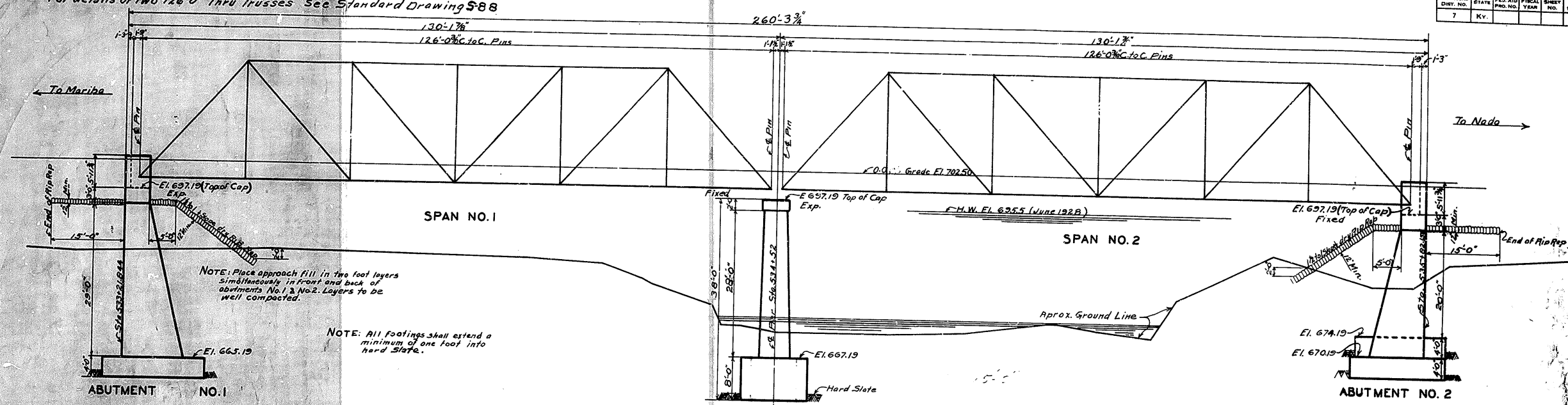


REFERENCE: For details of Abutments No.1 & No.2 see Sheet No.2
 For details of Pier see Sheet No.3
 For details of two 126'-0" Thru Trusses see Standard Drawing S-88

FED. ROAD DIST. NO.	STATE	FED. AID FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	Ky.			



NOTE: Place approach fill in two foot layers simultaneously in front and back of abutments No. 1 & No. 2. Layers to be well compacted.

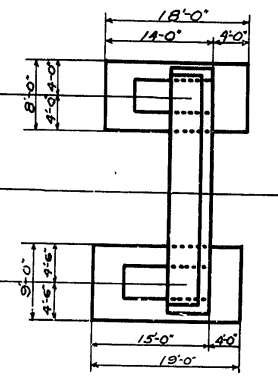
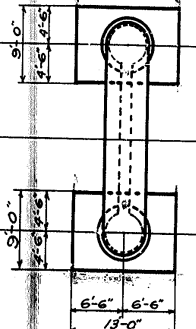
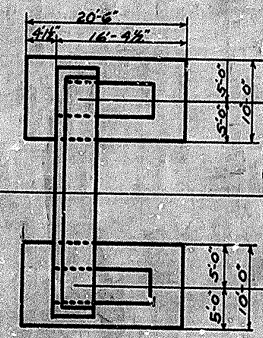
NOTE: All footings shall extend a minimum of one foot into hard slate.

ABUTMENT NO. 1

PIER NO. 1

ABUTMENT NO. 2

ELEVATION
 2-126'-0" SPANS - 20'-0" ROADWAY



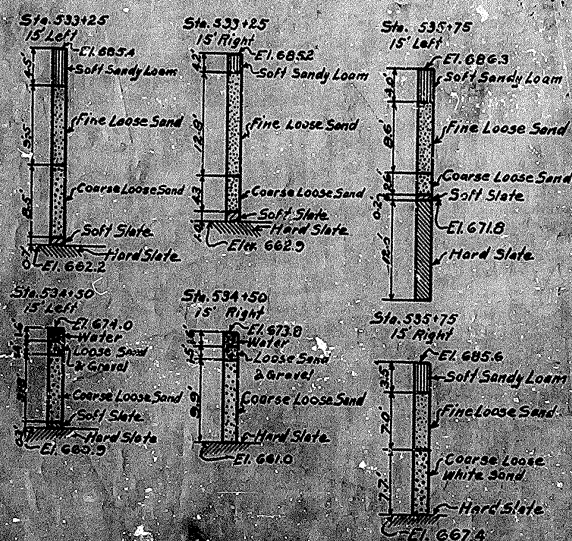
PLAN OF SUB STRUCTURE

GENERAL NOTE

Specifications: State Highway Department, Kentucky 1932, with Amendments.
 Concrete: Class "A" concrete shall be used thruout in Abutments, Pier and Floor Slab.
 Reinforcement: To be deformed bars. Dimensions from face of concrete are clear distances. Dimensions for bar spacing are center to center of bars. Bars shall be wired at all intersections, and held in correct position by precast mortar or concrete blocks, spaced not more than 50 diameters of supported bar.
 Wearing Surface: Top of concrete slab shall be finished in accordance with specifications for "Monolithic Concrete Wearing Surface".
 Beveled Edges: All exposed concrete edges to be chamfered unless otherwise shown on plans.
 Extra Labor: The substructure contractor shall not be paid for extra labor connected with the the preparation of bearing areas.
 Paint: Shop coat on all steel work to be Red Lead or Basic Sulphate Blue Lead.
 Expansion Joint Filler: This item furnished by superstructure contractor shall be included in the price bid per cu. yd. for Class "A" concrete. Copper Strip shall be included in price bid for Class "A" concrete.
 Standard Plans: Notes or requirements shown on Standard Plans for Thru Trusses in conflict with requirements shown hereon or with 1932 specifications are to be disregarded, and 1932 specifications and notes shown hereon shall govern.
 Boring Data: The subsurface data shown hereon was obtained by borings at the locations indicated and represents the best information available to the State Highway Commission as to the conditions existing at the site of work. This data is furnished for information only, and bidders must draw their own conclusions as to the conditions to be encountered. The State Highway Commission does not give any guarantee as to the accuracy of the data furnished or as to conditions which will actually be found to exist when the work is being executed.
 Anchor Bolts: Anchor bolts shall be furnished and set by superstructure contractor.
 Expansion Angles: Expansion Angles on parapet walls are to be furnished by the superstructure contractor, and placed by the substructure contractor. Contractor shall notify the Commission at least 60 days in advance of need of these angles.

TOTAL ESTIMATE OF QUANTITIES

SUPERSTRUCTURE			
STRUCTURAL STEEL	285700.	LBS.	
CONCRETE CLASS A	132.8	CU. YDS.	5-47
REINFORCEMENT	32130	LBS.	5-85
SUBSTRUCTURE			
CONCRETE CLASS A	435.9	CU. YDS.	
REINFORCEMENT	36380	LBS.	5-88
EXCAVATION (UNCLASSIFIED)	1800	CU. YDS.	
DRY RIP RAP - SEE ROAD PLANS.			



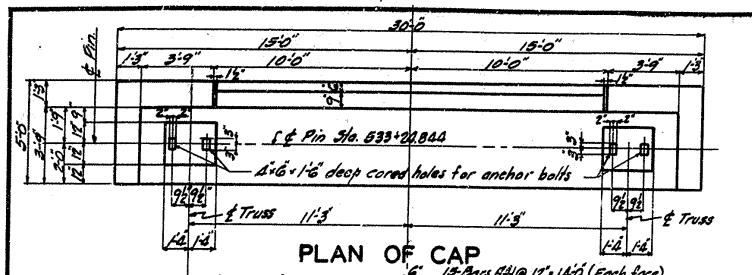
BORING DATA

SHEET 1. OF 3.

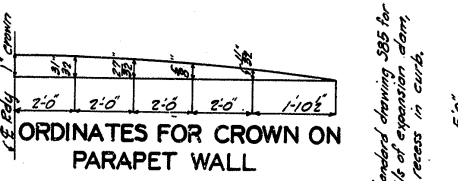
BRIDGE OVER RED RIVER.

COMMONWEALTH OF KENTUCKY
 STATE HIGHWAY DEPARTMENT
 FRANKFORT
 COUNTY OF
POWELL-MENIFEE
 MARIBA-NADA
 ROAD PROJECT NO.
 STATION 534+52

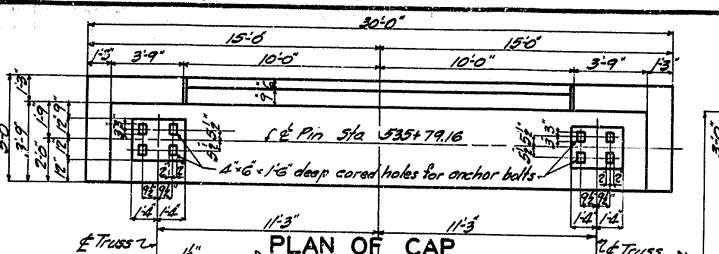
PSR. ROAD DIST. NO.	PLATE	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	KV.			



