Damages
Rockcastle County
Item No. 8-2009**

Contrary to Section 108.09, Liquidated Damages of \$5,000 per calendar day will be assessed for each day or fraction thereof work remains uncompleted beyond the Specified Completion Date. This project has a Fixed Completion Date of November 30, 2012.

In addition to the Liquidated Damages specified above, Liquidated Damages in the following amounts will be charged when a lane closure remains in place during the prohibited period outlined in the Traffic Control Plan:

Mainline: \$5,000 for the first hour or fraction thereof
 \$10,000 for the second hour or fraction thereof
 \$20,000 for any additional hour or fraction thereof

Ramps: \$500 for the first hour or fraction thereof
 \$1,500 any additional hour or fraction thereof

These hourly disincentives will still be in effect after the Fixed Completion Date and will be charged in addition to the \$5,000 per calendar day if warranted. The contractor is expected to make every effort to complete the work in order to open the ramp or mainline lane closure within a specified timeframe.

Contrary to Section 108.09 of the Standard Specifications, **the disincentive fee will be charged during those periods when seasonal limitations of the Contract prohibit the Contractor from working on a controlling item or operation. This includes the months from December through March.**

All liquidated damages will be applied cumulatively.

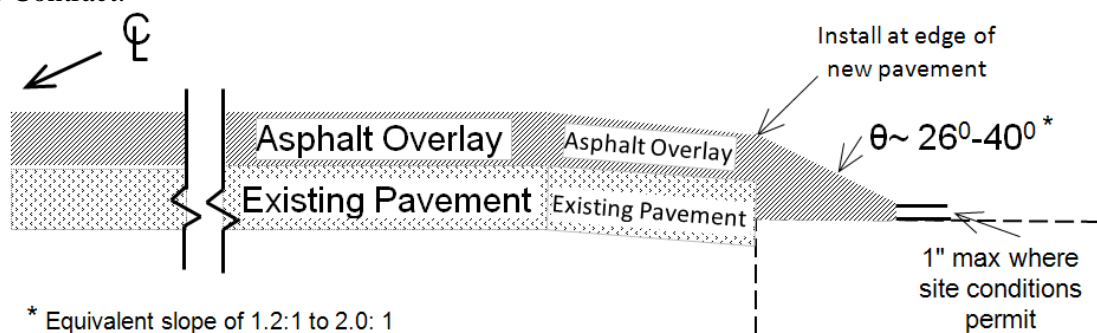
All other applicable portions of Section 108 apply.

**Special Note For:
Safety Pavement Edge
Rockcastle Co. Item No. 8-2009
(Experimental)**

CONSTRUCTION Attach a device to the screed of the paver that confines the material at the end gate and extrudes the asphalt material in such a way that results in a compacted wedge shape pavement edge of approximately 30 degrees (not steeper than 40 degrees). The device shall maintain contact to the road shoulder surface. It shall also allow for automatic transition to cross roads, driveways and obstructions. The device shall constrain the asphalt head reducing the area by 10 to 15% increasing the density of the extruded profile. The use of a single plate strike off is not allowed.

The contractor may use the TransTech Shoulder Wedge Maker, the Advant-Edger, Carlson Safety Edge End Gate, Troxler SafeTSlope Edge Smoother, or a similar device that produces the same wedge consolidation results. If the contractor uses a similar device, they must provide proof that the device has been used on previous projects with acceptable results or the contractor shall construct a test section prior to the beginning of work and demonstrate wedge compaction to the satisfaction of the Engineer. Short sections of handwork will be allowed when necessary for transitions and turnouts or otherwise authorized by the Engineer.

The following sketch is primarily for the computation of quantities; however, the wedge will result in a similar cross-section. Do not construct a shoulder for placing the wedge unless specified elsewhere in the Contract.



UNPAVED SHOULDER PREPARATION Prior to placing asphalt pavement, prepare the shoulder material, including removal of vegetation/sod, where the pavement wedge will be placed to provide a foundation that will support the placement of the pavement wedge according to Section 403.03.02 of the Standard Specifications.

COORDINATION A Preconstruction Conference will be required for this project. A representative from the Central Office Division of Traffic Operations – Traffic Engineering Safety Section should be in attendance. At this meeting, the Contractor is to provide the details of the device that will be used to place the wedge.

PAYMENT The Department will not measure preparation or construction of pavement wedge for separate payment, but make payment for the completed and accepted quantities of Asphalt Surface Mixtures according to Section 402 of the Standard Specifications.

Special Note For: Erosion Prevention and Sediment Control Rockcastle Co. Item No. 8-2009

The Contractor shall be responsible for filing the Kentucky Pollution Discharge Elimination System (KPDES) KYR10 permit Notice of Intent (NOI) with the Kentucky Division of Water (DOW) and any KPDES local Municipal Separate Storm Sewer System (MS4) program that has jurisdiction. The NOI shall name the contractor as the Facility Operator and include the KYTC Contract ID Number (CID) for reference.

The Contractor shall perform all temporary erosion/sediment control functions including: providing a Best Management Practice (BMP) Plan, conducting required inspections, modifying the BMP plan documents as construction progresses and documenting the installation and maintenance of BMPs in conformance with the KPDES KYR10 permit effective on August 1, 2009 or a permit re-issued to replace that KYR10 permit. This work shall be conducted in conformance with the requirements of Section 213 of KYTC 2012 Department of Highways, Standard Specifications for Road and Bridge Construction.

Contrary to Section 213.03.03, paragraph 2, the Engineer shall conduct inspections as needed to verify compliance with Section 213 of KYTC 2012 Department of Highways, Standard Specifications for Road and Bridge Construction. The Engineer's inspections shall be performed a minimum of once per month and within seven days after a storm of ½ inch or greater. Copies of the Engineer's inspections shall not be provided to the contractor unless improvements to the BMP's are required. The contractor shall initiate corrective action within 24 hours of any reported deficiency and complete the work within 5 days. The Engineer shall use Form TC 63-61 A for this report. Inspections performed by the Engineer do not relieve the Contractor of any responsibility for compliance with the KPDES permit.

Contrary to Section 213.05, bid items for temporary BMPs will not be listed and will be replaced with one lump sum item for the services. Payment will be pro-rated based on the Project Schedule as submitted by the Contractor and as agreed to by the Engineer.

