



CALL NO. 320

CONTRACT ID. 122438

LAUREL COUNTY

FED/STATE PROJECT NUMBER FE01 063 0080 010-012

DESCRIPTION RUSSELL DYCHE MEMORIAL HIGHWAY (KY 80)

WORK TYPE JPC PAVEMENT REPAIRS

PRIMARY COMPLETION DATE 10/31/2012

LETTING DATE: July 13, 2012

Sealed Bids will be received electronically through the Bid Express bidding service until 10:00 AM EASTERN DAYLIGHT TIME July 13, 2012. Bids will be publicly announced at 10:00 AM EASTERN DAYLIGHT TIME.

REQUIRED BID PROPOSAL GUARANTY: Not less than 5% of the total bid.

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PART I
SCOPE OF WORK

CONTRACT ID - 122438

ADMINISTRATIVE DISTRICT - 11

PROJECT(S) IDENTIFICATION AND DESCRIPTION:

COUNTY - LAUREL

PCN - MP06300801201

FE01 063 0080 010-012

RUSSELL DYCHE MEMORIAL HIGHWAY (KY 80) FROM 1120 FEET EAST OF THE INTERSECTION OF US 25 (MP 10.999) TO 560 FEET WEST OF THE INTERSECTION WITH US 25 (MP 11.317), A DISTANCE OF 0.32 MILES. JPC PAVEMENT REPAIRS.

GEOGRAPHIC COORDINATES LATITUDE 37^08'36" LONGITUDE 84^06'09"

AVERAGE DAILY TRAFFIC - 23800

AVERAGE MAINLINE WIDTH - 68.0 FEET

COMPLETION DATE(S):

COMPLETION DATE - October 31, 2012

APPLIES TO ENTIRE CONTRACT

CONTRACT NOTES

PROPOSAL ADDENDA

All addenda to this proposal must be applied when calculating bid and certified in the bid packet submitted to the Kentucky Department of Highways. Failure to use the correct and most recent addenda may result in the bid being rejected.

BID SUBMITTAL

Bidder must use the Department's Expedite Bidding Program available on the Internet web site of the Department of Highways, Division of Construction Procurement. (www.transportation.ky.gov/contract)

The Bidder must download the bid file located on the Bid Express website (www.bidx.com) to prepare a bid packet for submission to the Department. The bidder must submit electronically using Bid Express.

JOINT VENTURE BIDDING

Joint venture bidding is permissible. All companies in the joint venture must be prequalified in one of the work types in the Qualifications for Bidders for the project. The bidders must get a vendor ID for the joint venture from the Division of Construction Procurement and register the joint venture as a bidder on the project. Also, the joint venture must obtain a digital ID from Bid Express to submit a bid. A joint bid bond of 5% may be submitted for both companies or each company may submit a separate bond of 5%.

UNDERGROUND FACILITY DAMAGE PROTECTION

The contractor is advised that the Underground Facility Damage Protection Act of 1994, became law January 1, 1995. It is the contractor's responsibility to determine the impact of the act regarding this project, and take all steps necessary to be in compliance with the provision of the act.

SPECIAL NOTE FOR PIPE INSPECTION

Contrary to Section 701.03.08 of the 2012 Standard Specifications for Road and Bridge Construction and Kentucky Method 64-114, certification by the Kentucky Transportation Center for prequalified Contractors to perform laser/video inspection is not required on this contract. It will continue to be a requirement for the Contractor performing any laser/video pipe inspection to be prequalified for this specialized item with the Kentucky Transportation Cabinet-Division of Construction Procurement.

REGISTRATION WITH THE SECRETARY OF STATE BY A FOREIGN ENTITY

Pursuant to KRS 176.085(1)(b), an agency, department, office, or political subdivision of the Commonwealth of Kentucky shall not award a state contract to a person that is a foreign entity required by [KRS 14A.9-010](#) to obtain a certificate of authority to transact business in the Commonwealth (“certificate”) from the Secretary of State under [KRS 14A.9-030](#) unless the person produces the certificate within fourteen (14) days of the bid or proposal opening. If the foreign entity is not required to obtain a certificate as provided in [KRS 14A.9-010](#), the foreign entity should identify the applicable exception. Foreign entity is defined within [KRS 14A.1-070](#).

For all foreign entities required to obtain a certificate of authority to transact business in the Commonwealth, if a copy of the certificate is not received by the contracting agency within the time frame identified above, the foreign entity’s solicitation response shall be deemed non-responsive or the awarded contract shall be cancelled.

Businesses can register with the Secretary of State at <https://secure.kentucky.gov/sos/ftbr/welcome.aspx>.

SPECIAL NOTE FOR PROJECT QUESTIONS DURING ADVERTISEMENT

Questions about projects during the advertisement should be submitted in writing to the Division of Construction Procurement. This may be done by fax (502) 564-7299 or email to kytc.projectquestions@ky.gov. The Department will attempt to answer all submitted questions. The Department reserves the right not to answer if the question is not pertinent or does not aid in clarifying the project intent.

The deadline for posting answers will be 3:00 pm Eastern Daylight Time, the day preceding the Letting. Questions may be submitted until this deadline with the understanding that the later a question is submitted, the less likely an answer will be able to be provided.

The questions and answers will be posted for each Letting under the heading “Questions & Answers” on the Construction Procurement website (www.transportation.ky.gov/contract). The answers provided shall be considered part of this Special Note and, in case of a discrepancy, will govern over all other bidding documents.

ACCESS TO RECORDS

The contractor, as defined in KRS 45A.030 (9) agrees that the contracting agency, the Finance and Administration Cabinet, the Auditor of Public Accounts, and the Legislative Research Commission, or their duly authorized representatives, shall have access to any books, documents, papers, records, or other evidence, which are directly pertinent to this

contract for the purpose of financial audit or program review. Records and other prequalification information confidentially disclosed as part of the bid process shall not be deemed as directly pertinent to the contract and shall be exempt from disclosure as provided in KRS 61.878(1)(c). The contractor also recognizes that any books, documents, papers, records, or other evidence, received during a financial audit or program review shall be subject to the Kentucky Open Records Act, KRS 61.870 to 61.884.

In the event of a dispute between the contractor and the contracting agency, Attorney General, or the Auditor of Public Accounts over documents that are eligible for production and review, the Finance and Administration Cabinet shall review the dispute and issue a determination, in accordance with Secretary's Order 11-004. (See attachment)

10/18/2011

SPECIAL NOTE FOR RECIPROCAL PREFERENCE

Reciprocal preference to be given by public agencies to resident bidders

By reference, KRS 45A.490 to 45A.494 are incorporated herein and in compliance regarding the bidders residency. Bidders who want to claim resident bidder status should complete the Affidavit for Claiming Resident Bidder Status along with their bid in the Expedite Bidding Program. Submittal of the Affidavit should be done along with the bid in Bid Express.

03/01/2011

EXPEDITE PROJECT WORK ORDER

The Contractor may request that the Department expedite the work order for this project to allow for maximization of time to complete the work. In order for the Department to accomplish this task, the Contractor may be required to “hand carry” all required project documentation to facilitate the process. Immediately UPON NOTIFICATION OF AWARD OF THE CONTRACT, deliver required project documentation to:

Division of Construction Procurement
200 Mero St.
Frankfort, KY 40602

NATIONAL HIGHWAY

Be advised this project is on the NATIONAL HIGHWAY SYSTEM.

ASPHALT MIXTURE

Unless otherwise noted, the Department estimates the rate of application for all asphalt mixtures to be 110 lbs/sy per inch of depth.