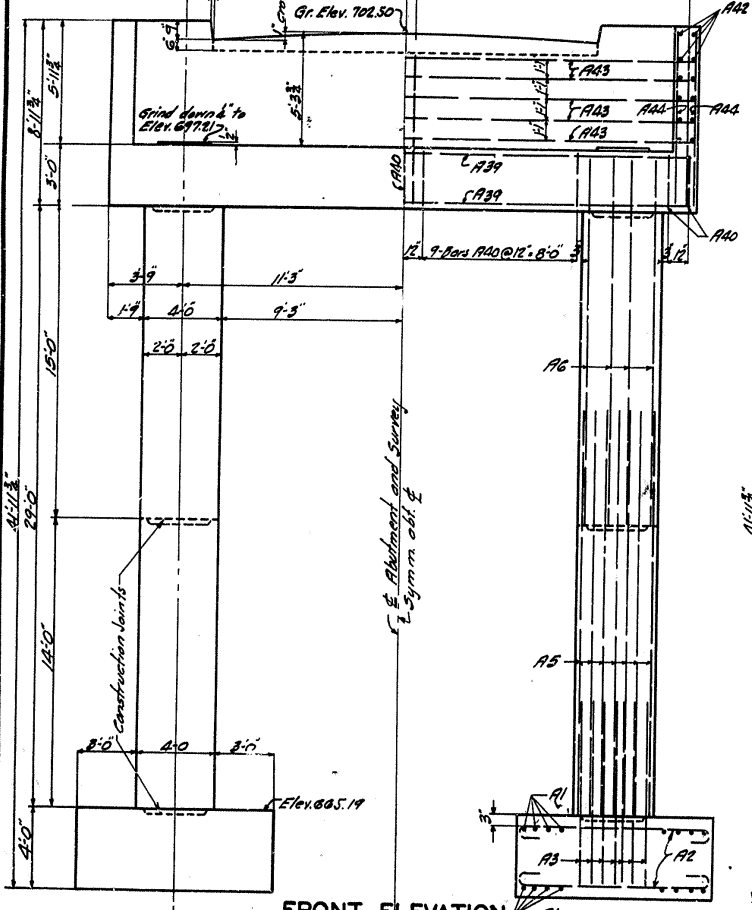
PLAN OF CAP



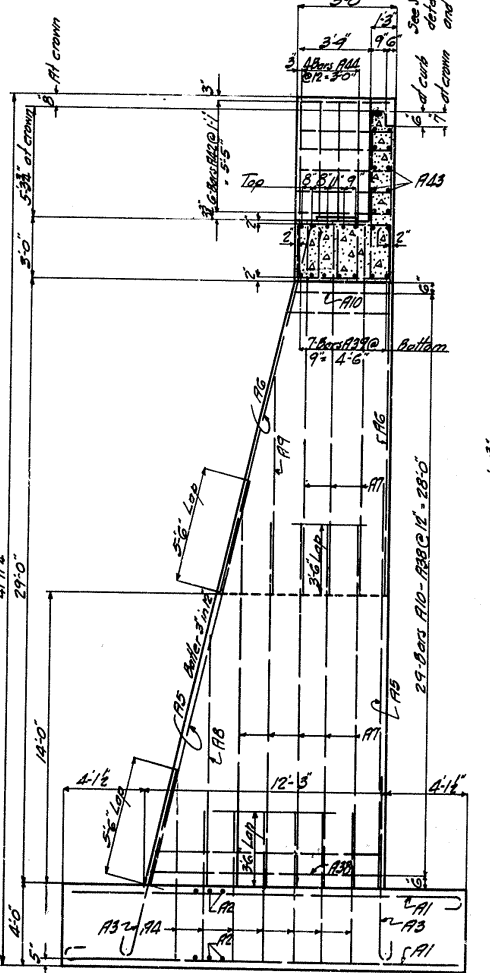
ORDINATES FOR CROWN ON PARAPET WALL



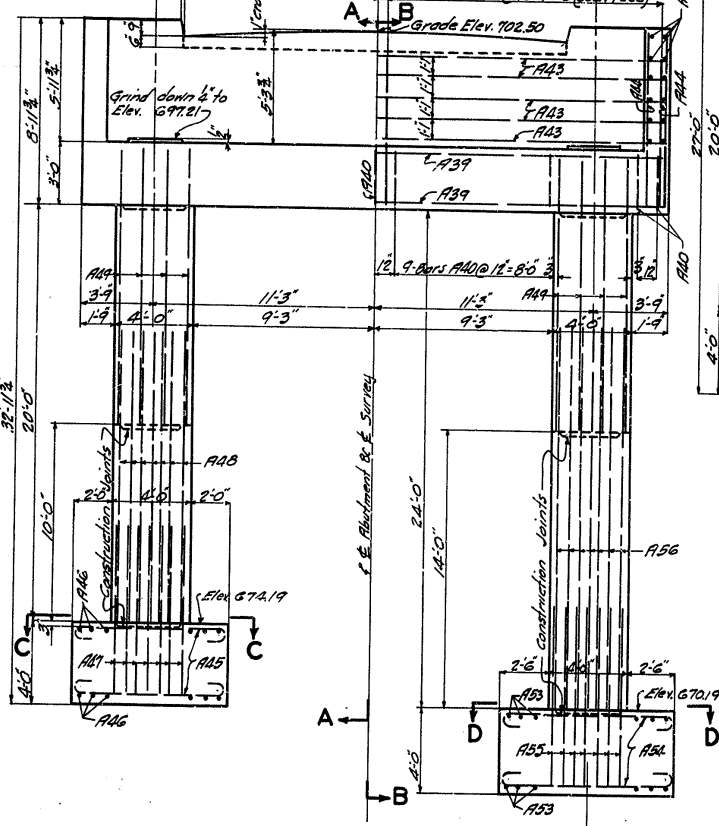
PLAN OF CAP



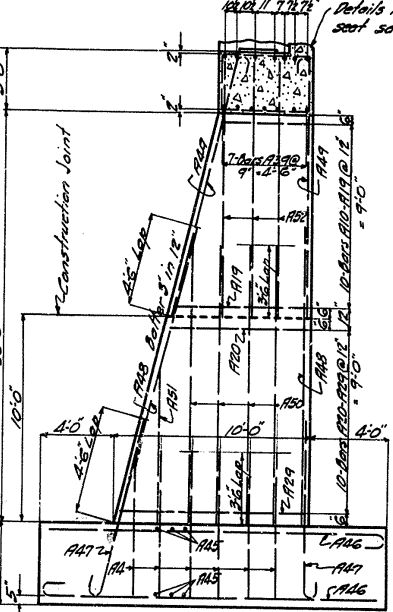
FRONT ELEVATION ABUTMENT NO. 1



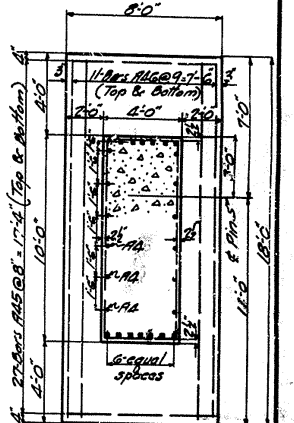
FRONT ELEVATION ABUTMENT NO. 2



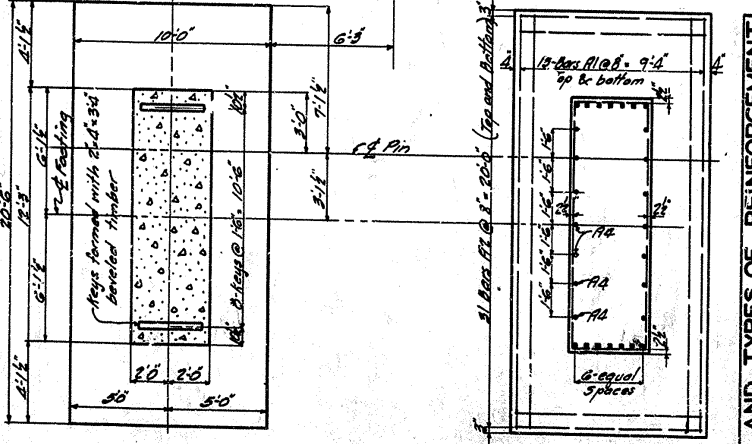
SECTION A-A



SECTION B-B



SECTION C-C



PLAN OF FOOTING

ITEM NO.	LOCATION	TYPE	BILL AND TYPES OF REINFORCEMENT		QUANTITY	REMARKS
			NO. 1	NO. 2		
1	Footings	TYPE 1	1	1	1	
2	Columns	TYPE 2	1	1	1	
3	Cap	TYPE 3	1	1	1	
4	Parapet	TYPE 4	1	1	1	
5	Reinforcing	TYPE 5	1	1	1	
6	Columns	TYPE 6	1	1	1	
7	Columns	TYPE 7	1	1	1	
8	Columns	TYPE 8	1	1	1	
9	Columns	TYPE 9	1	1	1	
10	Columns	TYPE 10	1	1	1	
11	Columns	TYPE 11	1	1	1	
12	Columns	TYPE 12	1	1	1	
13	Columns	TYPE 13	1	1	1	
14	Columns	TYPE 14	1	1	1	
15	Columns	TYPE 15	1	1	1	
16	Columns	TYPE 16	1	1	1	
17	Columns	TYPE 17	1	1	1	
18	Columns	TYPE 18	1	1	1	
19	Columns	TYPE 19	1	1	1	
20	Columns	TYPE 20	1	1	1	
21	Columns	TYPE 21	1	1	1	
22	Columns	TYPE 22	1	1	1	
23	Columns	TYPE 23	1	1	1	
24	Columns	TYPE 24	1	1	1	
25	Columns	TYPE 25	1	1	1	
26	Columns	TYPE 26	1	1	1	
27	Columns	TYPE 27	1	1	1	
28	Columns	TYPE 28	1	1	1	
29	Columns	TYPE 29	1	1	1	
30	Columns	TYPE 30	1	1	1	
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98	Columns	TYPE 98	1	1	1	
99	Columns	TYPE 99	1	1	1	
100	Columns	TYPE 100	1	1	1	

ESTIMATE OF QUANTITIES	
Concrete Class A	161.0 Cu. Yds.
Reinforcement	1200.0 Lbs.
ABUTMENT NO. 2	
Concrete Class A	1235 Cu. Yds.
Reinforcement	1114.0 Lbs.