The contractor shall be responsible for applying "good engineering practices" as required by the KPDES permit. The contractor may use any temporary BMPs with the approval of the KYTC Engineer.

The contractor shall provide the Engineer copies of all documents required by the KPDES permit at the time they are prepared.

The contractor shall be responsible for the examination of the soils to be encountered and make his own independent determination of the temporary BMPs that will be required to accomplish effective erosion prevention and sediment control.

The Contractor shall be responsible for filing the KPDES permit Notice of Termination (NOT) with the Kentucky DOW and any local MS4 program that has jurisdiction. The NOT shall be filed after the Engineer agrees that the project is stabilized or the project has been formally accepted.

Payment: Payment will be by lump sum under the bid item "K.P.D.E.S. Permit & Temporary Erosion Control".

Special Note For Pipe Debris Removal

The one-step removal method (as defined by the USACE) must be used for debris removal from the 60" pipe located at approximately Sta. 2950+94.55 and MP 58.94. Effective erosion controls must be employed at all times where applicable during the project to prevent degradation of waters of the Commonwealth.

Right-of-Way Certification Form

Revised 2/22/11

☐ Federal Funded

☒ Original

☒ State Funded

☐ Re-Certification

This form must be completed and submitted to FHWA with the PS&E package for federal-aid funded Interstate, Appalachia, and Major projects. This form shall also be submitted to FHWA for all federal-aid projects that fall under Conditions No. 2 or 3 outlined elsewhere in this form. When Condition No. 2 or 3 apply, KYTC shall resubmit this ROW Certification prior to construction contract Award. For all other federal-aid projects, this form shall be completed and retained in the KYTC project file.

Date: July 17, 2012

Project Name: Pavement Rehab-INT (P)

Letting Date: August 17, 2012

Project #: FD04 SPP 102 0075 055-059

County: Rockcastle

Item #: 08-2009.00

Federal #: N/A

Description of Project: Mill and Intermediate asphalt overlay on I-75 Northbound from MP 55.744 to MP 58.954

Projects that require **NO** new or additional right-of-way acquisitions and/or relocations

- ☒ The proposed transportation improvement will be built within the existing rights-of-way and there are no properties to be acquired, individuals, families, and businesses ("relocatees") to be relocated, or improvements to be removed as a part of this project.

Projects that require new or additional right-of-way acquisitions and/or relocations

- ☐ Per 23 CFR 635.309, the KYTC hereby certify that all relocatees have been relocated to decent, safe, and sanitary housing or that KYTC has made available to relocatees adequate replacement housing in accordance with the provisions of the current FHWA directive(s) covering the administration of the Highway Relocation Assistance Program and that at least one of the following three conditions has been met. (Check those that apply.)

- ☐ **Condition 1.** All necessary rights-of-way, including control of access rights when applicable, have been acquired including legal and physical possession. Trial or appeal of cases may be pending in court but legal possession has been obtained. There may be some improvements remaining on the right-of-way, but all occupants have vacated the lands and improvements, and KYTC has physical possession and the rights to remove, salvage, or demolish all improvements and enter on all land. Fair market value has been paid or deposited with the court.

- ☐ **Condition 2.** Although all necessary rights-of-way have not been fully acquired, the right to occupy and to use all rights-of-way required for the proper execution of the project has been acquired. Trial or appeal of some parcels may be pending in court and on other parcels full legal possession has not been obtained, but right of entry has been obtained, the occupants of all lands and improvements have vacated, and KYTC has physical possession and right to remove, salvage, or demolish all improvements. Fair market value has been paid or deposited with the court for most parcels. Fair market value for all pending parcels will be paid or deposited with the court prior to AWARD of construction contract. (See note 1 below.)

Note 1: The KYTC shall re-submit a right-of-way certification form for this project prior to AWARD of all Federal-Aid construction contracts. Award must not to be made until after KYTC has obtained full legal possession and fair market value for all parcels has been paid or deposited with the court and FHWA has concurred in the re-submitted right-of-way certification.

Right-of-Way Certification Form

Revised 2/22/11

- ☐ **Condition 3.** The acquisition or right of occupancy and use of a few remaining parcels are not complete and/or some parcels still have occupants. However, all remaining occupants have had replacement housing made available to them in accordance with 49 CFR 24.204. The KYTC is hereby requesting authorization to advertise this project for bids and to proceed with bid letting even though the necessary rights-of-way will not be fully acquired, and/or some occupants will not be relocated, and/or the fair market value will not be paid or deposited with the court for some parcels until after bid letting. KYTC will fully meet all the requirements outlined in 23 CFR 635.309(c)(3) and 49 CFR 24.102(j) and will expedite completion of all acquisitions, relocations, and full payments after bid letting and prior to AWARD of the construction contract or force account construction. A full explanation and reason for this request, including identification of each such parcel and dates on which acquisitions, payments, and relocations will be completed, is attached to this certification form for FHWA concurrence. (See note 2.)

Note 2: The KYTC may request authorization on this basis only in unique and unusual circumstances. Proceeding to bid letting shall be the exception and never become the rule. In all cases, the KYTC shall make extraordinary efforts to expedite completion of the acquisition, payment for all affected parcels, and the relocation of all relocatees prior to AWARD of all Federal-Aid construction contracts or force account construction.

Approved: Charles Hale
Printed Name

Charles Hale
Signature Right-of-Way Supervisor

Approved: DAVID L. ORR
Printed Name

David L. Orr 7/17/12
Signature KYTC, Director of ROW & Utilities

Approved: _____
Printed Name

Signature FHWA, ROW Officer (when applicable)

Right-of-Way Certification Form

Revised 2/22/11

Date: July 17, 2012

Project Name: Pavement Rehab-INT (P)

Project #: FD04 SPP 102 0075 055-059

Item #: 08-2009.00

Letting Date: August 17, 2012

County: Rockcastle

Federal #: N/A

This project has 0 total number of parcels to be acquired, and 0 total number of individuals or families to be relocated, as well as 0 total number of businesses to be relocated.

 Parcels where acquired by a signed fee simple deed and fair market value has been paid

 Parcels have been acquired by IOJ through condemnation and fair market value has been deposited with the court

 Parcels have not been acquired at this time (*explain below for each parcel*)

 Parcels have been acquired or have a "right of entry" but fair market value has not been paid or has not been deposited with the court (*explain below for each parcel*)

 Relocatees have not been relocated from parcels , , , , , , and (*explain below for each parcel*)

Parcel #	Name/Station	Explanation for delayed acquisition, delayed relocation, or delayed payment of fair market value	Proposed date of payment or of relocation

There are 0 billboards and/or 0 cemeteries involved on this project.

