



CALL NO. 302

CONTRACT ID. 251110

WARREN COUNTY

FED/STATE PROJECT NUMBER FD04 114 0234 010-011

**DESCRIPTION LEFT AND RIGHT TURN LANE EXTENSIONS ON KY 234 AT
LOVERS LANE**

WORK TYPE WIDENING

PRIMARY COMPLETION DATE 3/31/2026

LETTING DATE: June 26,2025

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME June 26,2025. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

NO PLANS ASSOCIATED WITH THIS PROJECT.

REQUIRED BID PROPOSAL GUARANTY: Not less than 5% of the total bid.

TABLE OF CONTENTS

PART I	SCOPE OF WORK <ul style="list-style-type: none">• PROJECT(S), COMPLETION DATE(S), & LIQUIDATED DAMAGES• CONTRACT NOTES• STATE CONTRACT NOTES• INCIDENTAL SURFACING• COMPACTION OPTION B• SPECIAL NOTE(S) APPLICABLE TO PROJECT• ELECTRONIC DELIVERY MANAGEMENT SYSTEM (E-TICKETING) ASPHALT• ELECTRONIC DELIVERY MANAGEMENT SYSTEM (E-TICKETING) AGGREGATE• DOLOMITIC POLISH-RESISTANT AGGREGATE CLASS A 0.38-IN AND 0.50-IN NOMINAL ASPHALT MIXTURES• DOUBLE ASPHALT SEAL COAT• RIGHT OF WAY CERTIFICATION• UTILITY IMPACT & RAIL CERTIFICATION NOTES
PART II	SPECIFICATIONS AND STANDARD DRAWINGS <ul style="list-style-type: none">• STANDARD AND SUPPLEMENTAL SPECIFICATIONS• [SN-1I] PORTABLE CHANGEABLE MESSAGE SIGNS
PART III	EMPLOYMENT, WAGE AND RECORD REQUIREMENTS <ul style="list-style-type: none">• LABOR AND WAGE REQUIREMENTS• EXECUTIVE BRANCH CODE OF ETHICS• KENTUCKY EQUAL EMPLOYMENT OPPORTUNITY ACT OF 1978 LOCALITY / STATE• PROJECT WAGE RATES / STATE FUNDED
PART IV	BID ITEMS

PART I
SCOPE OF WORK

ADMINISTRATIVE DISTRICT - 03

CONTRACT ID - 251110
FD04 114 0234 010-011
COUNTY - WARREN
PCN - DE11402342510
FD04 114 0234 010-011

LEFT AND RIGHT TURN LANE EXTENSIONS ON KY 234 AT LOVERS LANE LEFT AND RIGHT TURN LANE
EXTENSIONS ON KY 234 @ LOVERS LANE, A DISTANCE OF 0.60 MILES.WIDENING SYP NO. 03-80318.00.
GEOGRAPHIC COORDINATES LATITUDE 86:23:46.00 LONGITUDE 36:58:52.00
ADT

COMPLETION DATE(S):
COMPLETED BY 11/15/2025 SEE SPECIAL NOTES
COMPLETED BY 03/31/2026 APPLIES TO ENTIRE CONTRACT

CONTRACT NOTES

INSURANCE

Refer to Kentucky Standard Specifications for Road and Bridge Construction, current edition.

PROPOSAL ADDENDA

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

BID SUBMITTAL

Bidder must use the Department's electronic bidding software. The Bidder must download the bid file located on the Bid Express website (www.bidx.com) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

JOINT VENTURE BIDDING

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

UNDERGROUND FACILITY DAMAGE PROTECTION

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. When prescribed in said directives, the contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom shall be contacted through their individual Protection Notification Center. Non-compliance with these directives can result in the enforcement of penalties.

REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth ("certificate") from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the

foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity's solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by email to kytc.projectquestions@ky.gov. The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading "Questions & Answers" on the Construction Procurement website (www.transportation.ky.gov/construction-procurement). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

HARDWOOD REMOVAL RESTRICTIONS

The US Department of Agriculture has imposed a quarantine in Kentucky and several surrounding states, to prevent the spread of an invasive insect, the emerald ash borer. Hardwood cut in conjunction with the project may not be removed from the state. Chipping or burning on site is the preferred method of disposal.

INSTRUCTIONS FOR EXCESS MATERIAL SITES AND BORROW SITES

Identification of excess material sites and borrow sites shall be the responsibility of the Contractor. The Contractor shall be responsible for compliance with all applicable state and federal laws and may wish to consult with the US Fish and Wildlife Service to seek protection under Section 10 of the Endangered Species Act for these activities.

ACCESS TO RECORDS

The state agency certifies that it is in compliance with the provisions of KRS 45A.150, "Access to contractor's books, documents, papers, records, or other evidence directly pertinent to the contract." The Contractor, as defined in KRS 45A.030, agrees that the contracting agency, the

Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this agreement for the purpose of financial audit or program review. The Contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884. Records and other prequalification information confidentially disclosed as part of the bid process shall not be deemed as directly pertinent to the agreement and shall be exempt from disclosure as provided in KRS 61.878(1)(c).

BOYCOTT PROVISIONS

If applicable, the contractor represents that, pursuant to [KRS 45A.607](#), they are not currently engaged in, and will not for the duration of the contract engage in, the boycott of a person or an entity based in or doing business with a jurisdiction with which Kentucky can enjoy open trade. **Note:** The term Boycott does not include actions taken for bona fide business or economic reasons, or actions specifically required by federal or state law.

If applicable, the contractor verifies that, pursuant to KRS 41.480, they do not engage in, and will not for the duration of the contract engage in, in energy company boycotts as defined by KRS 41.472.

LOBBYING PROHIBITIONS

The contractor represents that they, and any subcontractor performing work under the contract, have not violated the agency restrictions contained in [KRS 11A.236](#) during the previous ten (10) years, and pledges to abide by the restrictions set forth in such statute for the duration of the contract awarded.

The contractor further represents that, pursuant to [KRS 45A.328](#), they have not procured an original, subsequent, or similar contract while employing an executive agency lobbyist who was convicted of a crime related to the original, subsequent, or similar contract within five (5) years of the conviction of the lobbyist.

Revised: 1/1/2025

1.0 BUY AMERICA REQUIREMENT.

Follow the “Buy America” provisions as required by 23 U.S.C. § 313 and 23 C.F.R. § 635.410. Except as expressly provided herein all manufacturing processes of steel or iron materials including but not limited to structural steel, guardrail materials, corrugated steel, culvert pipe, structural plate, prestressing strands, and steel reinforcing bars shall occur in the United States of America, including the application of:

- Coating,
- Galvanizing,
- Painting, and
- Other coating that protects or enhances the value of steel or iron products.

The following are exempt, unless processed or refined to include substantial amounts of steel or iron material, and may be used regardless of source in the domestic manufacturing process for steel or iron material:

- Pig iron,
- Processed, pelletized, and reduced iron ore material, or
- Processed alloys.

The Contractor shall submit a certification stating that all manufacturing processes involved with the production of steel or iron materials occurred in the United States.

Produce, mill, fabricate, and manufacture in the United States of America all aluminum components of bridges, tunnels, and large sign support systems, for which either shop fabrication, shop inspection, or certified mill test reports are required as the basis of acceptance by the Department.

Use foreign materials only under the following conditions:

- 1) When the materials are not permanently incorporated into the project; or
- 2) When the delivered cost of such materials used does not exceed 0.1 percent of the total Contract amount or \$2,500.00, whichever is greater.

The Contractor shall submit to the Engineer the origin and value of any foreign material used.

2.0 – BUILD AMERICA, BUY AMERICA (BABA)

Contractor shall comply with the Federal Highway Administration (FHWA) Buy America Requirement in 23 C.F.R. § 635.410 and all relevant provisions of the Build America, Buy America Act (BABA), contained within the Infrastructure Investment and Jobs Act, Pub. L. No. 117-58, §§ 70901-52 enacted November 15, 2021. The BABA requires iron, steel, manufactured products, and construction materials used in infrastructure projects funded by federal financial assistance to be produced in the United States. Comply with 2 C.F.R § 184.

BABA permits FHWA participation in the Contract only if domestic steel and iron will be used on the Project. To be considered domestic, all steel and iron used, and all products manufactured from steel and iron must be produced in the United States and all manufacturing processes, including application of a coating, for these materials must occur in the United States. Coating includes all processes that protect or enhance the value of the material to which the coating is applied. This requirement does not preclude a minimal use of foreign steel and iron materials, provided the cost of such materials does not exceed 0.1% of the total contract amount under the Contract or \$2,500.00 whichever is greater.

BABA permits FHWA participation in the Contract only if all “construction materials” as defined in the Act are made in the United States. The Buy America preference applies to the following construction materials incorporated into infrastructure projects: non-ferrous metals; plastic and polymer-based products (including polyvinylchloride, composite building materials, and polymers used in fiber optic cables); glass (including optic glass); Fiber optic cable; optical fiber; lumber; engineered wood; and drywall. Contractor will be

required to use construction materials produced in the United States on this Project. The Contractor shall submit a certification stating that all construction materials are certified to be BABA compliant.

3.0 FINAL RULE – FHWA’S BUY AMERICA REGULATION TO TERMINATE GENERAL APPLICABILITY WAIVER FOR MANUFACTURED PRODUCTS

- **March 17, 2025** (effective date): For all Federal-aid projects obligated on or after March 15, 2025, all iron or steel products, as defined in § 635.410(c)(1)(iii), must comply with FHWA’s Buy America requirements for steel and iron in § 635.410(b). In addition, for all Federal-aid projects obligated on or after March 15, 2025, per § 635.410(c)(2), articles, materials, and supplies should be classified as an iron or steel product, a manufactured product, or another product as specified by law or in 2 CFR part 184 (such other products specified by law or in 2 CFR part 184 include “excluded materials” and “construction materials”); an article, material, or supply must not be considered to fall into multiple categories.
- **October 1, 2025:** The final assembly requirement will become effective for Federal-aid projects obligated on or after October 1, 2025. This means that, for manufactured product to be Buy America compliant, for Federal-aid projects obligated on or after October 1, 2025, final assembly of the manufactured product must occur in the United States.
- **October 1, 2026:** The 55 percent requirement will become effective for Federal-aid projects obligated on or after October 1, 2026. This means that, for manufactured product to be Buy America-compliant, for Federal-aid projects obligated on or after October 1, 2026, all manufactured products permanently incorporated into the project must both be manufactured in the United States (satisfy the final assembly requirement) and have the cost of the components of the manufactured product that are mined, produced, or manufactured in the United States be greater than 55 percent of the total cost of all components of the manufactured product (satisfy the 55 percent requirement).

4.0 – ADDITIONAL REQUIREMENTS

The Contractor has completed and submitted, or shall complete and submit, to the Cabinet a Buy America/Build America, Buy America Certificate prior to the Cabinet issuing the notice to proceed, in the format below. After submittal, the Contractor is bound by its original certification.

A false certification is a criminal act in violation of 18 U.S.C. § 1001. The Contractor has the burden of proof to establish that it’s in compliance.

At the Contractor’s request, the Cabinet may, but is not obligated to, seek a waiver of Buy America requirements if grounds for the waiver exist under 23 C.F.R. § 635.410(c) or will comply with the applicable Buy America requirements if a waiver of those requirements is not available or not pursued by the Cabinet.

Please refer to the Federal Highway Administration’s Buy America webpage for more information.

[Buy America - Construction Program Guide - Contract Administration - Construction - Federal Highway Administration \(dot.gov\)](#)

Effective - June 26, 2025, Letting

BUY AMERICA / BUILD AMERICA, BUY AMERICA (ACT) MATERIALS CERTIFICATE OF COMPLIANCE

The Contractor hereby certifies that it will comply with all relevant provisions of the Build America, Buy America Act, contained within the Infrastructure Investment and Jobs Act, Pub. L. NO. 117-58, §§ 70901-52, the requirements of 23 U.S.C. § 313, 23 C.F.R. § 635.410 and 2 C.F.R § 184.

Date Submitted:_____

Contractor:_____

Signature:_____

Printed Name:_____

Title:_____

NOTE: THIS CERTIFICATION IS IN ADDITION TO ANY AND ALL REQUIREMENTS OUTLINED IN THE CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND/OR SPECIAL NOTES CONTAINED IN THE PROJECT PROPOSAL.

SPECIAL NOTE FOR RECIPROCAL PREFERENCE

RECIPROCAL PREFERENCE TO BE GIVEN BY PUBLIC AGENCIES TO RESIDENT BIDDERS

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the electronic bidding software. Submittal of the Affidavit should be done along the bid in Bid Express.

April 30, 2018

INCIDENTAL SURFACING

The Department has included in the quantities of asphalt mixtures established in the proposal estimated quantities required for resurfacing or surfacing mailbox turnouts, farm field entrances, residential and commercial entrances, curve widening, ramp gores and tapers, and road and street approaches, as applicable. Pave these areas to the limits as shown on Standard Drawing RPM-110-06 or as directed by the Engineer. In the event signal detectors are present in the intersecting streets or roads, pave the crossroads to the right of way limit or back of the signal detector, whichever is the farthest back of the mainline. Surface or resurface these areas as directed by the Engineer. The Department will not measure placing and compacting for separate payment but shall be incidental to the Contract unit price for the asphalt mixtures.

OPTION B

Be advised that the Department will control and accept compaction of asphalt mixtures furnished on this project under OPTION B in accordance with Sections 402 and 403.

TABLE OF CONTENTS

SPECIAL NOTES:

- Special Notes Applicable to Project (General Notes & Description of Work)
- Special Note for Completion Dates & Liquidated Damages
- Special Note for Staking
- Special Note for Erosion Control
- Traffic Control plan
- Standard Drawings that Apply

DETAIL SHEETS:	Detail Number
Vicinity Map.....	R1
Typical Section Sheets.....	R2A-R2B
General Summary & Paving Summary Sheet.....	R2C
Utility Owners Sheet.....	R2D
Plan & Profile Sheets.....	R3-R6
Coordinate Control Data Sheet.....	R7
Striping Plan Sheets.....	R8-R10
Lighting Plan Sheets.....	R11-R13
Light Pole Base.....	R14-R15
Structural Detail Sheet.....	R16
Sepia 008 – Inlaid Pavement Marker Arrangements Multi-Lane Roadways.....	R17
Right Turn Lane Cross Sections.....	X1-X9
Left Turn Lane Cross Sections.....	X10-X24

Special Notes Applicable to Project – General Notes & Description of Work

CAUTION

The information in this proposal and the type of work listed herein are approximate only and are not to be taken as an exact evaluation of the materials and conditions to be encountered during construction; the bidder must draw his/her own conclusions. The Department does not give any guarantee as to the accuracy of the data and no claim for money or time extension will be considered if the conditions encountered are not in accordance with the information shown.

EXISTING MAPPING AND CONTROL

All survey information was obtained by field survey and should be field verified prior to beginning work. Refer to the Special Note for Staking concerning staking operations required to control and construct the work.

ON-SITE INSPECTION

Before submitting a bid for the work, make a thorough inspection of the site and determine existing conditions so that the work can be expeditiously performed after a contract is awarded. The Department will consider submission of a bid to be evidence of this inspection having been made. The Department will not honor any claims for money or time extension resulting from site conditions.

RIGHT OF WAY LIMITS

The Department has not established the exact limits of the Right-of-Way. Unless a consent and release form is obtained from the adjoining property owner, limit work activities to the obvious Right-of-Way and staging areas secured by the Contractor at no additional cost to the Department. In the event that private improvements (i.e. fences, buildings, etc.) encroach upon the Right-of-Way, the contractor shall notify the Engineer and limit work activities in order to NOT disturb the improvements. If they become necessary, the Department will secure consent and releases from property owners through the Engineer. Be responsible for all encroachments onto private lands.

UTILITY COORDINATION

Coordination with Utility Companies. Locate all underground, above ground, and overhead utilities prior to beginning construction. Be responsible for contacting and maintaining liaison with all utility companies that have utilities located within the project limits. Do not disturb existing overhead or underground utilities. There are two existing street lights and associated underground electric service line in the center raised median that will be removed. Outside of these two street lights and electric service line it is not anticipated that any other utility facilities will need to be relocated and/or adjusted; however, in the event that it is discovered that the work does require that additional utilities be relocated and/or adjusted, the utility companies will work concurrently with the Contractor while relocating their facilities. The contractor will be responsible for repairing all utility damage that occurs as a result of construction operations at no additional cost to the Department. NOTIFY THE ENGINEER AND THE UTILITY OWNER(S) IMMEDIATELY WHEN IT IS DISCOVERED OR ANTICIPATED THAT ANY UTILITY CONFLICT COULD DELAY THE CONTRACTOR'S OPERATIONS. If the total delay exceeds ten working days, an extension of the specified completion date will be negotiated with the Contractor for delay to the Contractor's work; however, no extension will be granted for any delay caused by the Contractor's failure to notify the Engineer and/or the utility company as specified above when a conflict is discovered or anticipated as specified. This will be done by the contractor.

General Notes & Description of Work
Page 2 of 3

CONTROL

Perform all work under the absolute control of the Department of Highways. Obtain the Engineer's approval of all designs required to be furnished by the Contractor prior to incorporation into the work. The Department reserves the right to have other work performed by other contractors and its own forces and to permit public utility companies and others to do work during the construction within the limits of, or adjacent to, the project. Conduct operations and cooperate with such other parties so that interference with such other work will be reduced to a minimum. The Department will not honor any claims for money or time extension created by the operations of such other parties. Should a difference of opinion arise as to the rights of the Contractor and others working within the limits of, or adjacent to, the project, the Engineer will decide as to the respective rights of the various parties involved in order to assure the completion of the Department's work in general harmony and in a satisfactory manner, and his/her decision shall be final and binding upon the Contractor.

DESCRIPTION OF WORK

Except as specified herein, perform all work in accordance with the Department's Standard Specifications, Supplemental Specifications, applicable Special Notes and Special Provisions, and applicable Standard and Sepia Drawings, current editions. Furnish all materials, labor, equipment, and incidentals for the following work:

Turn Lane Extensions – KY 234 at Lovers Lane Intersection Improvements: The scope of this project includes the extension of both the existing right-turn and left-turn lanes serving traffic movements from KY 234 onto Lovers Lane. The purpose of the improvements is to enhance vehicular storage capacity (queue length) at the intersection. This will be achieved through the construction of additional full-depth asphalt pavement widening.

Roadway Widening and Pavement Construction: The proposed roadway widening will occur from Station 24+00 to Station 28+00 on the right-turn lane, and from Station 35+20 to Station 41+56.93 on the left-turn lane. Construction activities will include:

- Excavation and earthwork operations for subgrade preparation
- Installation of barrier-type curb and gutter
- Construction of a longitudinal edge key along the existing pavement joint
- Placement of a crushed stone rock roadbed and aggregate base course
- Placement of asphalt base course and asphalt surface course
- Final surface treatment including the installation of permanent pavement striping and markings in accordance with MUTCD standards

Drainage Structure Modification: At Station 37+30 Right, the top section of the existing DBI Type 13G drainage structure will be removed and retrofitted with a custom-fabricated manhole lid designed for site-specific hydraulic and structural requirements.

Signing: Existing signage at two specified locations within the project limits shall be removed (see detail sheets) and delivered to the District 3 Traffic Section. The District 3 Traffic Section will reinstall the signage upon completion of construction activities.

Street Lighting Removal Electrical Coordination and Utility Disconnection: Two existing street light poles are located within the center raised median at Sta. 37+80 and Sta. 39+37. These fixtures are to be removed by BGMU as part of the project scope. The Contractor shall coordinate with Bowling Green Municipal Utilities (BGMU) to arrange for the disconnection and de-energization of the electrical service lines

General Notes & Description of Work
Page 3 of 3

supplying these luminaires. Following electrical disconnection, BGMU will remove and retain the light poles. The Contractor is responsible for providing a minimum of 14 calendar days' advance notice to BGMU to schedule the required utility work.

Street Lighting Installation: At Station 37+80 and Station 39+37, the existing two street lights will be replaced with four new luminaires—two at each location—positioned on the outside shoulders. The contractor shall be responsible for the infrastructure associated with the new street lighting system, which includes the construction of reinforced concrete pole foundations, installation of electrical secondary vaults, and placement of electrical conduit and pull string necessary for each pole location. Upon completion of conduit installation, the contractor will coordinate with BGMU, acting as an electrical subcontractor, to perform the following tasks: wire pulling through the pre-installed conduit, erection of street light poles on the prepared foundations, and final connection and energization of the lighting fixtures.

SPECIAL NOTES FOR COMPLETION DATES & LIQUIDATED DAMAGES

Due to the lead time associated with the street light poles, this project will include an intermediate milestone. All construction activities—excluding the installation of the underground electrical conductor and the erection of four street light poles and luminaires (to be installed by BGMU)—must be complete by **November 15, 2025**.

The ultimate fixed completion date for this project, including all outstanding electrical and lighting installations, will be **March 31, 2026**.

The Contractor shall notify the Engineer two (2) weeks prior to beginning construction activities. The Department will assess Liquidated Damages for failure to complete construction on time.

In addition to the requirements of Section 108.09, the Department will assess Liquidated Damages in the amount of **\$5,000** per day for each day, or fraction of a day, for all lane closures that are in place beyond the time frame(s) noted in the Traffic Control Plan and approved by the Engineer.

In addition to the requirements of Section 108.09, the Department will assess Liquidated Damages in the amount of **\$1,000** per hour for each hour, or fraction of an hour, for any and all road closures that are in place beyond the time frame(s) noted in the Traffic Control Plan and approved by the Engineer.

Contrary to Section 108.09, Liquidated Damages will be assessed regardless of whether seasonal limitations prohibit the Contractor from performing work on the controlling operation.

All liquidated damages will be applied accumulatively.

All other applicable portions of Section 108 apply.

SPECIAL NOTE FOR STAKING

Perform Contractor Staking according to Section 201; except, in addition to the requirements of Section 201, perform the following:

1. Contrary to Section 201, perform items 1-3 usually performed by the Engineer.
2. Verify the dimensions, type, and quantities of the culvert pipes and/or entrance pipes as listed and detailed in the proposal, and determine flow line elevations and slopes necessary to provide positive drainage. Revise as necessary to accommodate the existing site conditions; to provide proper alignment of the drainage structures with existing and/or proposed ditches, stream channels, swales, and the roadway lines and grades; and to ensure positive drainage upon completion of the work.
3. Using stakes, paint marks on the pavement, mag nails, and/or any other means approved by the Engineer, the Contractor shall mark and/or stake the proposed sign locations in the field. NOTE: The proposed signs are listed in the proposal by approximate location and are NOT to be taken as the exact location for the signs. During staking operations the Contractor shall review the signing layout and existing field conditions and look for potential conflicts, including but not limited to utilities, driveways, visual obstructions, etc. When conflicts are found, adjust the staked location of signs to mitigate conflicts. Because the sign locations in the proposal are approximate and the location of some signs may need to be adjusted due to conflicts, during staking operations the Contractor shall refer to and utilize the information in the Manual on Uniform on Traffic Control Devices (MUTCD), current edition. The MUTCD cover items such as: appropriate sign location, advance placement distances, and spacing requirements for signing. The intent is for the proposed signs to be consistent with, and meet the requirements of, the MUTCD. Once the proposed sign locations have been staked, notify and coordinate with the District Traffic Engineer, and perform a review of the staked locations. Adjust the staked locations, as directed by the District Traffic Engineer and obtain approval of the final staked locations. This review will also be used to determine if there are any existing signs that require removal and/or relocation. Provide the District Traffic Engineer with 2 weeks of notice when a route will be ready for a review of the staked locations. NOTE: The District Traffic Engineer may determine that the proposed signing, including sign types and messages, needs to be adjusted and/or modified from what is shown in the proposal. Therefore, the Contractor shall not order any sign material for a route until the route has been staked and final sign location approval has been given by the District Traffic Engineer.
4. Using paint marks on the pavement, and/or any other means approved by the Engineer, the Contractor shall layout and pre-mark the proposed striping, pavement markings, etc. Adjust as necessary to accommodate the existing site conditions and to provide proper alignment of the proposed thru and turning lanes. Obtain approval of the pre-marked layout from the Engineer and/or District Traffic Engineer prior to installing the striping and/or pavement markings.
5. Prior to incorporating into the work, obtain the Engineers approval of all revisions determined by the Contractor.
6. Perform any and all other staking operations required to control and construct the work.

SPECIAL NOTE FOR EROSION CONTROL

A. DESCRIPTION

Perform all erosion and water pollution control work in accordance with any other notes in the Proposal, the Department's Standard and Interim Supplemental Specifications, the Special Provisions and Special Notes, and the Standard and Sepia Drawings, current editions, or as directed by the Engineer. Section references are to the Standard Specifications. This work shall consist of:

(1) Developing and preparing a Best Management Practices Plan (BMP) tailored to suit the specific construction phasing for each site within the project; (2) Preparing the project site for construction, including locating, furnishing, installing, and maintaining temporary and/or permanent erosion and water pollution control measures as required by the BMP prior to beginning any earth disturbing activity on the project site; (3) Clearing and grubbing and removal of all obstructions as required for construction; (4) Removing all erosion control devices when no longer needed; (5) Restoring all disturbed areas as nearly as possible to their original condition; (6) Preparing seedbeds and permanently seeding all disturbed areas; (7) Providing a Kentucky Erosion Prevention and Sediment Control Program (KEPSC) qualified inspector; and (8) Performing any other work to prevent erosion and/or water pollution as specified by this contract, required by the BMP, or as directed by the Engineer.

II. MATERIALS

Furnish materials in accordance with these notes, the Standard Specifications and Interim Supplemental Specifications, applicable Special Provisions and Special Notes, and the Standard and Sepia Drawings, current editions. Provide for all materials to be sampled and tested in accordance with the Department's Sampling Manual. Unless directed otherwise by the Engineer, make the materials available for sampling a sufficient time in advance of the use of the materials to allow for the necessary time for testing.

III. CONSTRUCTION

Be advised, these Erosion Control Notes do not constitute a BMP plan for the project. Jointly with the Engineer, prepare a site specific BMP plan for each drainage area within the project in accordance with Section 213. Provide a unique BMP at each project site using good engineering practices taking into account existing site conditions, the type of work to be performed, the construction phasing, methods, and the techniques to be utilized to complete the work. Be responsible for all erosion prevention, sediment control, and water pollution prevention measures required by the BMP for each site. Represent and warrant compliance with the Clean Water Act (33 USC Section 1251 et seq.), the 404 Permit, the 401 Water Quality Certification, and applicable state and local government agency laws, regulations, rules, specifications, and permits. Contrary to Section 105.05, in case of discrepancy between these notes, the Standard Specifications, Interim Supplemental Specifications, Special Provisions and Special Notes, Standard and Sepia Drawings, and such state and local government agency requirements, adhere to the most restrictive requirement.

Special Note for Erosion Control
Page 2 of 3

Conduct operations in such a manner as to minimize the amount of disturbed ground during each phase of the construction and limit the haul roads to the minimum required to perform the work. Preserve existing vegetation not required to be removed by the work or the contract. Seed and/or mulch disturbed areas at the earliest opportunity. Use silt fence, silt traps, temporary ditches, brush barriers, erosion control blankets, sodding, channel lining, and other erosion control measures in a timely manner as required by the BMP and as directed or approved by the Engineer. Prevent sediment laden water from leaving the project, entering an existing drainage structure, or entering a stream.

Provide for erosion control measures to be in place and functioning prior to any earth disturbance within a drainage area. Compute the volume and size of silt control devices necessary to control sediment during each phase of construction. All silt control devices shall be sized to retain a volume of 3,600 cubic feet per disturbed contributing acre. Remove sediment from silt traps before they become a maximum of $\frac{1}{2}$ full. Maintain silt fence by removing accumulated trappings and/or replacing the geotextile fabric when it becomes clogged, damaged, or deteriorated, or when directed by the Engineer. Properly dispose of all materials trapped by erosion control devices at approved sites off the right of way obtained by the Contractor at no additional cost to the Department. See the Special Provision for Waste and Borrow Sites.

As work progresses, add or remove erosion control measures as required by the BMP, applicable to the Contractor's project phasing, construction methods, and techniques. Update the volume calculations and modify the BMP as necessary throughout the duration of the project. Ensure that an updated BMP is kept on site and available for public inspection throughout the life of the project.

The required volume at each Silt Trap shall be computed based on the Up Gradient Contributing Areas that are disturbed and/or stabilized to the satisfaction of the Engineer. The required volume calculation for each Silt Trap shall be determined by the Contractor and verified by the Engineer. The required volume at each Silt Trap may be reduced by the following amounts:

- Up Gradient Areas not disturbed (acres)
- Up Gradient Areas that have been reclaimed and protected by Erosion Control Blanket or other ground protection material such as Temporary Mulch (acres)
- Up Gradient Areas that have been protected by Silt Fence (acres) – Areas protected by Silt Fence shall be computed at a maximum rate of 100 square feet per linear foot of Silt Fence
- Up Gradient Areas that have been protected by Silt Traps (acres)

The use of Temporary Mulch is encouraged.

Silt Trap Type B shall always be placed at the collection point prior to discharging into a Blue Line Stream or onto an adjacent Property Owner. Where overland flow exists, a Silt Fence or other filter devices may be used.

After all construction is complete, restore all disturbed areas in accordance with Section 212. Completely remove all temporary erosion control devices not required as part of the permanent erosion control from the construction site. Prior to removal, obtain the Engineer's concurrence of items to be removed. Grade the remaining exposed earth (both on and off the Right-of-Way) as nearly

Special Note for Erosion Control
Page 3 of 3

as possible to its original condition, or as directed by the Engineer. Prepare the seed bed areas and sow all exposed earthen areas with the applicable seed mixture(s) according to Section 212.03.03.

IV. MEASUREMENT

Erosion control countermeasures and seeding will be measured in accordance to each countermeasure used.

V. BASIS OF PAYMENT

Erosion control countermeasures and seeding will be paid in accordance to each countermeasure used.

TRAFFIC CONTROL PLAN

TRAFFIC CONTROL GENERAL

Except as provided herein, traffic shall be maintained in accordance with the current editions of the Manual on Uniform Traffic Control Devices (MUTCD), Standard Specifications, and the Standard and Sepia Drawings. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic".

Contrary to Section 106.01, traffic control devices used on this project may be new, or used in like new condition, at the beginning of the work and maintained in like new condition until completion of the work. Any temporary traffic control items, devices, materials, and incidentals shall remain the property of the contractor unless otherwise addressed, when no longer needed.

PROJECT PHASING & CONSTRUCTION PROCEDURES

Project maintenance of traffic and construction phasing shall be set-up in accordance with standard drawing TTC-115-04 and shall adhere to guidance set forth in the current addition the Manual for Uniform Traffic Control Devices (MUTCD). In general, construction operations shall proceed as follows:

- Phase 1: Once traffic control has been established (Sta. 36+20 to Sta. 41+65) allow Operation Pride one working day to remove plants and sponsor signs from the center median;
- Phase 2: Construction of the Right Turn Lane (Sta. 24+00 to Sta. 28+00);
- Phase 3: Construction of the Left Turn Lane (Sta. 35+20 to Sta. 41+56.93);
- Phase 4: Construction of the Proposed Street Lighting.