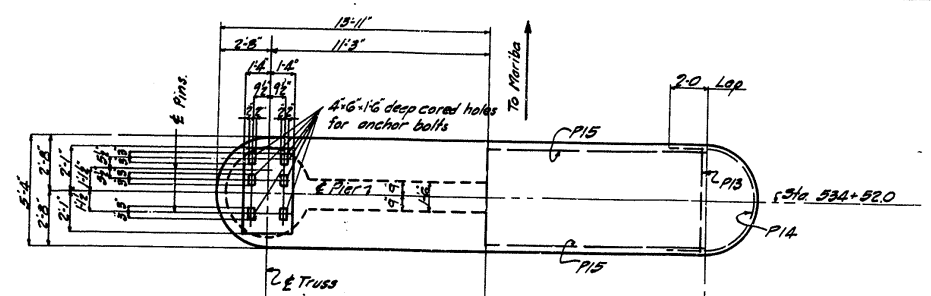
SHEET 2 OF 3
Red River Bridge

COMMONWEALTH OF KENTUCKY
STATE HIGHWAY DEPARTMENT
FRANKFORT COUNTY

POWELL-MENIFEE
MARIBA - NADA

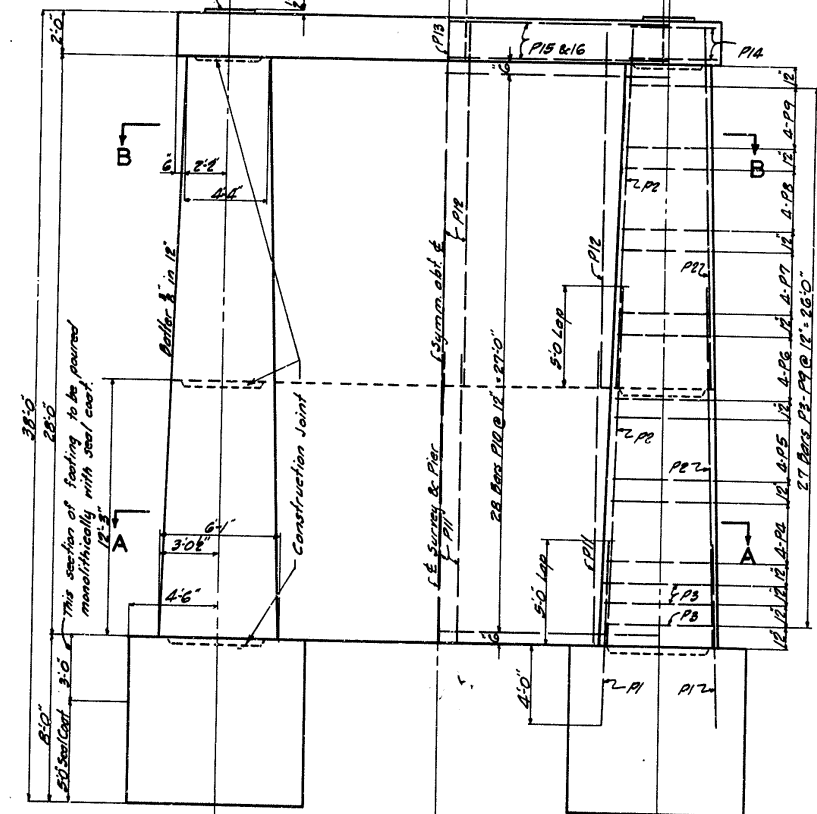
STATION 534 + 2 PROJECT NO. 99-40-2

REVISIONS BY DATE BY
 1. DATE: 1-2-54 BY: M. Meniffee
 2. DATE: 2-2-54 BY: M. Meniffee
 3. DATE: 7-2-54 BY: M. Meniffee

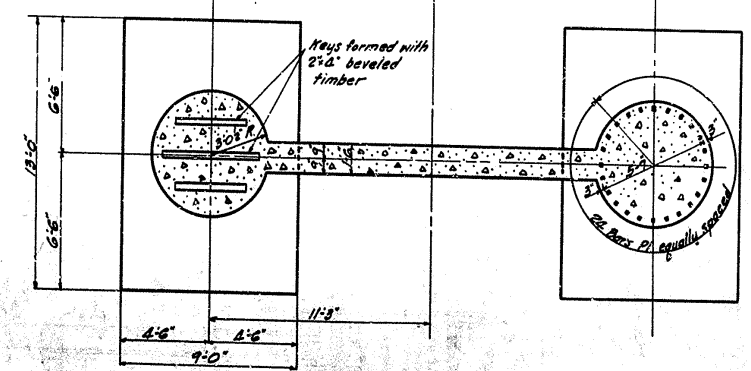


PLAN OF CAP

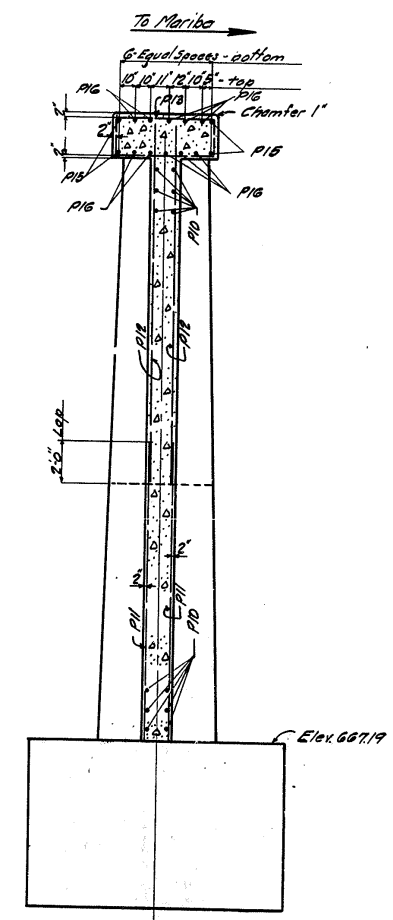
Block to be poured monolithically with cap and 2' higher. Grind down to Elev. 697.21



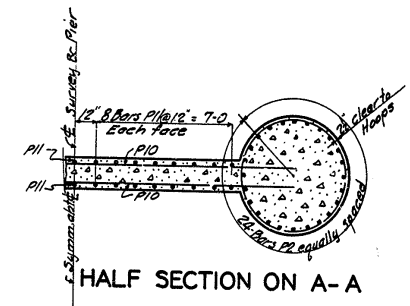
ELEVATION



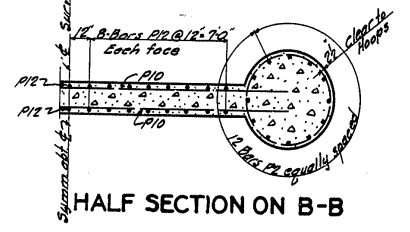
FOOTING PLAN



SECTION ON C



HALF SECTION ON A-A



HALF SECTION ON B-B

BILL AND TYPES OF REINFORCEMENT					
Mark	Type	Quantity	Unit	Location	Notes
P1	Str. 10	9.0		Columns	
P2	Str. 10	17.3		Columns	
P3	①	19.6		Columns	5.7
P4	①	18.9		Columns	3.4
P5	①	18.0		Columns	5.1
P6	①	17.2		Columns	4.10
P7	①	16.5		Columns	4.7
P8	①	15.8		Columns	4.4
P9	①	14.1		Columns	4.1
P10	Str. 20	22.6		Web Wall	
P11	Str. 20	14.3		Web Wall	
P12	Str. 20	17.6		Web Wall	
P13	②	13.6		Cap	
P14	③	1.9		Cap	
P15	Str. 10	22.6		Cap	
P16	Str. 10	26.0		Cap	

TYPE ① TYPE ③ TYPE ②
All bar bending dimensions are out to out.