There are 0 water or monitoring wells on parcels , , , , and . All have been acquired and are the responsibility of the project contractor to close/cap.

Form Effective Date: April 1, 2006

Last Revised: February 22, 2011

PART II

SPECIFICATIONS AND STANDARD DRAWINGS

SPECIFICATIONS REFERENCE

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2012* and *Standard Drawings, Edition of 2012 with the 2012 Revision*.

**Supplemental Specifications to the Standard Specifications for Road and
Bridge Construction, 2012 Edition**

(Effective with the August 17, 2012 Letting)

Subsection:	402.03.02 Contractor Quality Control and Department Acceptance.
Part:	D) Testing Responsibilities.
Number:	4) Density.
Revision:	Replace the second sentence of the Option A paragraph with the following: Perform coring by the end of the following work day.
Subsection:	606.03.17 Special Requirements for Latex Concrete Overlays.
Part:	A) Existing Bridges and New Structures.
Number:	1) Prewetting and Grout-Bond Coat.
Revision:	Add the following sentence to the last paragraph: Do not apply a grout-bond coat on bridge decks prepared by hydrodemolition.
Subsection:	609.03 Construction.
Revision:	Replace Subsection 609.03.01 with the following: 609.03.01 A) Swinging the Spans. Before placing concrete slabs on steel spans or precast concrete release the temporary erection supports under the bridge and swing the span free on its supports. 609.03.01 B) Lift Loops. Cut all lift loops flush with the top of the precast beam once the beam is placed in the final location and prior to placing steel reinforcement. At locations where lift loops are cut, paint the top of the beam with galvanized or epoxy paint.

SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS

This Special Note will apply when indicated on the plans or in the proposal.

1.0 DESCRIPTION. Furnish, install, operate, and maintain variable message signs at the locations shown on the plans or designated by the Engineer. Remove and retain possession of variable message signs when they are no longer needed on the project.

2.0 MATERIALS.

2.1 General. Use LED Variable Message Signs Class I, II, or III, as appropriate, from the Department's List of Approved Materials.

Unclassified signs may be submitted for approval by the Engineer. The Engineer may require a daytime and nighttime demonstration. The Engineer will make a final decision within 30 days after all required information is received.

2.2 Sign and Controls. All signs must:

- 1) Provide 3-line messages with each line being 8 characters long and at least 18 inches tall. Each character comprises 35 pixels.
- 2) Provide at least 40 preprogrammed messages available for use at any time. Provide for quick and easy change of the displayed message; editing of the message; and additions of new messages.
- 3) Provide a controller consisting of:
 - a) Keyboard or keypad.
 - b) Readout that mimics the actual sign display. (When LCD or LCD type readout is used, include backlighting and heating or otherwise arrange for viewing in cold temperatures.)
 - c) Non-volatile memory or suitable memory with battery backup for storing pre-programmed messages.
 - d) Logic circuitry to control the sequence of messages and flash rate.
- 4) Provide a serial interface that is capable of supporting complete remote control ability through land line and cellular telephone operation. Include communication software capable of immediately updating the message, providing complete sign status, and allowing message library queries and updates.
- 5) Allow a single person easily to raise the sign to a satisfactory height above the pavement during use, and lower the sign during travel.
- 6) Be Highway Orange on all exterior surfaces of the trailer, supports, and controller cabinet.
- 7) Provide operation in ambient temperatures from -30 to + 120 degrees Fahrenheit during snow, rain and other inclement weather.
- 8) Provide the driver board as part of a module. All modules are interchangeable, and have plug and socket arrangements for disconnection and reconnection. Printed circuit boards associated with driver boards have a conformable coating to protect against moisture.
- 9) Provide a sign case sealed against rain, snow, dust, insects, etc. The lens is UV stabilized clear plastic (polycarbonate, acrylic, or other approved material) angled to prevent glare.
- 10) Provide a flat black UV protected coating on the sign hardware, character PCB, and appropriate lens areas.
- 11) Provide a photocell control to provide automatic dimming.

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- 12) Allow an on-off flashing sequence at an adjustable rate.
- 13) Provide a sight to aim the message.
- 14) Provide a LED display color of approximately 590 nm amber.
- 15) Provide a controller that is password protected.
- 16) Provide a security device that prevents unauthorized individuals from accessing the controller.
- 17) Provide the following 3-line messages preprogrammed and available for use when the sign unit begins operation:

/KEEP/RIGHT/⇒⇒⇒/	/MIN/SPEED/**MPH/
/KEEP/LEFT/⇐⇐⇐/	/ICY/BRIDGE/AHEAD/ /ONE
/LOOSE/GRAVEL/AHEAD/	LANE/BRIDGE/AHEAD/
/RD WORK/NEXT/**MILES/	/ROUGH/ROAD/AHEAD/
/TWO WAY/TRAFFIC/AHEAD/	/MERGING/TRAFFIC/AHEAD/
/PAINT/CREW/AHEAD/	/NEXT/***/MILES/
/REDUCE/SPEED/**MPH/	/HEAVY/TRAFFIC/AHEAD/
/BRIDGE/WORK/***() FT/	/SPEED/LIMIT/**MPH/
/MAX/SPEED/**MPH/	/BUMP/AHEAD/
/SURVEY/PARTY/AHEAD/	/TWO/WAY/TRAFFIC/

*Insert numerals as directed by the Engineer.

Add other messages during the project when required by the Engineer.

2.3 Power.

- 1) Design solar panels to yield 10 percent or greater additional charge than sign consumption. Provide direct wiring for operation of the sign or arrow board from an external power source to provide energy backup for 21 days without sunlight and an on-board system charger with the ability to recharge completely discharged batteries in 24 hours.

3.0 CONSTRUCTION. Furnish and operate the variable message signs as designated on the plans or by the Engineer. Ensure the bottom of the message panel is a minimum of 7 feet above the roadway in urban areas and 5 feet above in rural areas when operating. Use Class I, II, or III signs on roads with a speed limit less than 55 mph. Use Class I or II signs on roads with speed limits 55 mph or greater.