DGA BASE

Unless otherwise noted, the Department estimates the rate of application for DGA Base to be 115 lbs/sy per inch of depth.

OPTION B

Be advised that the Department will control and accept compaction of asphalt mixtures furnished on this project under OPTION B in accordance with Sections 402 and 403.

Special Notes for JPC Pavement-10 IN/24

I. Description

Except as specified in these notes, perform all work according to the Department's 2012 Standard Specification, applicable special Provisions and Special Notes, Standard Drawings and Sepia Drawings, and the drawing else where in this proposal. Article references are to the Standard Specifications.

Furnish all materials, labor and equipment for the following work: (1) Shoulder strengthen; (2) Remove Traffic Island; (3) Remove and Replace JPC at the locations listed and/or directed by the engineer; (4) Pavement Markings; (5) Traffic Signal Loops; (6) Maintaining and controlling Traffic; and (7) All other work specified as part of this contract.

II. Materials

Except as specified in these notes or on these drawings, all materials will be according to the Standard Specifications and applicable Special Provisions and Special Notes. The Department will sample and test all materials according to the Department's sampling Manual and the Contractor will have the materials available for sampling a sufficient time in advance of their use, to allow for the necessary time for testing, unless otherwise specified in these notes.

A. Maintain and Control Traffic. See Traffic Control Plan.

B. JPC Pavement-10 IN/24. Use jointed plain concrete pavement 10 inch/24 for full depth replacement of concrete pavement as specified in the project typical section. Recommended slump for JPC Pavement-10 IN/24 is 3 inches. Either central mixing or truck mixing will be allowed.

C. Joint and Crack Sealing. For joints use Hot-Poured Elastic Joint Sealant conforming to Section 807.03.01.

D. Pavement Markings. All permanent markings on the concrete pavement shall be Durable Preformed Type I Tape that conforms to Section 836 of the Standard Specifications. All permanent markings on bituminous pavement shall be pave striping permanent paint that conforms to Section 842 of the Standard Specifications.

E. Traffic Signal Loops. See Special Notes for Installation of Traffic Loops.

III. Construction Methods

A. Maintain and Control Traffic. See Traffic Control Plan.

B. Site Preparation. The Contractor is responsible for all site preparation. This item will include, but is not limited to, incidental excavation and backfilling; removal of bituminous pavement; removal of all obstructions or any other items; disposal of materials; sweeping and removal of debris; shoulder preparation and restoration, temporary and permanent erosion and pollution control; and all incidentals. All Site preparation shall be only as approved or directed by the Engineer. Other than the bid items listed, no direct payment will be made for site preparation, but will be incidental to the other items of work.

C. Shoulder Preparation. Prior to placing any lane closures that require shifting traffic onto the existing shoulders, trench the area as shown in the typical section for preparation of bituminous pavement. Use trenched material where needed for subgrade embankment to construct the bituminous paved shoulder as outlined in the typical sections for the temporary traffic phases. Place trenched material for a shoulder wedge as needed to eliminate any shoulder edge drop off. While traffic is shifted onto the paved shoulder, maintain the shoulder. In order to construct the shoulder to final template, place DGA as needed in preparation of the chip & seal operation. Asphalt Materials, DGA and the Asphalt Seal Coat will be paid at the Contract unit bid price.

D. Concrete Pavement Removal and Replacement. Prior to placement of JPC Pavement-10 IN/24, saw cut the asphalt pavement 6 inches or as directed by the Engineer on KY80 and the Hal Rodgers Parkway to provide a neat clean edge. Remove the existing PCC Pavement by saw cut and lift method without disturbing the underlying base or damaging the adjacent pavement remaining in place. Do not over-saw into the existing PCC Pavement not intended to be removed.

It is not intended to disturb the underlying subgrade, however a quantity of Crushed Aggregate #2 and Geotextile Fabric Type III have been included for undercutting very poor, soft, and wet soils. These quantities are to be used sparingly and ONLY as directed by the Engineer. The excavation for these locations will be incidental to the unit bid price for Crushed Aggregate #2.

The existing nominal depth of the mainline JPC Pavement is assumed to be 10 inches. Additional payment will not be made for any existing pavement removed greater than 10 inches in thickness. The existing PCC Pavement grade/profile may be used as a guideline but forming of the new JPC Pavement shall be independent of the existing concrete pavement in order to ensure a smooth profile. The contractor is required to

present their method of construction for ensuring and establishing a smooth profile for the Engineer's approval. Remove and replace concrete pavement in a continuous operation in accordance with the traffic control plan and these notes. The nominal depth of the JPC Pavement shall be 10 inches, however, transition the finished grade of the JPC Pavement to match adjacent pavement that is to remain in place; therefore, the actual thickness of the pavement may be greater than 10 inches in some areas. The concrete shall be consolidated, struck off using a vibrating or roller screed and finished mechanically. **A concrete paver shall be used to construct the mainline JPC pavement except for the JPC Pavement in the middle of the KY80, Hal Rodgers Parkway and US25 intersection.** The entire concrete area is to be "straight edged" with a tool no less than 10-feet long to ensure the best possible ride. New longitudinal joints will be constructed at the edges of the permanent lane configuration. Use tie bars for the longitudinal joints per the Kentucky Standard Specification unless inadequate separation yields them impractical. Hook bolts may be used in that case at the expense of the contractor. Contrary to specification, construction joints will only be permitted at transverse joint locations unless approved by the engineer.

E. Joint Sealing. Saw, clean and seal transverse and longitudinal joints as designated by the Engineer. Construct joints as outlined in the specification and standard drawings. Joints shall not be exposed to traffic without being joint sealed. All joints shall be sealed with Hot Pour Elastic.

F. Saw Clean and Reseal Random Cracks. Saw-cut, clean and reseal random cracks with hot poured elastic material as directed by the Engineer.

G. Traffic Signal Loops. See special notes for installation of traffic loops. Contractor will be responsible for the protection of the lead wires from each loop to the junction box during additional phases of construction. This operation will be at no additional cost to the department.

H. Shoulder and Other Restoration. Restore any areas of the existing shoulders or other roadway features disturbed by the work or the Contractor's operations in like kind materials or as directed by the Engineer.

I. Disposal of Waste. Dispose of all cuttings, debris, and other waste off the right-of-way at approved sites obtained by the Contractor at no additional cost to the Department. No separate payment will be made for the disposal of waste and debris from the project, but will be incidental to the other items of the work.

J. Pavement Markings. Temporary pavement markings will conform to the permanent marking locations or the Traffic Control Exhibits as applicable. Temporary striping will be in accordance with Section 112 and the permanent striping will be in accordance with Section 112 and Section 714.

K. On-Site Inspection. Each contractor submitting a bid for this work will make a thorough inspection of the site prior to submitting a bid and will thoroughly familiarize themselves with the existing conditions so that the work can be expeditiously performed after a contract is awarded. Submission of a bid will be considered evidence of this inspection having been made. Any claims resulting from site conditions will not be honored by the Department.

L. Utility Clearance. It is not anticipated that utility facilities will need to be relocated and/or adjusted; however, in the event that work does require relocation and/or adjustment, the utility companies will work concurrently with the Contractor while relocating their facilities.

M. Lighting. See special notes for installation of Lighting. It is intended to only replace the control box utilizing the exiting service pole.

N. Final Dressing, Clean Up, and Seeding and Protection. After all work is completed, remove all waste and debris from the construction sites. Restore disturbed shoulders. Perform Class A final dressing on all disturbed areas. Sow disturbed earthen areas with Seed Mixture No. 1.