REVISIONS:
 DATE: _____
 BY: _____
 CHECKED BY: _____
 DATE: _____
 APPROVED BY: _____
 DATE: _____

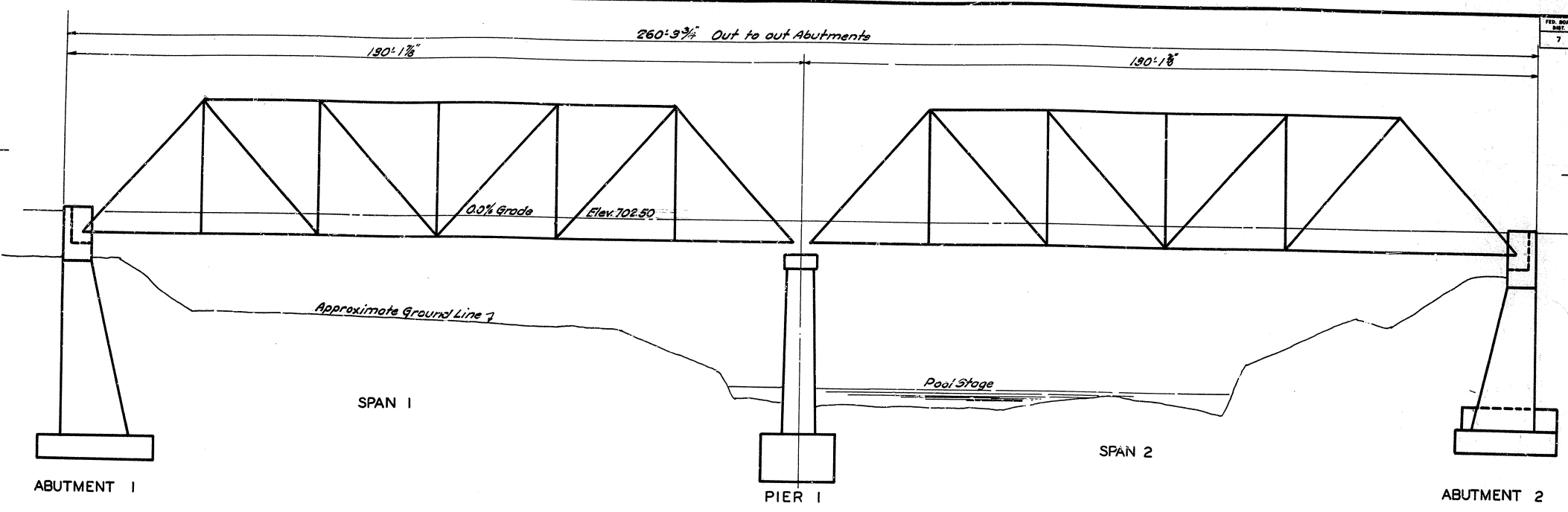
ESTIMATE OF QUANTITIES
 Concrete, Class A 1314 Cu Yds
 Reinforcement 10400 Lbs

PIER NO. 1
 SHEET 3 OF 3
 Bridge over Red River
COMMONWEALTH OF KENTUCKY
 STATE HIGHWAY DEPARTMENT
 FRANKFORT
 COUNTY OF
POWELL - MENIFFE
 MARIBA - NADA
 ROAD PROJECT NO. _____
 STATION 534+52

FED. ROAD DIST.	STATE	FISCAL YEAR	PROJECT NO.	SHEET NO.	TOTAL SHEETS
7	KY.				

TO MARI/BA

TO NADA



ELEVATION

GENERAL NOTE

SPECIFICATIONS: KENTUCKY DEPARTMENT OF HIGHWAYS 1936 STANDARD SPECIFICATIONS FOR STONE AND CONCRETE ROAD AND BRIDGE CONSTRUCTION, WITH AMENDMENTS.

SCOPE OF WORK: THE WORK TO BE DONE UNDER THIS CONTRACT WILL INCLUDE THE CLEANING, AND ONE COMPLETE COAT OF RED LEAD OR BLUE LEAD PAINT, AND FINISH PAINTING WITH TWO COMPLETE COATS OF APPROVED WHITE LEAD PAINT OF ALL EXPOSED METAL WORK IN THE SUPERSTRUCTURE.

IT IS THE DUTY OF THE CONTRACTOR TO FULLY PROTECT PEDESTRIAN, VEHICULAR OR OTHER TRAFFIC UPON OR UNDERNEATH THE STRUCTURE, AND TO PROTECT ALL PORTIONS OF THE STRUCTURE (SUPERSTRUCTURE AND SUBSTRUCTURES) AGAINST ANY DISTURBANCE FROM EQUIPMENT OR MATERIALS DURING CLEANING AND PAINTING OPERATIONS.

THE CLEANING SHALL CONSIST OF THE REMOVAL OF ALL SCALED, BROKEN, FLAKED, LOOSE AND MUST-STRIPPED PAINT ON ALL METAL WORK TO BARE METAL; THE CLEANING OF ALL RUSTED SURFACES TO BRIGHT, BARE METAL; AND THE REMOVAL OF ALL DIRT, OIL AND OTHER FOREIGN MATTER. CLEANING SHALL BE DONE BY MEANS OF COMPRESSED AIR, SCRAPERS OR WIRE BRUSHES.

SANDBLAST MAY BE USED FOR CLEANING. PROVIDED SATISFACTORY ARRANGEMENTS ARE MADE FOR THE PROTECTION OF PREVIOUSLY PAINTED SURFACES AND FOR THE PROTECTION OF PEDESTRIAN AND VEHICULAR TRAFFIC CROSSING THE BRIDGE. ALL GREASE, TAR OR OTHER SIMILAR STAINS SHALL BE REMOVED BY THE APPLICATION OF A SATISFACTORY SOLVENT TO SUCH SURFACES.

BASIC PAINTING SHALL CONSIST OF APPLYING TO ALL SURFACES ONE COMPLETE COAT OF RED LEAD OR BASIC SULPHATE BLUE LEAD PAINT.

THE FINISH PAINTING SHALL BE DONE AS REQUIRED BY THE SPECIFICATIONS, AND SHALL CONSIST OF ONE COMPLETE COAT OF NO. 1 WHITE LEAD PAINT AND ONE COMPLETE COAT OF NO. 2 WHITE LEAD PAINT APPLIED BY MEANS OF BRUSHES IF THE ORDER NOTES. SURFACES WHICH CANNOT BE SATISFACTORY PAINTED BY MEANS OF BRUSHES SHALL HAVE PAINT APPLIED BY MEANS OF SPRAYS OR OTHER APPROVED METHODS. SPRAYING MAY BE DONE ONLY WITH THE APPROVAL OF THE COMMISSIONER OF HIGHWAYS AND ONLY IN ORDER TO COVER SUCH SURFACES AS CANNOT BE SATISFACTORY PAINTED BY MEANS OF BRUSHES, SPRAYS OR OTHER APPROVED METHODS.

MARINE SPIRITS, BENZOL, OR OTHER COP-BACK MATERIAL WILL NOT BE ALLOWED ON THE JOB AT ANY TIME OR UNDER ANY CIRCUMSTANCES, EXCEPT UNDER THE DIRECT SUPERVISION OF THE ENGINEER, FOR USE IN CLEANING OR REMOVING GREASE OR OTHER SIMILAR MATERIALS.

THE PRESENCE OF OR THE USE OF ALUMINA SPIRITS, GASOLINE, KEROSENE, OR OTHER SIMILAR MATERIALS USED AS COP-BACK AGENT FOR THINNING THE PAINT, WILL BE SUFFICIENT CAUSE FOR REJECTING THE SURFACES PAINTED, AND FOR REQUIRING A COMPLETE RECLEANING AND REPAINTING OF THE SURFACES WITH SATISFACTORY MATERIALS. ALL PAINT SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS.

THE PAINT SHALL BE THOROUGHLY MIXED AND KEPT STIRRED WHILE BEING APPLIED, BUT A MECHANICAL MIXER AS SPECIFIED IN ARTICLES 5-2.3-4-3 OF THE SPECIFICATIONS WILL NOT BE REQUIRED.

ESTIMATE OF QUANTITIES: THE FOLLOWING IS A STATEMENT OF THE APPROXIMATE WEIGHTS OF THE METAL WORK TO BE PAINTED:

255,700 POUNDS OF STRUCTURAL STEEL (TOTAL)

TRAFFIC: THE CONTRACTOR SHALL NOT INTERFERE WITH THE MOVEMENT OF VEHICULAR OR FOOT TRAFFIC ACROSS THE BRIDGE, EXCEPT WHEN ABSOLUTELY NECESSARY.

PARTIAL PAYMENT: PARTIAL PAYMENTS WILL BE MADE ON ESTIMATES AS THE WORK PROGRESSES ON THE BASIS OF FORTY (40) PERCENT OF THE CONTRACT LUMP SUM PRICE WHEN THE CLEANING AND BASIC PAINTING IS COMPLETED ON THE ENTIRE BRIDGE AND THIRTY (30) PERCENT OF THE CONTRACT LUMP SUM PRICE WHEN ONE COAT OF NO. 1 WHITE LEAD PAINT IS COMPLETED ON THE ENTIRE BRIDGE. THE BALANCE WILL BE PAID WHEN ALL PAINTING HAS BEEN SATISFACTORY COMPLETED WITH THE PROVISION THAT TEN (10) PERCENT WILL BE RETAINED ON ALL ESTIMATES EXCEPT THE FINAL ESTIMATE. TWO OR THREE COMPLETE COATS ON A PORTION OF THE BRIDGE SHALL NOT BE CONSIDERED AS FULFILLING THE ABOVE IN LIEU OF ONE COMPLETE COAT OF PAINT ON THE ENTIRE BRIDGE.