Maintain the sign in proper working order, including repair of any damage done by others, until completion of the project. When the sign becomes inoperative, immediately repair or replace the sign. Repetitive problems with the same unit will be cause for rejection and replacement.

Use only project related messages and messages directed by the Engineer, unnecessary messages lessen the impact of the sign. Ensure the message is displayed in either one or 2 phases with each phase having no more than 3 lines of text. When no message is needed, but it is necessary to know if the sign is operable, flash only a pixel.

When the sign is not needed, move it outside the clear zone or where the Engineer directs. Variable Message Signs are the property of the Contractor and shall be removed from the project when no longer needed. The Department will not assume ownership of these signs.

4.0 MEASUREMENT. The final quantity of Variable Message Sign will be

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the actual number of individual signs acceptably furnished and operated during the project. The Department will not measure signs replaced due to damage or rejection.

5.0 PAYMENT. The Department will pay for the Variable Message Signs at the unit price each. The Department will not pay for signs replaced due to damage or rejection. Payment is full compensation for furnishing all materials, labor, equipment, and service necessary to, operate, move, repair, and maintain or replace the variable message signs. The Department will make payment for the completed and accepted quantities under the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
02671	Portable Changeable Message Sign	Each

Effective June 15, 2012

PART III

EMPLOYMENT, WAGE AND RECORD REQUIREMENTS

TRANSPORTATION CABINET DEPARTMENT OF HIGHWAYS

LABOR AND WAGE REQUIREMENTS APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS

- I. Application
- II. Nondiscrimination of Employees (KRS 344)
- III. Payment of Predetermined Minimum Wages
- IV. Statements and Payrolls

I. APPLICATION

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

II. NONDISCRIMINATION OF EMPLOYEES

AN ACT OF THE KENTUCKY GENERAL ASSEMBLY TO PREVENT DISCRIMINATION IN EMPLOYMENT KRS CHAPTER 344 EFFECTIVE JUNE 16, 1972

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (between forty and seventy); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age (between forty and seventy). The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, disability or age (between forty and seventy), except that such notice or advertisement may indicate a preference, limitation, or specification based on religion, or national origin when religion, or national origin is a bona fide occupational qualification for employment.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual

because of his race, color, religion, national origin, sex, disability or age (between forty and seventy), in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

III. PAYMENT OF PREDETERMINED MINIMUM WAGES

1. These special provisions are supplemented elsewhere in the contract by special provisions which set forth certain predetermined minimum wage rates. The contractor shall pay not less than those rates.

2. The minimum wage determination schedule shall be posted by the contractor, in a manner prescribed by the Department of Highways, at the site of the work in prominent places where it can be easily seen by the workers.

IV. STATEMENTS AND PAYROLLS

1. All contractors and subcontractors affected by the terms of KRS 337.505 to 337.550 shall keep full and accurate payroll records covering all disbursements of wages to their employees to whom they are required to pay not less than the prevailing rate of wages. Payrolls and basic records relating thereto will be maintained during the course of the work and preserved for a period of one (1) year from the date of completion of this contract.

2. The payroll records shall contain the name, address and social security number of each employee, his correct classification, rate of pay, daily and weekly number of hours worked, itemized deductions made and actual wages paid.

3. The contractor shall make his daily records available at the project site for inspection by the State Department of Highways contracting office or his authorized representative.

Periodic investigations shall be conducted as required to assure compliance with the labor provisions of the contract. Interrogation of employees and officials of the contractor shall be permitted during working hours.

Aggrieved workers, Highway Managers, Assistant District Engineers, Resident Engineers and Project Engineers shall report all complaints and violations to the Division of Contract Procurement.

The contractor shall be notified in writing of apparent violations. The contractor may correct the reported violations and notify the Department of Highways of the action taken or may request an informal hearing. The request for hearing shall be in writing within ten (10) days after receipt of the notice of the reported violation. The contractor may submit

records and information which will aid in determining the true facts relating to the reported violations.

Any person or organization aggrieved by the action taken or the findings established as a result of an informal hearing by the Division of Contract Procurement may request a formal hearing.

4. The wages of labor shall be paid in legal tender of the United States, except that this condition will be considered satisfied if payment is made by a negotiable check, on a solvent bank, which may be cashed readily by the employee in the local community for the full amount, without discount or collection charges of any kind. Where checks are used for payments, the contractor shall make all necessary arrangements for them to be cashed and shall give information regarding such arrangements.

5. No fee of any kind shall be asked or accepted by the contractor or any of his agents from any person as a condition of employment on the project.

6. No laborers shall be charged for any tools used in performing their respective duties except for reasonably avoidable loss or damage thereto.

7. Every employee on the work covered by this contract shall be permitted to lodge, board, and trade where and with whom he elects and neither the contractor nor his agents, nor his employees shall directly or indirectly require as a condition of employment that an employee shall lodge, board or trade at a particular place or with a particular person.

8. Every employee on the project covered by this contract shall be an employee of either the prime contractor or an approved subcontractor.

9. No charge shall be made for any transportation furnished by the contractor or his agents to any person employed on the work.

10. No individual shall be employed as a laborer or mechanic on this contract except on a wage basis, but this shall not be construed to prohibit the rental of teams, trucks or other equipment from individuals.

No Covered employee may be employed on the work except in accordance with the classification set forth in the schedule mentioned above; provided, however, that in the event additional classifications are required, application shall be made by the contractor to the Department of Highways and (1) the Department shall request appropriate classifications and rates from the proper agency, or (2) if there is urgent need for additional classification to avoid undue delay in the work, the contractor may employ such workmen at rates deemed comparable to rates established for similar classifications provided he has made written application through the Department of Highways, addressed to the proper agency, for the supplemental rates. The contractor shall retroactively adjust, upon receipt of the supplemental rates schedule, the wages of any employee paid less than the established rate and may adjust the wages of any employee overpaid.

11. No contractor or subcontractor contracting for any part of the contract work which may require or involve the employment of laborers or mechanics shall require or permit any laborer or mechanic in any work-week in which he is employed on such work, to work in excess of eight hours in any calendar day or in excess of forty hours in such work-week unless such laborer or mechanic receives compensation at a rate not less than one and one half times his basic rate of pay for all hours worked in excess of eight hours in any calendar day or in excess of forty hours in such work-week. A laborer, workman or mechanic and an employer may enter into a written agreement or a collective bargaining agreement to work more than eight (8) hours a calendar day but not more than ten (10) hours a calendar day for the straight time hourly rate. This agreement shall be in writing and shall be executed prior to the employee working in excess of eight (8) hours, but not more than ten (10) hours, in any one (1) calendar day.