Notes:

- Construction Phases 2, 3 & 4 may be constructed concurrently or consecutively. The contractor shall provide the engineer with a construction phasing plan for approval prior to beginning construction.
- During Phase 3 construction the contractor shall set up traffic control devices in a way to maximize the vehicular storage of the existing dual left turn lanes.

For all construction activities, maintain one 12 ft. lane of traffic on KY 234 in each direction at all times while maintaining access to the entrances and approach roads throughout construction. This may require part-width construction of certain elements. If approved by the Engineer, steel plates may be used to span trenches while flowable fill cures.

If traffic should be stopped due to construction operations, and a school bus or emergency vehicle on an official run arrives on the scene, make provisions for the passage of the school bus or emergency vehicle as quickly as possible.

LANE CLOSURES

Contrary to the Standard Specifications, long-term lane closures will not be measured for payment, but will be incidental to the Bid Item for Maintain and Control Traffic.

Traffic Control Plan

Page 2 of 9

Lane closures will be prohibited on the following dates and times:

Monday – Friday from 7:00AM to 7:00PM

Memorial Day Weekend: 7:00AM Saturday, May 24, 2025 – 7:00PM Monday, May 26, 2025

Thunderfest 2025: Noon Saturday, June 28, 2025 – 10:00PM Saturday, June 28, 2025

Independence Day Weekend: 7:00AM Friday, July 4, 2025 – 7:00 PM Sunday, July 6, 2025

Labor Day Weekend: 7:00AM Saturday, August 29, 2025 – 7:00PM Monday, September 1, 2025

At the discretion of the Engineer, additional days and hours may be specified when lane closures will not be allowed.

A maximum duration of 20 calendar days will be imposed for each lane closure on KY 234 related to the construction of any phase of the specified work, with a requirement to reopen the lanes to traffic within this time frame. Any lane closure, regardless of direction, will count toward the 20-day limit. Lane closures will not be restricted on Saturdays or Sundays, except during the designated holiday weekends listed above; however, closures occurring on these days will still count against the 20-calendar-day limit.

Liquidated Damages will be assessed at \$5,000 per day for each day (or prorated portion thereof), that the KY 234 lanes remain in place.

ROAD CLOSURES

When changing from one temporary traffic pattern to another, traffic may be halted for up to 15 minutes. Prior approval by the Engineer will be required for all road closures. It is the intent that all road closures be kept to a minimum time. The contractor is to schedule operations involving road closures so that all work proceeds in an expeditious manner. Liquidated Damages will be assessed at \$1,000 per hour (or prorated portion thereof), for each occurrence, of road closures exceeding 15 minutes.

SIGNS

Sign posts and splices shall be compliant with NCHRP 350 or MASH. Manufacturer's documentation validating this compliance shall be provided to the Engineer prior to installation. Signs, including any splices, shall be installed according to manufacturer's specifications and installation recommendations. Contrary to section 112.04.02, only long-term signs (signs intended to be continuously in place for more than 3 days) will be measured for payment. Short-term signs (signs intended to be left in place for 3 days or less) will not be measured for payment but will be incidental to Maintain and Control Traffic.

CHANGEABLE MESSAGE SIGNS

Provide changeable message signs at locations determined by the Engineer. The Engineer may vary the designated locations as the work progresses. The Engineer will determine the messages to be displayed. In the event of damage or mechanical/electrical failure, repair or replace the Changeable Message Sign within 24 hours. The Department will measure for payment the maximum number of Changeable Message Signs in concurrent use at the same time on a single day on all sections of the contract. The

Traffic Control Plan
Page 3 of 9

Department will measure individual Changeable Message Signs only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. The Department will not measure replacements for damaged Changeable Message Signs or for signs the Engineer directs be replaced due to poor condition or readability. Retain possession of the Changeable Message Signs upon completion of the work.

BARRICADES

The Department will not measure barricades used in lieu of barrels and cones for channelization or delineation, but shall be incidental to Maintain and Control Traffic according to Section 112.04.01.

TEMPORARY ENTRANCES

The Engineer will not require the Contractor to provide continuous access to farms, single family, duplex, or triplex residential properties during working hours; however, provide reasonable egress and ingress to each such property when actual operations are not in progress at that location. Limit the time during which a farm or residential entrance is blocked to the minimum length of time required for actual operations, not extended for the Contractor's convenience, and in no case exceeding six (6) hours. Notify all residents twenty-four hours in advance of any driveway or entrance closings and make any accommodations necessary to meet the access needs of disabled residents.

Except as allowed by the Phasing as specified in the plans, maintain direct access to all side streets and roads, schools, churches, commercial properties, and apartments or apartment complexes of four or more units at all times. Access to fire hydrants must also be maintained at all times

The Department will measure asphalt materials required to construct and maintain any temporary entrances which may be necessary to provide temporary access; however, the Department will not measure aggregates, excavation, and/or embankment, but shall be incidental to Maintain and Control Traffic. The Engineer will determine the type of surfacing material, asphalt or aggregate, to be used at each entrance.

PAVEMENT EDGE DROP-OFFS

Do not allow a pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation with an elevation difference greater than 1½". Place Warning signs (MUTCD W8-11 or W8-9A) in advance of and at 1500' intervals throughout the drop-off area. Dual post the signs on both sides of the traveled way. Wedge all transverse transitions between resurfaced and un-resurfaced areas which traffic may cross with asphalt mixture for leveling and wedging. Remove the wedges prior to placement of the final surface course.

Protect pavement edges that traffic is not expected to cross, except accidentally, as follows:

Less than 2" - No protection required.

Traffic Control Plan

Page 4 of 9

2" to 4" - Place plastic drums, vertical panels, or barricades every 50 feet. During daylight working hours only, the Engineer will allow the Contractor to use cones in lieu of plastic drums, panels, and barricades. Wedge the drop-off with DGA or asphalt mixture for leveling and wedging with a 1:1 or flatter slope in daylight hours, or 3:1 or flatter slope during nighttime hours, when work is not active in the drop-off area.

Greater than 4" - Protect drop-offs greater than 4 inches within 10 feet of traffic by placing drums, vertical panels, or barricades every 25 feet. The Engineer will not allow the use of cones in lieu of drums, vertical panels, or barricades for drop-offs greater than 4". Place Type III Barricades directly in front of the drop-off facing oncoming traffic in both directions of travel. Provide warning signs as shown on the Standard Drawings or as directed by the Engineer.

Pedestrians & Bicycles - Protect pedestrian and bicycle traffic as directed by the Engineer.

PAVEMENT MARKINGS

If there is to be a deviation from the existing striping plan and/or the proposed striping plan, the Engineer will furnish the Contractor an updated striping plan prior to placement of the final surface course. Install Temporary Striping according to Section 112 with the following exception:

If the Contractor's operations or phasing requires temporary markings that must subsequently be removed from the final surface course, use an approved removable lane tape; however, the Department will not measure removable lane tape for separate payment, but will measure and pay for removable lane tape as temporary striping.

The Contractor and Engineer should work together to determine any locations throughout the project requiring temporary pavement markings. The Engineer will make the final determination as to the quantities and placement of temporary pavement striping.

USE AND PLACEMENT OF CHANGEABLE MESSAGE SIGNS

The following policy is based upon current Changeable Message Signs (CMS) standards and practice from many sources, including the Federal Highway Administration (FHWA), other State Departments of Transportation, and Traffic Safety Associations. It is understood that each CMS installation or use requires individual consideration due to the specific location or purpose. However, there will be elements that are constant in nearly all applications. Accordingly these recommended guidelines bring a level of uniformity, while still being open to regional experience and engineering judgment.

Application

The primary purpose of CMS is to advise the driver of unexpected traffic and routing situations. Examples of applications where CMS can be effective include:

- Closures (road, lane, bridge, ramp, shoulder, interstate)
- Changes in alignment or surface conditions

Traffic Control Plan

Page 5 of 9

- Significant delays, congestion
- Construction/maintenance activities (delays, future activities)
- Detours/alternative routes
- Special events with traffic and safety implications
- Crash/incidents
- Vehicle restrictions (width, height, weight, flammable)
- Advance notice of new traffic control devices
- Real-time traffic conditions (must be kept up to date)
- Weather /driving conditions, environmental conditions, Roadway Weather Information Systems
- Emergency Situations
- Referral to Highway Advisory Radio (if available)
- Messages as approved by the County Engineer's Office

CMS should not be used for:

- Replacement of static signs (e.g. road work ahead), regulatory signage (e.g. speed limits), pavement markings, standard traffic control devices, conventional warning or guide signs.
- Replacement of lighted arrow board
- Advertising (Don't advertise the event unless clarifying "action" to be taken by driver – e.g. Speedway traffic next exit)
- Generic messages
- Test messages (portable signs only)
- Describe recurrent congestion (e.g. rush hour)
- Public service announcements (not traffic related)

Messages

Basic principles that are important to providing proper messages and insuring the proper operation of a CMS are:

- Visible for at least ½ mile under ideal daytime and nighttime conditions
- Legible from all lanes a minimum of 650 feet
- Entire message readable twice while traveling at the posted speed
- No more than two message panels should be used (three panels may be used on roadways where vehicles are traveling less than 45 mph). A panel is the message that fits on the face of the sign without flipping or scrolling.
- Each panel should convey a single thought; short and concise
- Do not use two unrelated panels on a sign
- Do not use the sign for two unrelated messages
- Should not scroll text horizontally or vertically
- Should not contain both the words left and right
- Use standardized abbreviations and messages
- Should be accurate and timely
- Avoid filler/unnecessary words and periods (hazardous, a, an, the)
- Avoid use of speed limits
- Use words (not numbers) for dates

Traffic Control Plan

Page 6 of 9

Placement

Placement of the CMS is important to insure that the sign is visible to the driver and provides ample time to take any necessary action. Some of the following principles may only be applicable to controlled access roadways. The basic principles of placement for a CMS are:

- When 2 signs are needed, place on same side of roadway and at least 1,000 feet apart
- Place behind semi-rigid/rigid protection (guardrail, barrier) or outside of the clear zone
- Place 1,000 feet in advance of work zone; at least one mile ahead of decision point
- Normally place on right side of roadway; but should be placed closest to the affected lane so that either side is acceptable
- Signs should not be dual mounted (one on each side of roadway facing same direction)
- Point trailer hitch downstream
- Secure to immovable object to prevent theft (if necessary)
- Do not place in sags or just beyond crest
- Check for reflection of sun to prevent the blinding of motorist
- Should be turned ~3 degrees outward from perpendicular to the edge of pavement
- Bottom of sign should be 7 feet above the elevation of edge of roadway
- Should be removed when not in use

Traffic Control Plan
Page 7 of 9

Standard Abbreviations

The following is a list of standard abbreviations to be used on CMS:

<u>Word</u>	<u>Abbrev</u>	<u>Example</u>
Access	ACCS	ACCIDENT AHEAD/ USE ACCS RD NEXT RIGHT
Alternate	ALT	ACCIDENT AHEAD/ USE ALT RTE NEXT RIGHT
Avenue	AVE	FIFTH AVE CLOSED/ DETOUR NEXT LEFT
Blocked	BLKD	FIFTH AVE BLKD/ MERGE LEFT
Boulevard	BLVD	MAIN BLVD CLOSED/ USE ALT RTE
Bridge	BRDG	SMITH BRDG CLOSED/ USE ALT RTE
Cardinal Directions	N, S, E, W	N I75 CLOSED/ DETOUR EXIT 30
Center	CNTR	CNTR LANE CLOSED/ MERGE LEFT
Commercial	COMM	OVRSZ COMM VEH/ USE I275
Condition	COND	ICY COND POSSIBLE
Congested	CONG	HVY CONG NEXT 3 MI
Construction	CONST	CONST WORK AHEAD/ EXPECT DELAYS
Downtown	DWNTN	DWNTN TRAF USE EX 40
Eastbound	E-BND	E-BND I64 CLOSED/ DETOUR EXIT 20
Emergency	EMER	EMER VEH AHEAD/ PREPARE TO STOP
Entrance, Enter	EX, EXT	DWNTN TRAF USE EX 40
Expressway	EXPWY	WTRSN EXPWY CLOSED/ DETOUR EXIT 10
Freeway	FRWY, FWY	GN SYNDR FWY CLOSED/ DETOUR EXIT 15
Hazardous Materials	HAZMAT	HAZMAT IN ROADWAY/ ALL TRAF EXIT 25
Highway	HWY	ACCIDENT ON AA HWY/ EXPECT DELAYS
Hour	HR	ACCIDENT ON AA HWY/ 2 HR DELAY
Information	INFO	TRAF INFO TUNE TO 1240 AM
Interstate	I	E-BND I64 CLOSED/ DETOUR EXIT 20
Lane	LN	LN CLOSED MERGE LEFT
Left	LFT	LANE CLOSED MERGE LFT
Local	LOC	LOC TRAF USE ALT RTE
Maintenance	MAINT	MAINT WRK ON BRDG/ SLOW
Major	MAJ	MAJ DELAYS I75/ USE ALT RTE
Mile	MI	ACCIDENT 3 MI AHEAD/ USE ALT RTE
Minor	MNR	ACCIDENT 3 MI MNR DELAY
Minutes	MIN	ACCIDENT 3 MI/ 30 MIN DELAY
Northbound	N-BND	N-BND I75 CLOSED/ DETOUR EXIT 50
Oversized	OVRSZ	OVRSZ COMM VEH/ USE I275 NEXT RIGHT
Parking	PKING	EVENT PKING NEXT RGT
Parkway	PKWY	CUM PKWAY TRAF/ DETOUR EXIT 60
Prepare	PREP	ACCIDENT 3 MI/ PREP TO STOP
Right	RGT	EVENT PKING NEXT RGT
Road	RD	HAZMAT IN RD/ ALL TRAF EXIT 25
Roadwork	RDWK	RDWK NEXT 4 MI/ POSSIBLE DELAYS
Route	RTE	MAJ DELAYS I75/ USE ALT RTE
Shoulder	SHLDR	SHLDR CLOSED NEXT 5 MI
Slippery	SLIP	SLIP COND POSSIBLE/ SLOW SPD
Southbound	S-BND	S-BND I75 CLOSED/ DETOUR EXIT 50
Speed	SPD	SLIP COND POSSIBLE/ SLOW SPD

Traffic Control Plan
Page 8 of 9

Standard Abbreviations (cont)

<u>Word</u>	<u>Abbrev</u>	<u>Example</u>
Street	ST	MAIN ST CLOSED/ USE ALT RTE
Traffic	TRAF	CUM PKWAY TRAF/ DETOUR EXIT 60
Vehicle	VEH	OVRSZ COMM VEH/ USE I275 NEXT RIGHT
Westbound	W-BND	W-BND I64 CLOSED/ DETOUR EXIT 50
Work	WRK	CONST WRK 2MI/ POSSIBLE DELAYS

Certain abbreviations are prone to inviting confusion because another word is abbreviated or could be abbreviated in the same way. DO NO USE THESE ABBREVIATIONS:

<u>Abbrev</u>	<u>Intended Word</u>	<u>Word Erroneously Given</u>
ACC	Accident	Access (Road)
CLRS	Clears	Colors
DLY	Delay	Daily
FDR	Feeder	Federal
L	Left	Lane (merge)
LOC	Local	Location
LT	Light (traffic)	Left
PARK	Parking	Park
POLL	Pollution (index)	Poll
RED	Reduce	Red
STAD	Stadium	Standard
TEMP	Temporary	Temperature
WRNG	Warning	Wrong

Typical Messages

The following is a list of typical messages used on CMS. The list consists of the reason or problem that you want the driver to be aware of and the action that you want the driver to take.

<u>Reason/Problem</u>	<u>Action</u>
ACCIDENT	ALL TRAFFIC EXIT RT
ACCIDENT/XX MILES	AVOID DELAY USE XX
XX ROAD CLOSED	CONSIDER ALT ROUTE
XX EXIT CLOSED	DETOUR
BRIDGE CLOSED	DETOUR XX MILES
BRIDGE/(SLIPPERY, ICE, ETC.)	DO NOT PASS
CENTER/LANE/CLOSED	EXPECT DELAYS
DELAY(S), MAJOR/DELAYS	FOLLOW ALT ROUTE
DEBRIS AHEAD	KEEP LEFT
DENSE FOG	KEEP RIGHT
DISABLED/VEHICLE	MERGE XX MILES
EMER/VEHICLES/ONLY	MERGE LEFT
EVENT PARKING	MERGE RIGHT
EXIT XX CLOSED	ONE-WAY TRAFFIC
FLAGGER XX MILES	PASS TO LEFT
FOG XX MILES	PASS TO RIGHT

Traffic Control Plan
Page 9 of 9

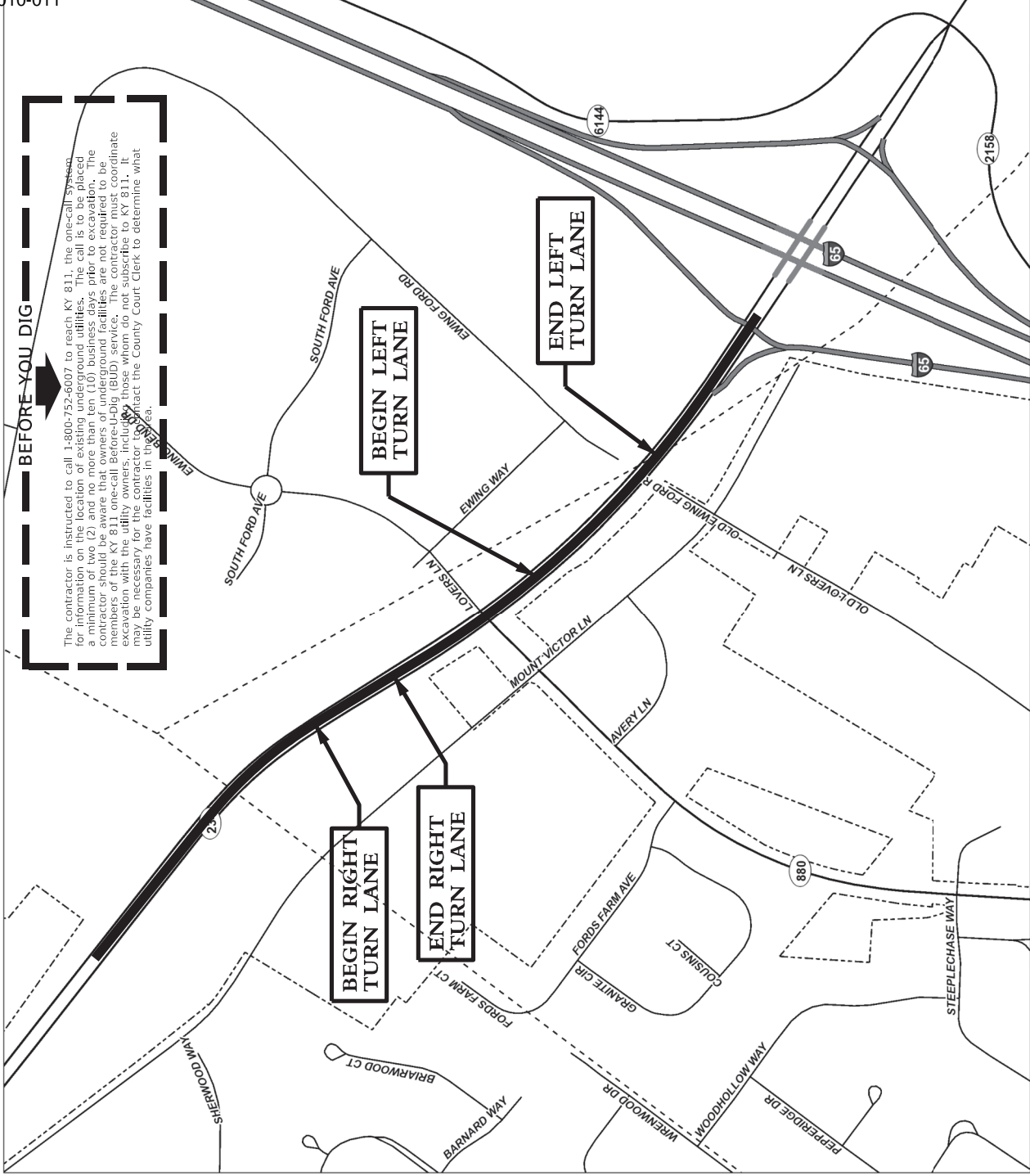
Typical Messages (cont)

Reason/Problem	Action
FREEWAY CLOSED	PREPARE TO STOP
FRESH OIL	REDUCE SPEED
HAZMAT SPILL	SLOW
ICE	SLOW DOWN
INCIDENT AHEAD	STAY IN LANE
LANES (NARROW, SHIFT, MERGE, ETC.)	STOP AHEAD
LEFT LANE CLOSED	STOP XX MILES
LEFT LANE NARROWS	TUNE RADIO 1610 AM
LEFT 2 LANES CLOSED	USE NN ROAD
LEFT SHOULDER CLOSED	USE CENTER LANE
LOOSE GRAVEL	USE DETOUR ROUTE
MEDIAN WORK XX MILES	USE LEFT TURN LANE
MOVING WORK ZONE, WORKERS IN ROADWAY	USE NEXT EXIT
NEXT EXIT CLOSED	USE RIGHT LANE
NO OVERSIZED LOADS	WATCH FOR FLAGGER
NO PASSING	
NO SHOULDER	
ONE LANE BRIDGE	
PEOPLE CROSSING	
RAMP CLOSED	
RAMP (SLIPPERY, ICE, ETC.)	
RIGHT LANE CLOSED	
RIGHT LANE NARROWS	
RIGHT SHOULDER CLOSED	
ROAD CLOSED	
ROAD CLOSED XX MILES	
ROAD (SLIPPERY, ICE, ETC.)	
ROAD WORK	
ROAD WORK (OR CONSTRUCTION) (TONIGHT, TODAY, TOMORROW, DATE)	
ROAD WORK XX MILES	
SHOULDER (SLIPPERY, ICE, SOFT, BLOCKED, ETC.)	
NEW SIGNAL XX MILES	
SLOW 1 (OR 2) - WAY TRAFFIC	
SOFT SHOULDER	
STALLED VEHICLES AHEAD	
TRAFFIC BACKUP	
TRAFFIC SLOWS	
TRUCK CROSSING	
TRUCKS ENTERING	
TOW TRUCK AHEAD	
UNEVEN LANES	
WATER ON ROAD	
WET PAINT	
WORK ZONE XX MILES	
WORKERS AHEAD	

2020 STANDARD DRAWINGS THAT APPLY	
ROADWAY	
~ DRAINAGE ~	
<u>PERFORATED PIPE</u>	
PERFORATED PIPE TYPES AND COVER HEIGHTS	RDP-001-06
PERFORATED PIPE FOR SUBGRADE DRAINAGE ON TWO-LANE (CLASS 2) AND MULTI-LANE ROADS.....	RDP-005-05
PERFORATED PIPE HEADWALLS.....	RD9-010-09
<u>TYPICAL DRAINAGE INSTALLATIONS</u>	
EROSION CONTROL BLANKET SLOPE INSTALLATION.....	RDI-040-01
EROSION CONTROL BLANKET CHANNEL INSTALLATION.....	RDI-041-01
<u>MANHOLES</u>	
FRAME AND LID TYPE 2	RDM-105-03
<u>MISCELLANEOUS DRAINAGE</u>	
TEMPORARY SILT FENCE.....	RDX-210-03
SILT TRAP - TYPE A	RDX-220-05
SILT TRAP - TYPE B.....	RDX-225-01
SILT TRAP - TYPE C.....	RDX-230-01
~ GENERAL ~	
<u>MISCELLANEOUS STANDARDS</u>	
MISCELLANEOUS STANDARDS	RGX-001-06
~ PAVEMENT ~	
<u>MEDIANS, CURBS, APPROACHES, ENTRANCES, ETC.</u>	
CURB AND GUTTER CURBS AND VALLEY GUTTER.....	RPM-010-06
TRAFFIC	
~ PERMANENT ~	
<u>RAISED PAVEMENT MARKERS</u>	
TYPICAL MARKINGS FOR ISLANDS AND MEDIAN.....	TPM-205
TYPICAL MARKINGS FOR TURN LANES	TPM-206
~ TEMPORARY ~	
<u>TRAFFIC CONTROL</u>	
LANE CLOSURE MULTI-LANE HIGHWAY CASE I.....	TTC-115-03
LANE CLOSURE MULTI-LANE HIGHWAY CASE II.....	TTC-120-03
SHOULDER CLOSURE.....	TTC-135-02
<u>DEVICES</u>	
DOUBLE FINES ZONE SIGNS	TTD-120-02
PAVEMENT CONDITION WARNING SIGNS.....	TTD-125-02

Standard Drawings That Apply
Page 2 of 2

<u>STRIPING OPERATIONS</u>	
MOBILE OPERATION FOR PAINT STRIPING CASE III	TTS-110-02
MOBILE OPERATION FOR DURABLE STRIPING CASE I.....	TTS-120-02



The contractor is instructed to call 1-800-752-8007 to reach KY 811, the one-call system for information on the location of existing underground utilities. The call is to be placed a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor should be aware that owners of underground facilities are not required to be members of the KY 811 one-call Before-You-Dig (BUD) service. The contractor must coordinate excavation with the utility owners, including those who do not subscribe to KY 811. It will be necessary for the contractor to contact the County Court Clerk to determine what utility companies have facilities in the area.

COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

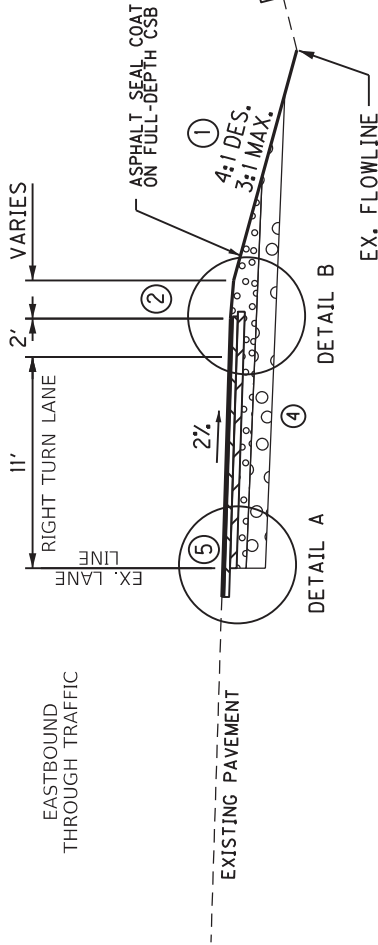
KENTUCKY
TRANSPORTATION
CABINET

DRAWING TITLE: VICINITY MAP SHEET

ITEM NO.	COUNTY OF
03-80318.00	WARREN
SHEET NO.	R1

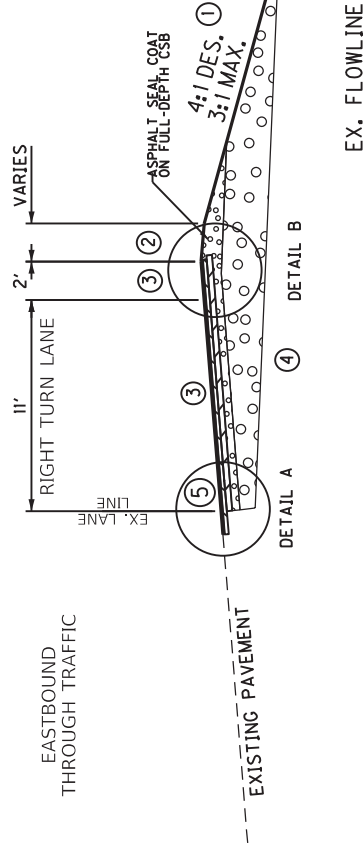
RIGHT TURN LANE TYPICAL SECTIONS

KY 234 NORMAL SECTION



STA. 24+00 TO STA. 26+45

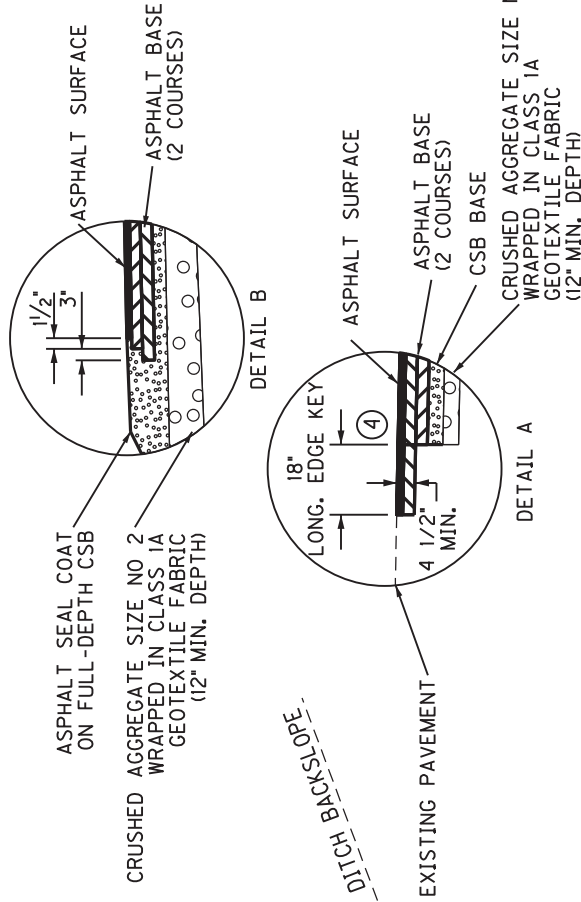
KY 234 SUPERELEVATED SECTION



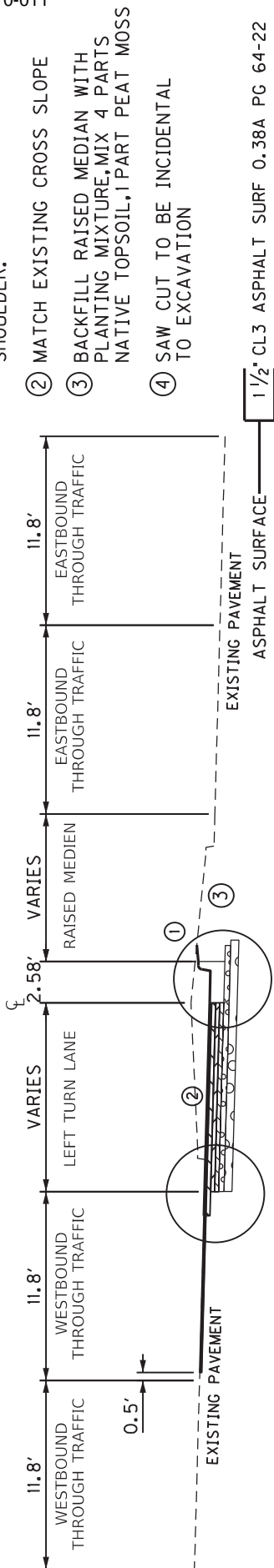
STA. 26+45 TO STA. 28+00

NOTES:

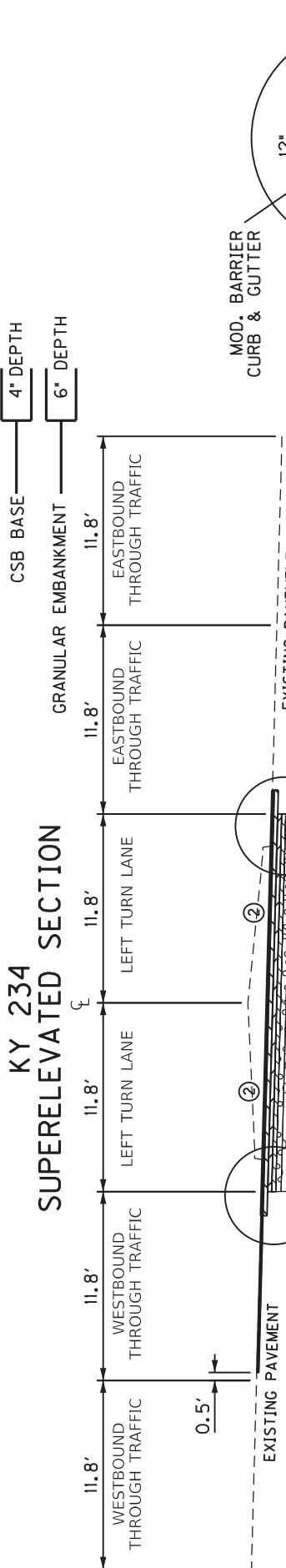
- SEE CROSS SECTIONS FOR SLOPES OUTSIDE THE LIMITS OF THE SHOULDER.
 - CSB AND EARTH SHOULDERS SHALL BE CONSTRUCTED WITH A 8% CROSS SLOPE.
 - MATCH EXISTING CROSS SLOPE
 - SUBGRADE TO BE CONSTRUCTED ON A 4% CROSS SLOPE AWAY FROM THE EXISTING PAVEMENT
 - SAW CUT TO BE INCIDENTAL TO EXCAVATION
- KY 234 PAVEMENT STA. 24+00 TO 28+00
- | | |
|------------------------------------|---|
| ASPHALT SURFACE | 1 1/2" CL3 ASPHALT SURF 0.38A PG 64-22 |
| ASPHALT BASE | 3" CL3 ASPHALT BASE 1.00D PG 64-22 |
| CSB BASE | 3 1/2" CL3 ASPHALT BASE 1.00D PG 64-22 |
| CSB DEPTH | 6" DEPTH |
| ASPHALT SEAL COAT (2 APPLICATIONS) | 2.4 lb/S.Y. ASPHALT SEALCOAT
20 lb/S.Y. SIZE 1/4" ASPHALT SEAL AGGREGATE |
| GRANULAR EMBANKMENT | 12" MIN. DEPTH (VARIABLE) |



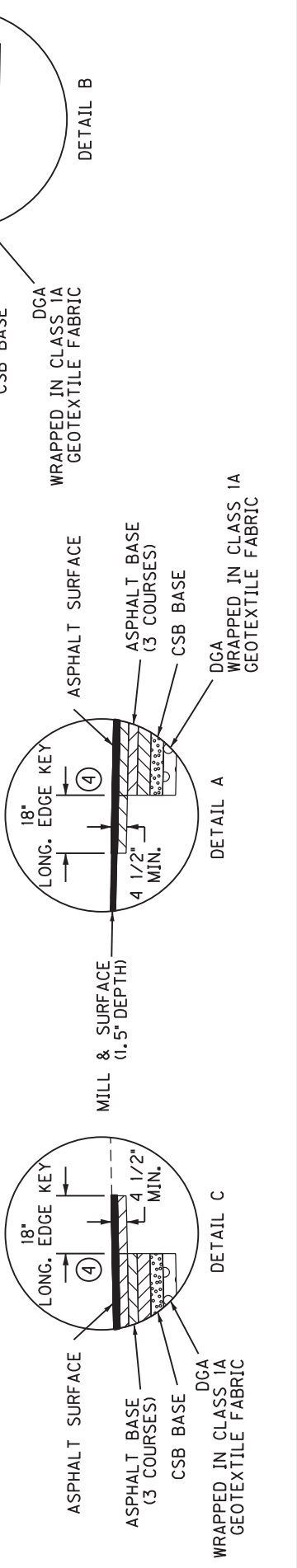
KY 234
SUPERELEVATED



DETAIL A
STA. 39+16.86 TO STA. 41+56.93



DETAIL A
DETAIL C
STA. 36+23 TO STA. 39+16.86



DRAWING TITLE: TYPICAL SECTIONS



COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS



DRAWING TITLE: TYPICAL SECTIONS

ITEM NO.
03-80318.00

SHEET NO.
R2B

COUNTY OF
WARREN



0' 5' 10' 20'

HORIZONTAL SCALE: 1"=10'

OpenRoads Designer v23.00.01.11

USER: Stewart.Llich

GENERAL SUMMARY

ITEM	DESCRIPTION	UNIT	RIGHT TURN	LEFT TURN	TOTAL	PROJECT	NOTES
20478ND	FRAME AND LID TY 2	EACH		1	1	1	SEE STRUCTURAL DETAIL R16
1791	ADJUST MANHOLE FRAME TO GRADE	EACH		1	1	1	
8100	CONCRETE-CLASS A	CY		0.3	0.3	0.3	SEE STRUCTURAL DETAIL R16
8150	STEEL REINFORCEMENT	LBS		36	36	36	SEE STRUCTURAL DETAIL R16
24108EC	BARRIER CURB AND GUTTER-MOD	LF		249	249	249	
2159	TEMP DITCH	LF	200		200	200	
2160	CLEAN TEMP DITCH	LF	100		100	100	
2200	ROADWAY EXCAVATION	CUYD	783	578	1361		
2604	GEOTEXTILE-FABRIC CLASS 1A	SQYD	2516	1868	4684		300 SY INCLUDED FOR SUBGRADE PREPARATION AS DIRECTED BY ENGINEER FOR DUST CONTROL WHILE MAINTAINING TRAFFIC
2242	WATER	MGAL	1		1	1	APPROX. 0.43 ACRES
2545	CLEARING AND GRUBBING	LS					
2562	TEMPORARY SIGNS	SOFT	300	300	600		
2569	DEMOBILIZATION	LS					
2650	MAINTAIN & CONTROL TRAFFIC	LS					
2671	PORTABLE CHANGEABLE MESSAGE SIGN	LS					
2676	MOBILIZATION FOR MILL & TEXT	EACH	2	2	4		TO BE PLACED AND PROGRAMMED AS DIRECTED BY ENGINEER
2677	ASPHALT PAVE MILLING & TEXTURING	EACH					
2701	TEMP SILT FENCE	TON	34	98	132		
2708	SILT TRAP TYPE A	LF	200		200		
2704	SILT TRAP TYPE B	EACH	1		1		
2705	SILT TRAP TYPE C	EACH	1		1		
2706	CLEAN SILT TRAP TYPE A	EACH	1		1		
2707	CLEAN SILT TRAP TYPE B	EACH	1		1		
2708	CLEAN SILT TRAP TYPE C	EACH	1		1		
2776	STAKING	LS					
2775	ARROW PANEL	EACH	1	1	2		
5950	EROSION CONTROL BLANKET	SOYD	259		259		
5952	TEMP MULCH	SOYD	781		781		
5953	TEMP SEEDING AND PROTECTION	SOYD	583		583		
5963	INITIAL FERTILIZER	TON	0.04		0.04		CALCULATED AT 0.157 TONS PER DISTURBED ACRE, APPROX. 0.28 ACRES
5964	MAINTENANCE FERTILIZER	TON	0.07		0.07		CALCULATED AT 0.25 TONS PER SEEDED ACRE, APPROX. 0.28 ACRES
5992	AGRICULTURAL LIMESTONE	TON	0.84		0.84		CALCULATED AT 3.0 TONS PER DISTURBED ACRE, APPROX. 0.28 ACRES
6510	PAVE STRIPING-TEMP PAINT-4 IN	LF	2800	2186	4986		
6542	PAVE STRIPING-THERMO-6 IN W	LF	1400	1093	2493		
6543	PAVE STRIPING-THERMO-6 IN Y	LF		1454	1454		
6549	PAVE STRIPING-TEMP REM TAPE-B	LF	300	300	600		
6550	PAVE STRIPING-TEMP REM TAPE-W	LF	300	300	600		
6568	PAVE STRIPING-THERMO STOP BAR-24IN	LF	15	26	41		
6569	PAVE MARKING-THERMO-CROSS-HATCH	SOFT	54	54	54		
6574	PAVE MARKING-THERMO-CURV ARROW	EACH	7	14	21		
6610	IN-LAID PAVEMENT MARKER-MW	EACH	16	26	42		
6612	IN-LAID PAVEMENT MARKER-BY	EACH	1	25	25		
21373ND	REMOVE SIGN	EACH	1	1	2		SIGNS TO BE RETURNED TO KYTC DISTRICT 3 TRAFFIC SECTION
21285ED	LONGITUDINAL EDGE KEY	LF	418	857	1275		
20071EC	JOINT ADHESIVE	LF	772	2105	2877		
4740	POLE BASE	EACH		4	4		
4794	CONDUIT-1 1/2 IN	LF		522	522		
4810	ELECTRICAL JUNCTION BOX	EACH		1	1		
22593ND	INSTALL LUMINAIRE POLE	EACH		4	4		SEE LIGHTING NOTES R11

DESCRIPTION	UNIT	NOTES
1.5" ASPHALT SURFACE 0.38A PG 64-22	CUYD	
1.5" ASPHALT SURFACE 1.00D PG 64-22	CUYD	
3" ASPHALT BASE 1.00D PG 64-22	CUYD	
3" ASPHALT BASE 1.00D PG 64-22	CUYD	
3" ASPHALT BASE 1.00D PG 64-22	CUYD	
6" DGA BASE	CUYD	
4" CSB BASE	CUYD	
ASPHALT MATERIAL FOR TACK	SOYD	
4163.37		

DESCRIPTION	UNIT	NOTES
1.5" ASPHALT SURFACE 0.38A PG 64-22	CUYD	
1.5" ASPHALT SURFACE 1.00D PG 64-22	CUYD	
3" ASPHALT BASE 1.00D PG 64-22	CUYD	
3 1/2" ASPHALT BASE 1.00D PG 64-22	CUYD	
6" CSB BASE	CUYD	
12" CRUSHED AGGREGATE NO 2	CUYD	
ASPHALT SEAL AGGREGATE & ASPHALT SEALCOAT	SOYD	
177.78		
515.53		
173.75		
54.31		
45.68		
22.73		
25.46		
MILLING & SURFACING	CUYD	
FULL DEPTH PAVEMENT	CUYD	
CALCULATED BY AVERAGE END AREA METHOD		
1105.06		

EARTHWORK QUANTITIES
COM (RIGHT TURN LANE) = 783 CY
COM (LEFT TURN LANE) = 578 CY
EMB (RIGHT TURN LANE) = 1 CY
EMB (LEFT TURN LANE) = 40 CY
TOTAL = 1320 CY WASTE

*ESTIMATE FOR EARTHWORK CALCULATIONS FOR DESIGN PURPOSES ONLY. THE CONTRACTOR IS ADVISED THAT EARTHWORK CALCULATION SHOWN ARE FOR INFORMATION ONLY. ASSUMPTIONS FOR SHRINKAGE AND SWELL FACTORS ARE THE CONTRACTORS RESPONSIBILITY.

PAVING SUMMARY

ITEM CODE	ITEM	UNIT	RIGHT TURN	LEFT TURN	PROJECT TOTALS	NOTES
0001	DGA BASE	TON		311.99	312	ESTIMATED AT 2.07 TONS/CUYD
0003	CSB BASE	TON	359.66	203.96	564	ESTIMATED AT 2.07 TONS/CUYD
0078	CRUSHED AGGREGATE SIZE NO 2	TON	695.97		796	ESTIMATED AT 1.35 TONS/CUYD, 100 TONS INCLUDED FOR SUBGRADE PREPARATION AS DIRECTED BY THE ENGINEER
0100	ASPHALT SEAL AGGREGATE	TON	3.56		4	ESTIMATED AT 20 LB PER SOYD AT 2 APPLICATIONS
0103	ASPHALT SEALCOAT	TON	0.43		1	ESTIMATED AT 2.4 LB PER SOYD AT 2 APPLICATIONS
0212	CL2 ASPHALT BASE 1.00D PG 64-22	TON	197.98	394.95	593	ESTIMATED AT 1.98 TONS/CUYD
22506ES403	CL3 ASPHALT SURFACE 0.38A PG 64-22	TON	95.42	211.78	308	ESTIMATED AT 1.98 TONS/CUYD
0356	ASPHALT MATERIAL FOR TACK	TON	0.46	1.75	3	ESTIMATED AT 0.84 LB/SOYD



COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS



DRAWING TITLE: GENERAL SUMMARY SHEET

UTILITY CONTACTS

ELECTRIC:
Bowling Green Municipal Utilities
Electric Division
801 Center Street, P.O. Box 10300
Bowling Green, Kentucky 42102-7300
Telephone: 270-782-1200
Eric Phillips, PDE, Electric Engineering Superintendent
Email: eric.phillips@bgmu.com
Telephone: 270-782-4386
Chris Matlock, Field Engineer
Telephone: 270-782-4342
Email: chris.matlock@bgmu.com

GAS:
Atmos Energy Corporation
Kentucky / Mid-States Division
3275 Highland Point Drive
Owensboro, Kentucky 42303
Regional Office & District Office
Ryne White, Sr. Engineer
Email: Ryne.White@atmosenergy.com
Telephone Number: 270-685-8140
Cellular Telephone: 270-929-1706
Mike Close, Operations Manager
Cellular: 270-469-0073
Doug Bower, Operations Supervisor
Telephone: 270-792-4383

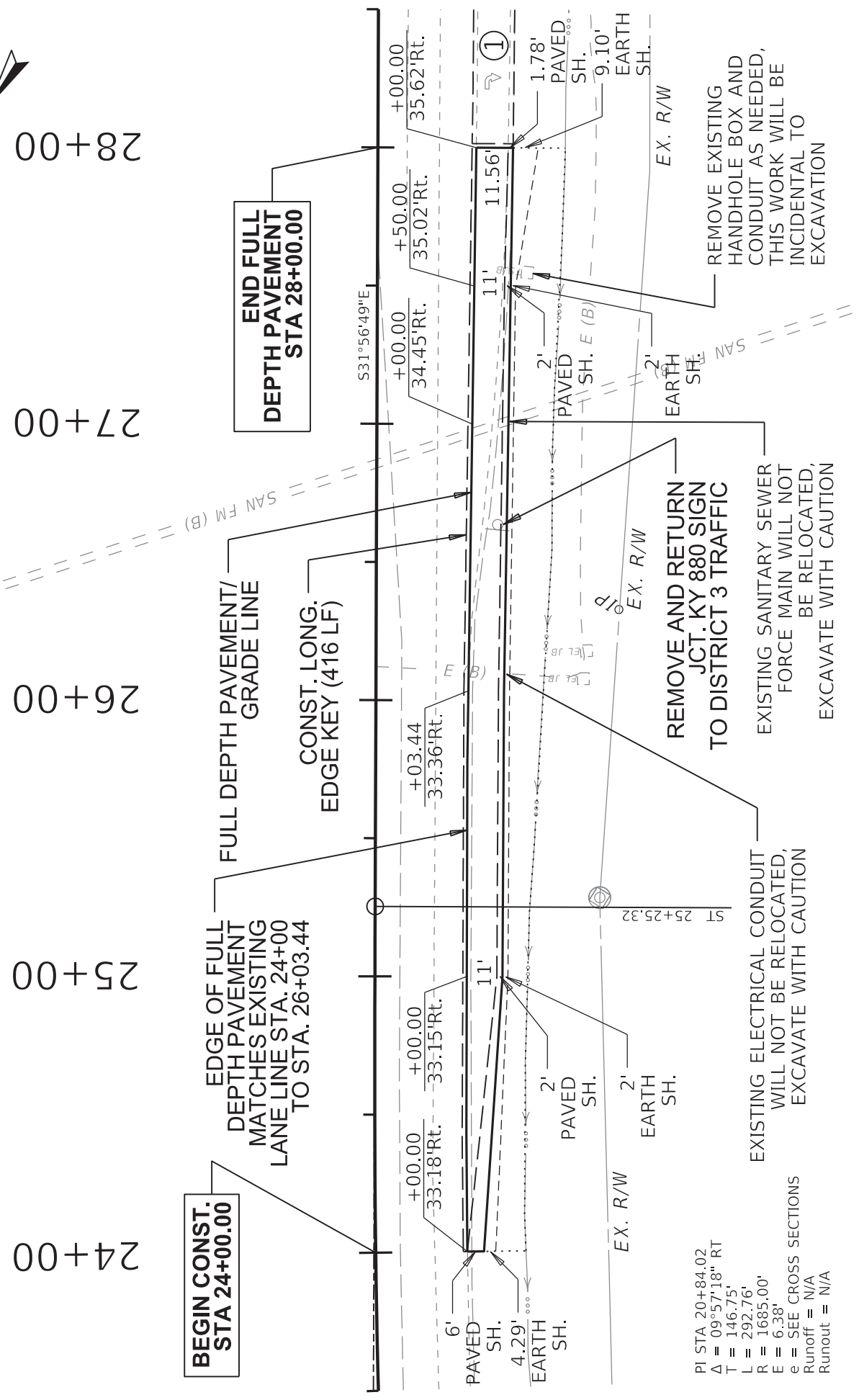
TELECOMMUNICATIONS:
Bowling Green Municipal Utilities
General Services Division
801 Center Street, P.O. Box 10300
Bowling Green, Kentucky 42102-4300
Telephone Number: 270-782-1200
Todd Crosby, General Services Division Manager
Email Address: todd.crosby@bgmu.com
Telephone: 270-782-4535
Cellular: 270-438-2841
Tice Edwards, Network Operations Supervisor
Email Address: tice.edwards@bgmu.com
Telephone: 270-782-4393
Cellular: 270-991-1529
Jason Reynolds, Engineer
Email: jason.reynolds@bgmu.com
Cellular: 270-779-6676

Telephone:
AT&T
Bowling Green Central Office
Travis Parsley, GEO Manager - OSP Plng & Engrg Design
1150 State Street, Bowling Green, Kentucky 42101
Telephone: 270-846-3196
Email: tp2087@att.com

CABLE TELEVISION:
Spectrum Cable
District Office
515 Double Springs Road
Bowling Green, Kentucky 42101
Telephone: 270-782-0903 or 270-782-0910
Cory Hendrick, Supervisor Manager
Telephone: 270-392-0605
Email: Cory.Hendrick@charter.com
Phillip Eadens, Construction Manager
Telephone: 1-270-392-1079
Email: phillip.eadens@charter.com
Michael Vincent, Construction Coordinator
Cellular: 1-270-557-9581
Email: Michael.Vincent@charter.com

WATER and SEWER:
Bowling Green Municipal Utilities
Water and Sewer Division
801 Center Street, P.O. Box 10300
Bowling Green, Kentucky 42102-7300
Telephone Number: 270-782-1200
Mark Iverson, General Manager
Email Address: mark.iverson@bgmu.com
Joe Pavoni, P.E., Systems Manager
Email: joe.pavoni@bgmu.com
Scott Neighbors, Engineering Superintendent
Email: scott.neighbors@bgmu.com
Telephone: 270-782-4371
Cellular: 270-282-1824
Rodney Sullivan, Engineering
Email: rodney.sullivan@bgmu.com
Telephone: 270-782-4382
Cellular: 270-799-1711

① BEGIN MILL AND SURFACE FOR THE RIGHT TURN LANE, STA. 28+01.50 TO APPROX. 5' PAST THE EXISTING STOP BAR AT LOVERS LANE, STA. 31+54 (DEPTH 1.5", WIDTH 18" OUTSIDE OF THE LANE LINE TO THE EDGE OF PAVEMENT, 611 SY)



PI STA 20+84.02
Δ = 09°57'18" RT
T = 146.75'
L = 292.76'
R = 1685.00'
E = 6.38'
e = SEE CROSS SECTIONS
Runoff = N/A
Runout = N/A

COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

DRAWING TITLE: RIGHT TURN LANE PLAN SHEET
STA: 23+50.00 TO STA: 28+50.00

ITEM NO.
03-0166.00

COUNTY OF
WARREN

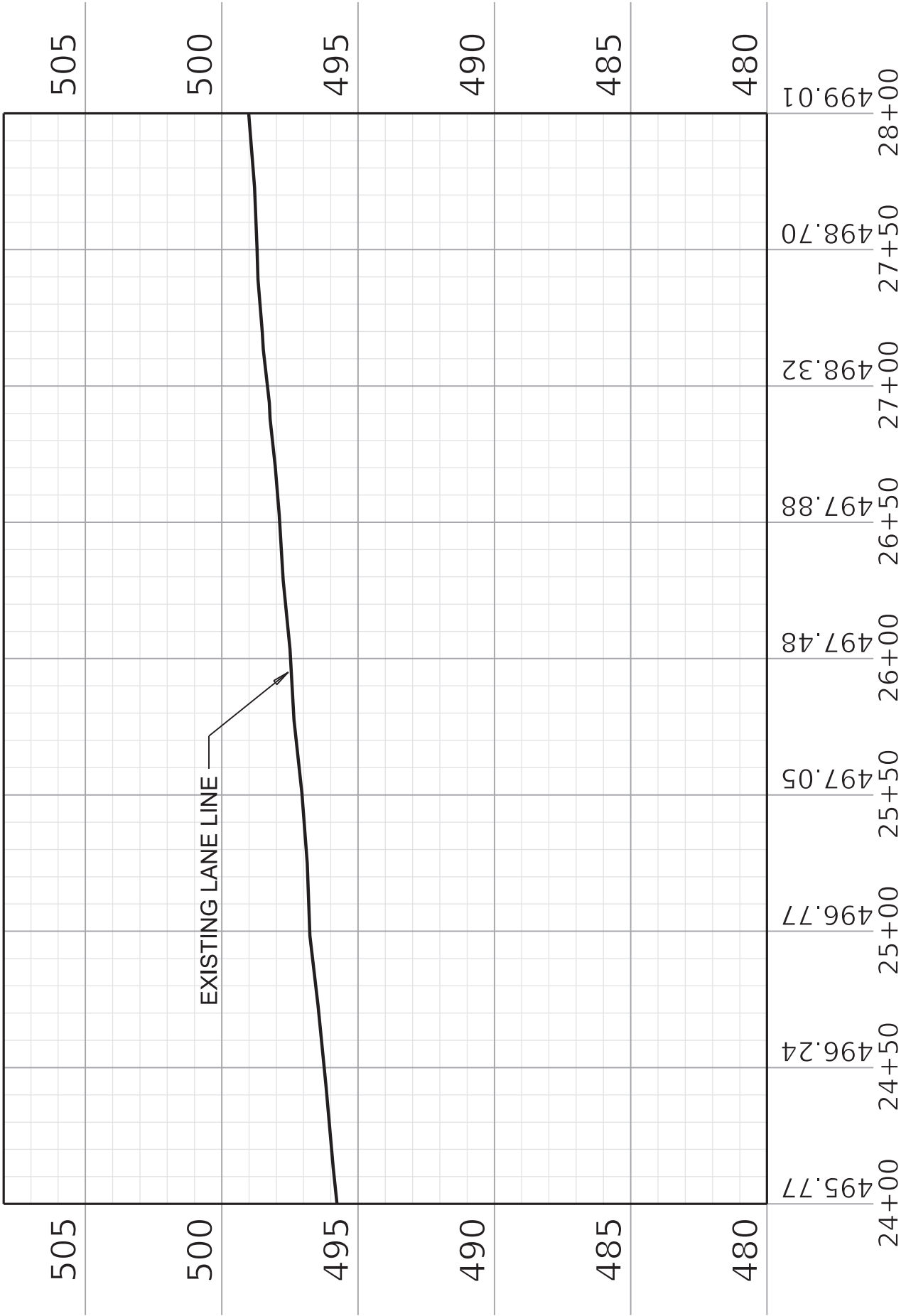
SHEET NO.
R3

0' 25' 50' 100'

HORIZONTAL SCALE: 1"=50'

USER: StewartLich

OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.LICH\ID241933903_80318_00_R_PLAN_PROFILE.DGN





COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS



TEAM KENTUCKY
Infrastructure

DRAWING TITLE: RIGHT TURN LANE PROFILE SHEET
STA: 24+00.00 TO STA: 28+00.00

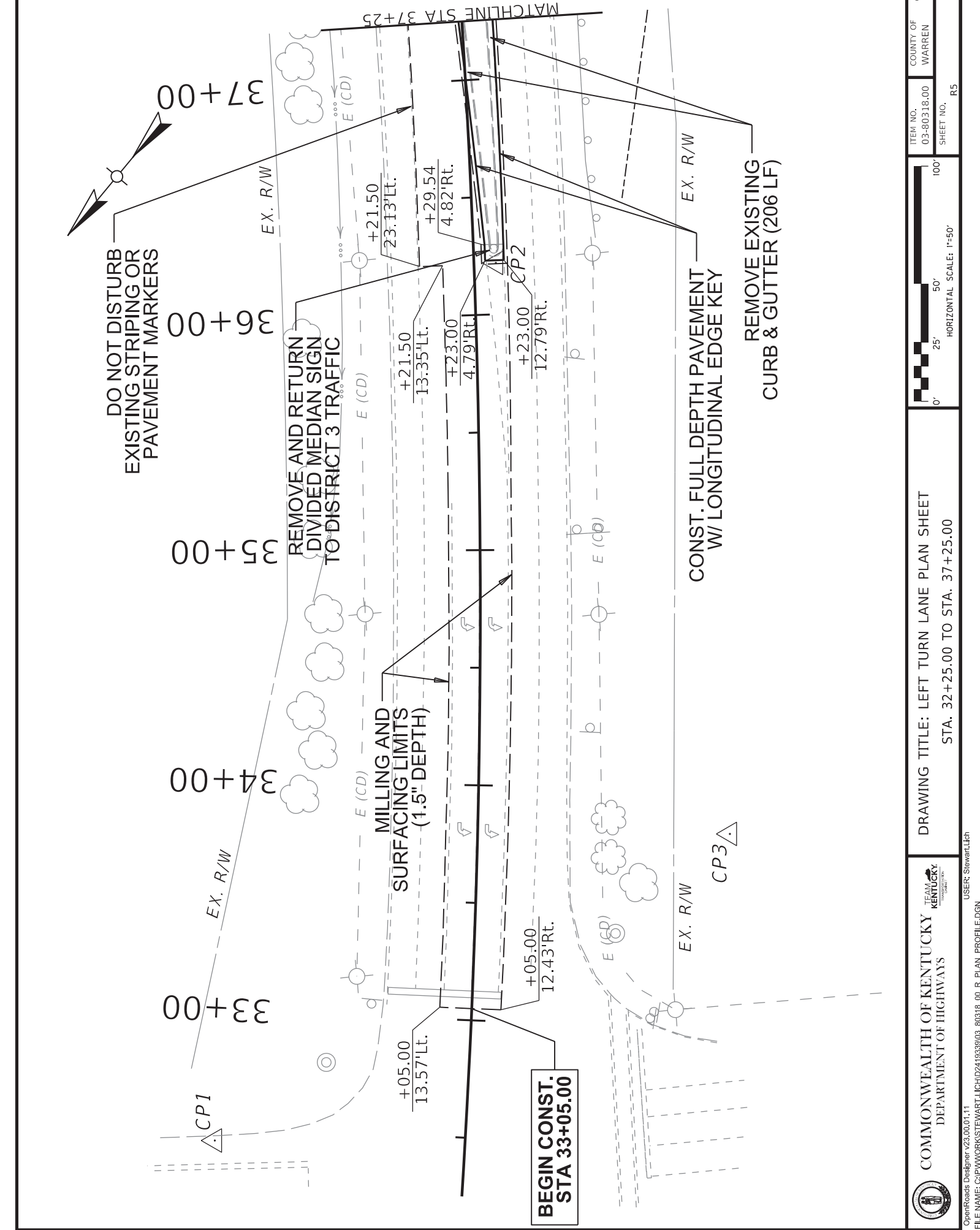
HORIZONTAL SCALE: 1"=50'

VERTICAL SCALE: 1"=5'

ITEM NO.
03-80318.00

COUNTY OF
WARREN

SHEET NO.
R4





COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

TEAM KENTUCKY
CONSULTANTS

OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.LIC\H241933903_80318_00_R_PLAN_PROFILE.DGN

USER: Stewart.Lich

DRAWING TITLE: LEFT TURN LANE PLAN SHEET
STA: 32+25.00 TO STA: 37+25.00

ITEM NO.
03-80318.00

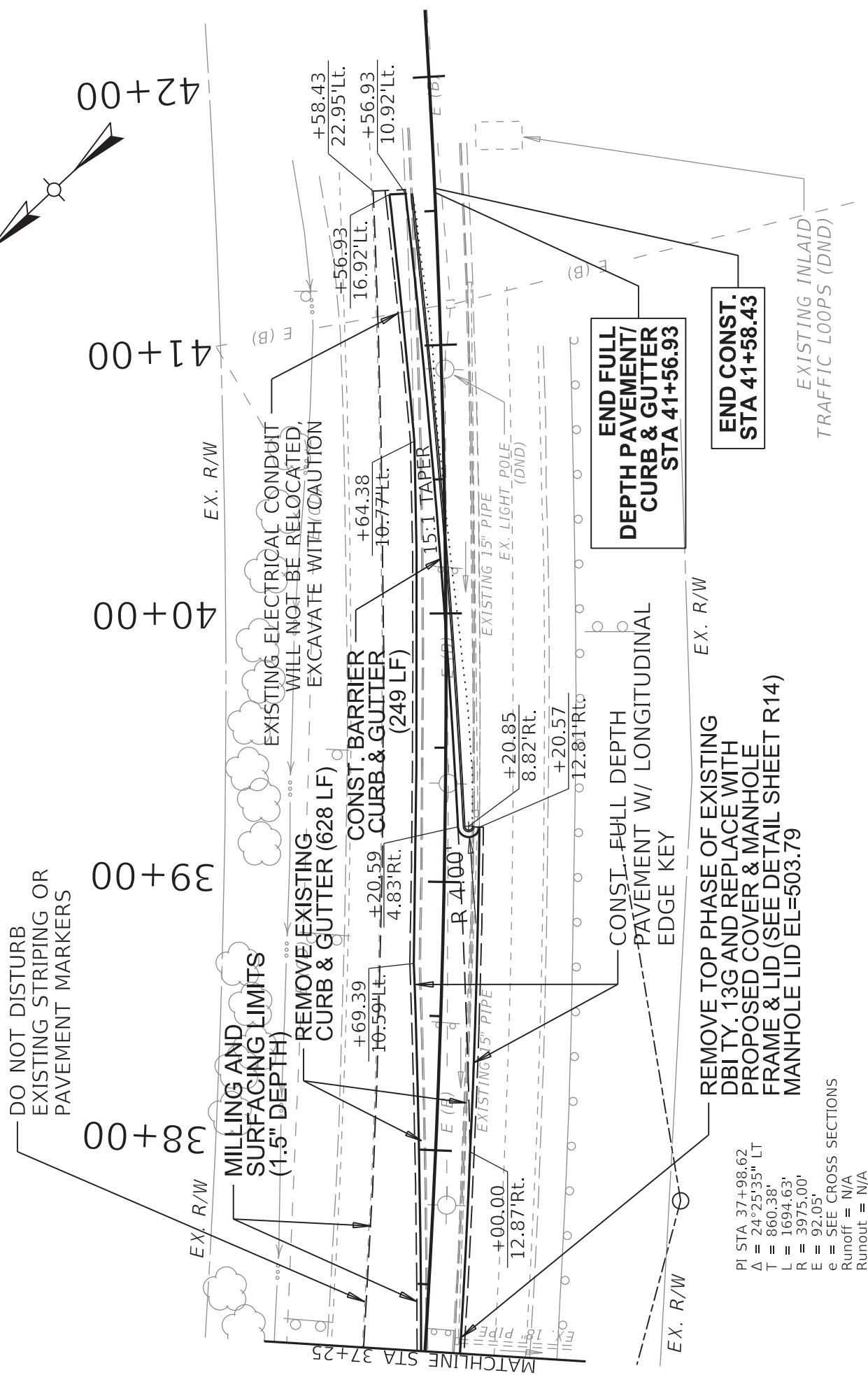
COUNTY OF
WARREN

SHEET NO.
R5



0' 25' 50' 100'