O. Coordination of Work. Be advised that other projects may be in progress within or in the near vicinity of this project. The traffic control of those projects may affect this project and the traffic control of this project may affect those projects. The Contractor shall coordinate the work on this project with the work of the other contractors. In case of a conflict, the Engineer will determine the relative priority to give to work phasing on the various projects.

IV. Method of Measurement

A. Maintain and Control Traffic. See Traffic Control Plan.

B. Site Preparation. Other than the bid items listed, site preparation will not be measured for payment, but will be incidental to the other items of the work.

C. Dense Graded Aggregate. DGA Base for wedging shoulder edge drop-offs and subgrade corrections will be measured for payment.

D. Asphalt Mixtures. Asphalt mixtures will be measured for payment.

E. Remove Traffic Island. Remove Traffic Island will be measured for payment in square yards.

F. Remove PCC Pavement. Removed PCC Pavement will be measured for payment in square yards, regardless of thickness.

G. JPC Pavement-10 IN/24. See Section 501 and 502 of the Standard Specifications.

H. Joint Sealing. The Department will not measure Joint Sealing for payment, but shall be incidental to the bid item JPC Pavement-10 IN/24.

I. Signal Loops. See Special Notes for Installation of Traffic Loops.

J. Smooth Dowels, Deformed Tie Bars and Hook Bolts. Smooth dowels, deformed tie bars and hook bolts will not be measured for payment, but will be incidental to JPC Pavement-10 IN/24.

K. Remove Pavement. Removed asphalt pavement needed for the construction of the new JPC Pavement along the right shoulder will be incidental to the work except for the asphalt mixture needed to replace the removed pavement shall be paid direct on a tonnage basis. The removed asphalt pavement in the median will be paid on the square yard basis.

L. Lighting. See Special Notes for Installation of Lighting.

M. Restoration. All items of restoration will not be measured for payment, but shall be incidental to the other items of work.

N. Final Dressing, Clean Up, and Seeding and Protection. Final dressing, Clean Up, and Seeding and Protection will not be measured for separate payment, but shall be incidental to the other items of work.

Signal General Notes

The Standard Specifications for Road and Bridge Construction, current edition, and other special notes and specifications will apply on this project.

Conduit shall include furnishing and installing specified conduit in accordance with specifications. This item includes conduit fittings, pipe/test plugs, expansion joints, clamps, and weatherheads.

Junction box shall include furnishing and installing specified junction box in accordance with the specifications. This item includes concrete (if required), #57 aggregate, and backfilling and restoration of disturbed areas to the satisfaction of the resident engineer.

Trenching and backfilling shall include excavation, backfilling, and the restoration of disturbed areas to original condition. Incidental to this item shall be furnishing and installing underground utility warning tape (if required).

Wire or cable shall include furnishing and installing specified wire or cable within conduit, saw slot, or overhead as indicated on the detail sheets. Incidental to this item shall be the furnishing and installing of splice boots, cable rings or other hardware required for installing cable. The contractor shall install all cable or wire runs splice-free from the controller to each loop wire, signal head, pedestrian head/detector, camera, blankout sign and audible pedestrian head/detector the cable or wire is feeding. Exceptions to this must be approved by the engineer or as specified on the plans.

Install signal control type 170 shall include installing a 170 controller and cabinet as specified on the detail sheets. This item includes the construction of the concrete base (if required), mounting the cabinet to the base or pole, and the connection of signals and detectors. All materials necessary to build the base shall be provided by the contractor. Incidental to this item shall be installing KYTC supplied induction loop amplifiers, pedestrian isolators, load switches and master controller 170. This item also includes installing the electrical service. All materials, inspections and fees necessary for the installation of the electrical service shall be provided by the contractor.

Maintain existing signal operation shall include furnishing, installing and maintaining necessary signal equipment (to include poles, cables, wires, loops, etc.). These materials are not included in the list of quantities. The cost for this item shall be incidental to the project.

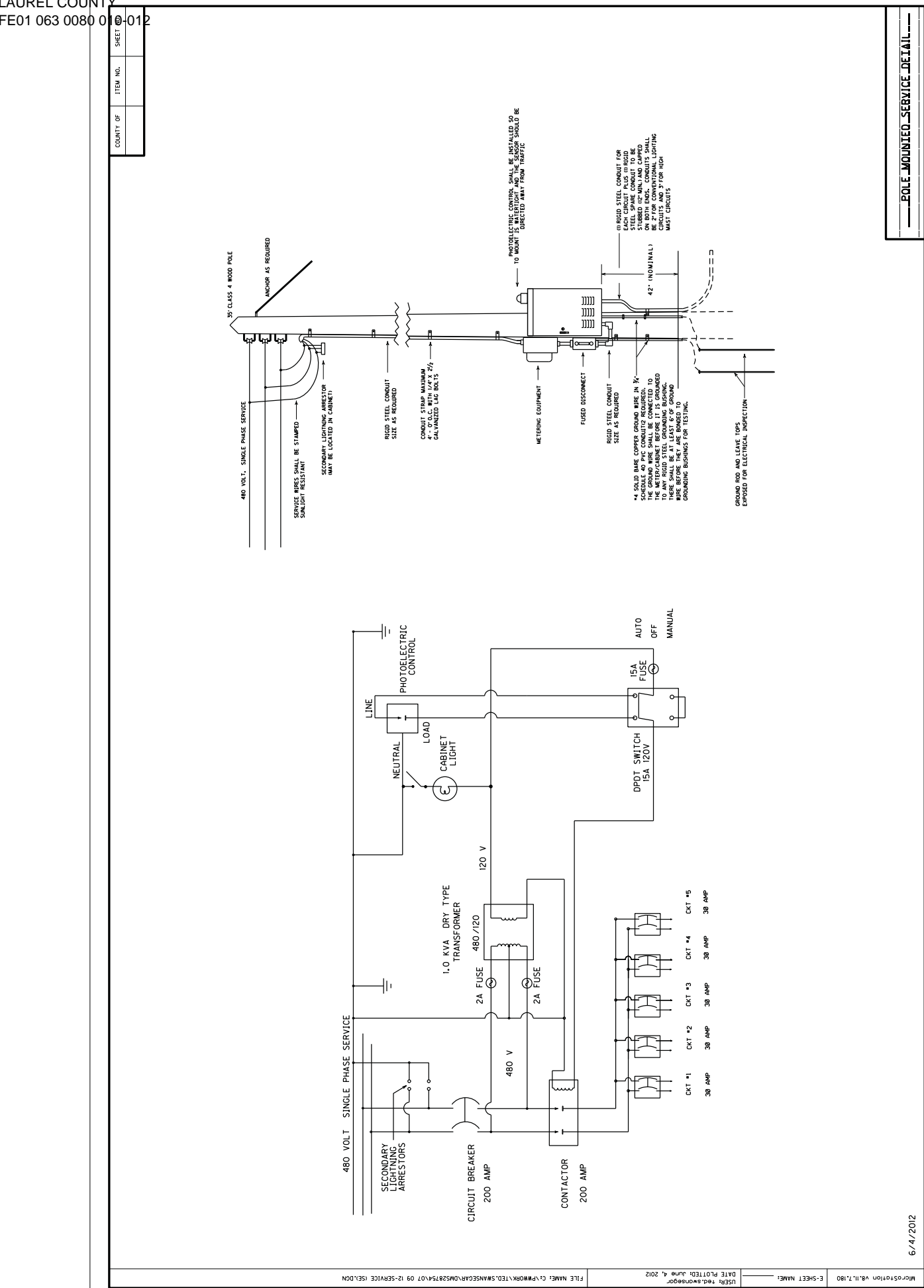
Remove signal equipment shall include the removal of all signal equipment not to be reused including all obsolete wiring. Control equipment and signal heads shall be returned to the Department of Highways as directed by the engineer. All other equipment and materials shall be disposed of by the contractor off the project.