12. Payments to the contractor may be suspended or withheld due to failure of the contractor to pay any laborer or

mechanic employed or working on the site of the work, all or part of the wages required under the terms of the contract. The Department may suspend or withhold payments only after the contractor has been given written notice of the alleged violation and the contractor has failed to comply with the wage determination of the Department of Highways.

13. Contractors and subcontractors shall comply with the sections of Kentucky Revised Statutes, Chapter 337 relating to contracts for Public Works.

Revised 2-16-95

EXECUTIVE BRANCH CODE OF ETHICS

In the 1992 regular legislative session, the General Assembly passed and Governor Brereton Jones signed Senate Bill 63 (codified as KRS 11A), the Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (6) provides:

No present or former public servant shall, within six (6) months of following termination of his office or employment, accept employment, compensation or other economic benefit from any person or business that contracts or does business with the state in matters in which he was directly involved during his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, provided that, for a period of six (6) months, he personally refrains from working on any matter in which he was directly involved in state government. This subsection shall not prohibit the performance of ministerial functions, including, but not limited to, filing tax returns, filing applications for permits or licenses, or filing incorporation papers.

KRS 11A.040 (8) states:

A former public servant shall not represent a person in a matter before a state agency in which the former public servant was directly involved, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not as a way to obtain private benefits.

If you have worked for the executive branch of state government within the past six months, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, Room 136, Capitol Building, 700 Capitol Avenue, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Kentucky Equal Employment Opportunity Act of 1978

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:
<https://www.eProcurement.ky.gov>.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **finance.contractcompliance@ky.gov** or by phone at 502-564-2874.

KENTUCKY LABOR CABINET
PREVAILING WAGE DETERMINATION
CURRENT REVISION
HIGHWAY CONSTRUCTION LOCALITY NO. II

Determination No. CR-11-II-HWY

Project No.
Highway

Date of Determination: August 4, 2011

This schedule of the prevailing rate of wages for Locality No. II including the counties of ADAIR, BARREN, BELL, BREATHITT, CASEY, CLAY, CLINTON, CUMBERLAND, ESTILL, FLOYD, GARRARD, GREEN, HARLAN, HART, JACKSON, JOHNSON, KNOTT, KNOX, LAUREL, LAWRENCE, LEE, LESLIE, LETCHER, LINCOLN, MCCREARY, MAGOFFIN, MARTIN, MENIFEE, METCALFE, MONROE, MORGAN, OWSLEY, PERRY, PIKE, POWELL, PULASKI, ROCKCASTLE, RUSSELL, TAYLOR, WAYNE, WHITLEY, and WOLFE has been determined in accordance with the provisions of KRS 337.505 to 337.550. This determination shall be referred to as Prevailing Wage Determination No. CR-11-II-HWY.

The following schedule of rates is to be used for highway construction projects advertised or awarded by the Kentucky Transportation Cabinet. This includes any contracts for the relocation of any utilities or other incidental construction projects advertised or awarded by public authorities as a result of the highway construction project.

Apprentices or trainees shall be permitted to work in accordance with Administrative Regulations adopted by the Commissioner of the Department of Workplace Standards. Copies of these regulations will be furnished upon request to any interested person.

Overtime is to be computed at not less than one and one-half (1 1/2) times the indicated BASE RATE for all hours worked in excess of eight (8) hours per day, or in excess of forty (40) hours per week. However, KRS 337.540 permits an employee and employer to agree, in writing, that the employee will be compensated at a straight time base rate for hours worked in excess of eight (8) hours in any one calendar day, but not more than ten (10) hours worked in any one calendar day, if such written agreement is prior to the over eight (8) hours in a calendar day actually being worked, or where provided for in a collective bargaining agreement. The fringe benefit rate is to be paid for each hour worked at a straight time rate for all hours worked. Fringe benefit amounts are applicable for all hours worked except when otherwise noted. Welders will receive rate for craft in which welding is incidental.

No laborer, workman or mechanic shall be paid at a rate less than that of the General Laborer except those classified as bona fide apprentices registered with the Kentucky State Apprenticeship Supervisor unless otherwise specified in this schedule of wage rates.


Michael Donta, Deputy Commissioner
Department of Workplace Standards

CLASSIFICATIONS

RATE AND FRINGE BENEFITS

BOILERMAKERS:

BASE RATE	\$24.65
FRINGE BENEFIT	12.94

BRICKLAYERS:

Bricklayers:

BASE RATE	\$22.90
FRINGE BENEFITS	8.50

Stone Mason:

BASE RATE	\$21.50
FRINGE BENEFITS	8.50

CARPENTERS:

Carpenters:

BASE RATE	\$22.40
FRINGE BENEFITS	8.75

Piledrivers:

BASE RATE	\$22.05
FRINGE BENEFITS	8.75

CEMENT MASONS:

BASE RATE	\$21.25
FRINGE BENEFITS	8.50

ELECTRICIANS:

*BASE RATE	\$29.36
FRINGE BENEFITS	10.55

*When workmen are required to work from bosum chairs, trusses, stacks, tanks, scaffolds, catwalks, radio and T.V. towers, structural steel (open, unprotected, unfloored raw steel), and bridges or similar hazardous locations where workmen are subject to a direct fall, except where using JLG's and bucket trucks up to 75 feet: Add 25% to workman's base rate for 50 to 75 feet, and add 50% to workman's base rate for over 75 feet.