HORIZONTAL SCALE: 1"=50'



COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.LIC\HD241933903_80318_00_R_PLAN_PROFILE.DGN

USER: Stewart.Lic

DRAWING TITLE: LEFT TURN LANE PLAN SHEET
STA: 37+25.00 TO STA: 42+25.00

ITEM NO. 03-80318.00
SHEET NO. R6

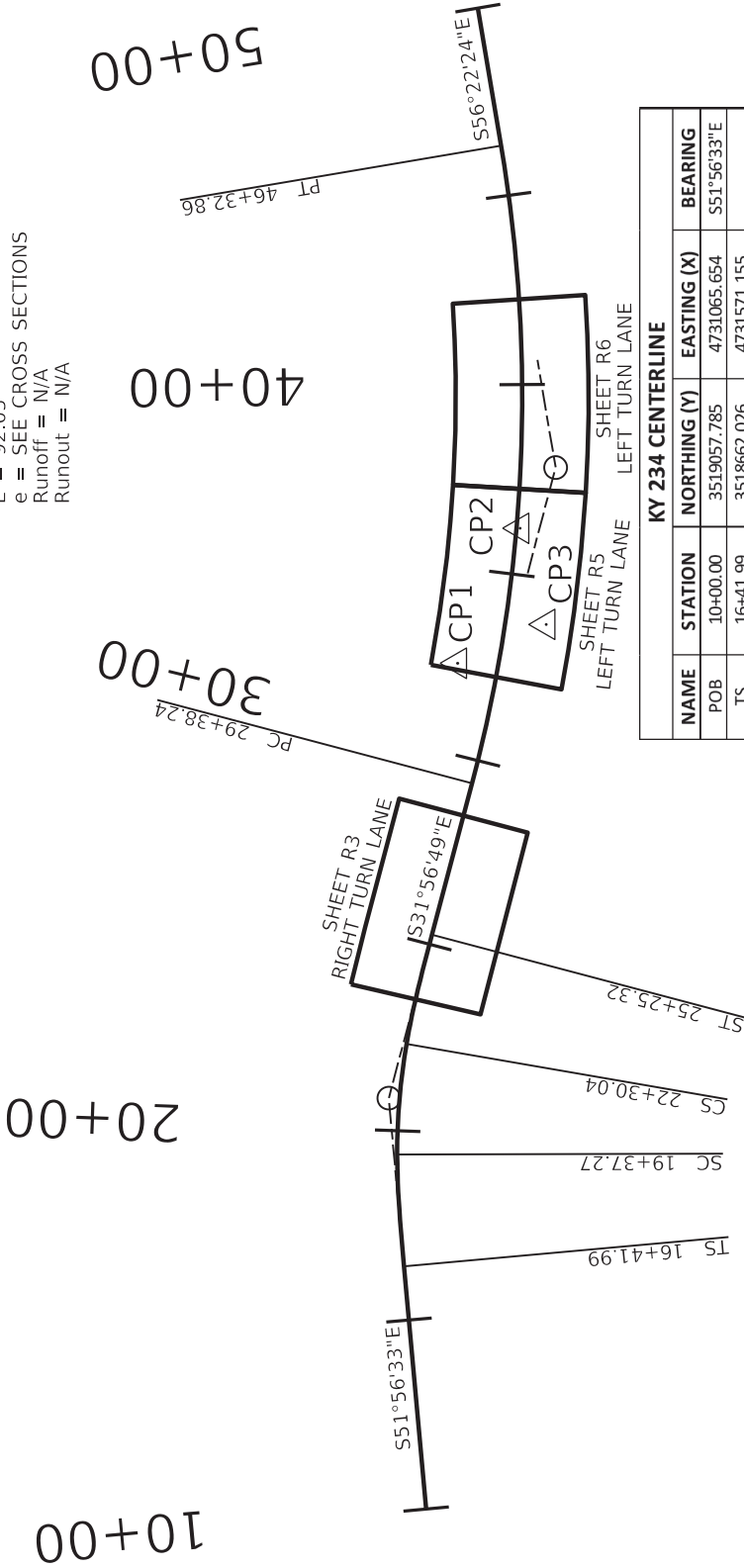
COUNTY OF WARREN

0' 25' 50' 100'
HORIZONTAL SCALE: 1"=50'

0' 25' 50' 100'
VERTICAL SCALE: 1"=50'

CONTROL POINTS				
CP NUMBER	TYPE	NORTHING (Y)	EASTING (X)	OFFSET
CP 1	IRON PIN & CAP	3517470.796	4732631.935	106.28 LT.
CP 2	IRON PIN & CAP	3517107.265	4732778.262	9.35 RT.
CP 3	IRON PIN & CAP	3517230.617	4732546.633	107.24 RT.

PI STA 37+98.62
Δ = 24°25'35" LT
T = 860.38'
L = 1694.63'
R = 3975.00'
e = 92.05'
e = SEE CROSS SECTIONS
Runoff = N/A
Runout = N/A



KY 234 CENTERLINE				
NAME	STATION	NORTHING (Y)	EASTING (X)	BEARING
POB	10+00.00	3519057.785	4731065.654	S51°56'33"E
TS	16+41.99	3518662.026	4731571.155	
SC	19+37.27	3518473.352	4731798.164	
PI	20+84.02	3518373.123	4731905.354	S31°56'49"E
CS	22+30.04	3518255.873	4731993.603	
ST	25+25.32	3518010.070	4732157.040	
PC	29+38.24	3517659.689	4732375.532	
PI	37+98.62	3516929.621	4732830.791	
PT	46+32.86	3516453.159	4733547.201	
POE	50+00.00	3516249.847	4733852.902	S56°22'24"E

PROJECT COORDINATES

Coordinates for horizontal control were obtained from GPS methods and adjusted to the National NAD83/FBN System.

Coordinates shown are Project Datum of Ground Coordinates and are in U.S. Survey Feet.

Coordinates are based on State Plane Coordinate System Single Zone.

COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

TEAM KENTUCKY
Infrastructure

OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.LIC\HD241933903_80318_00_R_CC.DGN

DRAWING TITLE: COORDINATE CONTROL & SHEET LAYOUT

ITEM NO.
03-80318.00

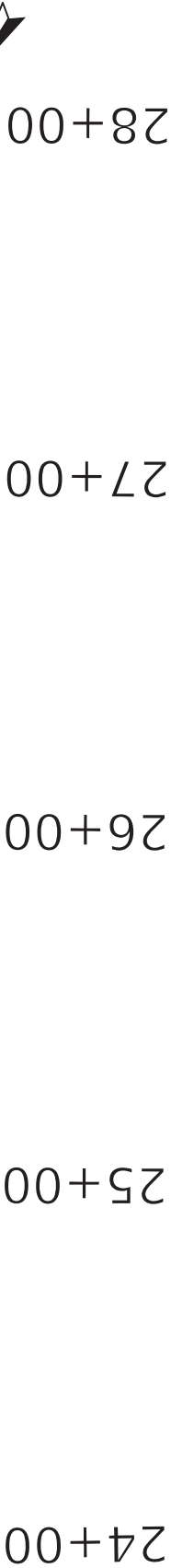
COUNTY OF
WARREN

SHEET NO.
R7

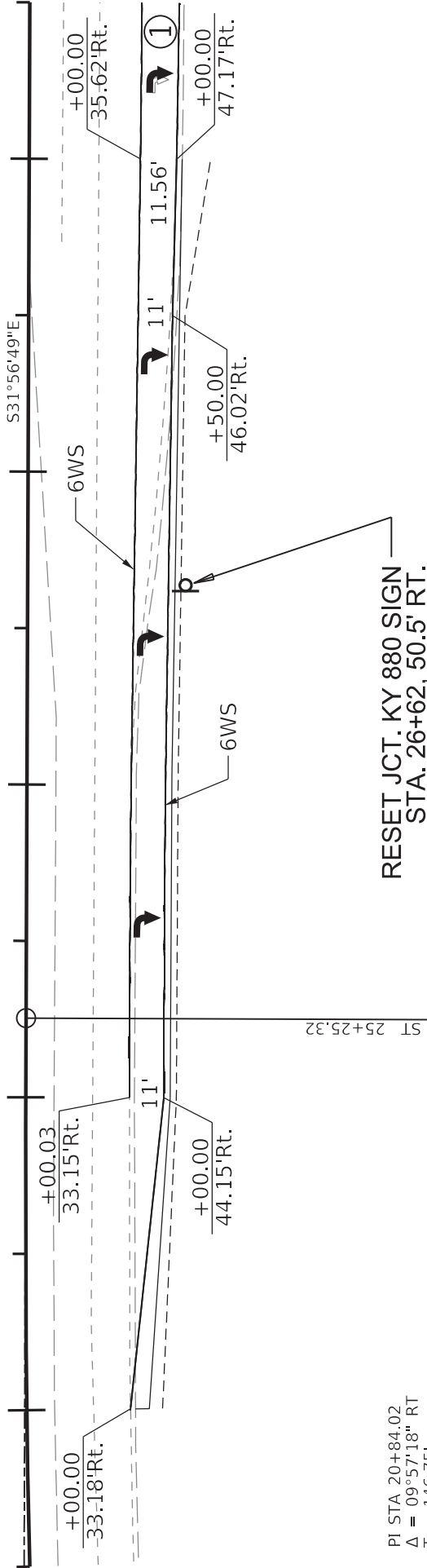
0' 250' 500' 1000'

HORIZONTAL SCALE: 1"=500'

① THE RIGHT TURN LANE STRIPING WILL EXTEND OFF OF THIS PLAN SHEET AND WILL NOT BE DETAILED IN THE PROPOSAL. STRIPING SHALL EXTEND TO THE EXISTING STOP BAR AT KY 234 AND LOVERS LANE.



KY 234 - PERMENANT PAVEMENT MARKINGS			
LABEL	DESCRIPTION	QUANTITY	UNIT
GWS	6" WHITE-SOLID THERMO	1400	LF
24SB	24" STOP BAR THERMO	15	LF
	INLAID PAVEMENT MARKER - MW	16	EACH
	RIGHT TURN THERMO SYMBOL	7	EACH



PI STA 20+84.02
Δ = 09°57'18" RT
T = 146.75'
L = 292.76'
R = 1685.00'
E = 6.38'
e = SEE CROSS SECTIONS
Runoff = N/A
Runout = N/A

COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

TEAM KENTUCKY

OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.LICH\241933903_80318_00_R_STRIPING_PLAN.DGN

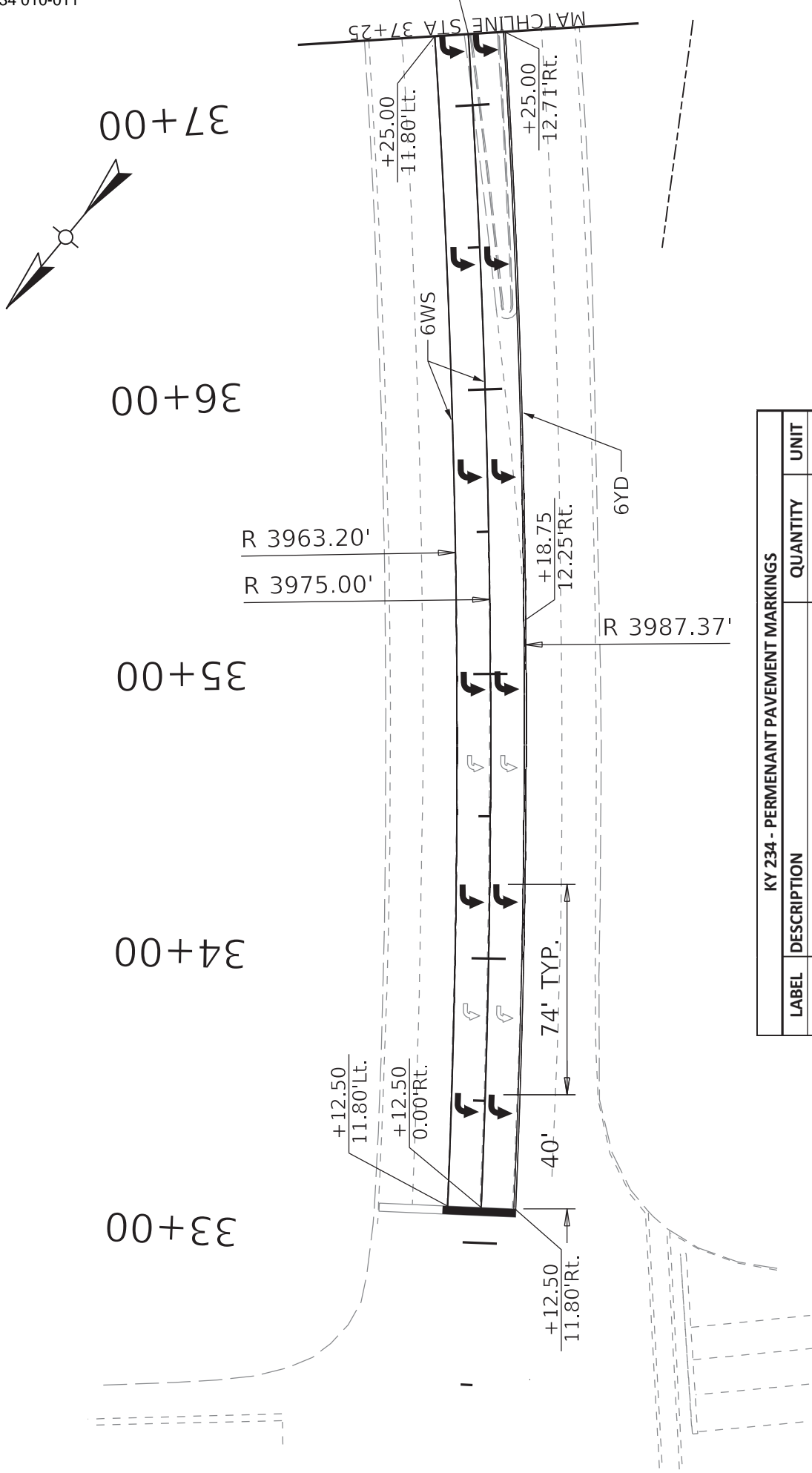
DRAWING TITLE: RIGHT TURN LANE DETAIL SHEET
STA: 23+50.00 TO STA: 28+50.00

ITEM NO.
03-80318.00

COUNTY OF
WARREN

SHEET NO.
R8

HORIZONTAL SCALE: 1"=50'



KY 234 - PERMENANT PAVEMENT MARKINGS		
LABEL	DESCRIPTION	UNIT
6WS	6" WHITE-SOLID THERMO	LF
6YD	6" DOUBLE YELLOW-SOLID THERMO	LF
24SB	24" STOP BAR THERMO	LF
	INLAID PAVEMENT MARKER - MW	EACH
	INLAID PAVEMENT MARKER - BY	EACH
	LEFT TURN THERMO SYMBOL	EACH

COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

TEAM KENTUCKY

OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.LIC\ID241933903_80318_00_R_STRIPING_PLAN.DGN

DRAWING TITLE: LEFT TURN LANE STRIPING SHEET
STA. 32+25.00 TO STA. 37+25.00

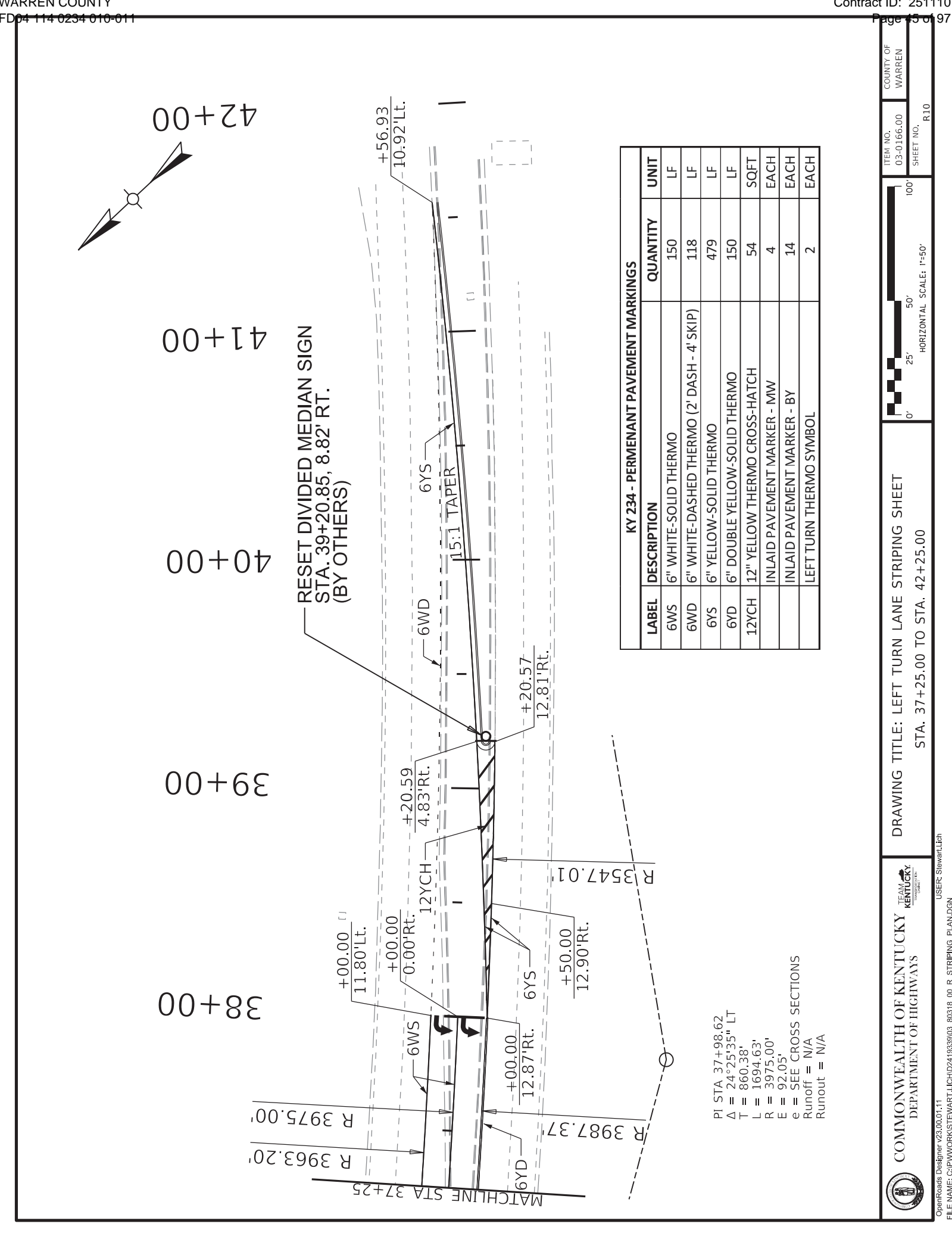
ITEM NO.
03-80318.00

COUNTY OF
WARREN

SHEET NO.
R9

0' 25' 50' 100'

HORIZONTAL SCALE: 1"=50'



KY 234 - PERMENANT PAVEMENT MARKINGS		
LABEL	DESCRIPTION	UNIT
6WS	6" WHITE-SOLID THERMO	LF
6WD	6" WHITE-DASHED THERMO (2' DASH - 4' SKIP)	LF
6YS	6" YELLOW-SOLID THERMO	LF
6YD	6" DOUBLE YELLOW-SOLID THERMO	LF
12YCH	12" YELLOW THERMO CROSS-HATCH	SQFT
	INLAID PAVEMENT MARKER - MW	EACH
	INLAID PAVEMENT MARKER - BY	EACH
	LEFT TURN THERMO SYMBOL	EACH

PI STA 37+98.62
Δ = 24°25'35" LT
T = 860.38'
L = 1694.63'
R = 3975.00'
E = 92.05'
e = SEE CROSS SECTIONS
Runoff = N/A
Runout = N/A

COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

DRAWING TITLE: LEFT TURN LANE STRIPING SHEET
STA. 37+25.00 TO STA. 42+25.00

ITEM NO.
03-0166.00

COUNTY OF
WARREN

SHEET NO.
R10

100'

50'

25'

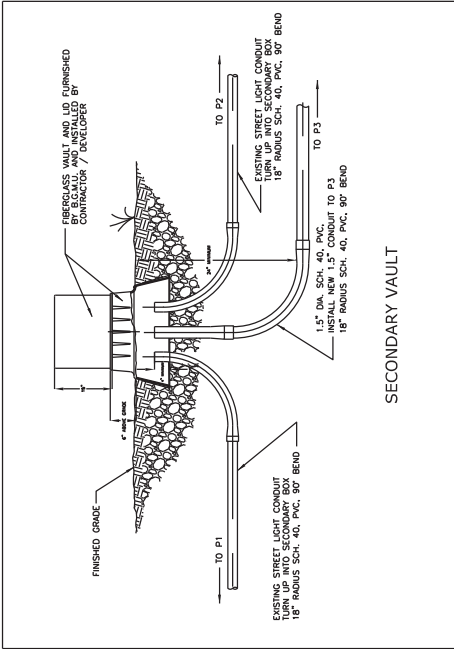
0'

HORIZONTAL SCALE: 1"=50'

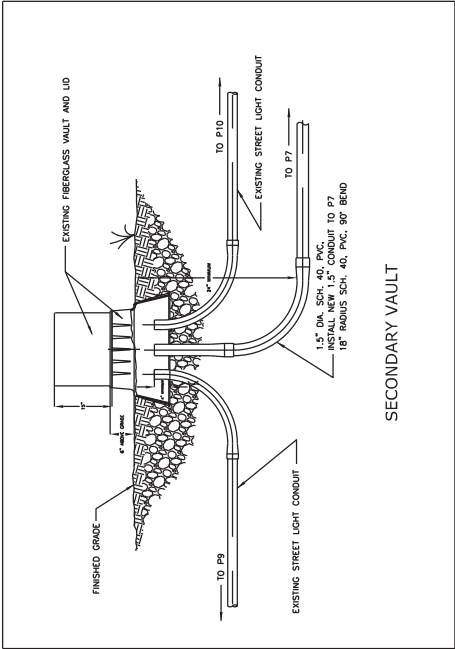
OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.LIC\ID241933903_80318_00_R_STRIPING_PLAN.DGN

USER: StewartLich

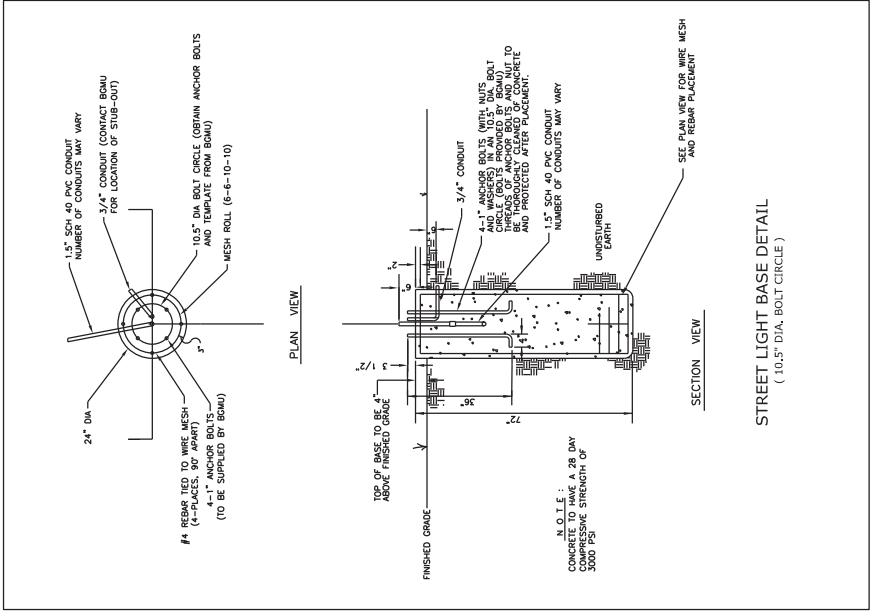
V1



V2



20' DECORATIVE POLE

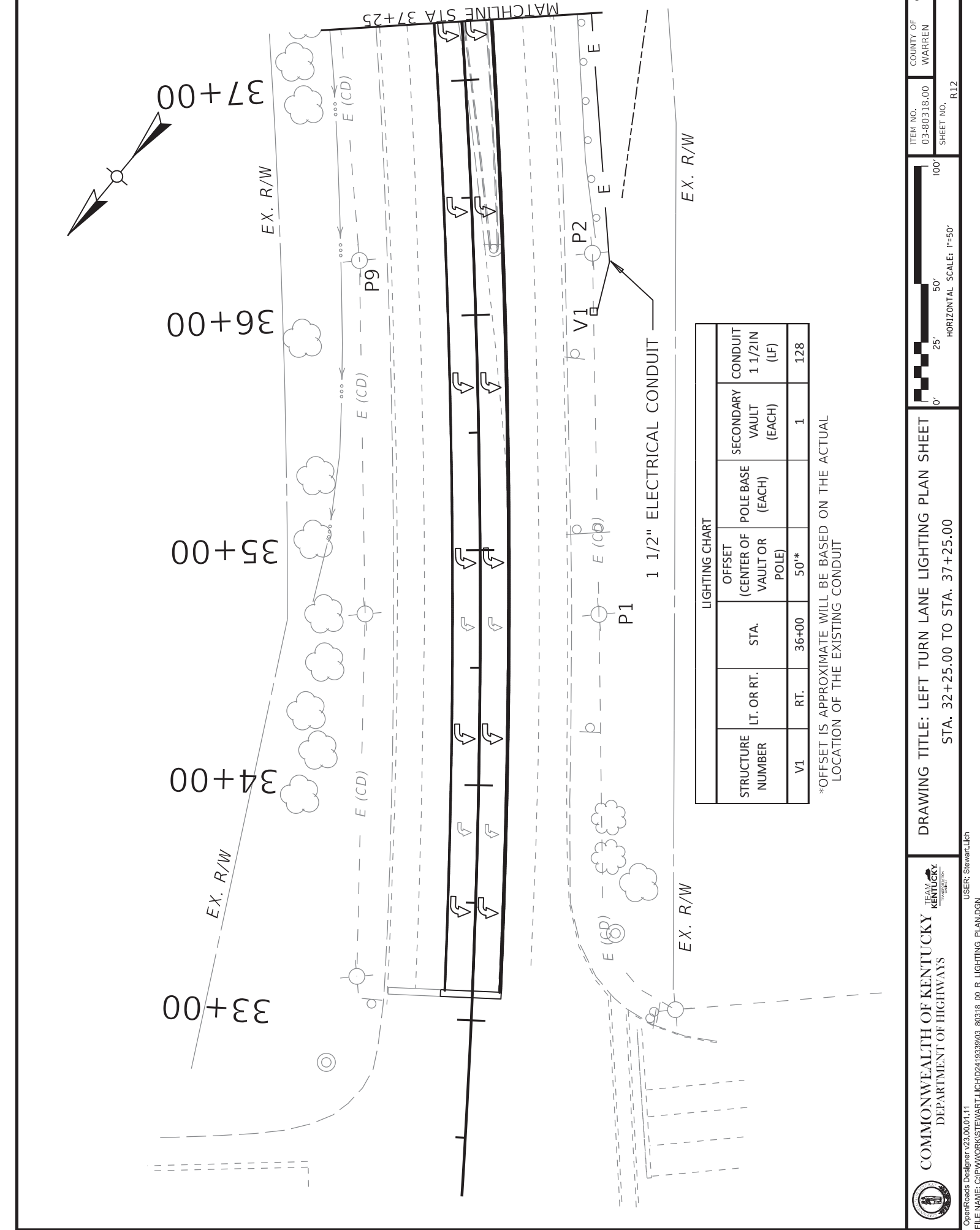


BGMU WILL REMOVE P5 AND P6 POLES AND LUMINAIRES
BGMU WILL REMOVE UNDERGROUND CONDUCTOR BETWEEN P5 AND P6, AND BETWEEN P6 AND V3, AND BETWEEN P1 AND P2

CONTRACTOR WILL BUILD A NEW CONCRETE POLE BASE AT P3, P4, P7, AND P8 (BGMU WILL PROVIDE ANCHOR BOLTS AND BOLT PATTERN TEMPLATE)
CONTRACTOR WILL LOCATE AND TURN UP STREET LIGHT CONDUIT BETWEEN P1 AND P2, AND INSTALL A SECONDARY VAULT AT V1
CONTRACTOR WILL INSTALL 1.5" SCH 40 CONDUIT FROM V1 TO P3, AND FROM P3 TO P4
CONTRACTOR WILL INSTALL 1.5" SCH 40 CONDUIT FROM V2 TO P7, AND FROM P7 TO P8
CONTRACTOR WILL INSTALL PULL STRING IN ALL STREET LIGHT CONDUIT REQUIRING NEW CONDUCTOR
CONTRACTOR WILL SUPPLY ALL LABOR AND MATERIALS REQUIRED FOR THE INSTALLATION OF SECONDARY VAULT, POLE BASES AND CONDUIT AS DESCRIBED AND DETAILED ABOVE

BGMU WILL INSTALL ALL NEW UNDERGROUND CONDUCTOR, AND SET FOUR STREET LIGHT POLES AND LUMINAIRES
ALL LABOR AND MATERIALS FOR THE WORK PERFORMED BY BGMU WILL BE INCIDENTAL TO BID ITEM 22939ND