The Contractor shall be responsible for picking up install items from the Frankfort pole yard and delivering these items to the site. The Contractor shall contact Frankfort pole yard personnel (502-564-3820) and arrange to pick up install items a minimum of two (2)

working days prior to arrival. Failure to provide pole yard personnel this advance notice could result in long delays or refusal to distribute equipment upon arrival.

8/2004

COUNTY OF	ITEM NO.	SHEET
		18



Lighting General Notes

The Standard Specifications for Road and Bridge Construction, current edition, and other special notes and specifications will apply on this project.

Remove lighting shall include the control equipment. It is intended for the service pole to remain in place.

Lighting control equipment shall include supplying and installing the control cabinet with a 4 circuit 60 amp phase service. The control equipment includes all items shown on the pole mounted service detail sheet.

SPECIAL NOTE FOR STAKING

It is intended to replace the existing concrete pavement at the same line and grade with new JPC Pavement. Therefore, the contractor is required to perform a field survey of the existing pavement in order to establish the existing cross slopes, transitions, alignments, and profile necessary to construct the project. Irregularities in the existing pavement are to be eliminated with the construction of a smooth line and grade of the new JPC pavement to ensure the best rideability as possible. The field survey is to be approved by the engineer.

The Department will measure "Staking" as a Lump Sum item. Payment at the contract unit price shall be full compensation for all labor, materials, equipment and incidentals necessary to complete the survey and establish grade during construction.

SPECIAL NOTES FOR TRAFFIC SIGNAL PREFORMED LOOP REPLACEMENT

I. DESCRIPTION. Loop replacement shall be performed in accordance with the Department's Standard Specifications (current edition), applicable Standard Drawings, and applicable Special Provisions except as hereafter specified. Article references are to the Standard Specifications.

The Contractor shall furnish all materials, labor, and equipment for the replacement of traffic signal loop(s), and junction boxes (if the contract specifies quantities for this bid item elsewhere), and maintaining and controlling traffic, and all other work specified as part of this contract.

II. MATERIALS. See section 835 of the Standard Specifications.

All preformed loop wire shall be 16-gauge THWN stranded copper, single conductor in a 2-4-2 configuration for Quadrapole and 3 turns for a standard as shown on the Quadrapole and Standard Loop detail. The loop and home run shall be housed in a class A oil resistant heavy-duty reinforced rubber hose with a 250-PSI internal pressure rating. Hose for the loop and home run assembly shall be one continuous piece. The 3/8" I.D. (5/8" O.D.) hose shall be factory assembled. Preformed loops and home runs shall be pre-wired. The loop configurations and homerun lengths shall be assembled for the specific application.

Hose tee connections shall be high temperature synthetic rubber. The tee shall be of proper size to attach directly to the hose, minimizing the glue joints. The tee shall have the same flexible properties as the hose to insure that the whole assembly can conform to pavement movement and shifting without cracking or breaking.

III. CONSTRUCTION. See section 723 of the Standard Specifications.

The electrical contractor shall coordinate with the general contractor and inspector to ensure the loops are installed prior to any milling work being performed. The contractor shall be responsible for the removal of existing lead-in cable.

Preformed Quadrapole Loops, Preformed Loops and Preformed Loop Lead-In locations shall be coordinated with the Contractor and the Engineer prior to any work being performed. The Contractor shall be careful to avoid pavement sections where potholes, cracks, or any other roadway flaws exist.

Hose for the preformed loops and home run assembly shall be one continuous piece and shall be extended splice-free to the controller or junction box. Preformed loop cables shown as extended to junction boxes by means of preformed home run cables shall be spliced into loop lead-in cable at the boxes. Loop lead-in cable shall be extended splice-free from the junction box to controller.

The preformed loop dimension shall be 6' x 30' Quadrapole or 6' x 6', as specified. Center and mark each loop in the lane such that its sides are parallel and perpendicular to the direction of traffic.

Each Contractor submitting a bid for this work shall make a thorough inspection of the site prior to submitting his bid and shall thoroughly familiarize himself with existing conditions so that the work can be expeditiously performed after a Contract is awarded. Submission of a bid will be considered evidence of this inspection having been made. Any claims resulting from site conditions will not be honored by the Department.

Information provided in this proposal and the types and quantities of work listed are not to be taken as an accurate or complete evaluation of the material and conditions to be encountered during construction. The bidder must draw his own conclusion as to the conditions encountered. The Department does not give any guarantee as to the accuracy of the data and no claim will be considered for additional compensation if the conditions encountered are not in accordance with the information shown.

It is not anticipated that utility facilities will need to be relocated and/or adjusted; however, in the event that it is discovered that the work does require that utilities be relocated and/or adjusted, the utility companies will work concurrently with the Contractor while relocating their facilities.

Sow all disturbed earthen areas with Seed Mixture No. 1.

Asphalt or Concrete(4 inches or less) Installation

Starting at the tee joint, saw-cut a $\frac{3}{4}$ " wide slot to a depth of 6" below the final surface of pavement for three sides of the loop leaving the center leg and the shortest and farthest leg from the home run tee joint for the last saw-cuts. After the three sides have been saw-cut, lay loop in slot to mark the center leg and the fourth side. Pull the loop out of the slot to saw-cut the center leg and the fourth side. Saw-cut a $\frac{3}{4}$ " wide slot for the center leg and the fourth side. Clean any debris, water and loose particles from the slot with compressed air.

Make the saw-cut for the home run slot from each loop to the transition conduit $\frac{3}{4}$ " wide and 6" deep. Clean any debris, water, and loose particles from the slot with compressed air.

Insert the preformed loop wire and home run lead-in into the bottom of the loop slot. Extend the preformed home run lead-in cable splice-free to the junction box or cabinet. No exceptions to this shall be considered.

There shall be a minimum of 6' between loops in adjacent lanes for 12' wide lanes. Once the preformed loop is installed in the roadway, hand place 1" backer rod in the saw slot to ensure preformed loop will not rise out of slot. Contractor shall then fill the saw slot with non-shrink grout until level with road surface. The non-shrink grout shall be incidental to the Loop Saw, Slot and Fill bid item. See Asphalt saw slot detail.

Concrete(with more than 4 inches) Installation

Lay the preformed loop wire and home run lead-in on the compacted aggregate prior to pouring the new concrete. There shall be a minimum of six feet between loops in adjacent lanes for 12 foot wide lanes.