LINEMAN:

*BASE RATE	\$30.09
FRINGE BENEFITS	10.94

EQUIPMENT OPERATOR:

*BASE RATE	\$26.90
FRINGE BENEFITS	10.31

GROUNDSMAN:

*BASE RATE	\$17.79
FRINGE BENEFITS	8.51

IRONWORKERS:

BASE RATE	\$ 25.77
FRINGE BENEFITS	18.54

CLASSIFICATIONS

RATE AND FRINGE BENEFITS

LABORERS:

General laborer, flagman, steam jenny:	BASE RATE	\$20.84
	FRINGE BENEFITS	8.75
Batch truck dumper, deck hand or scow man, hand blade operator:	BASE RATE	\$20.84
	FRINGE BENEFITS	8.75
Power driven tool operator of the following: wagon drill, chain saw, sand blaster, concrete chipper, pavement breaker, vibrator, power wheelbarrow, power buggy, sewer pipe layer, bottom men, dry cement handler, concrete rubber, mason tender:	BASE RATE	\$21.09
	FRINGE BENEFITS	8.75
Asphalt lute and rakerman, side rail setter:	BASE RATE	\$21.14
	FRINGE BENEFITS	8.75
Gunnite nozzle man, gunnite operator:	BASE RATE	\$21.14
	FRINGE BENEFITS	8.75
Tunnel laborer (free air):	BASE RATE	\$21.14
	FRINGE BENEFITS	8.75
Tunnel mucker (free air):	BASE RATE	\$21.74
	FRINGE BENEFITS	8.75
Tunnel miner, blaster and driller (free air):	BASE RATE	\$21.74
	FRINGE BENEFITS	8.75
Caisson worker:	BASE RATE	\$21.74
	FRINGE BENEFITS	8.75
Powderman:	BASE RATE	\$21.44
	FRINGE BENEFITS	8.75
Drill operator of percussion type drills which are both powered and propelled by an independent air supply:	BASE RATE	\$22.64
	FRINGE BENEFITS	8.75

CLASSIFICATIONS

RATE AND FRINGE BENEFITS

OPERATING ENGINEERS:

Group A:

Auto patrol, batcher plant, bituminous paver, cable-way, clamshell, concrete mixer (21 cu. ft. or over), concrete pump, crane, crusher plant, derrick, derrick boat, ditching and trenching machine, dragline, dredge engineer, elevator (regardless of ownership when used for hoisting any building material), elevating grader and all types of loaders, hoe-type machine, hoisting engine, locomotive, LeTourneau or carry-all scoop, bulldozer, mechanic, orangepeel bucket, piledriver, power blade, roller (bituminous), roller (earth), roller (rock), scarifier, shovel, tractor shovel, truck crane, well points, winch truck, push dozer, grout pump, high lift, fork lift (regardless of lift height), all types of boom cats, multiple operator, core drill, tow or push boat, A-Frame winch truck, concrete paver, gradeall, hoist, hyster, material pump, pumpcrete, ross carrier, sheepfoot, sideboom, throttle-valve man, rotary drill, power generator, mucking machine, rock spreader attached to equipment, scoopmobile, KeCal loader, tower cranes (French, German and other types), hydrocrane, tugger, backfiller guries, self-propelled compactor, self-contained hydraulic percussion drill:

BASE RATE	\$23.80
FRINGE BENEFITS	8.75

Group B:

All air compressors (200 cu. ft. per min. or greater capacity), bituminous mixer, concrete mixer (under 21 cu. ft.), welding machine, form grader, tractor (50 H.P. and over), bull float, finish machine, outboard motor boat, brakeman, mechanic helper, whirly oiler, tractair and road widening trencher, articulating trucks:

BASE RATE	\$21.55
FRINGE BENEFITS	8.75

Group B2:

Greaser on grease facilities servicing heavy equipment:

BASE RATE	\$21.90
FRINGE BENEFITS	8.75

Group C:

Bituminous distributor, cement gun, conveyor, mud jack, paving joint machine, pump, tamping machine, tractors (under 50 H.P.), vibrator, oiler, air compressors (under 200 cu. ft. per min. capacity), concrete saw, burlap and curing machine, hydro seeder, power form handling equipment, deckhand oiler, hydraulic post driver:

BASE RATE	\$21.31
FRINGE BENEFITS	8.75

PAINTERS:

All Excluding Bridges:

BASE RATE	\$19.92
FRINGE BENEFITS	9.57

Bridges:

BASE RATE	\$23.92
FRINGE BENEFITS	10.07

<u>CLASSIFICATIONS</u>	<u>RATE AND FRINGE BENEFITS</u>	
PLUMBERS:	BASE RATE	\$22.52
	FRINGE BENEFITS	7.80
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SHEET METAL:	BASE RATE	\$20.40
	FRINGE BENEFITS	7.80
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TRUCK DRIVERS:		
Truck helper and warehouseman:	BASE RATE	\$21.10
	FRINGE BENEFITS	8.75
Driver, winch truck and A-Frame when used in transporting materials:	BASE RATE	\$21.46
	FRINGE BENEFITS	8.75
Driver, (semi-trailer or pole trailer), driver (dump truck, tandem axle), driver of distributor:	BASE RATE	\$21.45
	FRINGE BENEFITS	8.75
Driver on mixer trucks (all types):	BASE RATE	\$21.45
	FRINGE BENEFITS	8.75
Truck mechanic:	BASE RATE	\$21.38
	FRINGE BENEFITS	8.75
Driver (3 tons and under), tire changer and truck mechanic helper:	BASE RATE	\$21.15
	FRINGE BENEFITS	8.75
Driver on pavement breakers:	BASE RATE	\$21.46
	FRINGE BENEFITS	8.75
Driver (over 3 tons), driver (truck mounted rotary drill):	BASE RATE	\$21.45
	FRINGE BENEFITS	8.75
Driver, Euclid and other heavy earth moving equipment and Low Boy:	BASE RATE	\$21.46
	FRINGE BENEFITS	8.75
Greaser on greasing facilities:	BASE RATE	\$21.15
	FRINGE BENEFITS	8.75
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ERRATUM

Refer to the Locality Number and Determination Number listed below published by the Kentucky Labor Cabinet, Division of Employment Standards, Apprenticeship and Mediation dated August 4, 2011.

Locality: Highway Construction Locality No. II, including the following counties: Adair, Barren, Breathitt, Casey, Clay, Clinton, Cumberland, Estill, Floyd, Garrard, Green, Harlan, Hart, Jackson, Johnson, Knott, Knox, Laurel, Lawrence, Lee, Leslie, Letcher, Lincoln, McCreary, Magoffin, Martin, Menifee, Metcalfe, Monroe, Morgan, Owsley, Perry, Pike, Powell, Pulaski, Rockcastle, Russell, Taylor, Wayne, Whitley and Wolfe.