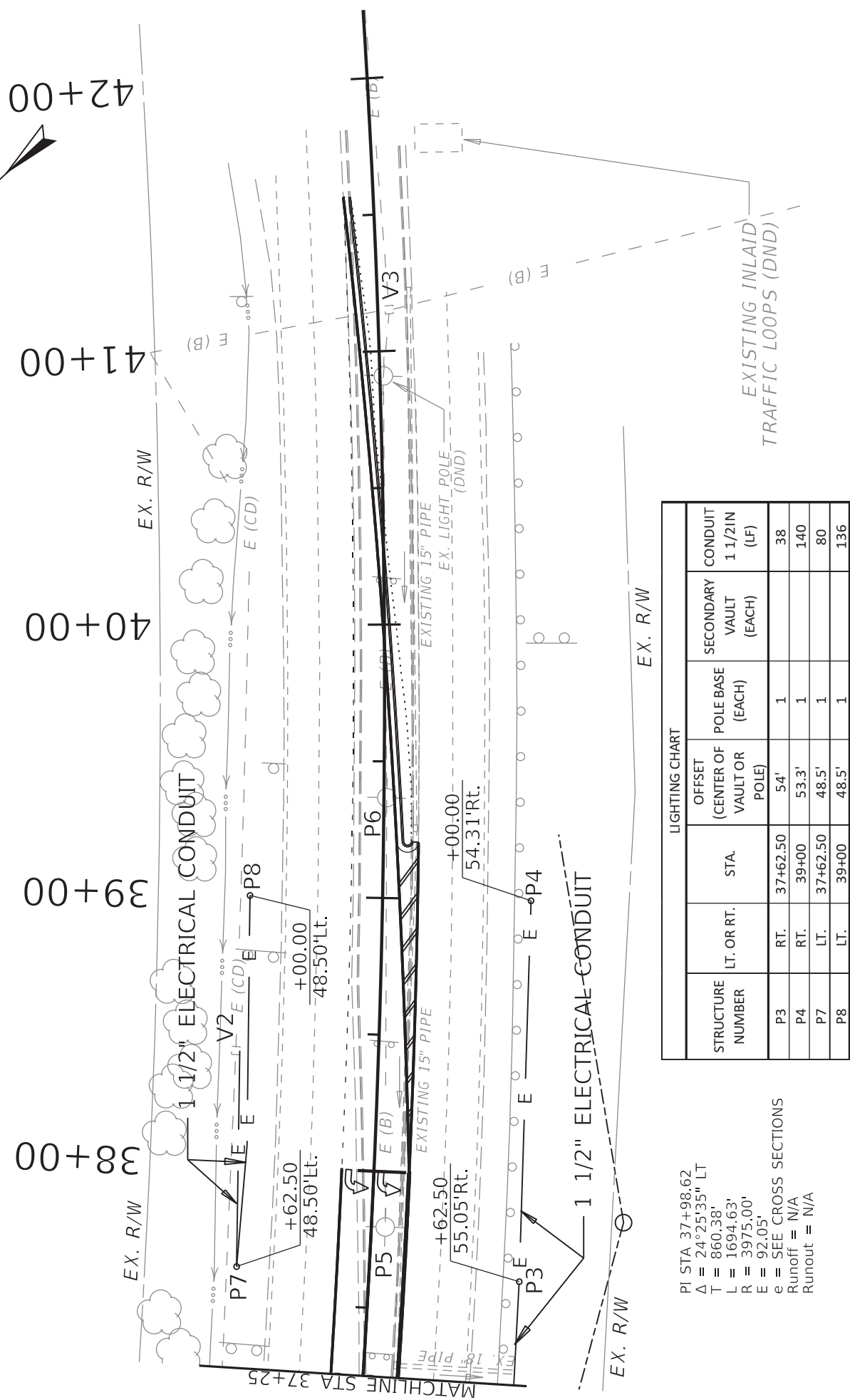
BGMU'S LABOR AND MATERIAL COSTS, INCLUDING THE PRICE OF NEW POLES AND LUMINAIRES, IS \$20,979.84



LIGHTING CHART				
STRUCTURE NUMBER	LT. OR RT.	STA.	OFFSET (CENTER OF VAULT OR POLE)	CONDUIT 1 1/2" (LF)
V1	RT.	36+00	50'	128

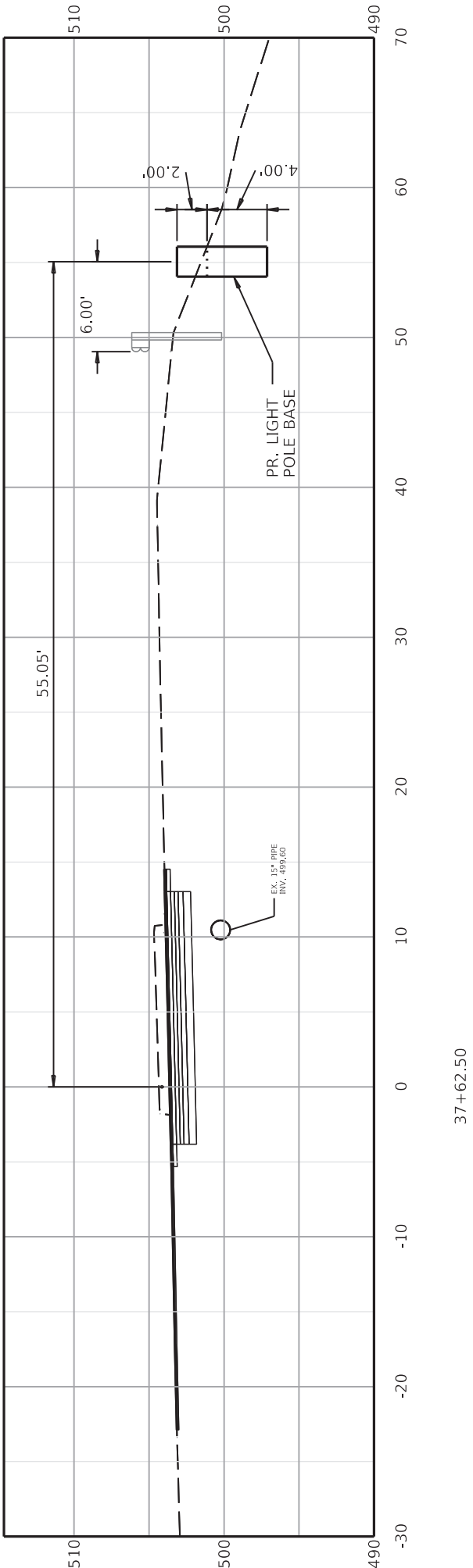
*OFFSET IS APPROXIMATE WILL BE BASED ON THE ACTUAL LOCATION OF THE EXISTING CONDUIT

FINAL POLE BASE LOCATIONS SHALL BE APPROVED BY THE ENGINEER



LIGHTING CHART				
STRUCTURE NUMBER	LT. OR RT.	STA.	OFFSET (CENTER OF VAULT OR POLE)	CONDUIT 1 1/2IN (LF)
P3	RT.	37+62.50	54'	38
P4	RT.	39+00	53.3'	140
P7	LT.	37+62.50	48.5'	80
P8	LT.	39+00	48.5'	136

PI STA 37+98.62
Δ = 24°25'35" LT
T = 860.38'
L = 1694.63'
R = 3975.00'
E = 92.05'
e = SEE CROSS SECTIONS
Runoff = N/A
Runout = N/A





COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

TEAM KENTUCKY
Infrastructure

COMMONWEALTH OF KENTUCKY
COUNTY OF WARREN

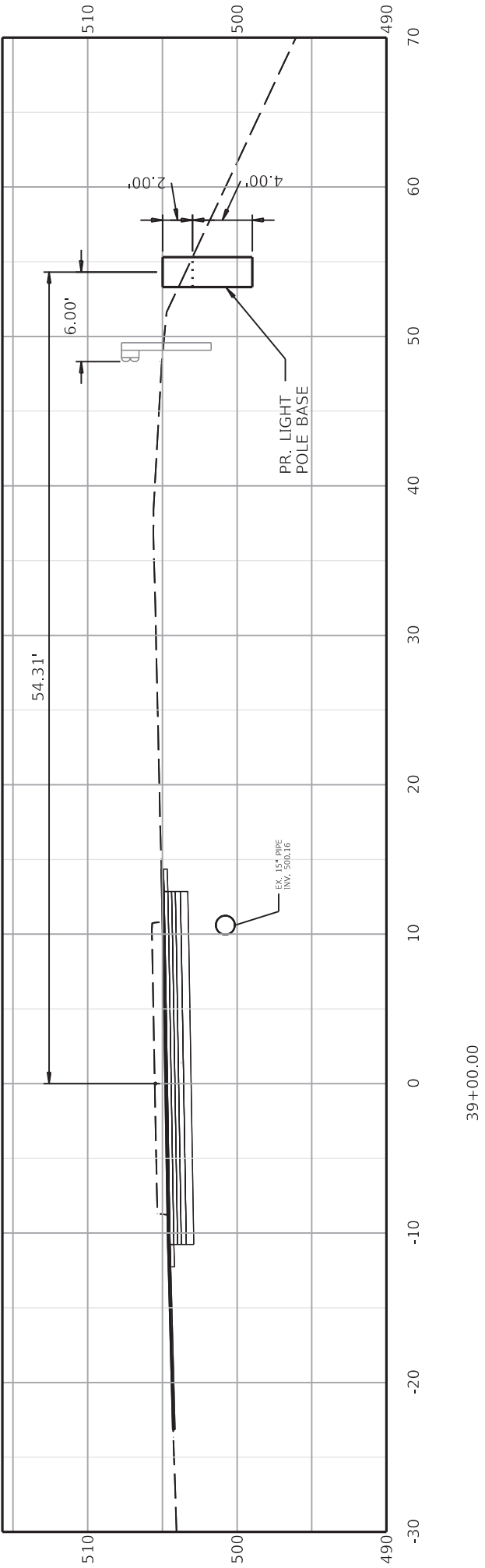
ITEM NO.
03-80318.00

SHEET NO.
R14

DRAWING TITLE: LIGHT POLE BASE
STA. 37+62.50 TO STA. 37+62.50

HORIZONTAL SCALE: 1"=10'

OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.LIC\HID241933903_80318_00_UTILITY_X_SECTIONS_LT_TURN.DGN
USER: Stewart.Lich



	COMMONWEALTH OF KENTUCKY DEPARTMENT OF HIGHWAYS	DRAWING TITLE: LIGHT POLE BASE STA: 39+00.00 TO STA: 39+00.00	HORIZONTAL SCALE: 1"=10'	ITEM NO. 03-80318.00	COUNTY OF WARREN
				SHEET NO. R15	

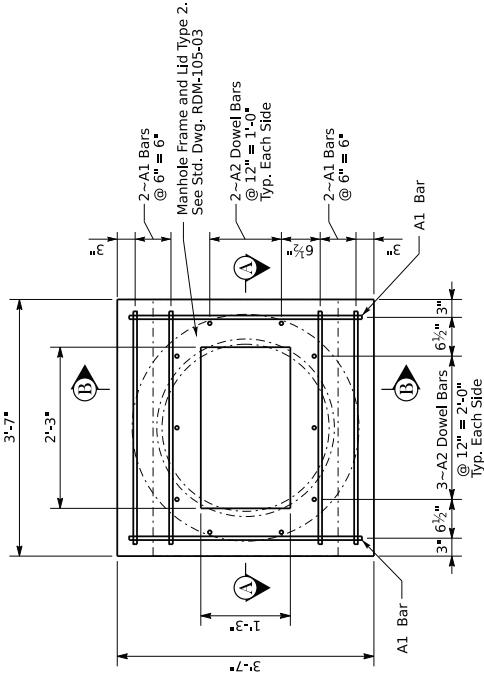
BILL OF REINFORCEMENT

MARK	TYPE	NO.	SIZE	LENGTH	LOCATION	A	B	C	D
A1	Str.	6	5	3'-3"	Slab				
A2	Str.	10	5	1'-6"	Slab				

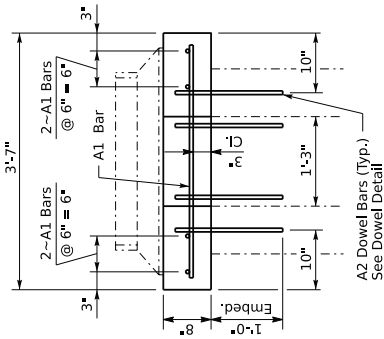
BID CODE	ITEM	QUANTITY
8100	Class A Conc.	0.3 CY
8150	Reinforcement	36 LB

NOTES:

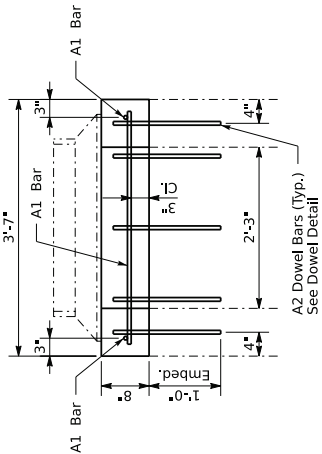
- Remove the top phase of the existing drop box inlet.
- Center the drop box cap and the manhole frame over the throat of the existing drop box inlet.



PLAN



SECTION B-B



SECTION A-A

GENERAL NOTES

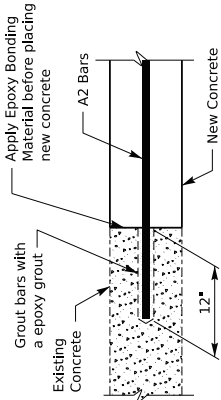
SPECIFICATIONS: All references to the standard Specifications are to the current edition of the Kentucky Department of Highways Standard Specifications for Road and Bridge Construction Supplemental Specifications. All references to the AASHTO are to the current edition of the AASHTO LRFD Bridge Design Specifications, with Interims.

COMPLETION OF THE STRUCTURE: The Contractor is required to complete the structure in accordance with the plans and specifications. Material, labor or construction operations, not otherwise specified, are to be included in the bid item most appropriate to the work involved. This may include cofferdams, shoring, excavations, backfilling, removal of all or parts of existing structures, phase construction, incidental materials, labor, or anything else required to complete the structure.

REINFORCEMENT: Dimensions shown from the face of concrete to bars are to center of bars unless otherwise shown. Spacing of bars is from center to center of bars. Clear distance to face of concrete is 2" unless otherwise noted. Any reinforcing bars designated by suffix (s) in a Bill of Reinforcement shall be considered a stirrup for purposes of bend diameters.

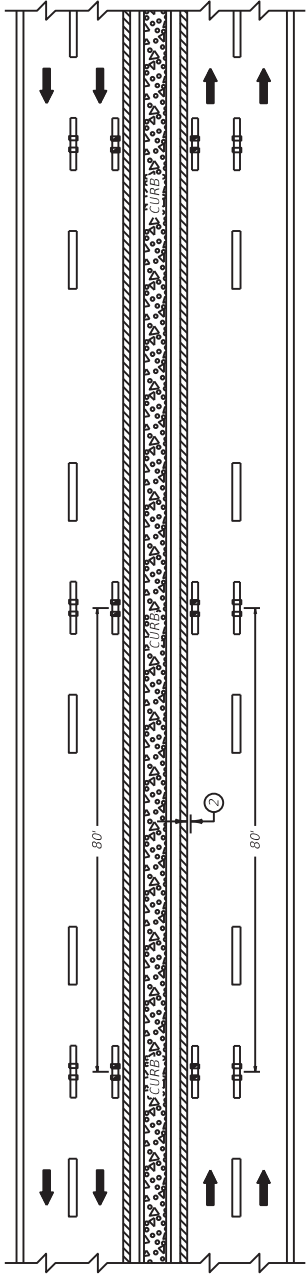
DIMENSIONS: Dimensions are for a normal temperature of 60 degrees Fahrenheit. Layout dimensions are horizontal measurements.

CONCRETE: Class "A" concrete shall be used throughout.



DOWEL DETAILS

NOTE: The cost of drilling holes, grouting, and epoxy bonding material shall be incidental to the cost of Class "A" Concrete.



ARRANGEMENT "E" (DIVIDED HIGHWAY WITH CURB WITHIN 8' OF DRIVING LANE)

~ NOTES ~

- 1. MARKERS INSTALLED ALONG LANE LINES SHALL BE PLACED BETWEEN AND IN LINE WITH THE SKIPS.
- 2. MARKERS INSTALLED ALONG EDGE LINES SHALL BE PLACED SO THAT THE NEAR EDGE OF THE GROOVE IS NO MORE THAN 1" FROM THE NEAR EDGE OF THE LINE.
- 3. MARKERS SHALL NOT BE INSTALLED ON TOP OF THE PAVEMENT JOINT. OFFSET MARKERS A MINIMUM OF 2' FROM THE PAVEMENT JOINT. ENSURE THAT THE FINISHED LINE OF MARKERS IS STRAIGHT WITH MINIMAL LATERAL DEVIATION. MARKERS MAY BE ELIMINATED OR PLACEMENT ADJUSTED AT THE DISCRETION OF THE ENGINEER.
- 4. MARKERS SHALL BE INSTALLED AT 40' SPACING ALONG SOLID WHITE AUXILIARY LINES. MARKER COLOR SHALL MATCH THE MARKERS INSTALLED ALONG THE WHITE LANE LINES.

BID ITEMS
06610 - INLAID PAVEMENT MARKER - MW
06611 - INLAID PAVEMENT MARKER - MY

LEGEND

MONO-DIRECTIONAL PAVEMENT MARKER (YELLOW)

MONO-DIRECTIONAL PAVEMENT MARKER (WHITE)

MARKINGS (YELLOW)

MARKINGS (WHITE)

DRAWING NOT TO SCALE

SUBMITTER: DESIGN PRINCIPAL DATE:



COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

Graphics: Uniglobe v15.18.209

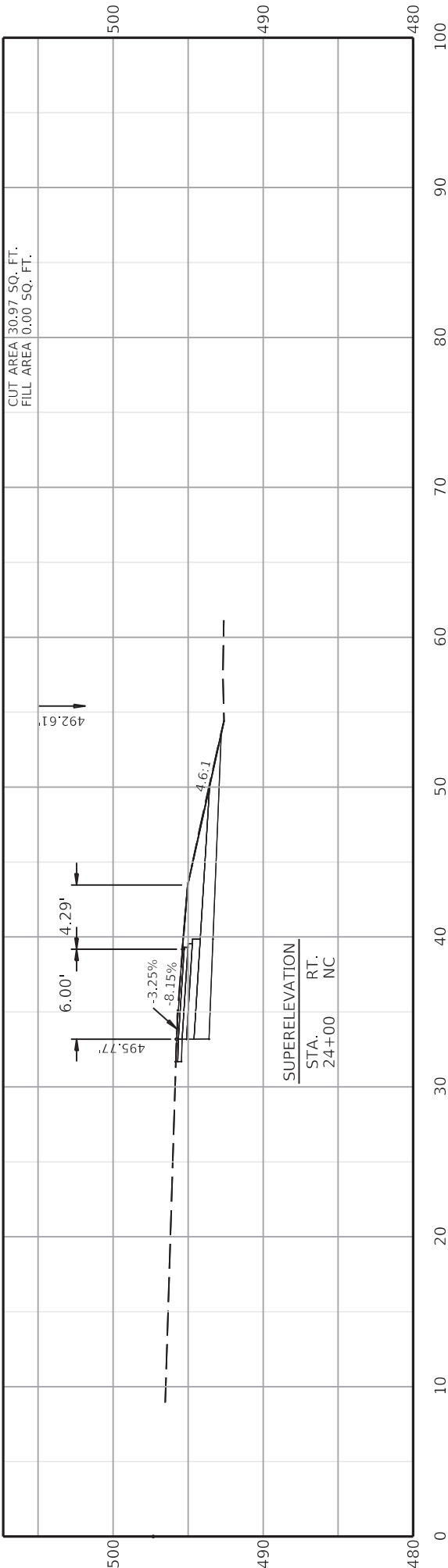
DATE PLOTTED: 8/7/2015 15:31:17 AM

USER: cwilliams@ky.gov

FILE NAME: C:\WORK\015\WARREN\CD04193\SEP008.DWG

DRAWING TITLE: SEPIA 008 - INLAID PAVEMENT MARKER ARRANGEMENTS MULTI-LANE ROADWAYS

ITEM NO. COUNTY OF WARREN
03-00318.00
SHEET NO. R17



24+00.00



COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

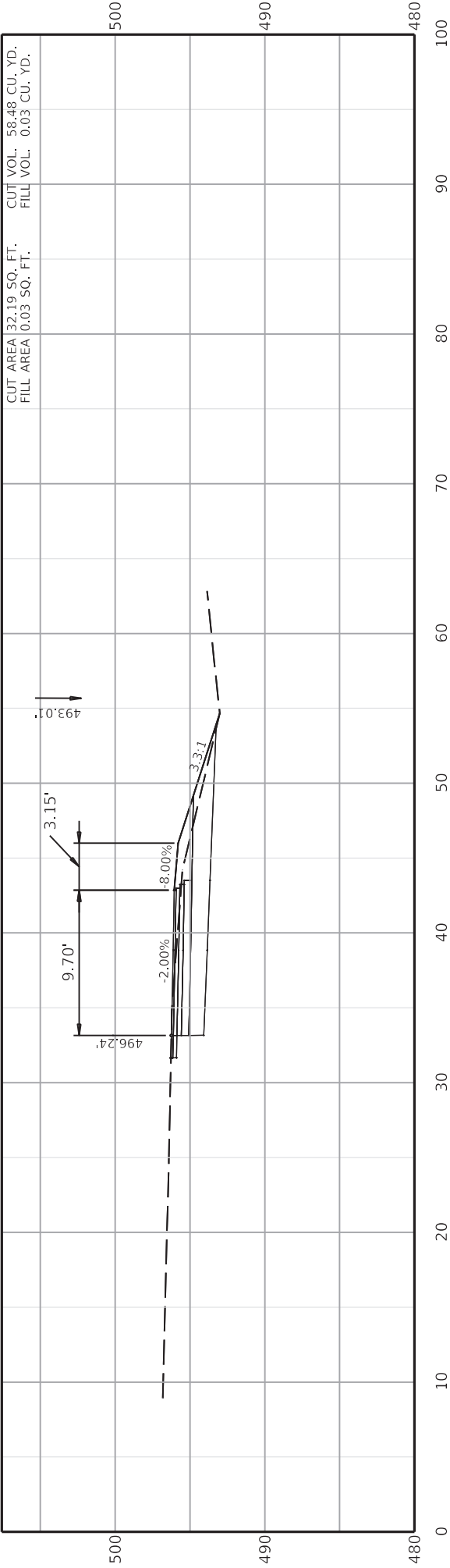
DRAWING TITLE: RIGHT TURN LANE CROSS SECTIONS
STA: 24+00.00 TO STA: 24+00.00

HORIZONTAL SCALE: 1"=10'

ITEM NO.
03-80318.00

COUNTY OF
WARREN

SHEET NO. X1





COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

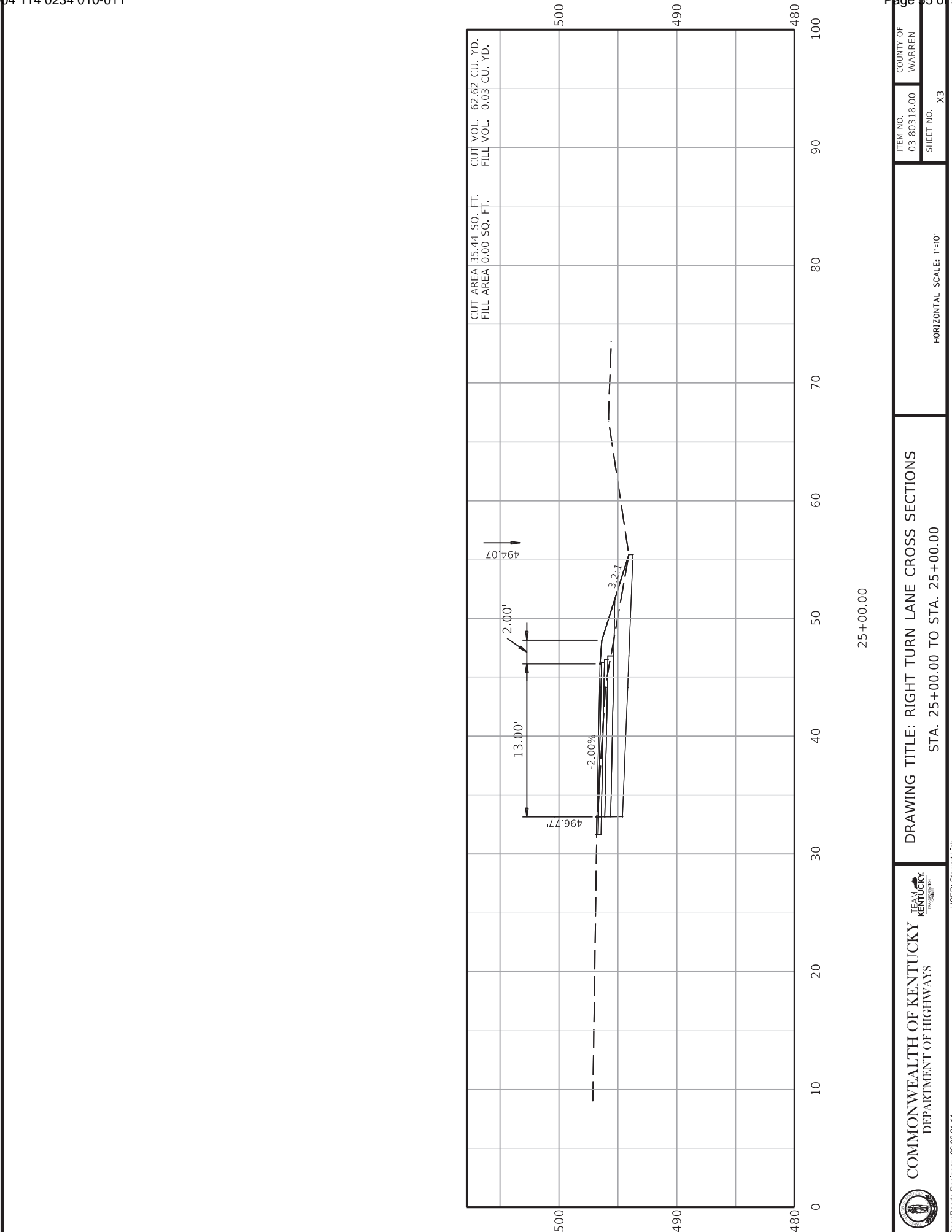
DRAWING TITLE: RIGHT TURN LANE CROSS SECTIONS
STA: 24+50.00 TO STA: 24+50.00

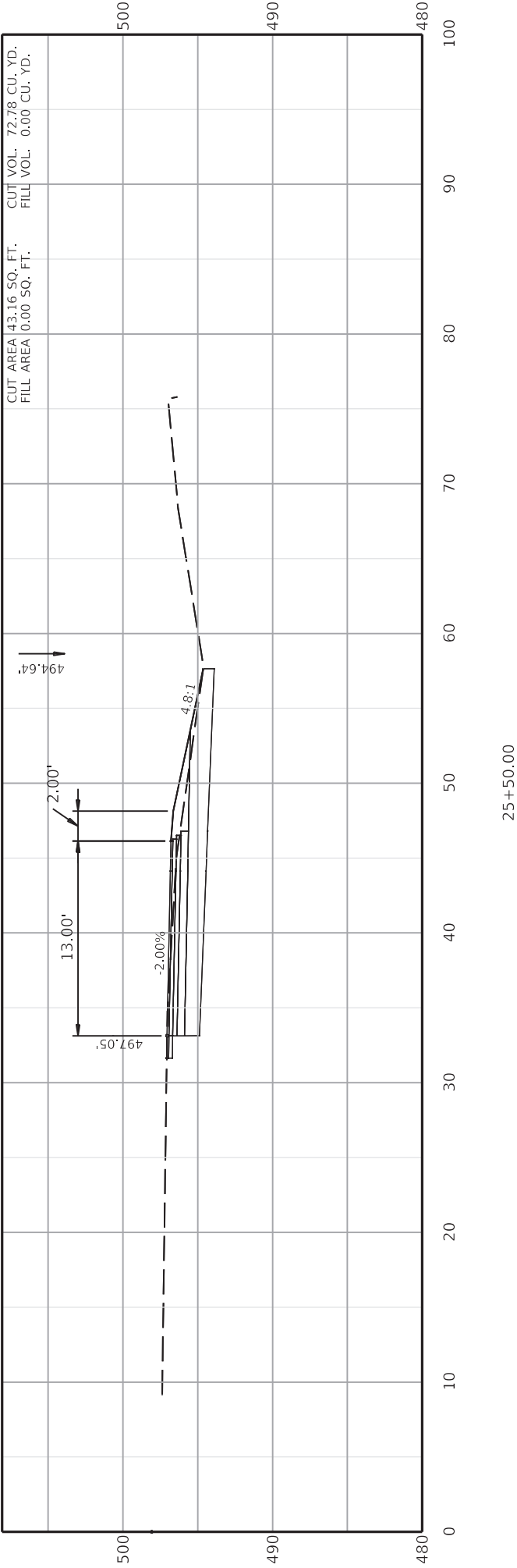
HORIZONTAL SCALE: 1"=10'