IV. MEASUREMENT. See Section 723.04 in the Standard Specifications.

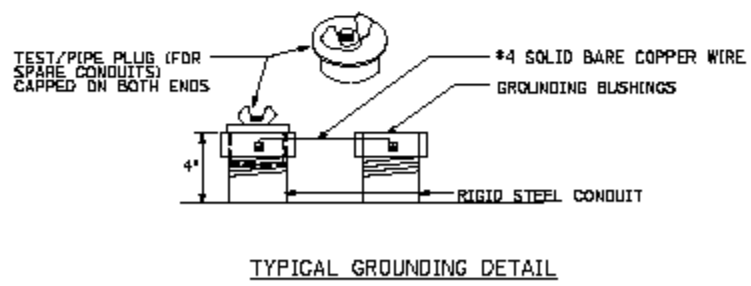
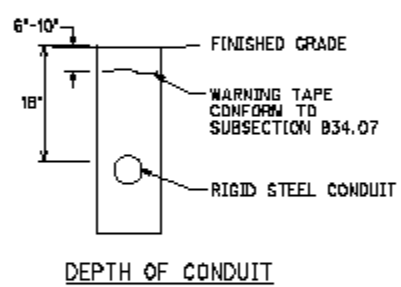
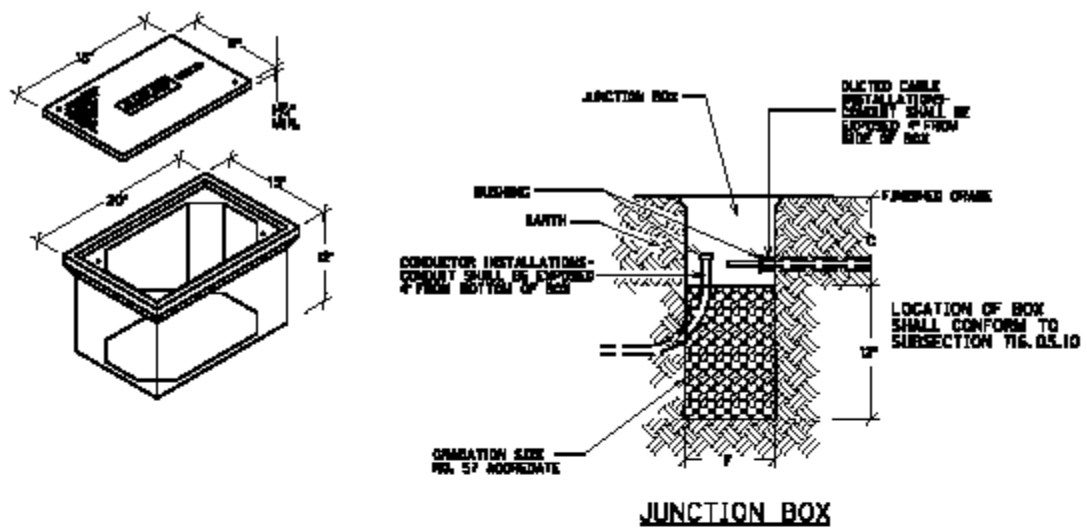
Bid notes not included in Section 723 in the Standard Specifications or that are contrary

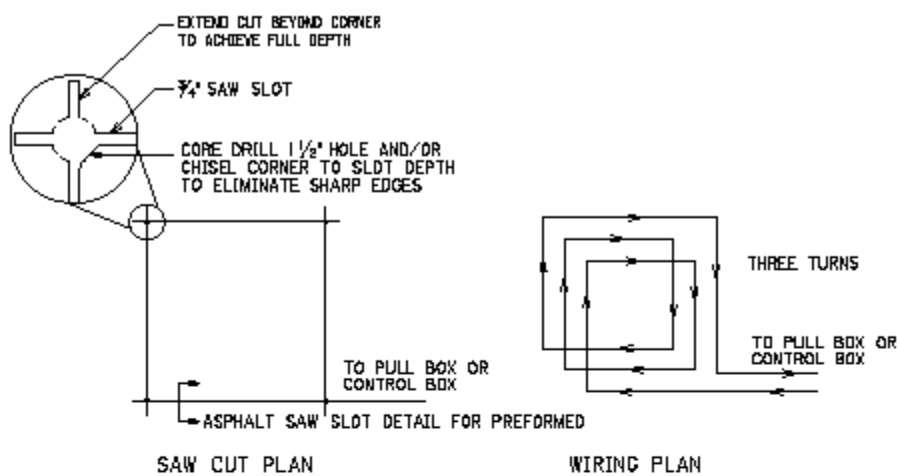
Loop saw slot and fill shall include sawing, cleaning saw slot as well as furnishing and installing loop sealant, backer rod and non-shrink grout as shown on the details. The contractor shall saw according to the dimensions shown on the detail sheets and not cut out any sections of pavement by over-sawing any slot. The ¾” conduit referenced in the Loop Wire Transition details is incidental to this project and not a separate pay item.

Preformed Quadrapole Loops, Preformed Loops and Preformed Loop Lead-In shall include furnishing and installing preformed quadrapole loops, preformed loops and preformed loop lead-in. Items installed in saw slots shall be installed as shown on the Saw Slot detail. All connections and fittings required for a full and complete installation of the loops are incidental to this item.

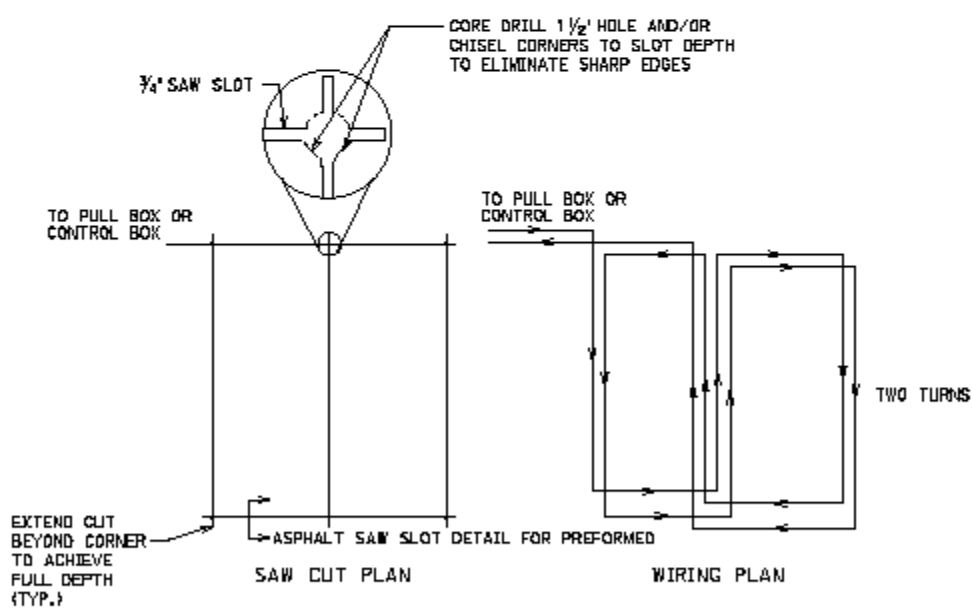
V. PAYMENT. The Department will make payment for completed and accepted quantities under the following:

Code	Pay Item	Pay Unit
4792	Conduit 1”	Linear Foot
4793	Conduit 1 ¼”	Linear Foot
4795	Conduit 2”	Linear Foot
4811	Junction Box Type B	Each
4820	Trenching and Backfilling	Linear Foot
4850	Cable-No. 14/1 Pair	Linear Foot
4895	Loop Saw Slot and Fill	Linear Foot
4894	Preformed Loop Lead-In	Linear Foot
20453NS835	Preformed Quadrapole Loops	Linear Foot
20452NS835	Preformed Loops	Linear Foot