Determination Number: CR-II-II-HWY

DELETE:

Ironworkers	BASE RATE	\$25.77
	FRINGE BENEFIT	18.54

INSERT:

Ironworker (Structural)	BASE RATE	\$22.50
	FRINGE BENEFIT	8.75

Ironworker (Reinforcing)	BASE RATE	\$22.30
	FRINGE BENEFIT	8.75



Michael L. Dixon, Commissioner
Department of Workplace Standards
Kentucky Labor Cabinet
Frankfort, KY 40601

This 8th day of November, 2011.

ERRATUM

Refer to the Locality Number and Determination Number listed below published by the Kentucky Labor Cabinet, Division of Employment Standards, Apprenticeship and Mediation dated August 4, 2011.

Locality: Highway Construction Locality No. II, including the following counties: Adair, Barren, Breathitt, Casey, Clay, Clinton, Cumberland, Estill, Floyd, Garrard, Green, Harlan, Hart, Jackson, Johnson, Knott, Knox, Laurel, Lawrence, Lee, Leslie, Letcher, Lincoln, McCreary, Magoffin, Martin, Menifee, Metcalfe, Monroe, Morgan, Owsley, Perry, Pike, Powell, Pulaski, Rockcastle, Russell, Taylor, Wayne, Whitley and Wolfe.

Determination Number: CR-II-II-HWY

DELETE:

Ironworker (Structural)	BASE RATE	\$22.50
	FRINGE BENEFIT	8.75
Ironworker (Reinforcing)	BASE RATE	\$22.30
	FRINGE BENEFIT	8.75

INSERT:

Ironworkers	BASE RATE	\$25.77
	FRINGE BENEFIT	18.54


Michael L. Dixon, Commissioner
Department of Workplace Standards
Kentucky Labor Cabinet
Frankfort, KY 40601

This 4TH day of May, 2012.

Kentucky Determination No. CR-11-II-HWY dated August 04, 2011

Fringe benefit amounts are applicable for all hours worked except when otherwise noted.

No laborer, workman or mechanic shall be paid at a rate less than that of the General Laborer except those classified as bona fide apprentices registered with the Kentucky State Apprenticeship Supervisor unless otherwise specified in this schedule of wage rates.

These rates are listed pursuant to the Kentucky Determination No. CR-11-II HWY dated August 04, 2011, erratum dated November 8, 2011, and erratum dated May 4, 2012. Apprentices or trainees shall be permitted to work as such subject to Administrative Regulations adopted by the Commissioner of Workplace Standards. Copies of these regulations will be furnished upon request from any interested person.

Before using apprentices on the job the contractor shall present to the Contracting Officer written evidence of registration of such employees in a program of a State apprenticeship and training agency approved and recognized by the U. S. Bureau of Apprenticeship and Training. In the absence of such a State agency, the contractor shall submit evidence of approval and registration by the U. S. Bureau of Apprenticeship and Training.

The contractor shall submit to the Contracting Officer, written evidence of the established apprenticeship-journeyman ratios and wage rates in the project area, which will be the basis for establishing such ratios and rates for the project under the applicable contract provisions.

Kentucky Determination No. CR-11-II-HWY dated August 04, 2011

TO: EMPLOYERS/EMPLOYEES

PREVAILING WAGE SCHEDULE:

The wages indicated on this wage schedule are the least permitted to be paid for the occupations indicated. When an employee works in more than one classification, the employer must record the number of hours worked in each classification at the prescribed hourly base rate.

OVERTIME:

Overtime is to be paid after an employee works eight (8) hours a day or forty (40) hours a week, whichever gives the employee the greater wages. At least time and one-half the base rate is required for all overtime. A laborer, workman or mechanic and an employer may enter into a written agreement or a collective bargaining agreement to work more than eight (8) hours a calendar day but not more than ten (10) hours a calendar day for the straight time hourly rate.

Wage violations or questions should be directed to the designated Engineer or to the undersigned.

Ryan Griffith, Director
Division of Construction Procurement
Frankfort, Kentucky 40622

PART IV

INSURANCE

INSURANCE

The Contractor shall procure and maintain the following insurance in addition to the insurance required by law:

- 1) Commercial General Liability-Occurrence form – not less than \$2,000,000 General aggregate, \$2,000,000 Products & Completed Aggregate, \$1,000,000 Personal & Advertising, \$1,000,000 each occurrence.
- 2) Automobile Liability- \$1,000,000 per accident
- 3) Employers Liability:
 - a) \$100,000 Each Accident Bodily Injury
 - b) \$500,000 Policy limit Bodily Injury by Disease
 - c) \$100,000 Each Employee Bodily Injury by Disease
- 4) The insurance required above must be evidenced by a Certificate of Insurance and this Certificate of Insurance must contain one of the following statements:
 - a) "policy contains no deductible clauses."
 - b) "policy contains _____ (amount) deductible property damage clause but company will pay claim and collect the deductible from the insured."
- 5) KENTUCKY WORKMEN'S COMPENSATION INSURANCE. The contractor shall furnish evidence of coverage of all his employees or give evidence of self-insurance by submitting a copy of a certificate issued by the Workmen's Compensation Board.

The cost of insurance is incidental to all contract items. All subcontractors must meet the same minimum insurance requirements.

PART V

BID ITEMS

CONTRACT ID: 121028
COUNTY: ROCKCASTLE
PROPOSAL: FD04 SPP 102 0075 055-059

PAGE: 1
LETTING: 08/17/12
CALL NO: 304

LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
SECTION 0001 ROADWAY					
0010	00001	DGA BASE	2,044.000 TON		
0020	00018	DRAINAGE BLANKET-TYPE II-ASPH	573.000 TON		
0030	00078	CRUSHED AGGREGATE SIZE NO 2	29.000 TON		
0040	00100	ASPHALT SEAL AGGREGATE	579.000 TON		
0050	00194	LEVELING & WEDGING PG76-22	85.000 TON		
0060	00214	CL3 ASPH BASE 1.00D PG64-22	6,489.000 TON		
0070	00214	CL3 ASPH BASE 1.00D PG64-22 FOR EDGE DRAIN	526.000 TON		
0080	00219	CL4 ASPH BASE 1.00D PG76-22	10,690.000 TON		
0090	00291	EMULSIFIED ASPHALT RS-2	69.000 TON		
0100	00339	CL3 ASPH SURF 0.38D PG64-22	1,389.000 TON		
0110	00342	CL4 ASPH SURF 0.38A PG76-22	3,548.000 TON		
0120	00522	STORM SEWER PIPE-18 IN	912.000 LF		
0130	01000	PERFORATED PIPE-4 IN	16,500.000 LF		
0140	01010	NON-PERFORATED PIPE-4 IN	330.000 LF		
0150	01015	INSPECT & CERTIFY EDGE DRAIN SYSTEM	(1.00) LS		
0160	01020	PERF PIPE HEADWALL TY 1-4 IN	19.000 EACH		
0170	01028	PERF PIPE HEADWALL TY 3-4 IN	10.000 EACH		
0180	01480	CURB BOX INLET TYPE B	3.000 EACH		
0190	01484	CURB BOX INLET TYPE B-T	1.000 EACH		
0200	01490	DROP BOX INLET TYPE 1	2.000 EACH		