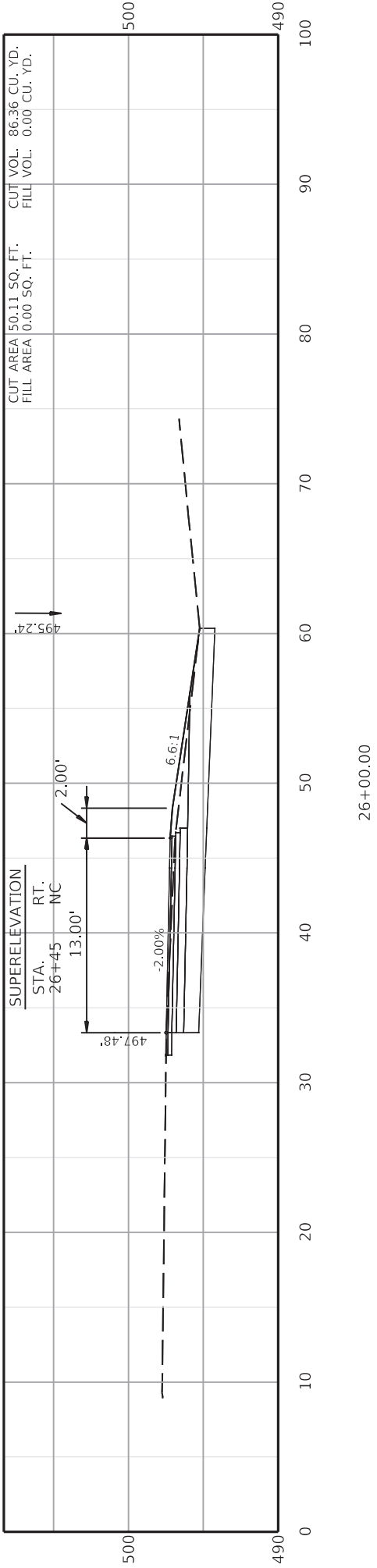
ITEM NO.
03-80318.00

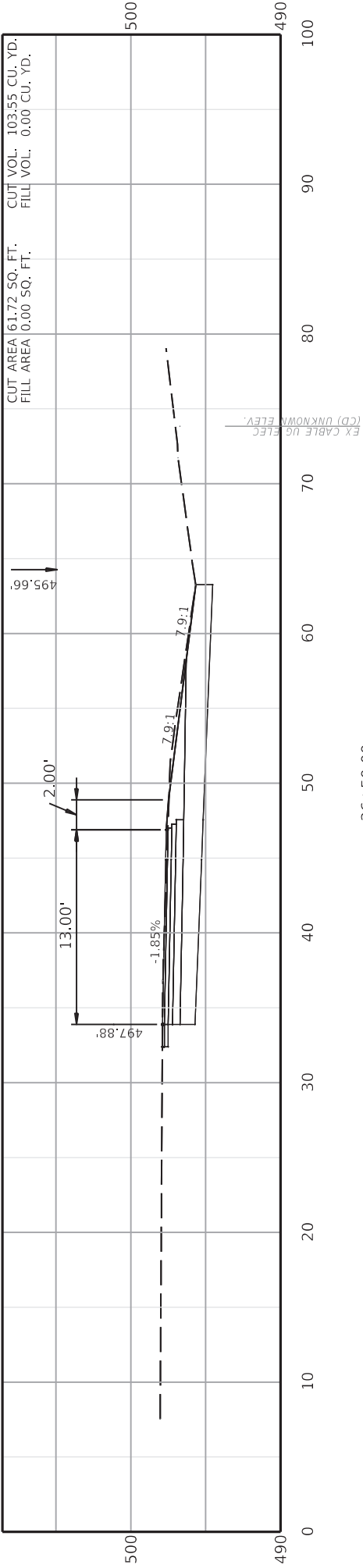
COUNTY OF
WARREN

SHEET NO.
X2











COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

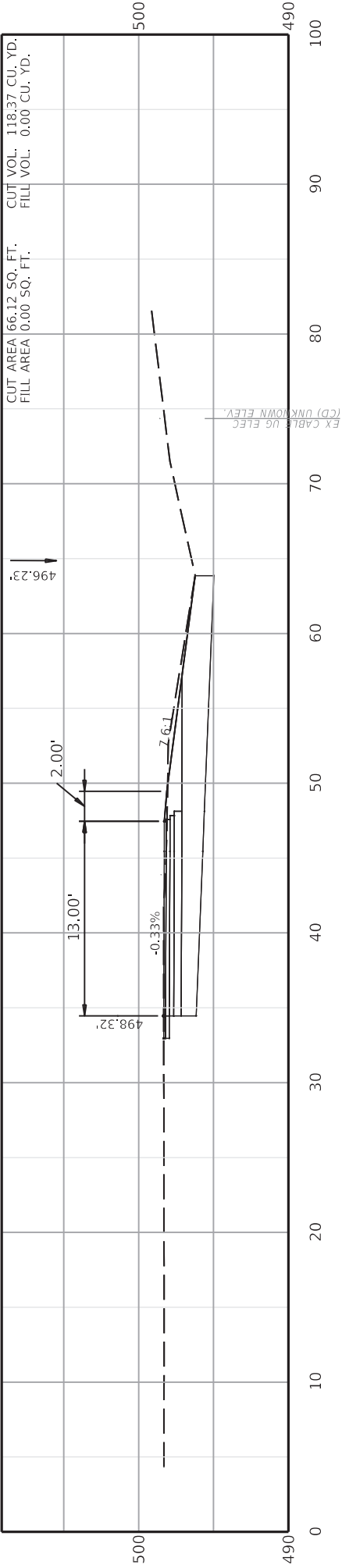
DRAWING TITLE: RIGHT TURN LANE CROSS SECTIONS
STA: 26+50.00 TO STA: 26+50.00

HORIZONTAL SCALE: 1"=10'

ITEM NO.
03-80318.00

COUNTY OF
WARREN

SHEET NO.
X6



27+00.00

DRAWING TITLE: RIGHT TURN LANE CROSS SECTIONS
STA: 27+00.00 TO STA: 27+00.00

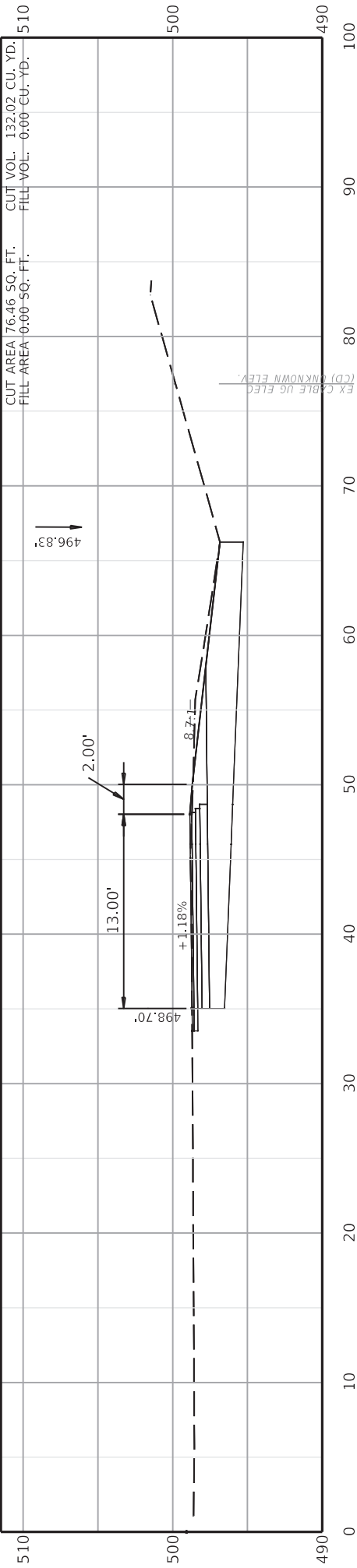
HORIZONTAL SCALE: 1"=10'

ITEM NO.
03-80318.00
SHEET NO. X7

COUNTY OF
WARREN

COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS
TEAM KENTUCKY
USER: StewartLich

OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.LICH\241933903_80318_00_X_SECTIONS_RT_TURN.DGN



COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

DRAWING TITLE: RIGHT TURN LANE CROSS SECTIONS
STA: 27+50.00 TO STA: 27+50.00

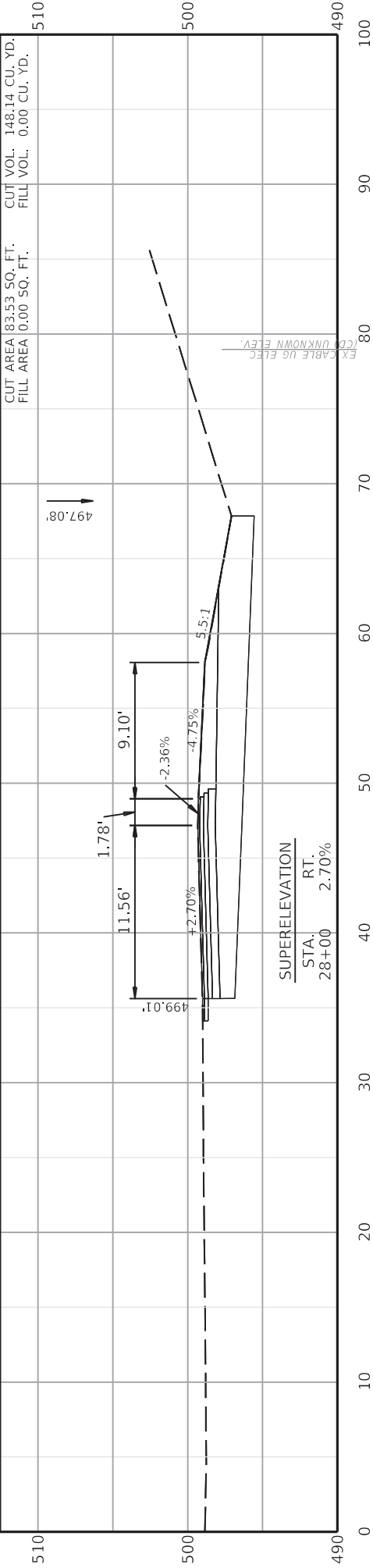
ITEM NO.
03-80318.00

SHEET NO.
X8

COUNTY OF
WARREN

HORIZONTAL SCALE: 1"=10'

RIGHT TURN LANE TOTALS
CUT VOL. 783 CU. YD.
FILL VOL. 1 CU. YD.



28+00.00

DRAWING TITLE: RIGHT TURN LANE CROSS SECTIONS

STA: 28+00.00 TO STA: 28+00.00

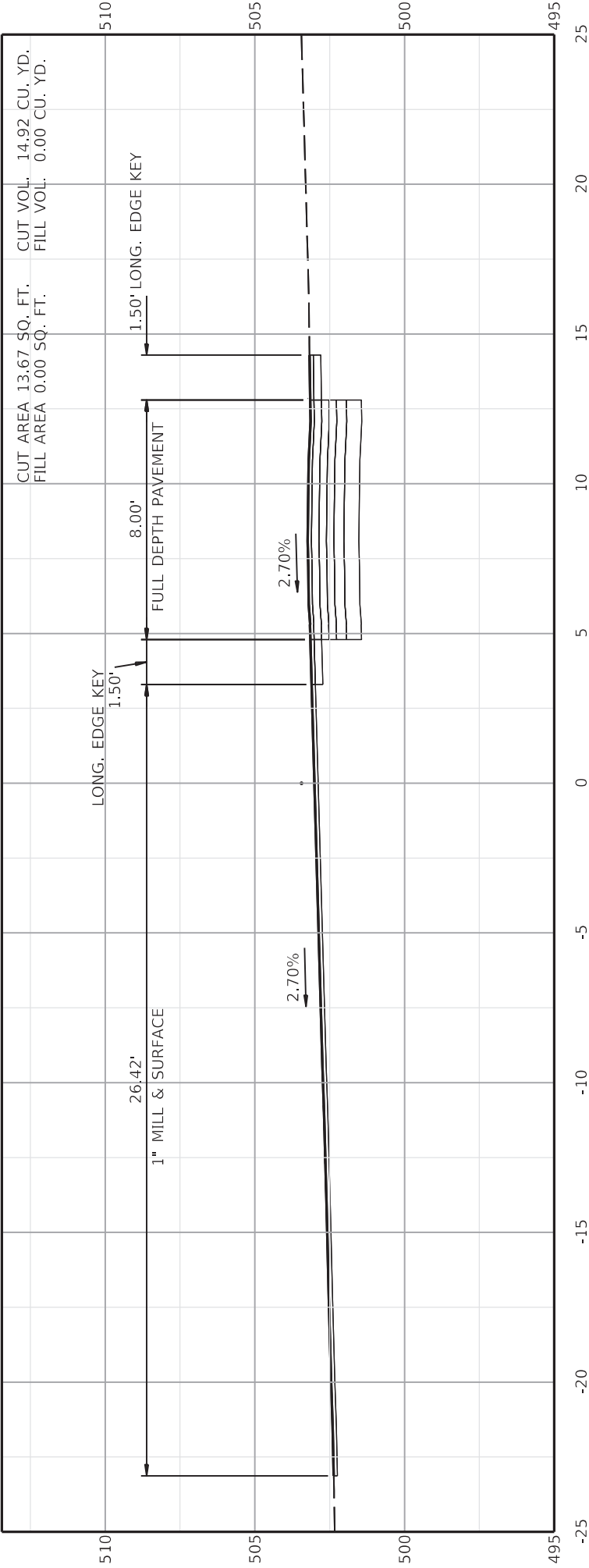
HORIZONTAL SCALE: 1"=10'

COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

COUNTY OF
WARREN

ITEM NO.
03-80318.00

SHEET NO.
X9

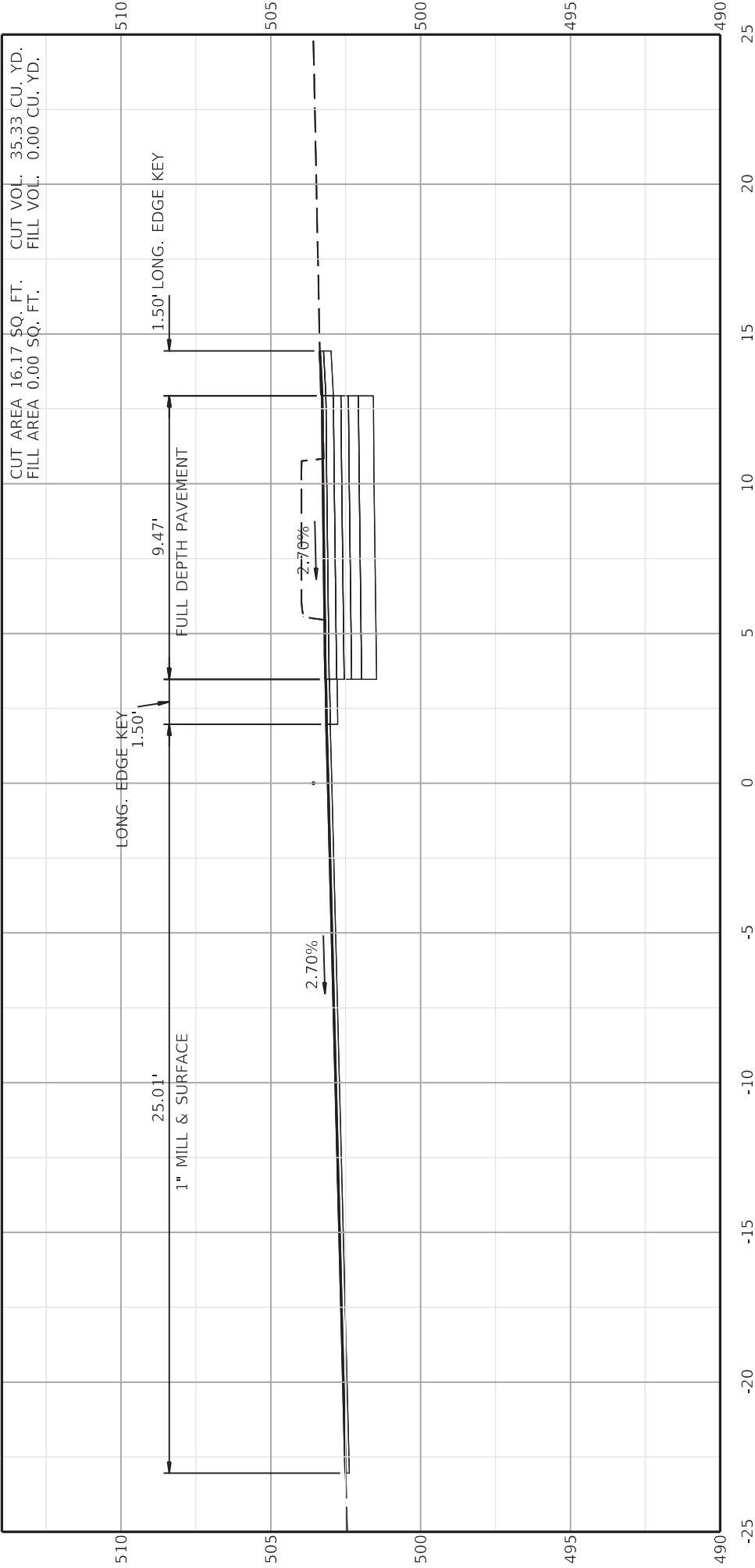


36+23.00

DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS
STA. 36+23.00 TO STA. 36+23.00



	ITEM NO. 03-80318.00	COUNTY OF WARREN
	SHEET NO. X10	





COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

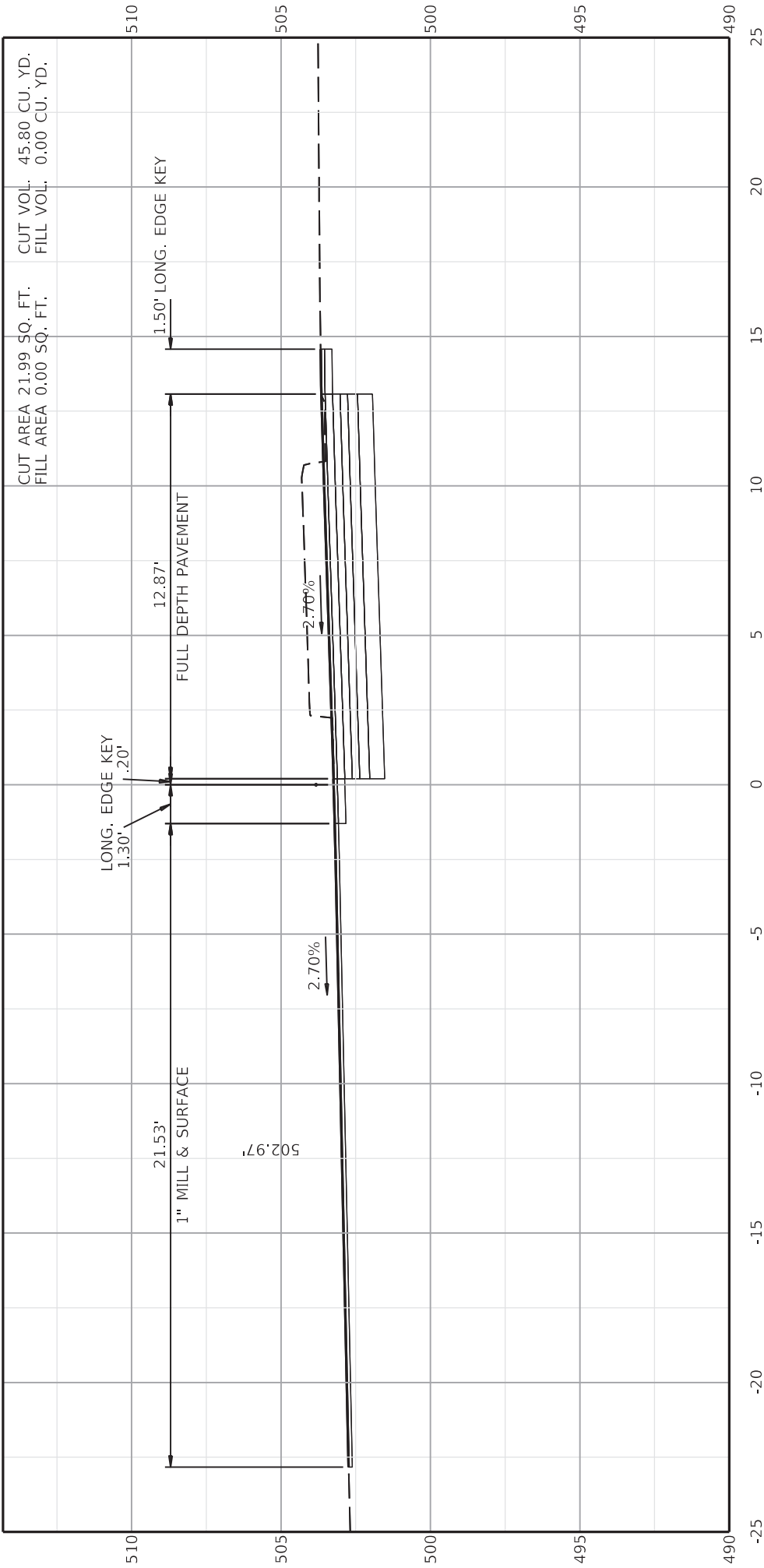
DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS
STA. 36+50.00 TO STA. 36+50.00

ITEM NO.
03-80318.00

SHEET NO.
X11

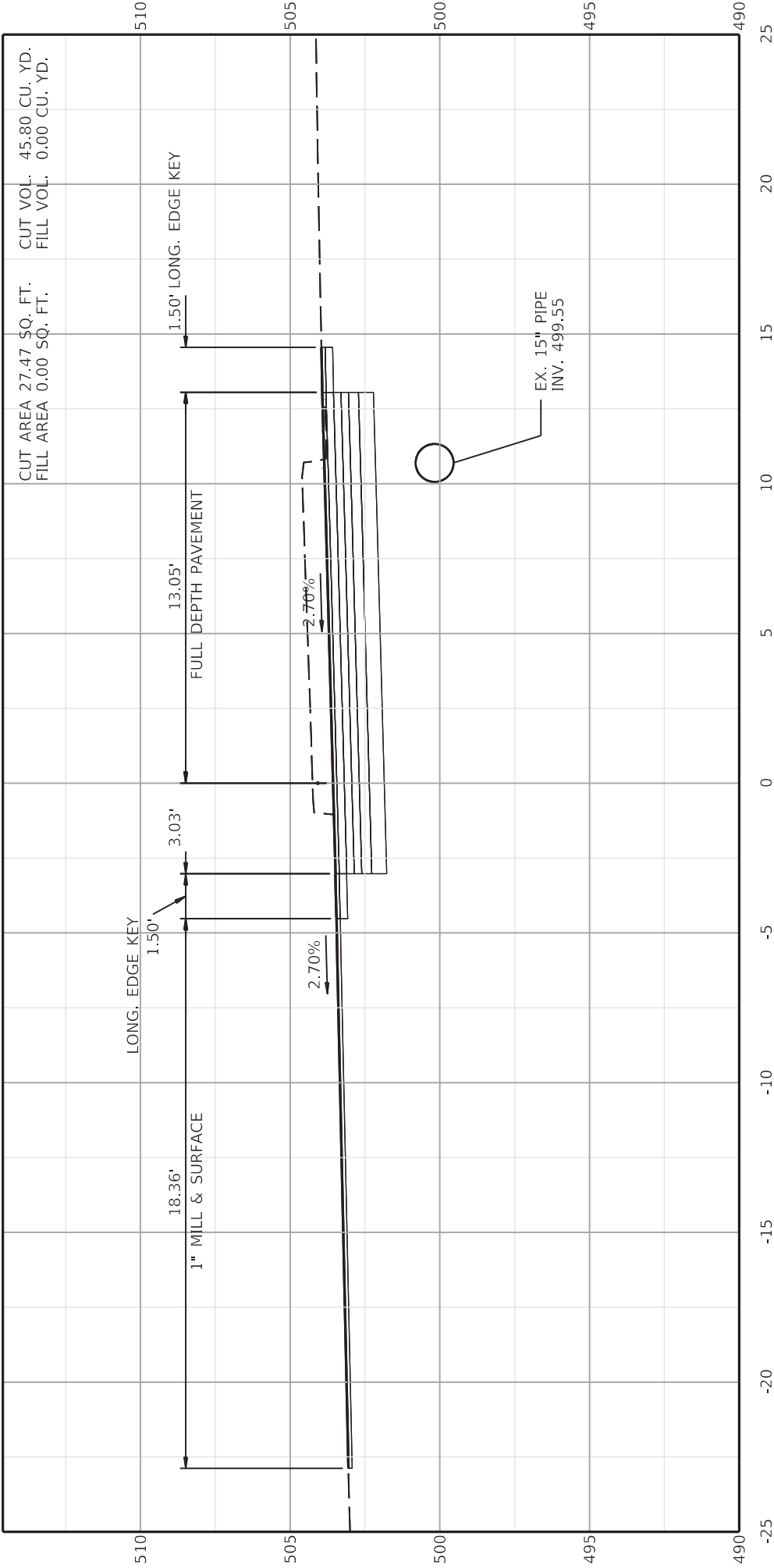
COUNTY OF
WARREN

HORIZONTAL SCALE: 1"=5'



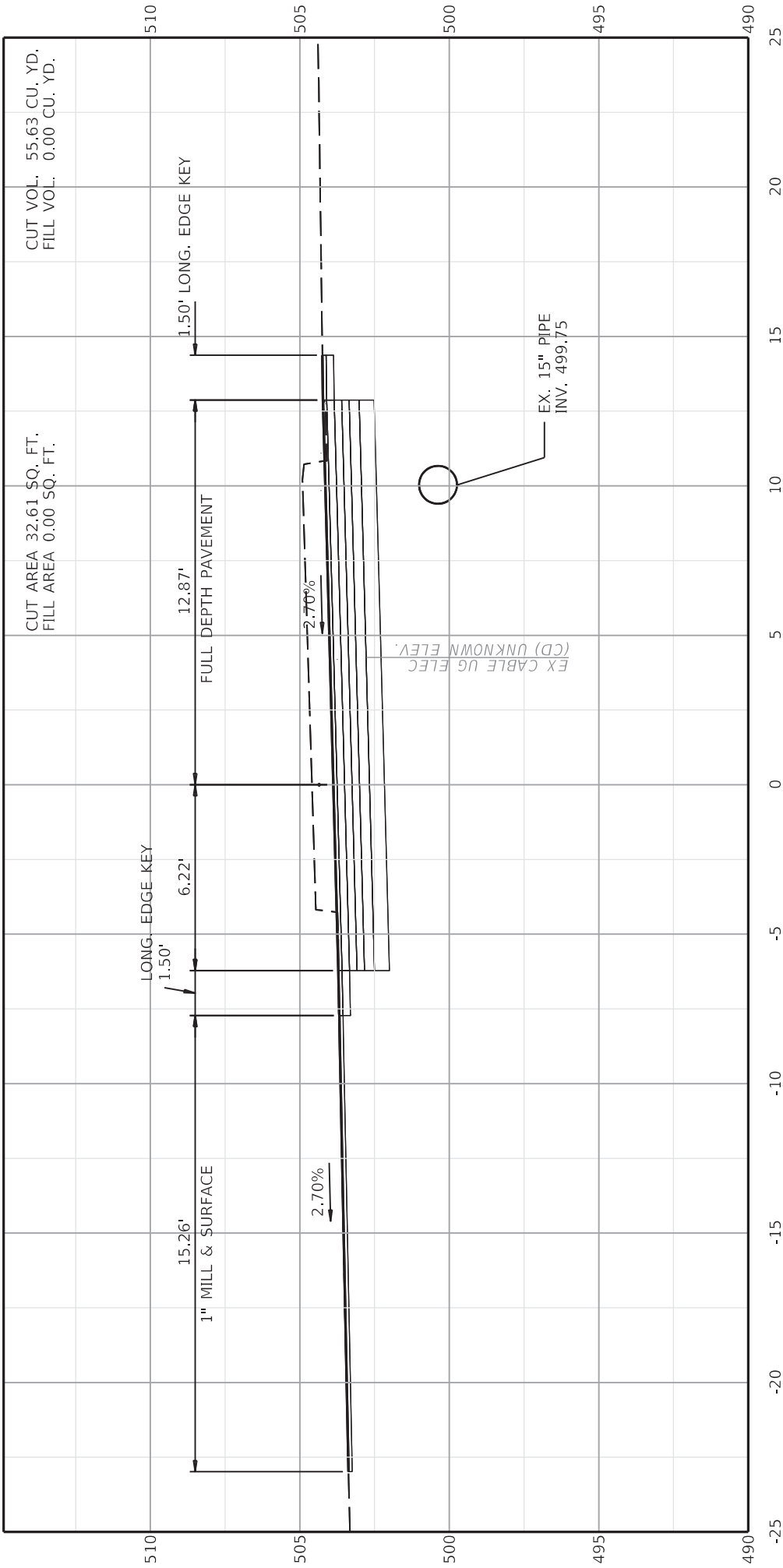
37+00.00

	COMMONWEALTH OF KENTUCKY DEPARTMENT OF HIGHWAYS	DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS STA: 37+00.00 TO STA: 37+00.00		HORIZONTAL SCALE: 1"=5'	
		ITEM NO. 03-80318.00	COUNTY OF WARREN	SHEET NO. X12	



37+50.00

	COMMONWEALTH OF KENTUCKY DEPARTMENT OF HIGHWAYS	DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS STA. 37+50.00 TO STA. 37+50.00	HORIZONTAL SCALE: 1"=5'	
			ITEM NO. 03-80318.00	COUNTY OF WARREN
			SHEET NO. X13	



COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

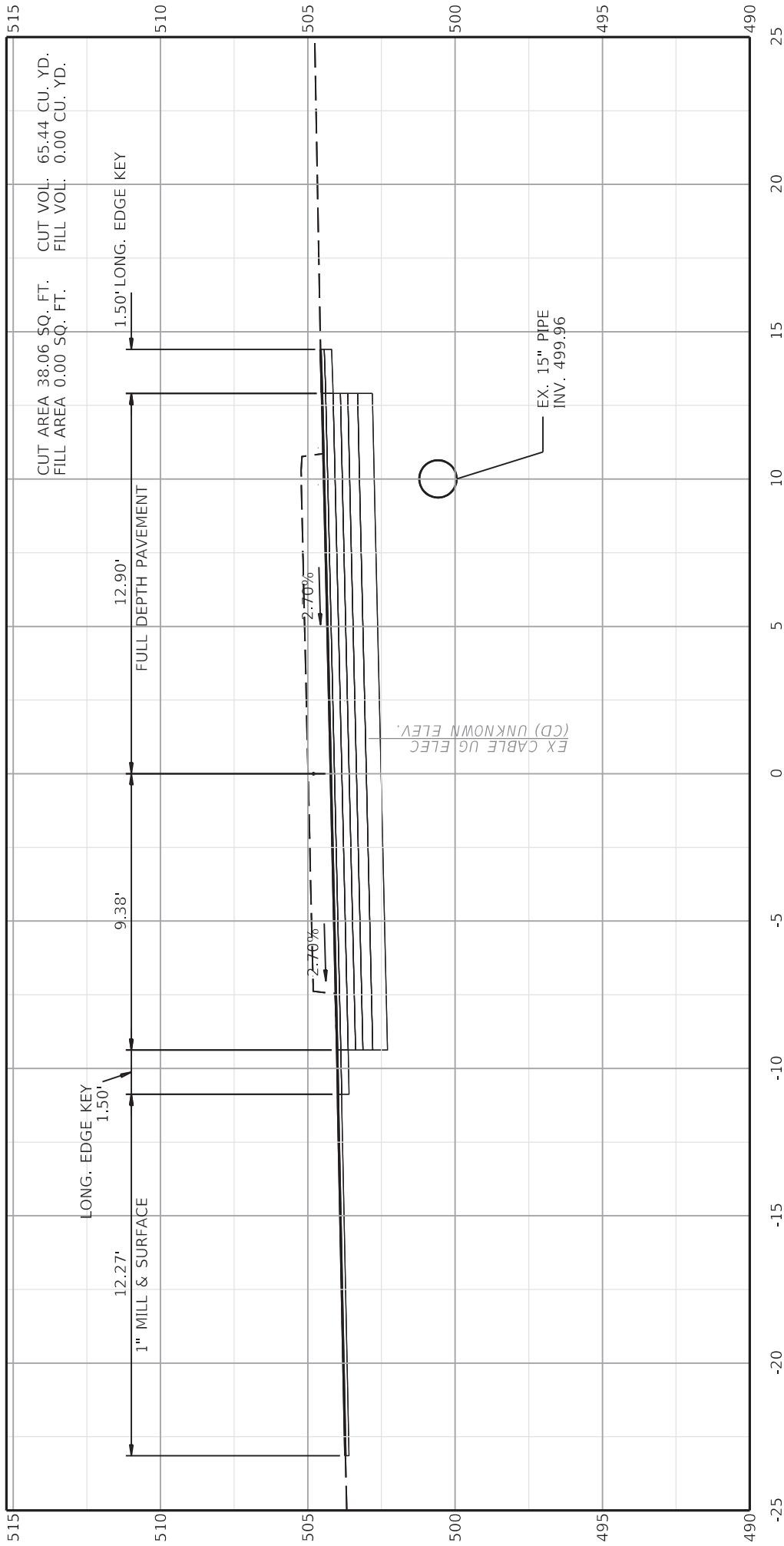
DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS
STA. 38+00.00 TO STA. 38+00.00