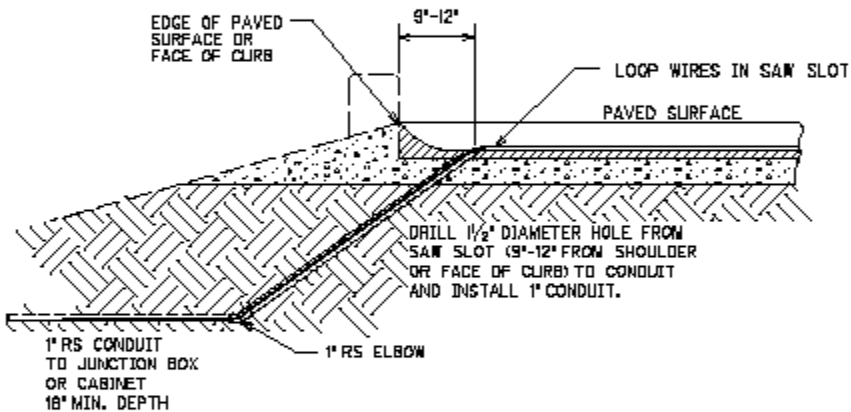
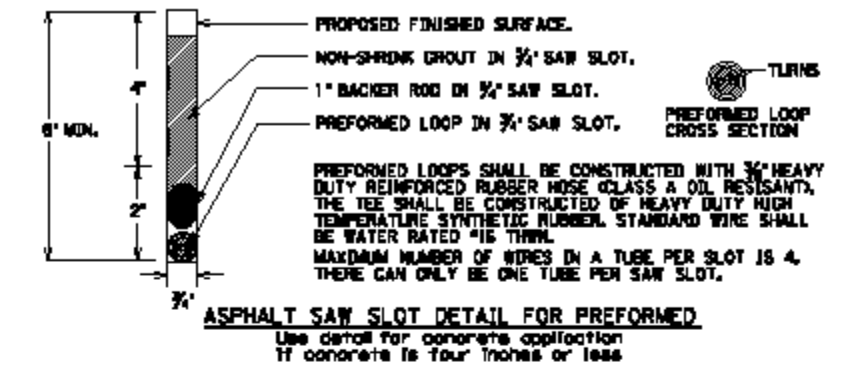




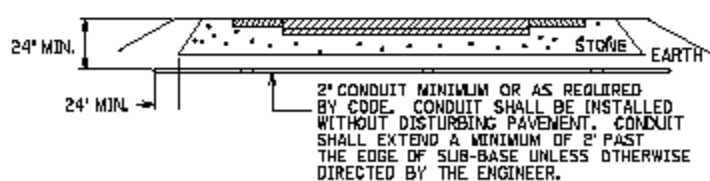
6'X6' LOOP



6'X30' QUADRAPOLE LOOP

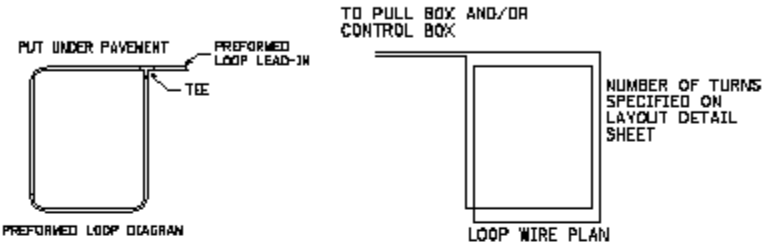


SAW SLOT EDGE OF PAVEMENT TRANSITION



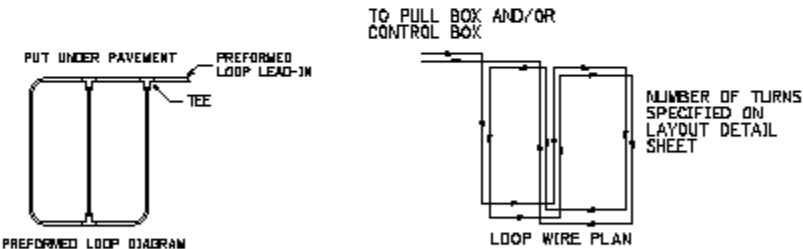
CONDUIT UNDER EXISTING PAVEMENT DETAIL

PREFORMED LOOP LEAD-IN SHALL BE TWISTED WITH THREE TO FIVE TURNS PER FOOT UNTIL TERMINATED AT FIELD CONNECTIONS IN THE CABINET OR CONNECTED TO SHIELDED CABLE.



STANDARD PREFORMED LOOP
*ALL LOOPS THAT ARE NOT QUADRAPOLES SHALL BE STANDARD AND HAVE 3 TURNS

PREFORMED LOOP LEAD-IN SHALL BE TWISTED WITH THREE TO FIVE TURNS PER FOOT UNTIL TERMINATED AT FIELD CONNECTIONS IN THE CABINET OR CONNECTED TO SHIELDED CABLE.



QUADRAPOLE PREFORMED LOOP
*ALL 6'x30' LOOPS SHALL BE QUADRAPOLE AND SHALL HAVE A 2-4-2 CONFIGURATION

SPECIAL NOTE FOR LIQUIDATED DAMAGES

Liquidated Damages will be assessed as outlined in Section 108.09 of the Kentucky Standard Specifications for the specified completion date of the project.

An additional liquidated Damage will be assessed equal to that specified in section 108.09 of the Kentucky Standard Specifications for each calendar day past **24 days** that it takes to complete the JPC pavement including the joints being sealed in Phase 4a – 4D of the project phasing.

All liquidated damages will be applied accumulatively.

All other applicable portions of Section 108 apply.

SPECIAL PROVISION FOR WASTE AND BORROW SITES

Obtain U.S. Army Corps of Engineer's approval before utilizing a waste or borrow site that involves "Waters of the United States". The Corps of Engineers defines "Waters of the United States" as perennial or intermittent streams, ponds or wetlands. The Corps of Engineers also considers ephemeral streams, typically dry except during rainfall but having a defined drainage channel, to be jurisdictional waters. Direct questions concerning any potential impacts to "Waters of the United States" to the attention of the appropriate District Office for the Corps of Engineers for a determination prior to disturbance. Be responsible for any fees associated with obtaining approval for waste and borrow sites from the U.S. Army Corps of Engineer or other appropriate regulatory agencies.

1-296 Waste & Borrow Sites
01/02/2012

SPECIAL NOTE FOR DOUBLE ASPHALT SEAL COAT

Use RS-2 or RS-2C asphalt material that is compatible with the seal aggregate. Apply the first course of asphalt seal coat at the rate of 3.2 lbs/sy of asphalt and 30 lbs/sy of size #78 seal coat aggregate. Apply the second course at 2.8 lbs/sy of asphalt and 20 lbs/sy of size #9M seal coat aggregate. The Engineer may adjust the rate of application as conditions warrant. Use caution in applying liquid asphalt material to avoid over spray getting on curbs, gutter, barrier walls, bridges, guardrail, and other roadway appurtenances.

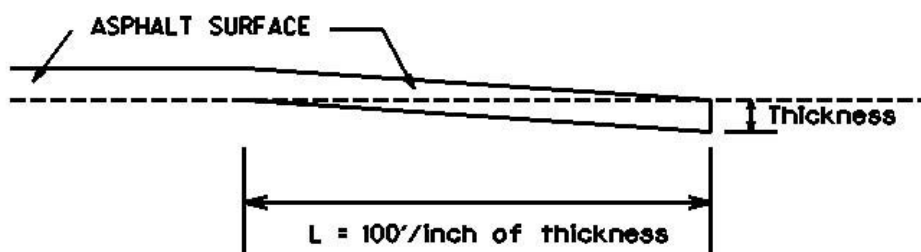
The Department will not measure any surface preparation required prior to applying the asphalt seal coat, but shall be incidental to “Asphalt Material for Asphalt Seal Coat”.