DESIGNED BY	DATE	REVIEWED	DATE
CHECKED BY	DATE	REVIEWED	DATE
APPROVED BY	DATE	REVIEWED	DATE

BRIDGE OVER RED RIVER

COMMONWEALTH OF KENTUCKY
 DEPARTMENT OF HIGHWAYS
 FRANKFORT
 COUNTY OF
POWELL-MENIFEE
 MARI/BA - NADA

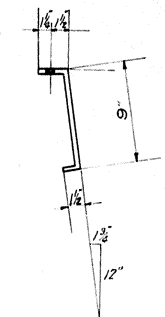
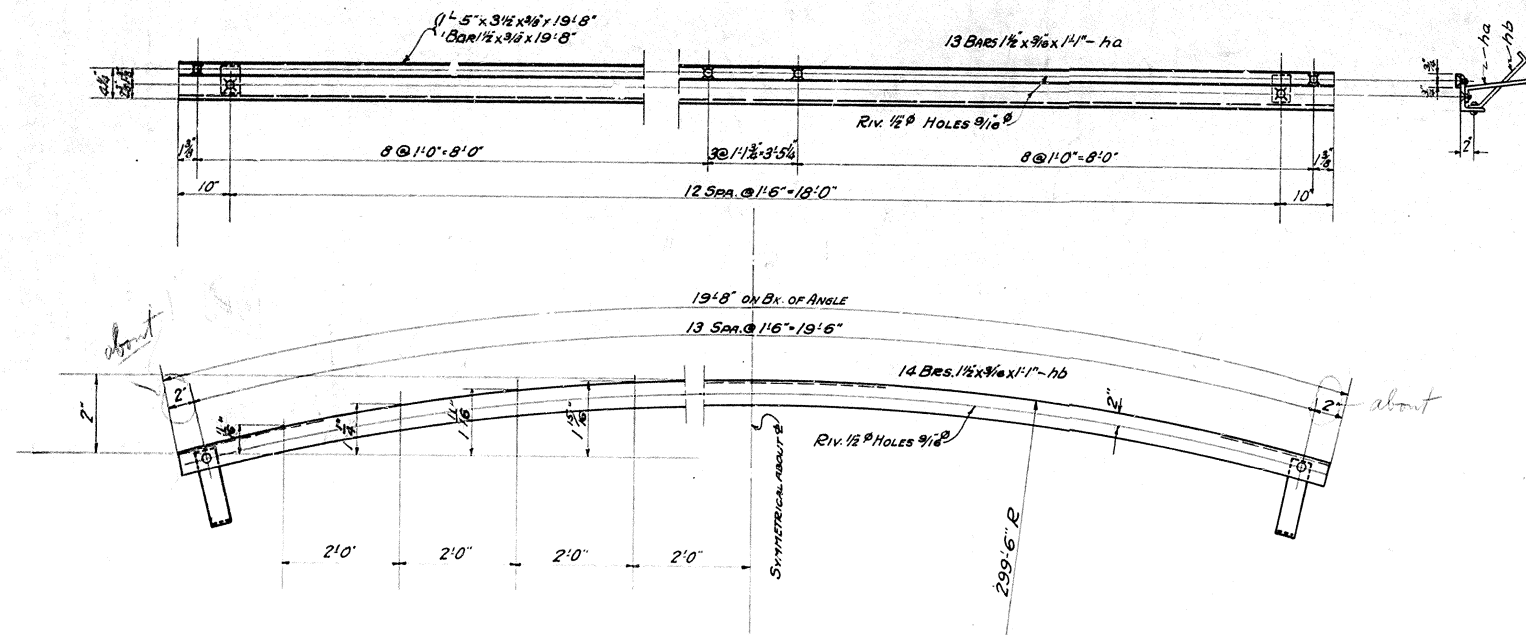
STATION 594+52 ROAD PROJECT NO. 4-P-99-40-2

DATE 7/18/43 SUBMITTED BY H.C. Neal BRIDGE ENGINEER

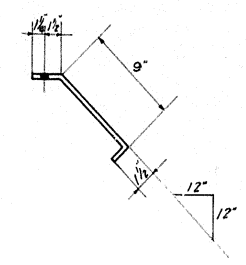
DATE 7/19/43 APPROVED BY J. Smith STATE HIGHWAY ENGINEER

BRIDGE NUMBER 99-40-2

NO. 0281



DETAIL OF BARS ha



DETAIL OF BARS hb

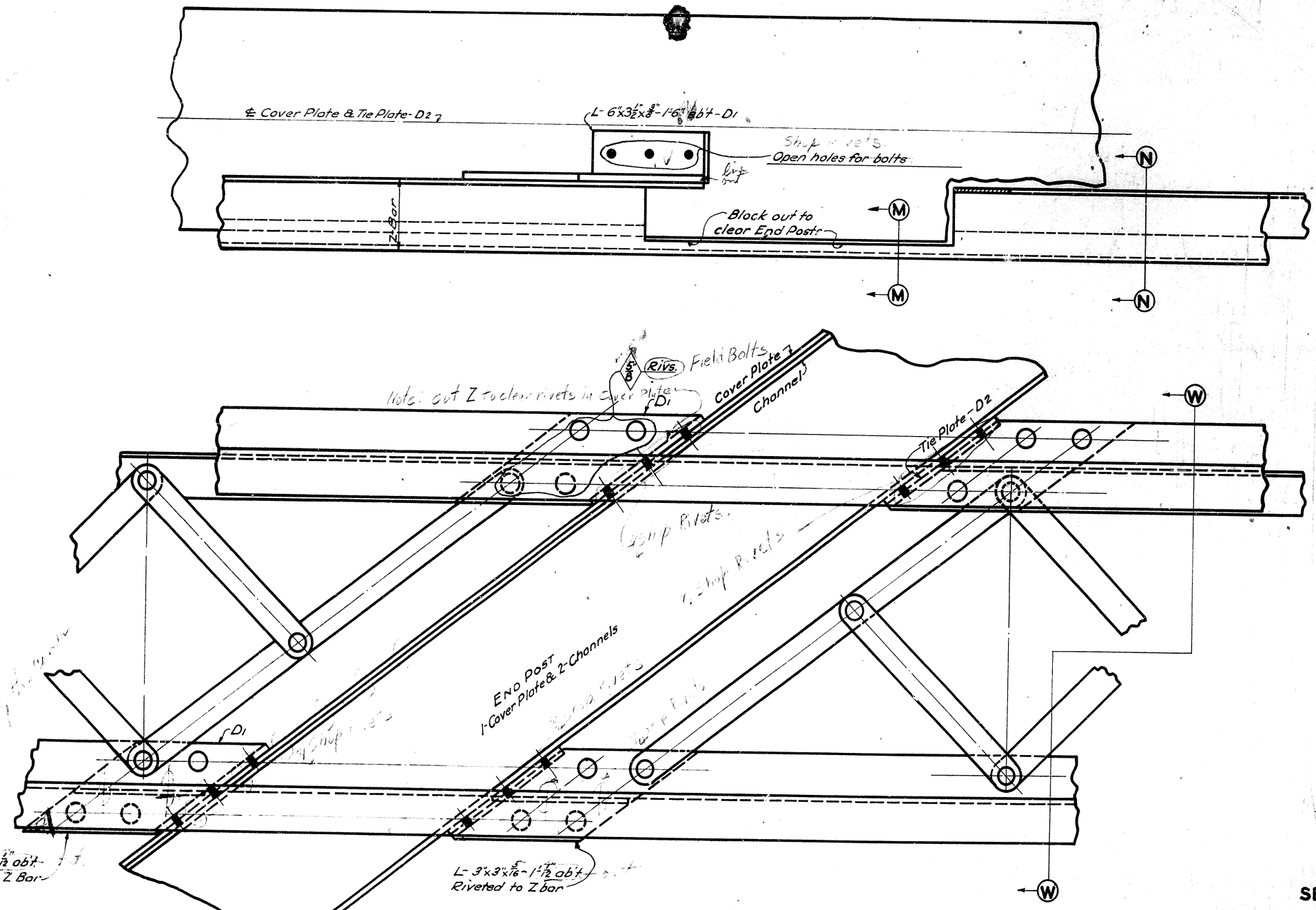
DETAIL OF EXPANSION ANGLE ON PARAPET WALL AT END OF STEEL SPANS

DRAWING NO.	INDEX
S-31	S-2
DESIGNED BY	
CHECKED BY	
REVISED	

COMMONWEALTH OF KENTUCKY
 STATE HIGHWAY DEPARTMENT
 FRANKFORT
**EXPANSION ANGLES
 FOR
 STEEL SPANS - 20 FT. ROADWAY**
 SHEET NO. | OF | SHEETS
 APPROVED BY _____
 APPROVED BY _____
 APPROVED BY _____

BRIDGE

PER. ROAD DIST.	STATE	PER. ASST.	PER. INSP.	DATE
7	KY.			



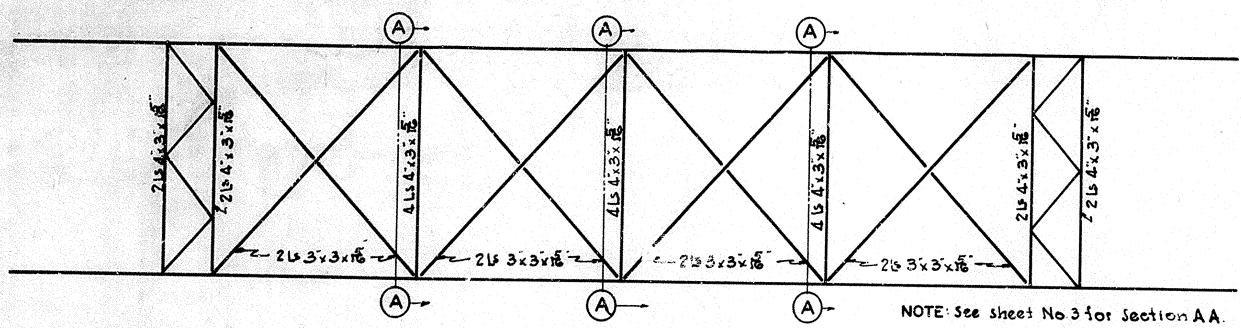
TYPICAL DETAILS SHOWING CONNECTIONS OF HANDRAIL TO END POST.

End Section of Handrail shall be fitted to the End Posts and match marked while the Truss is assembled in Shop. The Connections must fit accurately and the open holes reamed if necessary.