HORIZONTAL SCALE: 1"=5'

ITEM NO.
03-80318.00

COUNTY OF
WARREN

SHEET NO.
X14



COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

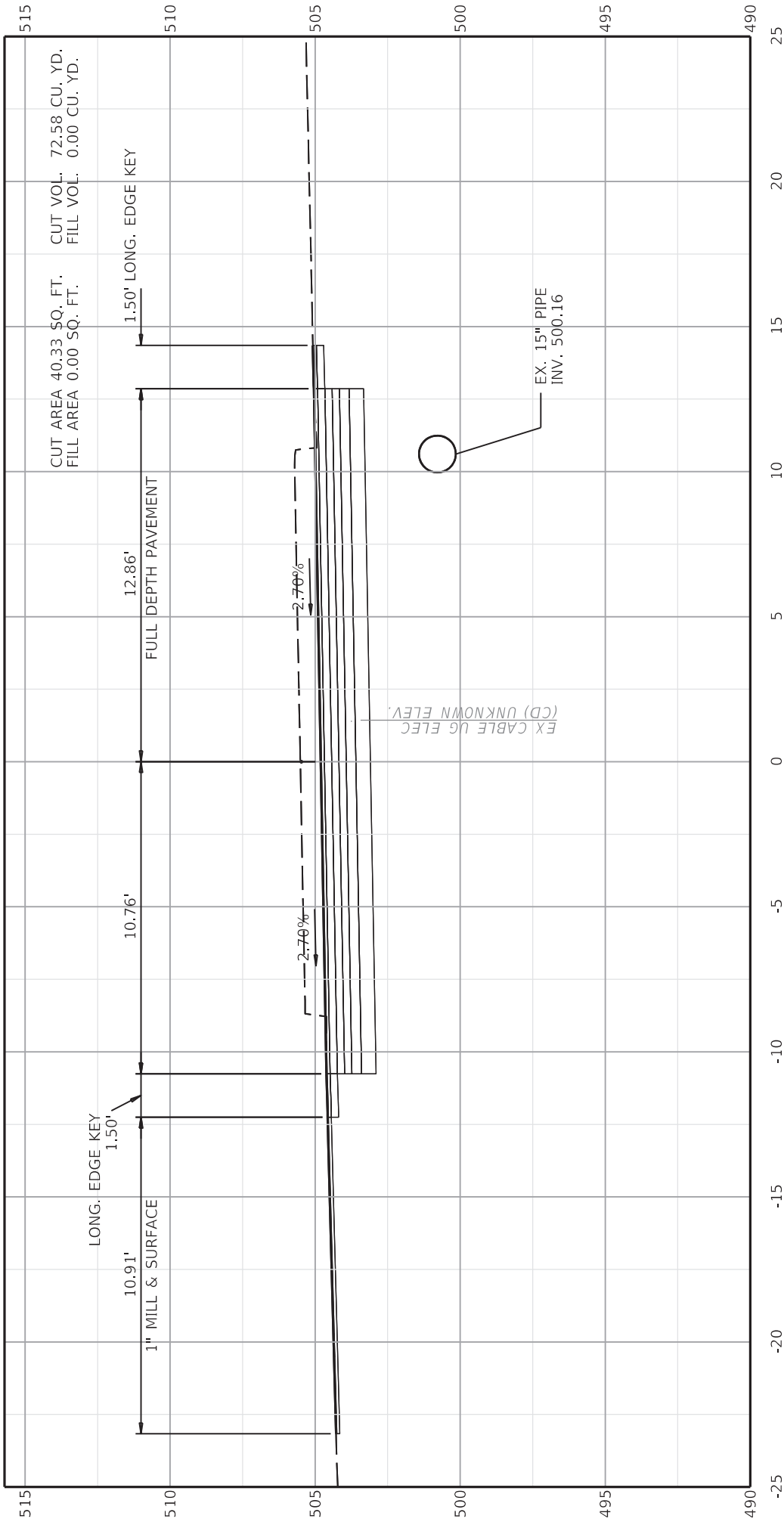
DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS
STA. 38+50.00 TO STA. 38+50.00

ITEM NO.
03-80318.00

COUNTY OF
WARREN

SHEET NO. X15

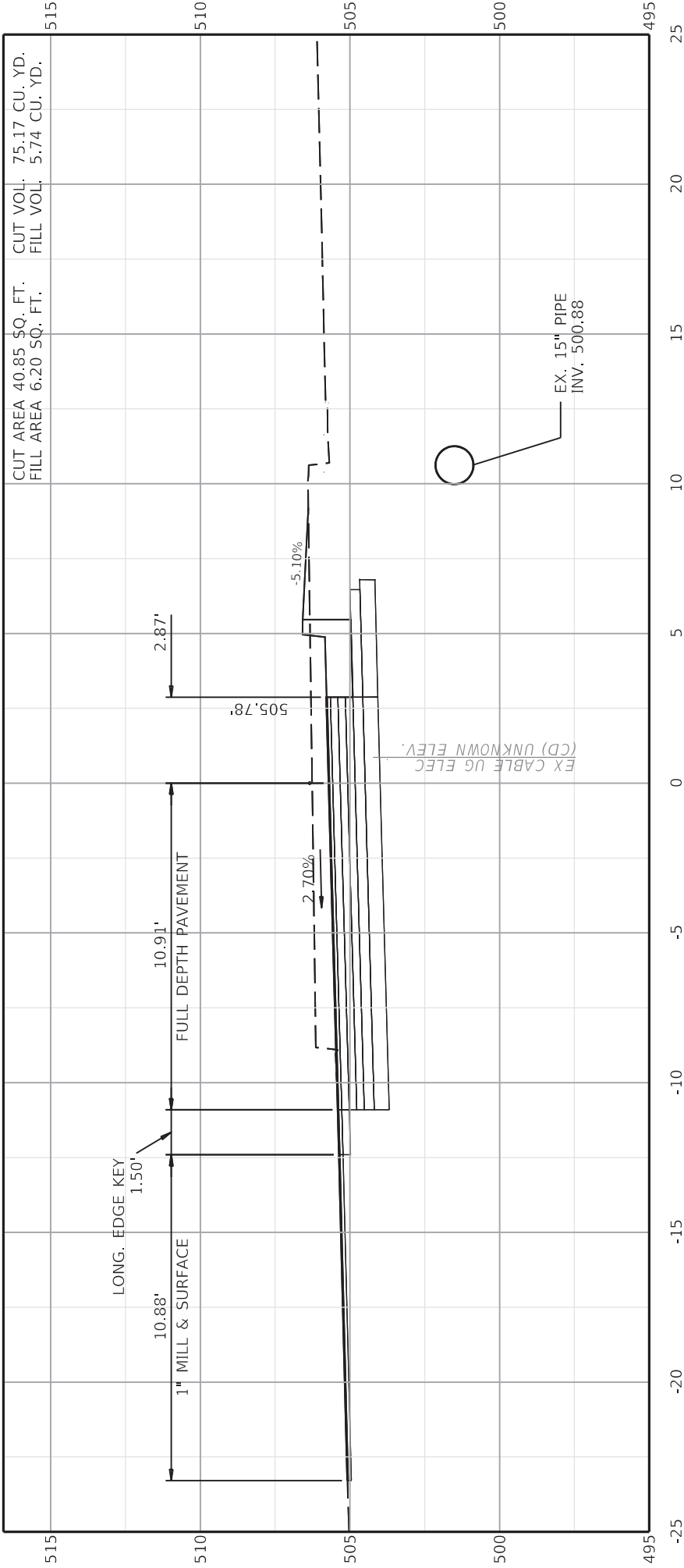
HORIZONTAL SCALE: 1"=5'



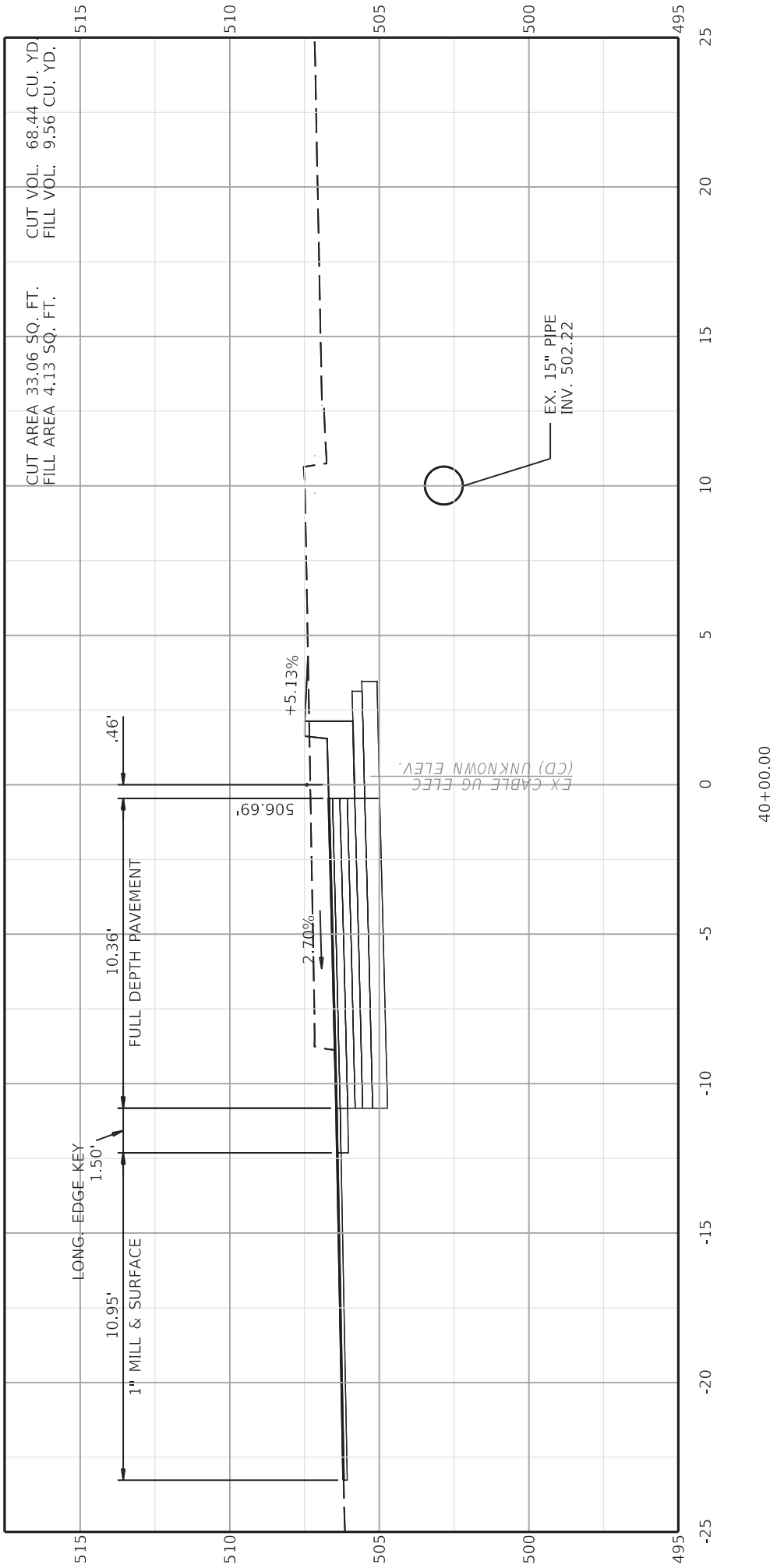
39+00.00

 COMMONWEALTH OF KENTUCKY DEPARTMENT OF HIGHWAYS	DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS		ITEM NO. 03-80318.00 SHEET NO. X16	COUNTY OF WARREN
	STA: 39+00.00 TO STA: 39+00.00			

Horizontal Scale: 1"=5'



 COMMONWEALTH OF KENTUCKY DEPARTMENT OF HIGHWAYS	 TEAM KENTUCKY <small>DESIGNED BY KENTUCKY</small>	DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS		ITEM NO. 03-80318.00	COUNTY OF WARREN
		STA. 39+50.00 TO STA. 39+50.00			
		HORIZONTAL SCALE: 1"=5'		SHEET NO. X17	





COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

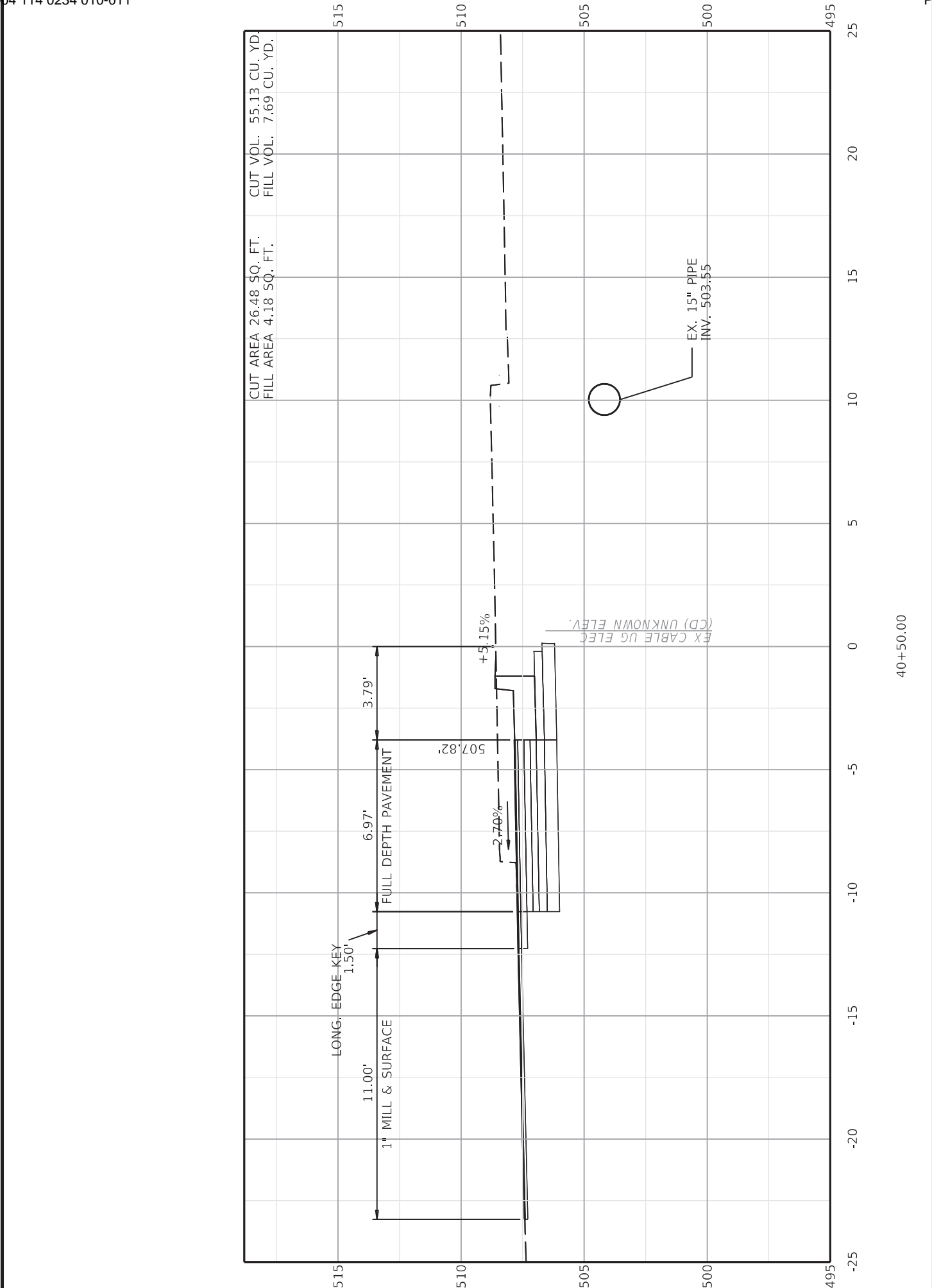
TEAM KENTUCKY
Infrastructure

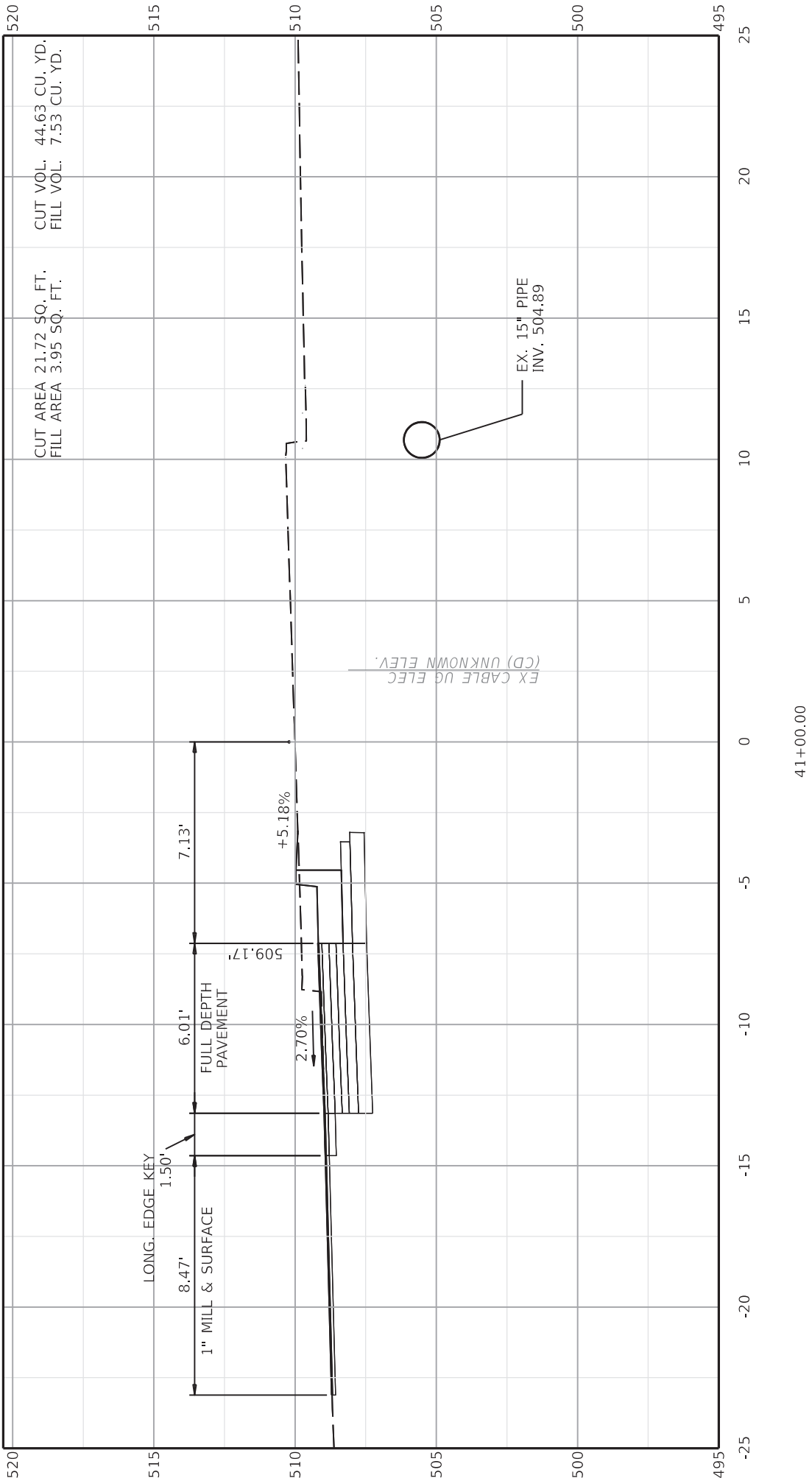
OpenRoads Designer v23.00.01.11
FILE NAME: C:\P\WORK\STEWART.L\CHD241933903_80318_00_X_SECTIONS_L\T_TURN.DGN

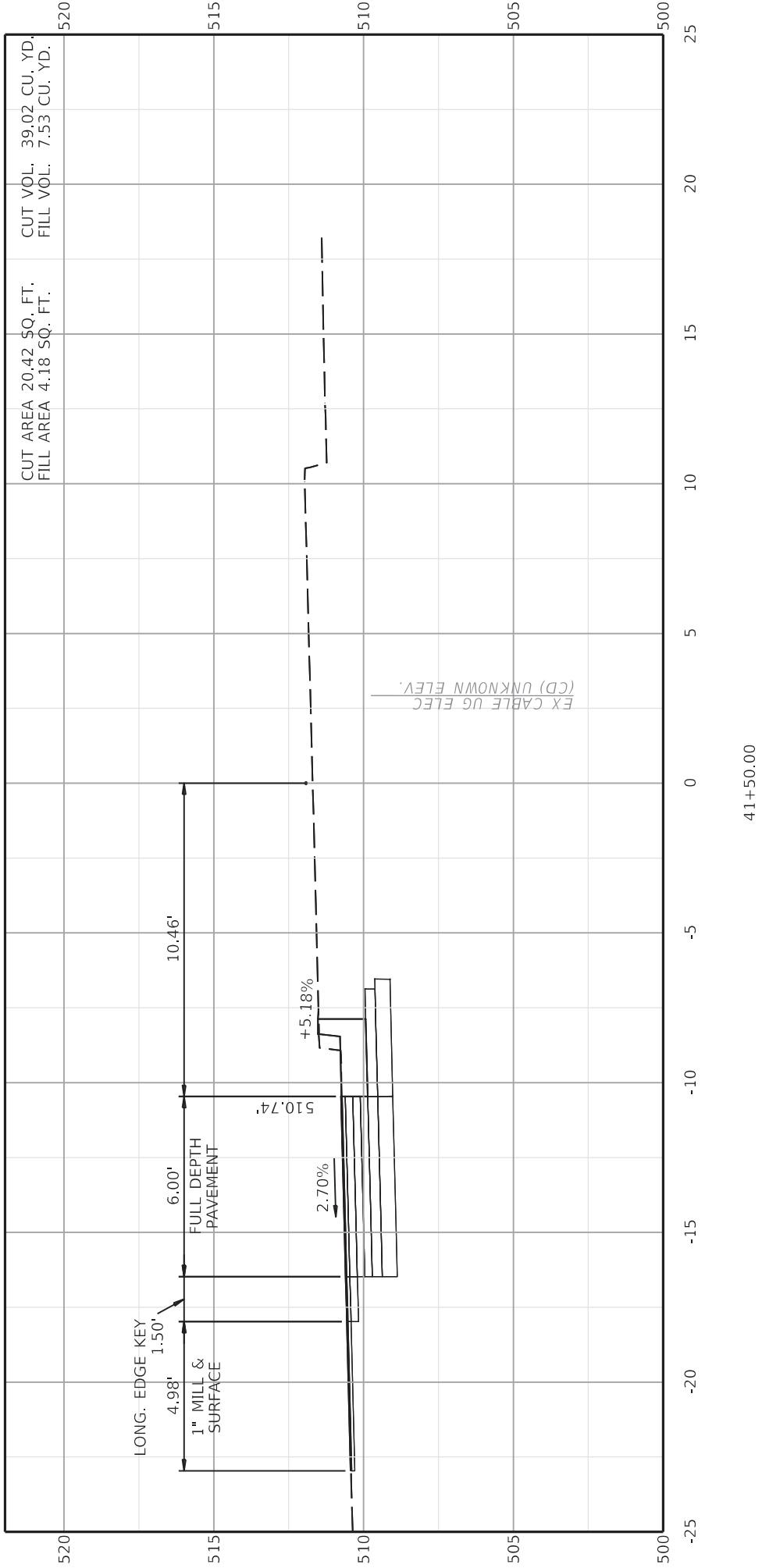
DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS
STA. 40+00.00 TO STA. 40+00.00

ITEM NO. 03-80318.00
COUNTY OF WARREN
SHEET NO. X18

HORIZONTAL SCALE: 1"=5'







DRAWING TITLE: LEFT TURN LANE CROSS SECTIONS

STA. 41+50.00 TO STA. 41+50.00

HORIZONTAL SCALE: 1"=5'

ITEM NO.	COUNTY OF
03-80318.00	WARREN
SHEET NO.	X21

COMMONWEALTH OF KENTUCKY
DEPARTMENT OF HIGHWAYS

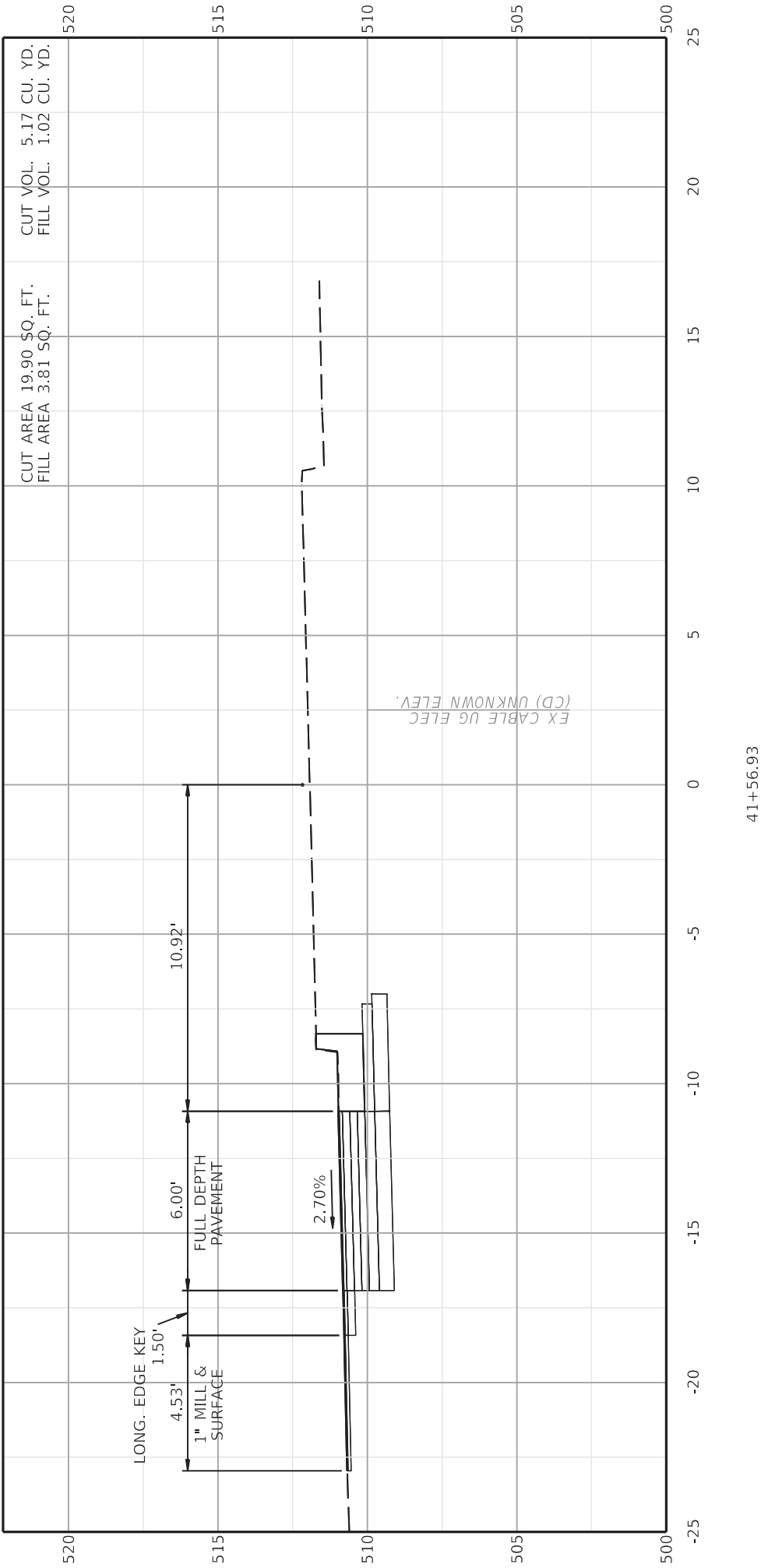


PROJECT TOTALS

CUT VOL. 1361 CU. YD.
FILL VOL. 41 CU. YD.

LEFT TURN LANE TOTALS

CUT VOL. 578 CU. YD.
FILL VOL. 40 CU. YD.



SPECIAL NOTE FOR ELECTRONIC DELIVERY MANAGEMENT SYSTEM (e-Ticketing) ASPHALT

This Special Note will apply when indicated on the plans or in the proposal. Section references herein are to the Department's Standard Specifications for Road and Bridge Construction current edition.

1.0 DESCRIPTION. Incorporate an e-Ticketing Delivery Software for weighed asphalt material delivered to the project to report loads and provide daily running totals of weighed asphalt material for pay items and incidental work during the construction processes from the point of measurement and loading to the point of incorporation to the project.

2.0 MATERIALS AND EQUIPMENT. Contractor shall supply material data in JavaScript Object Notation (JSON) documents to the KYTC e-Ticketing Delivery Software (KYTC e-Ticketing Portal) via Application Programming Interface (API) or direct connection. Test and verify that ticket data can be shared from the original source no fewer than 30 days prior to material placement activities. An e-Ticketing Delivery Software supplier can provide a qualified representative for on-site technical assistance during the initial setup, pre-construction verifications, and data management and processing as needed during the Project to maintain material data delivery capabilities. Virtual meetings may be hosted in lieu of on-site meetings when deemed appropriate by the Engineer.

Provide e-Ticketing Delivery Software that will meet the following:

1. The e-Ticketing Delivery Software shall be fully integrated with the Contractor's Load Read-Out scale system at the material source location.
2. The e-Ticketing Delivery Software shall provide real-time delivery to KYTC e-Ticketing Portal.
3. Transmit any updates to the ticket data within 5 minutes of a change.

3.0 CONSTRUCTION. Provide the Engineer with the manufacturer's specifications and all required documentation for data access at the pre-construction conference.

A. Construction Requirements

1. Install and operate software in accordance with the manufacturer's specifications.
2. Verify that all pertinent information is provided by the software within the requirements of this Special Note.