1-3215 Double Asphalt Seal Coat
01/02/2012

SPECIAL NOTE FOR EDGE KEY

Construct Edge Keys at the beginning of project, end of project, at railroad crossings, and at ramps, as applicable. Unless specified in the Contract or directed by the Engineer, do not construct edge keys at intersecting streets, roads, alleys, or entrances. Cut out the existing asphalt surface to the required depth and width shown on the drawing and heel the new surface into the existing surface. The Department will make payment for this work at the Contract unit price per ton for Asphalt Pavement Milling and Texturing, which shall be full compensation for all labor, materials, equipment, and incidentals for removal and disposal of the existing asphalt surface required to construct the edge key.

EDGE KEY



Thickness = 1.0 Inches

L = 100 LF

L= Length of Edge Key

**SPECIAL NOTE FOR
ASPHALT MILLING AND TEXTURING**

Begin paving operations within **48 hours** of commencement of the milling operation. Continue paving operations continuously until completed. If paving operations are not begun within this time period, the Department will assess liquidated damages at the rate prescribed by Section 108.09 until such time as paving operations are begun.

Contrary to Section 408, the Department will retain possession of the material obtained from the milling operations. Deliver this material to the State Maintenance facility in Laurel County.

NOTICE TO CONTRACTOR: The Department considers transfer of millings to the state maintenance facility to be a part of the construction project, therefore truck operators are subject to receiving prevailing wages.

SPECIAL NOTES FOR TRAFFIC ISLAND REMOVAL

Remove existing traffic islands as shown on the drawings or listed in the summary. Saw cut the existing pavement, asphalt surface, base, DGA or PCC pavement (if present). Excavate to an approximate depth of 6 inches below the bottom of the existing pavement level if DGA is not encountered. If no stone is encountered, then place 6 inches of DGA at the removal area. Remove and dispose of all materials. Do not damage existing culvert pipes and any existing underground utilities. Repair or restore any damaged items at no additional cost to the Department. Waste all removed materials off the Right of Way at sites obtained by the Contractor.

The area of removal will be replaced with JPC Pavement 10"/24. The placement of this material is outlined in the traffic control plan and according to the Kentucky Standard Specifications and Standard Drawings.

Payment at the Contract unit prices per square yard for "Remove Traffic Island" and per square yard for "JPC Pavement 10"/24" shall be full compensation for all labor, materials, equipment, and incidentals for saw cutting pavement, excavating and disposing of all materials, furnishing and placing the JPC Pavement, and all other items necessary to complete the work to the satisfaction of the Engineer.

SPECIAL NOTE FOR TYPICAL SECTION DIMENSIONS

Consider the dimensions shown on the typical sections for pavement and shoulder widths and thickness' to be nominal or typical dimensions. The Engineer may direct or approve varying the actual dimensions to be constructed to fit existing conditions. Do not widen existing pavement or shoulders unless specified elsewhere in this proposal or directed by the engineer.

1-3725 Typical Section Dimensions
01/02/2012

TRAFFIC CONTROL PLAN

TRAFFIC CONTROL GENERAL

Except as provided herein, traffic shall be maintained in accordance with the current Standard Specifications and the Standard Drawings, current editions. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic". All lane closures used on the project will be in compliance with the appropriate Standard Drawings. DO NOT use cones for lane closures or shoulder closures. Channelizing drum spacing shall be 20-foot during all phases of construction.

Contrary to Section 106.01, traffic control devices used on this project may be new, or used in like new condition, at the beginning of the work and maintained in like new condition until completion of the work. Traffic control devices used on this project must conform to the *Manual on Uniform Traffic Control Devices*, current edition.

The Contractor will be responsible for the movement of traffic signals to accommodate the changing traffic control scheme for the duration of the project. Any splicing, wiring, hooks, brackets and all other incidentals required to move the traffic signal heads to accommodate the new traffic control scheme shall be incidental to the pay item maintain & control traffic. Contractor shall notify the Project Engineer and District Traffic Engineer of plans to switch the traffic control phasing 48 hours prior to the operation. District traffic operations will adjust signal timing / phasing as required for the various traffic control schemes.

PROJECT PHASING & CONSTRUCTION PROCEDURES

Maintain a minimum of one traffic lane (mainline) in each direction at all times during construction. The clear lane width shall be 12 Feet. If traffic should be stopped due to construction operations, and a school bus on an official run arrives on the scene, the Contractor shall make provisions for the passage of the bus as quickly as possible.

CONSTRUCTION PHASING

PHASE 1 – SHOULDER TRENCHING AND ASPHALT PLACEMENT

Trench the Westbound and Eastbound right shoulder at varying widths from 0 feet to 10 feet wide on KY80. The depth of the trenching will be 7 inches. Trenching material will be used for subgrade embankment where necessary and to eliminate any shoulder edge drop-off. Two courses of 3.5 inch Class 2 Asphalt Base 0.75D PG64-22 will be placed. The Right Eastbound and Westbound lane of KY 80 and the Hal Rodgers Parkway and the right left turn lane of US25 Northbound will be closed during this phase of construction.

PHASE 2 – JPC REMOVAL AND PLACEMENT IN EASTBOUND AND WESTBOUND LEFT THRU LANES, LEFT TURN LANES AND MEDIAN

Channelize traffic using drums to one lane Eastbound and Westbound on KY80 and the Hal Rodgers Parkway utilizing the asphalt paved right shoulders and right thru lanes. Close the left most turning lane on US 25 Northbound and Southbound, and the right lane on US25 Southbound and Northbound prior to the concrete removal area. The clear lane width will be 12 feet. Notification of oversized wide load restrictions shall be directed to KVE and Permits. Remove the PCC pavement areas from the Eastbound left thru lane, the Westbound left thru lane except in the middle of the intersection of KY80, Hal Rodgers Parkway and US25, the left turn lanes and median, prepare the subbase if necessary and place the new JPC Pavement 10 IN - 24 hour. All JPC Pavement will conform to Section 502 in the Kentucky Standard Specifications. A six foot buffer zone as shown in the typical sections is desired in all construction work areas for constructability and separation from the traveling public.

PHASE 3 – JPC REMOVAL AND PLACEMENT IN EASTBOUND RIGHT THRU LANE, WESTBOUND RIGHT THRU LANE, RIGHT TURN LANE ONTO US25 NORTHBOUND AND RAMP ONTO US25 SOUTHBOUND.

Channelize traffic using drums to one lane Eastbound and Westbound in the new JPC pavement area on KY80 and the Hal Rodgers Parkway. Close the right left turn lane on US 25 Northbound and Southbound while keeping the right thru lane on US25 Northbound and Southbound closed as outlined in phase 2 of the traffic control plan. In addition, the State Police Road and KY6259 will be closed to thru traffic. A clear lane width of 12 feet will be maintained during construction. Remove the PCC pavement from the Eastbound and Westbound right thru lane on KY80 and the Hal Rodgers Parkway, the Eastbound ramp onto US25 Southbound, the Westbound right turn lane on the Hal Rodgers Parkway and the concrete aprons for the State Police Road and KY6259; prepare the subbase if necessary; and place the new 10" JPC Pavement 24 hour. All JPC Pavement will conform to Section 502 in the Kentucky Standard Specifications. Construct asphalt shoulders as per the typical section along the KY80 ramp Eastbound onto US25 Southbound and fill any voids between the asphalt shoulder and the new JPC Pavement with asphalt base.

PHASE 4A – 4D: JPC REMOVAL AND PLACEMENT IN THE INTERSECTION OF KY80, HAL RODGERS PARKWAY AND US25.