DESIGNED BY L.C. GAZDAR, MAY 1922, REVISED BY S.H. MOYER, MAY 1932, REVISED BY R.D. GILHAM, MAY 1938
 CHECKED BY GIBBS, S.O.B. - THROUGH CHECKED, S.H.C., MAY 1922-24

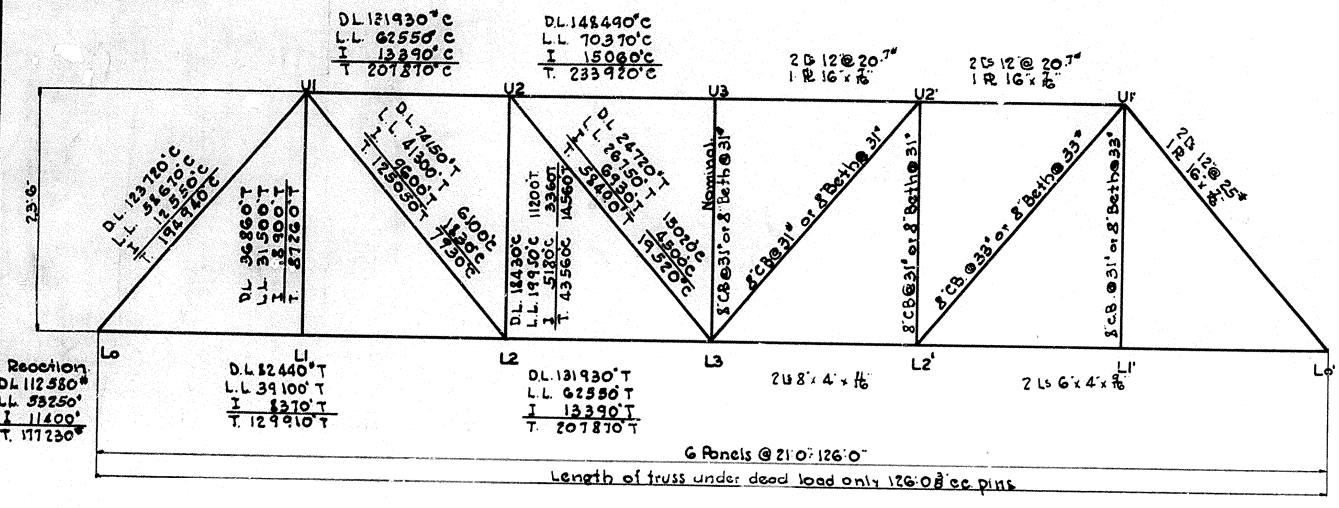
COMMONWEALTH OF KENTUCKY
 STATE HIGHWAY DEPARTMENT
 TYPICAL HANDRAIL
 DETAILS
 FOR STEEL SPANS

BRIDGE



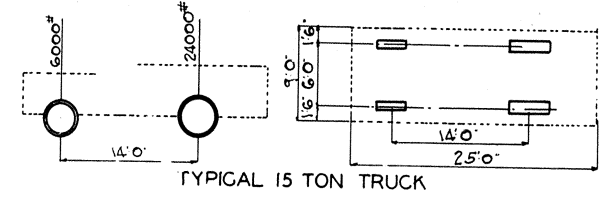
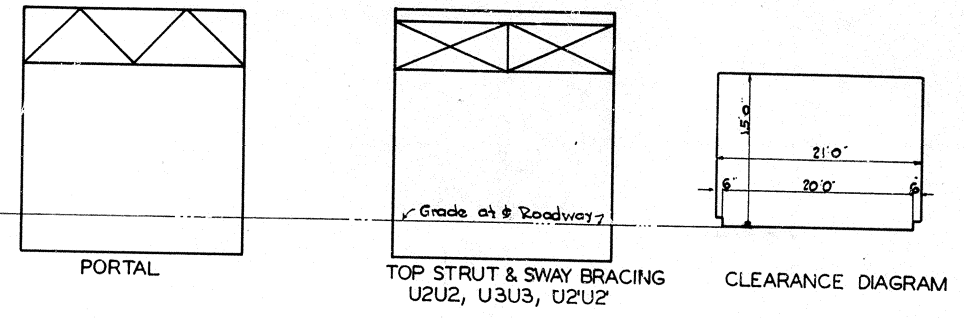
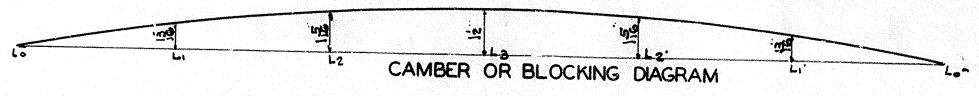
PORTALS AND TOP LATERALS

NOTE: See sheet No 3 for Section AA.



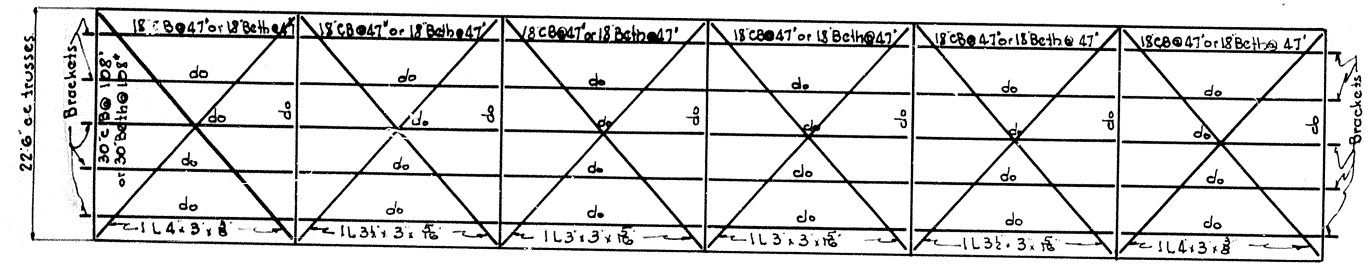
Reaction:
DL 112580°
LL 53250°
T 11400°
T 177230°

6 Panels @ 21'0" = 126'0"
Length of truss under dead load only 126'0" see pins



ASSUMED LOADING
 Dead Load: 3510 per lin. ft. of span including an allowance of 15 lbs. per sq. ft. of roadway for future surfacing.
 Live Load: 1600 lbs. per lin. ft. of span or 83 lbs. per sq. ft. of roadway for span fully loaded.
 Impact: 15150 where L = loaded length except as provided in specifications.

GENERAL NOTE
 Specifications: State Highway Department Kentucky 1932 and subsequent amendments
 Loading: Loading H15: 15 ton trucks. Two lanes of traffic.
 Concrete: Class A in floor slab and curbs.
 Corners: All exposed concrete corners to be chamfered 1"
 Wearing Surface: Top of concrete slab to be finished in accordance with specifications for monolithic wearing surface.
 Rivets: 2" except as noted, and to be countersunk, flattened or clipped as may be required for clearance.
 Expansion Angles: Contractor shall immediately prepare detailed shop drawings of expansion angles for substructure or approaches and submit to the Commission for approval.
 Alternate Sections: Alternate sections shown may be used. If used must be shown on approved detailed shop drawing.
 Painting: Material and workmanship for one shop and two field coats to be in accordance with specifications.



BOTTOM LATERALS AND FLOOR SYSTEM

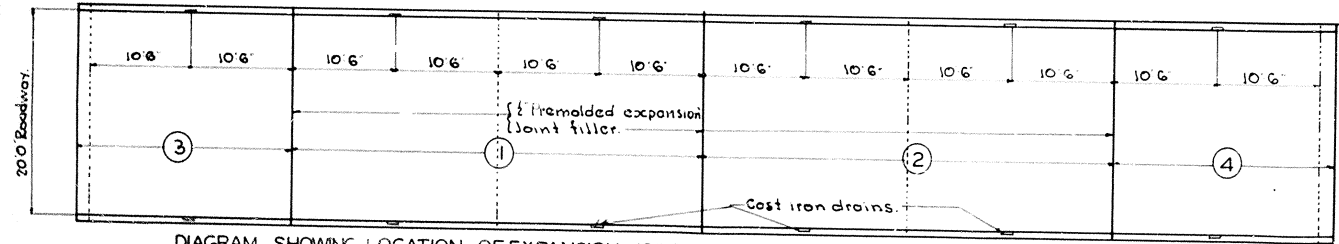


DIAGRAM SHOWING LOCATION OF EXPANSION JOINTS AND CAST IRON DRAINS IN CONCRETE FLOOR
 Sections of concrete floor to be placed in order shown working from the center towards the ends of the span. Section 4 may be placed before section 3. 24 ounce soft commercial grade sheet copper strip to be placed over floor beams over which there is an expansion joint in the floor. Cost of strip to be included in price bid for class A concrete.

ESTIMATE OF QUANTITIES
 (for one span only)
 Structural steel 139100 lbs.*
 Cast steel 2300 lbs.
 Forged steel 300 lbs.
 Cast iron drains 700 lbs.
 Total weight 142400 lbs.
 Concrete Class A 667 cu yds.
 Steel reinforcement 16080 lbs.
 * Add weight shown in table on sheet S85 for total structural steel weight.