B. Data Deliverables

Provide to the Engineer a means in which to gather report summaries by way of iOS apps, web pages, or any other method at the disposal of the Engineer. The Engineer may request data at any time during the project.

1. Asphalt Material

a. Real-time Continuous Data Items

Provide the Engineer access to JSON documents capable of being transmitted through the KYTC's e-Ticketing Portal that displays the following information in real-time with a web-based system compatible with iOS and Windows environments.

- Each Truck
 - Supplier Name
 - Supplier Address
 - Supplier Phone
 - Plant location
 - Date
 - Time at source
 - Project Location

- Contract ID#
- Carrier Name
- Unique Truck ID
- Description of Material
- Mix Design Number
- Gross, Tare and Net Weight
- Weighmaster

4.0 MEASUREMENT. The Department will not measure the electronic delivery management system.

5.0 PAYMENT. The Department will not measure this work for payment and will consider all items contained in this note to be incidental to the asphalt mixtures on the project, as applicable.

May 5, 2025

SPECIAL NOTE FOR ELECTRONIC DELIVERY MANAGEMENT SYSTEM (e-Ticketing) AGGREGATE

This Special Note will apply when indicated on the plans or in the proposal. Section references herein are to the Department's Standard Specifications for Road and Bridge Construction current edition.

1.0 DESCRIPTION. Incorporate an e-Ticketing Delivery Software for weighed aggregate material delivered to the project to report loads and provide daily running totals of weighed aggregate material for pay items and incidental work during the construction processes from the point of measurement and loading to the point of incorporation to the project.

2.0 MATERIALS AND EQUIPMENT. Contractor shall supply material data in JavaScript Object Notation (JSON) documents to the KYTC e-Ticketing Delivery Software (KYTC e-Ticketing Portal) via Application Programming Interface (API) or direct connection. Test and verify that ticket data can be shared from the original source no fewer than 30 days prior to material placement activities. An e-Ticketing Delivery Software supplier can provide a qualified representative for on-site technical assistance during the initial setup, pre-construction verifications, and data management and processing as needed during the Project to maintain material data delivery capabilities. Virtual meetings may be hosted in lieu of on-site meetings when deemed appropriate by the Engineer.

Provide e-Ticketing Delivery Software that will meet the following:

1. The e-Ticketing Delivery Software shall be fully integrated with the Contractor's Load Read-Out scale system at the material source location.
2. The e-Ticketing Delivery Software shall provide real-time delivery to KYTC e-Ticketing Portal.
3. Transmit any updates to the ticket data within 5 minutes of a change.

3.0 CONSTRUCTION. Provide the Engineer with the manufacturer's specifications and all required documentation for data access at the pre-construction conference.

A. Construction Requirements

1. Install and operate software in accordance with the manufacturer's specifications.
2. Verify that all pertinent information is provided by the software within the requirements of this Special Note.

B. Data Deliverables

Provide to the Engineer a means in which to gather report summaries by way of iOS apps, web pages, or any other method at the disposal of the Engineer. The Engineer may request data at any time during the project.

1. Aggregate Material

a. Real-time Continuous Data Items

Provide the Engineer access to JSON documents capable of being transmitted through the KYTC's e-Ticketing Portal that displays the following information in real-time with a web-based system compatible with iOS and Windows environments.

- Each Truck
 - Supplier Name
 - Supplier Address
 - Supplier Phone
 - Plant location
 - Date
 - Time at source
 - Project Location

- Contract ID#
- Carrier Name
- Unique Truck ID
- Description of Material
- Load Number
- Gross, Tare and Net Weight
- Weighmaster

4.0 MEASUREMENT. The Department will measure the electronic delivery management system as a lump sum item.

5.0 PAYMENT. The Department will make payment for the completed and accepted quantities under the following:

1. Payment is full compensation for all work associated with providing all required equipment, training, and documentation.
2. Payment will be full compensation for costs related to providing the e-Ticketing Delivery Software, including integration with plant load-out systems, and report viewing/exporting process. All quality control procedures including the software representative’s technical support and on-site training shall be included in the Contract lump sum price.

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
26248EC	ELECTRONIC DELIVERY MGMT SYSTEM-AGG	LS

May 5, 2025

SPECIAL NOTE FOR DOLOMITIC POLISH-RESISTANT AGGREGATE IN CLASS A 0.38-IN. AND 0.50-IN. NOMINAL ASPHALT MIXTURES

Contrary to Subsection 403.03.03, when utilizing a dolomitic polish-resistant aggregate as the coarse portion of the Class A 0.38-in. or 0.50-in.-nominal asphalt surface mixture, provide an asphalt mixture conforming to the following requirements:

- 70 percent of total combined aggregate is Class A polish-resistant aggregate.
- Any coarse aggregate utilized in the mixture shall be classified as Class A polish-resistant.
- Non-dolomitic substitutes from other Class A sources may be used as direct substitutes
- All mixes must have DFT testing/results submitted to Division of Materials with any supporting documentation prior to completion of the project.

Dynamic Friction Testing Procedure. Prepare samples for DFT analysis in accordance with PP 104. Friction testing shall be conducted by an AASHTO-accredited facility and data shall be provided in accordance with ASTM E1911 conforming to the following three-wheel polishing schedule. Variations to the testing frequency or methodology shall be coordinated with Division of Materials prior to testing.

<i>Polishing Cycles</i>
5,000
25,000
75,000
150,000

SPECIAL NOTE FOR DOUBLE ASPHALT SEAL COAT

Use RS-2 or RS-2C asphalt material that is compatible with the seal aggregate. Apply the first course of asphalt seal coat at the rate of 3.2 lbs/sy of asphalt and 30 lbs/sy of size #78 seal coat aggregate. Apply the second course at 2.8 lbs/sy of asphalt and 20 lbs/sy of size #9M seal coat aggregate. The Engineer may adjust the rate of application as conditions warrant. Use caution in applying liquid asphalt material to avoid over spray getting on curbs, gutter, barrier walls, bridges, guardrail, and other roadway appurtenances.

The Department will not measure any surface preparation required prior to applying the asphalt seal coat, but shall be incidental to “Asphalt Material for Asphalt Seal Coat”.

1-3215 Double Asphalt Seal Coat
01/02/2012



KENTUCKY TRANSPORTATION CABINET
Department of Highways
DIVISION OF RIGHT OF WAY & UTILITIES

TC 62-226
Rev. 01/2016
Page 1 of 1

RIGHT OF WAY CERTIFICATION

<input checked="" type="checkbox"/>	Original	<input type="checkbox"/>	Re-Certification	RIGHT OF WAY CERTIFICATION	
ITEM #		COUNTY		PROJECT # (STATE)	PROJECT # (FEDERAL)
3-80318.00		Warren		1100 FD04 114 1834501D	
PROJECT DESCRIPTION					
KY 234 Left and Right Turn Lane Extensions at Lovers Lane					
<input checked="" type="checkbox"/>	No Additional Right of Way Required				
Construction will be within the limits of the existing right of way. The right of way was acquired in accordance to FHWA regulations under the Uniform Relocation Assistance and Real Property Acquisitions Policy Act of 1970, as amended. No additional right of way or relocation assistance were required for this project.					
<input type="checkbox"/>	Condition # 1 (Additional Right of Way Required and Cleared)				
All necessary right of way, including control of access rights when applicable, have been acquired including legal and physical possession. Trial or appeal of cases may be pending in court but legal possession has been obtained. There may be some improvements remaining on the right-of-way, but all occupants have vacated the lands and improvements, and KYTC has physical possession and the rights to remove, salvage, or demolish all improvements and enter on all land. Just Compensation has been paid or deposited with the court. All relocations have been relocated to decent, safe, and sanitary housing or that KYTC has made available to displaced persons adequate replacement housing in accordance with the provisions of the current FHWA directive.					
<input type="checkbox"/>	Condition # 2 (Additional Right of Way Required with Exception)				
The right of way has not been fully acquired, the right to occupy and to use all rights-of-way required for the proper execution of the project has been acquired. Some parcels may be pending in court and on other parcels full legal possession has not been obtained, but right of entry has been obtained, the occupants of all lands and improvements have vacated, and KYTC has physical possession and right to remove, salvage, or demolish all improvements. Just Compensation has been paid or deposited with the court for most parcels. Just Compensation for all pending parcels will be paid or deposited with the court prior to AWARD of construction contract					
<input type="checkbox"/>	Condition # 3 (Additional Right of Way Required with Exception)				
The acquisition or right of occupancy and use of a few remaining parcels are not complete and/or some parcels still have occupants. All remaining occupants have had replacement housing made available to them in accordance with 49 CFR 24.204. KYTC is hereby requesting authorization to advertise this project for bids and to proceed with bid letting even though the necessary right of way will not be fully acquired, and/or some occupants will not be relocated, and/or the just compensation will not be paid or deposited with the court for some parcels until after bid letting. KYTC will fully meet all the requirements outlined in 23 CFR 635.309(c)(3) and 49 CFR 24.102(j) and will expedite completion of all acquisitions, relocations, and full payments after bid letting and prior to AWARD of the construction contract or force account construction.					
Total Number of Parcels on Project		0	EXCEPTION (S) Parcel #		ANTICIPATED DATE OF POSSESSION WITH EXPLANATION
Number of Parcels That Have Been Acquired					
Signed Deed					
Condemnation					
Signed ROE					
Notes/ Comments (Text is limited. Use additional sheet if necessary.)					
LPA RW Project Manager			Right of Way Supervisor		
Printed Name				Printed Name	
Signature				Signature	
Date				Date	
Right of Way Director			FHWA		
Printed Name				Printed Name	
Signature				Signature	
Date				Date	

Digitally signed by Kelly Divine
Date: 2025.05.16 10:20:38
-05'00'

Mike Russell
5/15/2025

UTILITIES AND RAIL CERTIFICATION NOTE

<p>WARREN COUNTY NO FEDERAL NUMBER STATE PROJECT NUMBER: FD04 114 0234 010-011 MILE POINT: 10.200 TO 10.800 SCOPE: REDUCE CONGESTION AND INCREASE MOBILITY ON KY 234 FROM THE EXIT 26 INTERCHANGE WITH I 65 TO KY 880. (2024CCN) ITEM NUMBER: 03-80318.00</p>
--

PROJECT NOTES ON UTILITIES

Please Note: The information presented in this Utility Note is informational in nature and the information contained herein is not guaranteed.

The contractor will be responsible for contacting all utility facility owners on the subject project to coordinate his activities. The contractor will coordinate his activities to minimize and, where possible, avoid conflicts with utility facilities. Due to the nature of the work proposed, it is unlikely to conflict with the existing utilities beyond minor facility adjustments. Where conflicts with utility facilities are unavoidable, the contractor will coordinate any necessary relocation work with the facility owner and Resident Engineer. The Kentucky Transportation Cabinet maintains the right to remove or alter portions of this contract if a utility conflict occurs. The utility facilities as noted in the previous section(s) have been determined using data garnered by varied means and with varying degrees of accuracy: from the facility owners, a result of S.U.E., field inspections, and/or reviews of record drawings. The facilities defined may not be inclusive of all utilities in the project scope and are not Level A quality, unless specified as such. It is the contractor's responsibility to verify all utilities and their respective locations before excavating.

The contractor shall make every effort to protect underground facilities from damage as prescribed in the Underground Facility Damage Protection Act of 1994, Kentucky Revised Statute KRS 367.4901 to 367.4917. It is the contractor's responsibility to determine and take steps necessary to be in compliance with federal and state damage prevention directives. The contractor is instructed to contact KY 811 for the location of existing underground utilities. Contact shall be made a minimum of two (2) and no more than ten (10) business days prior to excavation. The contractor shall submit Excavation Locate Requests to the Kentucky Contact Center (KY 811) via web ticket entry. The submission of this request does not relieve the contractor from the responsibility of contacting non-member facility owners, whom are to be contacted through their individual Protection Notification Center.

UTILITIES AND RAIL CERTIFICATION NOTE

<p>WARREN COUNTY NO FEDERAL NUMBER STATE PROJECT NUMBER: FD04 114 0234 010-011 MILE POINT: 10.200 TO 10.800 SCOPE: REDUCE CONGESTION AND INCREASE MOBILITY ON KY 234 FROM THE EXIT 26 INTERCHANGE WITH I 65 TO KY 880. (2024CCN) ITEM NUMBER: 03-80318.00</p>
--

PROJECT NOTES ON UTILITIES Continued

It may be necessary for the contractor to contact the County Court Clerk to determine what utility companies have facilities in the area. Non-compliance with these directives can result in the enforcement of penalties.

Utility coordination efforts determined that no significant utility relocation work is required to complete the project. Any work pertaining to these utility facilities is defined in the bid package and is to be carried out as instructed by the Kentucky Transportation Cabinet. The contractor will be responsible for any coordination or adjustments that are discussed or quantified in the proposal.

**NOTE: DO NOT DISTURB THE FOLLOWING FACILITIES LOCATED WITHIN THE PROJECT
DISTURB LIMITS**

Bowling Green Municipal Utilities - Electric

Bowling Green Municipal Utilities - Water and Sewer

The Contractor is fully responsible for protection of all utilities listed above

**THE FOLLOWING FACILITY OWNERS ARE RELOCATING/ADJUSTING THEIR FACILITIES
WITHIN THE PROJECT LIMITS AND WILL BE COMPLETE PRIOR TO CONSTRUCTION**

Not Applicable

UTILITIES AND RAIL CERTIFICATION NOTE

WARREN COUNTY

NO FEDERAL NUMBER

STATE PROJECT NUMBER: FD04 114 0234 010-011

MILE POINT: 10.200 TO 10.800

SCOPE: REDUCE CONGESTION AND INCREASE MOBILITY ON KY 234
FROM THE EXIT 26 INTERCHANGE WITH I 65 TO KY 880. (2024CCN)

ITEM NUMBER: 03-80318.00

THE FOLLOWING FACILITY OWNERS HAVE FACILITIES TO BE RELOCATED/ADJUSTED BY THE OWNER OR THEIR SUBCONTRACTOR AND IS TO BE COORDINATED WITH THE ROAD CONTRACT

Not Applicable

THE FOLLOWING FACILITY OWNERS HAVE FACILITIES TO BE RELOCATED/ADJUSTED BY THE ROAD CONTRACTOR AS INCLUDED IN THIS CONTRACT

Not Applicable

RAIL COMPANIES HAVE FACILITIES IN CONJUNCTION WITH THIS PROJECT AS NOTED

☒ No Rail Involvement ☐ Rail Involved ☐ Rail Adjacent

AREA FACILITY OWNER CONTACT LIST

Facility Owner	Address	Contact Name	Phone	Email
Bowling Green Municipal Utilities - Electric	P.O. Box 10300 Bowling Green KY 42102	Eric Phillips	2707827386	eric.phillips@bgmu.com
Bowling Green Municipal Utilities - Water and Sewer	P.O. Box 10300 Bowling Green KY 42102	Rodney Sullivan	2707821200	rodney.sullivan@bgmu.com

PART II

SPECIFICATIONS AND STANDARD DRAWINGS

STANDARD SPECIFICATIONS

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2019* and *Standard Drawings, Edition of 2020*.

SUPPLEMENTAL SPECIFICATIONS

The contractor shall use the Supplemental Specifications that are effective at the time of letting. The Supplemental Specifications can be found at the following link:
<http://transportation.ky.gov/Construction/Pages/Kentucky-Standard-Specifications.aspx>

SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS

This Special Note will apply when indicated on the plans or in the proposal.

1.0 DESCRIPTION. Furnish, install, operate, and maintain variable message signs at the locations shown on the plans or designated by the Engineer. Remove and retain possession of variable message signs when they are no longer needed on the project.

2.0 MATERIALS.

2.1 General. Use LED Variable Message Signs Class I, II, or III, as appropriate, from the Department's List of Approved Materials.

Unclassified signs may be submitted for approval by the Engineer. The Engineer may require a daytime and nighttime demonstration. The Engineer will make a final decision within 30 days after all required information is received.

2.2 Sign and Controls. All signs must:

- 1) Provide 3-line messages with each line being 8 characters long and at least 18 inches tall. Each character comprises 35 pixels.
- 2) Provide at least 40 preprogrammed messages available for use at any time. Provide for quick and easy change of the displayed message; editing of the message; and additions of new messages.
- 3) Provide a controller consisting of:
 - a) Keyboard or keypad.
 - b) Readout that mimics the actual sign display. (When LCD or LCD type readout is used, include backlighting and heating or otherwise arrange for viewing in cold temperatures.)
 - c) Non-volatile memory or suitable memory with battery backup for storing pre-programmed messages.
 - d) Logic circuitry to control the sequence of messages and flash rate.
- 4) Provide a serial interface that is capable of supporting complete remote control ability through land line and cellular telephone operation. Include communication software capable of immediately updating the message, providing complete sign status, and allowing message library queries and updates.
- 5) Allow a single person easily to raise the sign to a satisfactory height above the pavement during use, and lower the sign during travel.
- 6) Be Highway Orange on all exterior surfaces of the trailer, supports, and controller cabinet.
- 7) Provide operation in ambient temperatures from -30 to + 120 degrees Fahrenheit during snow, rain and other inclement weather.
- 8) Provide the driver board as part of a module. All modules are interchangeable, and have plug and socket arrangements for disconnection and reconnection. Printed circuit boards associated with driver boards have a conformable coating to protect against moisture.
- 9) Provide a sign case sealed against rain, snow, dust, insects, etc. The lens is UV stabilized clear plastic (polycarbonate, acrylic, or other approved material) angled to prevent glare.
- 10) Provide a flat black UV protected coating on the sign hardware, character PCB, and appropriate lens areas.
- 11) Provide a photocell control to provide automatic dimming.

11

- 12) Allow an on-off flashing sequence at an adjustable rate.
- 13) Provide a sight to aim the message.
- 14) Provide a LED display color of approximately 590 nm amber.
- 15) Provide a controller that is password protected.
- 16) Provide a security device that prevents unauthorized individuals from accessing the controller.
- 17) Provide the following 3-line messages preprogrammed and available for use when the sign unit begins operation:

/KEEP/RIGHT/⇒⇒⇒/	/MIN/SPEED/**MPH/
/KEEP/LEFT/⇐⇐⇐/	/ICY/BRIDGE/AHEAD/ /ONE
/LOOSE/GRAVEL/AHEAD/	LANE/BRIDGE/AHEAD/
/RD WORK/NEXT/**MILES/	/ROUGH/ROAD/AHEAD/
/TWO WAY/TRAFFIC/AHEAD/	/MERGING/TRAFFIC/AHEAD/
/PAINT/CREW/AHEAD/	/NEXT/***/MILES/
/REDUCE/SPEED/**MPH/	/HEAVY/TRAFFIC/AHEAD/
/BRIDGE/WORK/***() FT/	/SPEED/LIMIT/**MPH/
/MAX/SPEED/**MPH/	/BUMP/AHEAD/
/SURVEY/PARTY/AHEAD/	/TWO/WAY/TRAFFIC/

*Insert numerals as directed by the Engineer.

Add other messages during the project when required by the Engineer.

2.3 Power.

- 1) Design solar panels to yield 10 percent or greater additional charge than sign consumption. Provide direct wiring for operation of the sign or arrow board from an external power source to provide energy backup for 21 days without sunlight and an on-board system charger with the ability to recharge completely discharged batteries in 24 hours.

3.0 CONSTRUCTION. Furnish and operate the variable message signs as designated on the plans or by the Engineer. Ensure the bottom of the message panel is a minimum of 7 feet above the roadway in urban areas and 5 feet above in rural areas when operating. Use Class I, II, or III signs on roads with a speed limit less than 55 mph. Use Class I or II signs on roads with speed limits 55 mph or greater.

Maintain the sign in proper working order, including repair of any damage done by others, until completion of the project. When the sign becomes inoperative, immediately repair or replace the sign. Repetitive problems with the same unit will be cause for rejection and replacement.

Use only project related messages and messages directed by the Engineer, unnecessary messages lessen the impact of the sign. Ensure the message is displayed in either one or 2 phases with each phase having no more than 3 lines of text. When no message is needed, but it is necessary to know if the sign is operable, flash only a pixel.

When the sign is not needed, move it outside the clear zone or where the Engineer directs. Variable Message Signs are the property of the Contractor and shall be removed from the project when no longer needed. The Department will not assume ownership of these signs.

4.0 MEASUREMENT. The final quantity of Variable Message Sign will be

11
the actual number of individual signs acceptably furnished and operated during the project. The Department will not measure signs replaced due to damage or rejection.

5.0 PAYMENT. The Department will pay for the Variable Message Signs at the unit price each. The Department will not pay for signs replaced due to damage or rejection. Payment is full compensation for furnishing all materials, labor, equipment, and service necessary to, operate, move, repair, and maintain or replace the variable message signs. The Department will make payment for the completed and accepted quantities under the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
02671	Portable Changeable Message Sign	Each

Effective June 15, 2012

PART III

EMPLOYMENT, WAGE AND RECORD REQUIREMENTS

**TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS**

**LABOR AND WAGE REQUIREMENTS
APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS**

- I. Application
- II. Nondiscrimination of Employees (KRS 344)

I. APPLICATION

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual because of his race, color, religion, national origin, sex, disability or age forty (40) and over, in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

Revised: January 25, 2017

II. NONDISCRIMINATION OF EMPLOYEES

**AN ACT OF THE KENTUCKY
GENERAL ASSEMBLY TO PREVENT
DISCRIMINATION IN EMPLOYMENT
KRS CHAPTER 344
EFFECTIVE JUNE 16, 1972**

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (forty and above); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age forty (40) and over. The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, except that such a notice or advertisement may indicate a preference, limitation, or specification based on religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, when religion, national origin, sex, or age forty (40) and over, or because the person is a qualified individual with a disability, is a bona fide occupational qualification for employment.

EXECUTIVE BRANCH CODE OF ETHICS

The Executive Branch Code of Ethics created by Kentucky Revised Statutes (KRS) Chapter 11A, effective July 14, 1992, establishes the ethical standards that govern the conduct of all executive branch employees. The Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (7) provides:

A present or former public servant listed in KRS 11A.010(9)(a) to (g) shall not, within one (1) year following termination of his or her office or employment, accept employment, compensation, or other economic benefit from any person or business that contracts or does business with, or is regulated by, the state in matters in which he was directly involved during the last thirty-six (36) months of his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, or for which he received, prior to his state employment, a professional degree or license, provided that, for a period of one (1) year, he or she personally refrains from working on any matter in which he was directly involved during the last thirty-six (36) months of his or her tenure in state government. This subsection shall not prohibit the performance of ministerial functions, including but not limited to filing tax returns, filing applications for permits or licenses, or filing incorporation papers, nor shall it prohibit the former officer or public servant from receiving public funds disbursed through entitlement programs.

KRS 11A.040 (9) states:

A former public servant shall not represent a person or business before a state agency in a matter in which the former public servant was directly involved during the last thirty-six (36) months of his tenure, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not to obtain private benefits.

If you have worked for the executive branch of state government within the past year, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, 1025 Capital Center Drive, Suite 105, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Revised: March 11, 2025

Kentucky Equal Employment Opportunity Act of 1978

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:
<https://www.eProcurement.ky.gov>.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **finance.contractcompliance@ky.gov** or by phone at 502-564-2874.

EMPLOYEE RIGHTS UNDER THE FAIR LABOR STANDARDS ACT

THE UNITED STATES DEPARTMENT OF LABOR WAGE AND HOUR DIVISION

FEDERAL MINIMUM WAGE

\$7.25

 PER HOUR

BEGINNING JULY 24, 2009

OVERTIME PAY

At least $1\frac{1}{2}$ times your regular rate of pay for all hours worked over 40 in a workweek.

CHILD LABOR

An employee must be at least **16** years old to work in most non-farm jobs and at least **18** to work in non-farm jobs declared hazardous by the Secretary of Labor.

Youths **14** and **15** years old may work outside school hours in various non-manufacturing, non-mining, non-hazardous jobs under the following conditions:

No more than

- **3** hours on a school day or **18** hours in a school week;
- **8** hours on a non-school day or **40** hours in a non-school week.

Also, work may not begin before **7 a.m.** or end after **7 p.m.**, except from June 1 through Labor Day, when evening hours are extended to **9 p.m.** Different rules apply in agricultural employment.

TIP CREDIT

Employers of "tipped employees" must pay a cash wage of at least \$2.13 per hour if they claim a tip credit against their minimum wage obligation. If an employee's tips combined with the employer's cash wage of at least \$2.13 per hour do not equal the minimum hourly wage, the employer must make up the difference. Certain other conditions must also be met.

ENFORCEMENT

The Department of Labor may recover back wages either administratively or through court action, for the employees that have been underpaid in violation of the law. Violations may result in civil or criminal action.

Employers may be assessed civil money penalties of up to \$1,100 for each willful or repeated violation of the minimum wage or overtime pay provisions of the law and up to \$11,000 for each employee who is the subject of a violation of the Act's child labor provisions. In addition, a civil money penalty of up to \$50,000 may be assessed for each child labor violation that causes the death or serious injury of any minor employee, and such assessments may be doubled, up to \$100,000, when the violations are determined to be willful or repeated. The law also prohibits discriminating against or discharging workers who file a complaint or participate in any proceeding under the Act.

ADDITIONAL INFORMATION

- Certain occupations and establishments are exempt from the minimum wage and/or overtime pay provisions.
- Special provisions apply to workers in American Samoa and the Commonwealth of the Northern Mariana Islands.
- Some state laws provide greater employee protections; employers must comply with both.
- The law requires employers to display this poster where employees can readily see it.
- Employees under 20 years of age may be paid \$4.25 per hour during their first 90 consecutive calendar days of employment with an employer.
- Certain full-time students, student learners, apprentices, and workers with disabilities may be paid less than the minimum wage under special certificates issued by the Department of Labor.

For additional information:



1-866-4-USWAGE

(1-866-487-9243)

TTY: 1-877-889-5627



WWW.WAGEHOUR.DOL.GOV

PART IV

BID ITEMS

Section: 0001 - PAVING

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0010	00001		DGA BASE	312.00	TON		\$	
0020	00003		CRUSHED STONE BASE	564.00	TON		\$	
0030	00078		CRUSHED AGGREGATE SIZE NO 2	796.00	TON		\$	
0040	00100		ASPHALT SEAL AGGREGATE	4.00	TON		\$	
0050	00103		ASPHALT SEAL COAT	1.00	TON		\$	
0060	00212		CL2 ASPH BASE 1.00D PG64-22	593.00	TON		\$	
0070	00356		ASPHALT MATERIAL FOR TACK	3.00	TON		\$	
0080	22906ES403		CL3 ASPH SURF 0.38A PG64-22	308.00	TON		\$	

Section: 0002 - ROADWAY

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0090	01791		ADJUST MANHOLE FRAME TO GRADE	1.00	EACH		\$	
0100	02159		TEMP DITCH	200.00	LF		\$	
0110	02160		CLEAN TEMP DITCH	100.00	LF		\$	
0120	02200		ROADWAY EXCAVATION	1,361.00	CUYD		\$	
0130	02242		WATER	1.00	MGAL		\$	
0140	02545		CLEARING AND GRUBBING 0.43 ACRES	1.00	LS		\$	
0150	02562		TEMPORARY SIGNS	600.00	SQFT		\$	
0160	02604		FABRIC-GEOTEXTILE CLASS 1A	4,684.00	SQYD		\$	
0170	02650		MAINTAIN & CONTROL TRAFFIC	1.00	LS		\$	
0180	02671		PORTABLE CHANGEABLE MESSAGE SIGN	4.00	EACH		\$	
0190	02676		MOBILIZATION FOR MILL & TEXT	1.00	LS		\$	
0200	02677		ASPHALT PAVE MILLING & TEXTURING	132.00	TON		\$	
0210	02701		TEMP SILT FENCE	200.00	LF		\$	
0220	02703		SILT TRAP TYPE A	1.00	EACH		\$	
0230	02704		SILT TRAP TYPE B	1.00	EACH		\$	
0240	02705		SILT TRAP TYPE C	1.00	EACH		\$	
0250	02706		CLEAN SILT TRAP TYPE A	1.00	EACH		\$	
0260	02707		CLEAN SILT TRAP TYPE B	1.00	EACH		\$	
0270	02708		CLEAN SILT TRAP TYPE C	1.00	EACH		\$	
0280	02726		STAKING	1.00	LS		\$	
0290	02775		ARROW PANEL	2.00	EACH		\$	
0300	05950		EROSION CONTROL BLANKET	259.00	SQYD		\$	
0310	05952		TEMP MULCH	781.00	SQYD		\$	
0320	05953		TEMP SEEDING AND PROTECTION	583.00	SQYD		\$	
0330	05963		INITIAL FERTILIZER	.04	TON		\$	
0340	05964		MAINTENANCE FERTILIZER	.07	TON		\$	
0350	05992		AGRICULTURAL LIMESTONE	.84	TON		\$	
0360	06510		PAVE STRIPING-TEMP PAINT-4 IN	4,986.00	LF		\$	
0370	06542		PAVE STRIPING-THERMO-6 IN W	2,493.00	LF		\$	
0380	06543		PAVE STRIPING-THERMO-6 IN Y	1,454.00	LF		\$	
0390	06549		PAVE STRIPING-TEMP REM TAPE-B	600.00	LF		\$	
0400	06550		PAVE STRIPING-TEMP REM TAPE-W	600.00	LF		\$	
0410	06568		PAVE MARKING-THERMO STOP BAR-24IN	41.00	LF		\$	

Report Date 5/23/25

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0420	06569		PAVE MARKING-THERMO CROSS-HATCH	54.00	SQFT		\$	
0430	06574		PAVE MARKING-THERMO CURV ARROW	21.00	EACH		\$	
0440	06610		INLAID PAVEMENT MARKER-MW	42.00	EACH		\$	
0450	06612		INLAID PAVEMENT MARKER-BY	25.00	EACH		\$	
0460	08100		CONCRETE-CLASS A	.30	CUYD		\$	
0470	08150		STEEL REINFORCEMENT	36.00	LB		\$	
0480	20071EC		JOINT ADHESIVE	2,877.00	LF		\$	
0490	20478ND		FRAME AND LID TY 2	1.00	EACH		\$	
0500	21289ED		LONGITUDINAL EDGE KEY	1,275.00	LF		\$	
0510	21373ND		REMOVE SIGN	2.00	EACH		\$	
0520	24109EC		BARRIER CURB AND GUTTER-MOD	249.00	LF		\$	
0530	26248EC		ELECTRONIC DELIVERY MGMT SYSTEM - AGG	1.00	LS		\$	

Section: 0003 - LIGHTING

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0540	04740		POLE BASE	4.00	EACH		\$	
0550	04794		CONDUIT-1 1/2 IN	522.00	LF		\$	
0560	04810		ELECTRICAL JUNCTION BOX	1.00	EACH		\$	
0570	22939ND		INSTALL LUMINAIRE POLE	4.00	EACH		\$	

Section: 0004 - DEMOBILIZATION &/OR MOBILIZATION

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRIC	FP	AMOUNT
0580	02569		DEMOBILIZATION	1.00	LS		\$	