ROCKCASTLE COUNTY
FD04 SPP 102 0075 055-059

KENTUCKY TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS
FRANKFORT, KY 40622

CONTRACT ID: 121028
COUNTY: ROCKCASTLE
PROPOSAL: FD04 SPP 102 0075 055-059

PAGE: 2
LETTING: 08/17/12
CALL NO: 304

LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
0210	01660	SPRING BOX INLET TYPE A	1.000 EACH		
0220	01691	FLUME INLET TYPE 2	2.000 EACH		
0230	01756	MANHOLE TYPE A	1.000 EACH		
0240	01877	SPECIAL HEADER CURB	1,181.000 LF		
0250	01890	ISLAND HEADER CURB TYPE 1	320.000 LF		
0260	01904	REMOVE CURB	20.000 LF		
0270	01982	DELINEATOR FOR GUARDRAIL MONO DIRECTIONA	WHITE 87.000 EACH		
0280	01983	DELINEATOR FOR GUARDRAIL MONO DIRECTIONA	YELLOW 3.000 EACH		
0290	02014	BARRICADE-TYPE III	6.000 EACH		
0300	02220	FLOWABLE FILL	4.000 CU YD		
0310	02230	EMBANKMENT IN PLACE	3,200.000 CU YD		
0320	02237	DITCHING	11,864.000 LF		
0330	02352	GUARDRAIL-STEEL W BEAM-D FACE	275.000 LF		
0340	02360	GUARDRAIL TERMINAL SECTION NO 1	2.000 EACH		
0350	02363	GUARDRAIL CONNECTOR TO BRIDGE END TY A	2.000 EACH		
0360	02365	CRASH CUSHION TYPE IX-A	2.000 EACH		
0370	02367	GUARDRAIL END TREATMENT TYPE 1	3.000 EACH		
0380	02369	GUARDRAIL END TREATMENT TYPE 2A	6.000 EACH		
0390	02373	GUARDRAIL END TREATMENT TYPE 3	1.000 EACH		
0400	02381	REMOVE GUARDRAIL	8,712.500 LF		
0410	02483	CHANNEL LINING CLASS II	576.000 TON		

KENTUCKY TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS
FRANKFORT, KY 40622

CONTRACT ID: 121028
COUNTY: ROCKCASTLE
PROPOSAL: FD04 SPP 102 0075 055-059

PAGE: 3
LETTING: 08/17/12
CALL NO: 304

LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
0420	02484	CHANNEL LINING CLASS III	267.000 TON		
0430	02562	SIGNS	500.000 SQFT		
0440	02565	OBJECT MARKER TYPE 2	2.000 EACH		
0450	02650	MAINTAIN & CONTROL TRAFFIC	(1.00) LS		
0460	02671	PORTABLE CHANGEABLE MESSAGE SIGN	2.000 EACH		
0470	02676	MOBILIZATION FOR MILL & TEXT	(1.00) LS		
0480	02677	ASPHALT PAVE MILLING & TEXTURING	7,122.000 TON		
0490	02696	SHOULDER RUMBLE STRIPS-SAWED	34,400.000 LF		
0500	02775	ARROW PANEL	2.000 EACH		
0510	03262	CLEAN PIPE STRUCTURE	1.000 EACH		
0520	05950	EROSION CONTROL BLANKET	11,200.000 SQYD		
0530	06412	STEEL POST MILE MARKERS	3.000 EACH		
0540	06417	FLEXIBLE DELINEATOR POST-W	172.000 EACH		
0550	06418	FLEXIBLE DELINEATOR POST-Y	12.000 EACH		
0560	06511	PAVE STRIPING-TEMP PAINT-6 IN	43,000.000 LF		
0570	06568	PAVE MARKING-THERMO STOP BAR-24IN	52.000 LF		
0580	06592	PAVEMENT MARKER TYPE V-B W/R	257.000 EACH		
0590	06593	PAVEMENT MARKER TYPE V-B Y/R	28.000 EACH		
0600	06600	REMOVE PAVEMENT MARKER TYPE V	257.000 EACH		
0610	08100	CONCRETE-CLASS A	1.000 CUYD		
0620	08150	STEEL REINFORCEMENT	8.000 LB		

KENTUCKY TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS
FRANKFORT, KY 40622

CONTRACT ID: 121028
COUNTY: ROCKCASTLE
PROPOSAL: FD04 SPP 102 0075 055-059

PAGE: 4
LETTING: 08/17/12
CALL NO: 304

LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
0630	10020NS	FUEL ADJUSTMENT	31,854.000 DOLL	1.00	31,854.00
0640	10030NS	ASPHALT ADJUSTMENT	56,132.000 DOLL	1.00	56,132.00
0650	20757ED	PAVEMENT REPAIR	2,220.000 SQYD		
0660	21802EN	G/R STEEL W BEAM-S FACE (7 FT POST)	8,687.500 LF		
0670	23143ED	KPDES PERMIT AND TEMP EROSION CONTROL	(1.00) LS		
0680	23237EN10W	WATERBLAST STRIPE REMOVAL	17,200.000 LF		
0690	24189ER	DURABLE WATERBORNE MARKING-6 IN W	21,080.000 LF		
0700	24190ER	DURABLE WATERBORNE MARKING-6 IN Y	18,303.000 LF		
0710	24191ER	DURABLE WATERBORNE MARKING-12 IN W	840.000 LF		
SECTION 0002 MOB AND DEMOB					
0720	02568	MOBILIZATION (NO MORE THAN 5%)	LUMP		
0730	02569	DEMOBILIZATION (AT LEAST 1.5%)	LUMP		
		TOTAL BID			