S47
S85

COMMONWEALTH OF KENTUCKY
 STATE HIGHWAY DEPARTMENT
STANDARD STEEL SPAN
 126'-0" SPAN — 20'-0" ROADWAY
THRU TRUSS
 SHEET NO 1 OF 3

DESIGNED BY S88
CHECKED BY S2



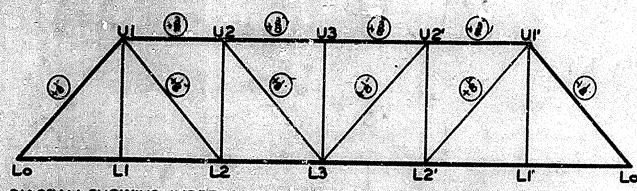


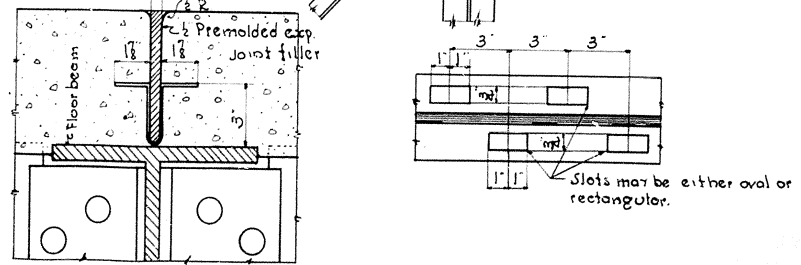
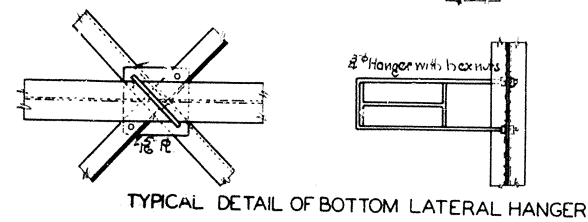
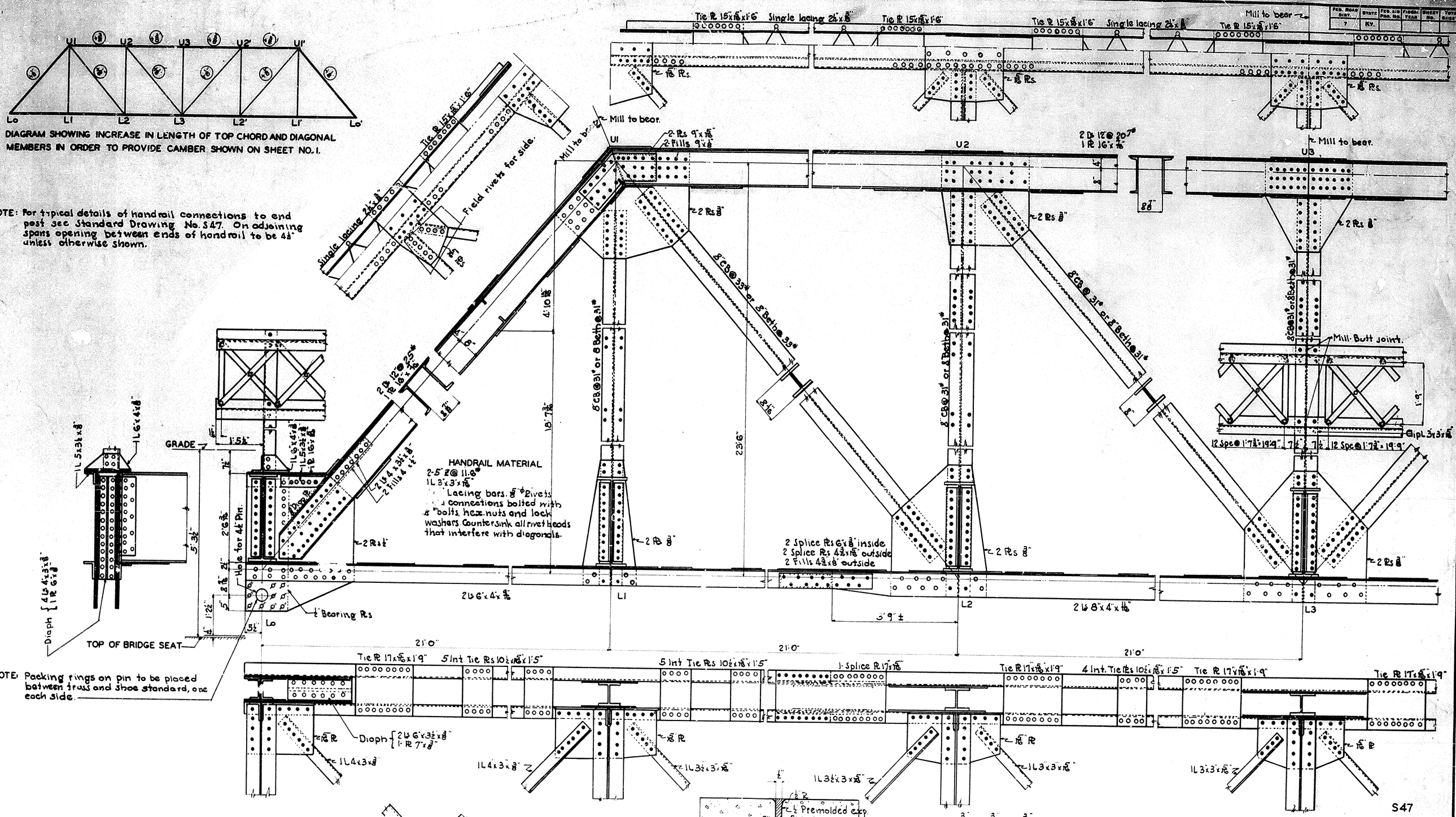
DIAGRAM SHOWING INCREASE IN LENGTH OF TOP CHORD AND DIAGONAL MEMBERS IN ORDER TO PROVIDE CAMBER SHOWN ON SHEET NO. 1.

NOTE: For typical details of handrail connections to end post see Standard Drawing No. S47. On adjoining spans opening between ends of handrail to be 4" unless otherwise shown.

DESIGNED BY: [blank] DATE: [blank]
 CHECKED BY: [blank] DATE: [blank]
 DRAWN BY: [blank] DATE: [blank]

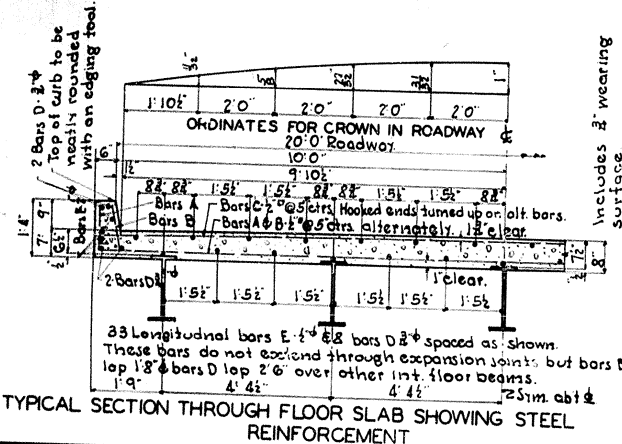
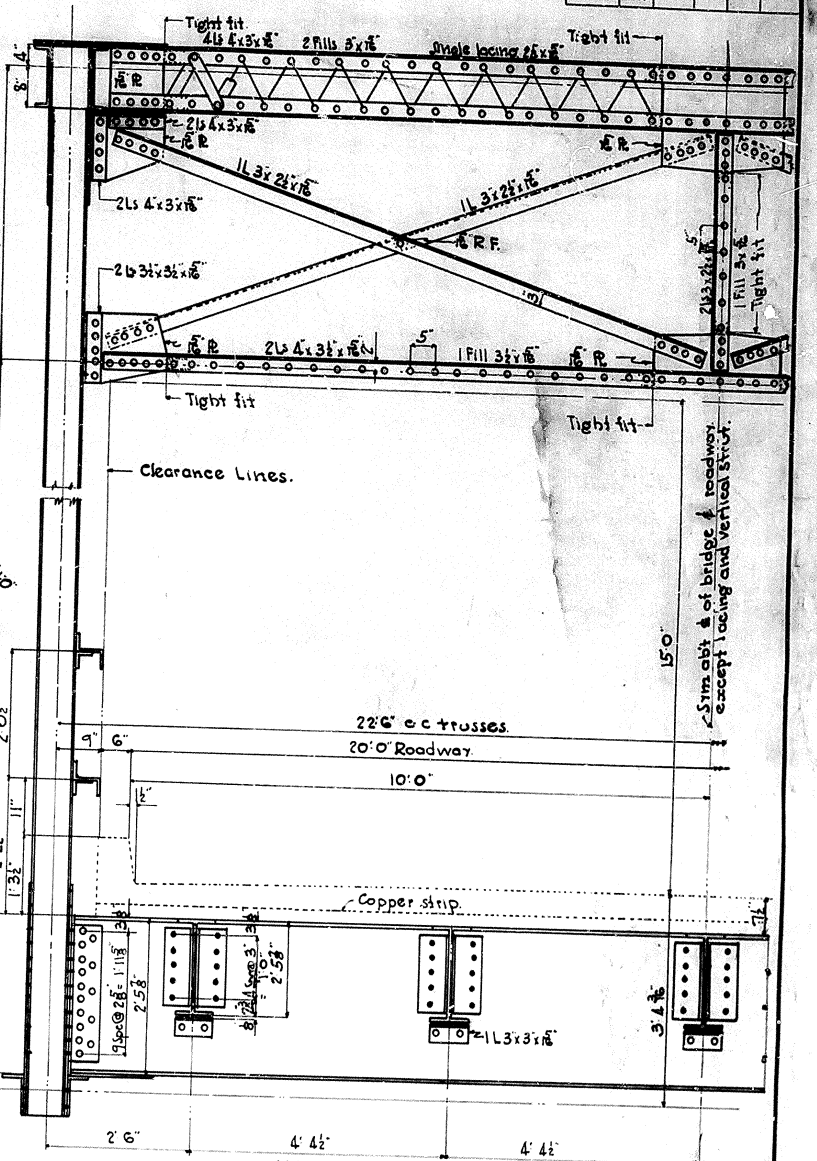
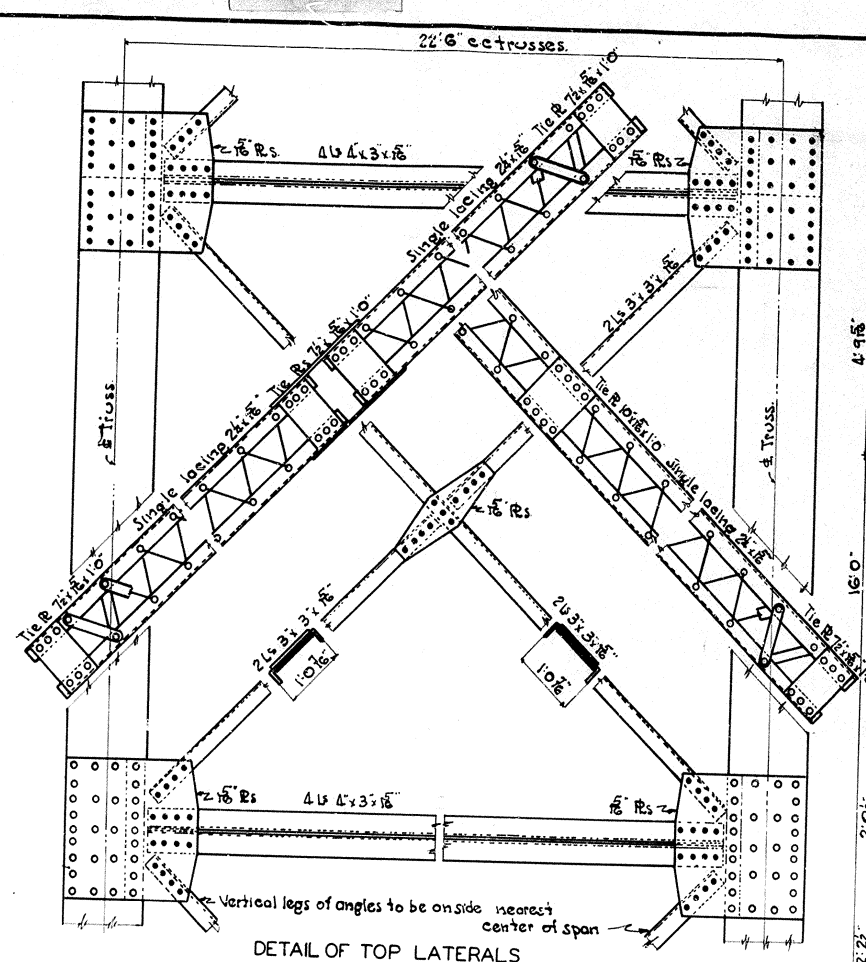
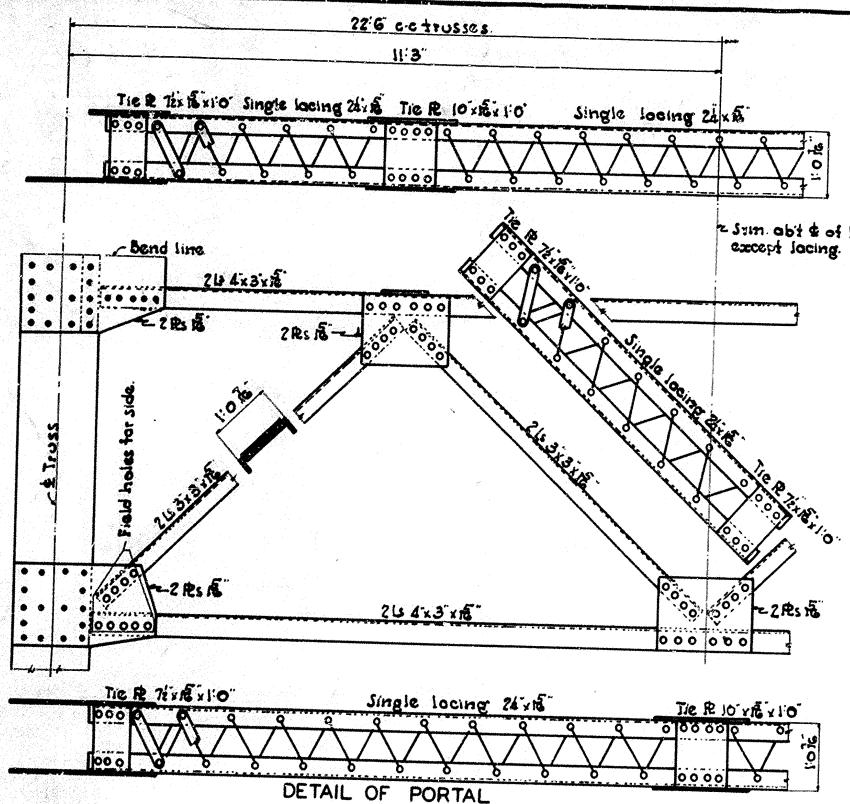
NOTE: Packing rings on pin to be placed between truss and shoe standard, one each side.

NOTE: Rivets in top and bottom chord splices may be changed from field to shop rivets or vice versa if more convenient for erection.

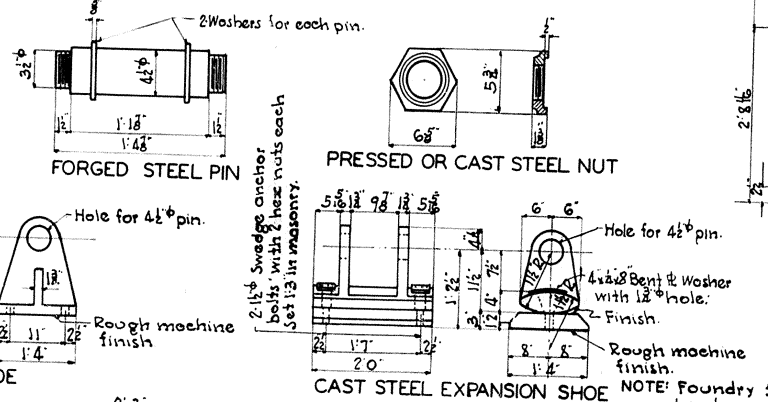


COMMONWEALTH OF KENTUCKY
 STATE HIGHWAY DEPARTMENT
STANDARD STEEL SPAN
126'-0" SPAN — 20'-0" ROADWAY
THRU TRUSS
 SHEET NO. 2 OF 3

-S88	S2
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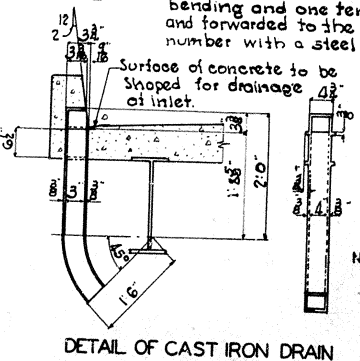
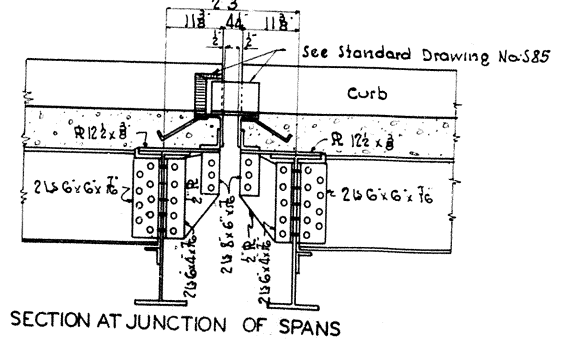
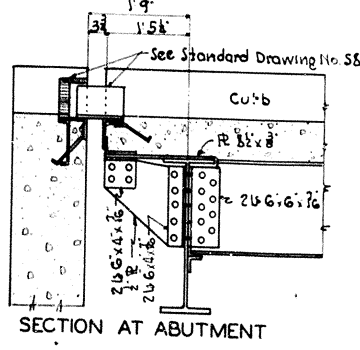
HORIZONTAL SECTION THROUGH CURB SHOWING BEVEL FOR EXPOSED EDGES AT EXPANSION JOINTS
 At top surface of roadway slab on each side of expansion joint use a 2" radius edging tool



BILL OF STEEL REINFORCEMENT IN FLOOR SLAB AND CURBS ONE SPAN

Mark	Type	No.	Size	Length	Location
A	1	154	1/2"	23 0	Bottom of slab
B	2	154	1/2"	22 1	Bottom of slab
C	3	308	1/2"	21 8	Top of slab
D	Str	32	1/2"	22 2	Curb Sect 1 & 2
E	Str	16	1/2"	22 4	Curb Sect 3 & 4
F	Str	132	1/2"	21 9	Slab Sect 1 & 2
G	Str	66	1/2"	22 4	Slab Sect 3 & 4

This bill of steel reinforcement is for a single span bridge. At junction of spans on multiple span bridges make corrections for change in length of end sections of concrete floor.



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126'-0" SPAN — 20'-0" ROADWAY
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 SHEET NO. 3 OF 3