Channelize traffic beginning approximately 500 foot prior to the intersection to accommodate one lane traffic in all directions except for one left turn lane on US25 Northbound. During this phase of construction, all permanent pavement markings within approximately 500 foot of the intersection on US25 will be removed and traffic maintained using barrels and temporary removable tape.

PHASE 5 – ASPHALT SHOULDER PAVING, SIGNAL EQUIPMENT, SHOULDERING AND PERMANENT PAVEMENT MARKINGS

Perform asphalt milling and texturing on the right shoulders and modified edge keys along KY80 and the Hal Rodgers Parkway to receive asphalt surface as shown on the typical sections. Mill the existing asphalt shoulder to 1 inch below the final grade of the JPC Pavement. Place 1 inch of Class 2 Asphalt Surface 0.38D PG64-22 on all asphalt base shoulders and the existing shoulders on the Hal Rodgers parkway. Regrade/reshape the DGA Base shoulders on all legs of the US25, KY80 and the Hal Rodgers Intersection and construct an asphalt seal coat 4-feet in width in two applications. The clear lane width will be 12 feet. The Quadrapole Loops and Loop Lead-Ins constructed under the JPC Pavement will be connected at a junction box and then wired to the controller cabinet. The contractor will remove and install the lighting control box and the signal control box. Lane closures will be shortened, reduced to a shoulder closure, or removed as appropriate, when the Contractor does not have active operations requiring a lane closure. Once the loops and lighting equipment are installed, the permanent pavement markings shall be placed using lane closures and mobile operations as required the entire length of the project.

LANE CLOSURES

Limit the lengths of lane closures to only that needed for actual operations in accordance with the phasing specified herein, or as directed by the Engineer. Lane closures must be monitored 24 hours a day by the contractor in order to provide safe travel for the general public and to ensure protection of the work zone.

SIGNS

Contrary to Section 112.04.02 and 112.04.03, Low Shoulder Signs will not be measured for payment, but shall be incidental to Maintain and Control Traffic. Contrary to section 112.04.02, only long term construction signs (signs intended to be continuously in place for more than 3 days) will be measured for payment; short term signs (signs intended to be left in place 3 days or less) will not be measured for payment but shall be incidental to Maintain and Control Traffic.

Traffic control signs in addition to normal lane closure signing detailed on the Standard Drawings may be required by the Engineer. Contrary to Section 112, Individual signs will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated for the duration of the project. Replacements for damaged signs or signs directed to be replaced by the Engineer due to poor legibility or reflectivity will not be measured for payment. A quantity of signs has been included for lane shifts and lane splits. These signs will be paid once no matter how many times they are moved or relocated. **All legs of the intersection shall be reduced to a 35mph speed limit during all construction activities.**

Relocate and reset or cover existing permanent signs as required by the work. Obtain the Engineer's approval before removing or covering an existing sign. The Department will not

measure relocating and resetting or covering existing permanent signs, but shall be incidental to Maintain and Control Traffic.

COORDINATION OF WORK

The Contractor is advised that other projects may be in progress within or in the near vicinity of this project. The traffic control of those projects may affect this project and the traffic control of this project may affect those projects. The Contractor will coordinate the work on this project with the work of the other contractors. In case of conflict, the Engineer will determine the relative priority for each project.

BARRICADES

Barricades used in lieu of barrels and cones for channelization or delineation will be incidental to Maintain and Control Traffic according to Section 112.04.01. Barricades used to protect pavement removal areas will be bid as each according to Section 112.04.04.

Individual barricades will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged barricades or barricades directed to be replaced by the Engineer due to poor legibility or reflectivity will not be measured for payment.

CHANGEABLE MESSAGE SIGNS

Provide changeable message signs in advance of and within the project at locations determined by the Engineer. If work is in progress concurrently in both directions or if more than one lane closure is in place in the same direction of travel, provide additional changeable message signs as directed by the Engineer. Place changeable message signs one mile in advance of the anticipated queue at each lane closure. As the actual queue lengthens and/or shortens, relocate or provide additional changeable message signs so that traffic has warning of slowed or stopped traffic at least one mile but not more than two miles before reaching the end of the actual queue. The Engineer may vary the designated locations as the work progresses. The Engineer will determine the messages to be displayed. In the event of damage or mechanical/electrical failure, repair or replace the Changeable Message Sign within 24 hours. The Department will measure for payment the maximum number of Changeable Message Signs in concurrent use at the same time on a single day on all sections of the contract. The Department will measure individual Changeable Message Signs only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. The Department will not measure replacements for damaged Changeable Message Signs or for signs the Engineer directs be replaced due to poor condition or readability. Retain possession of the Changeable Message Signs upon completion of the work.

ARROW PANEL

Use arrow panels as shown on the Standard Drawings or as directed by the Engineer. The Department will measure for payment the maximum number of arrow panels in concurrent use at the same time on a single day on all sections of the contract. Individual arrow panels will be measured only once for payment, regardless of how many times they are set, reset, removed, and relocated during the duration of the project. Replacements for damaged arrow panels directed by the Engineer to be replaced due to poor condition or readability will not be measured for payment. Arrow panels will remain the property of the Contractor after construction is complete.

TEMPORARY ENTRANCES

The Contractor will not be required to provide continuous access to farms, single family, duplex, or triplex residential properties during working hours; however, provide reasonable egress and ingress to each such property when actual operations are not in progress at that location. The time during which a farm or residential entrance is blocked shall be the minimum length of time required for actual operations, shall not be extended for the Contractor's convenience, and in no case shall exceed six (6) hours. Notify all residents twenty-four hours in advance of any driveway or entrance closings and make any accommodations necessary to meet the access needs of disabled residents.

Except as allowed by the Phasing as specified above, maintain direct access to all side streets and roads, schools, churches, commercial properties and apartments or apartment complexes of four or more units at all times.

Payment will be allowed at the unit price bid for all asphalt materials required to construct and maintain any temporary entrances which may be necessary to provide temporary access; however, no direct payment will be allowed for aggregates, excavation and/or embankment needed. The Engineer will determine the type of surfacing material, asphalt or aggregate, to be used at each entrance.

TRAFFIC COORDINATOR

Designate an employee to serve as Traffic Coordinator. The Traffic Coordinator will inspect the project maintenance of traffic once every two hours during the Contractor's operations and at any time a lane closure is in place. The Traffic Coordinator will report all incidents throughout the work zone to the Engineer on the project. The Contractor will furnish the name and telephone number where the Traffic Coordinator can be contacted at all times.

During any period when a lane closure is in place, the Traffic Coordinator will arrange for personnel to be present on the project at all times to inspect the traffic control, maintain the signing and devices, and relocate variable message boards as queue lengths change. The personnel will have access on the project to a radio or telephone to be used in case of emergencies or accidents.

TRAFFIC LOOP INSTALLATION

All items required for lane closures related to this item of work shall be considered incidental to bid item "Maintain and Control Traffic". Install Traffic signal loops as per special notes. The Contractor shall coordinate the placement of the traffic loops with the Traffic Engineer.

TYPE I TAPE INTERSECTION MARKINGS

Consider the locations listed as approximate only. Prior to construction, locate and document the locations of the existing markings. After construction, replace the markings at their approximate existing locations or as directed by Engineer. Place markings not existing prior to construction as directed by the Engineer.

PAVEMENT MARKINGS

Coordinate the installation of all temporary and permanent striping and Durable Type I Tape with the Engineer. If there is a deviation from the existing striping plan, a striping plan for the pavement shall be provided to the Contractor prior to the installation of any temporary or permanent markings. Do not install temporary pavement striping, permanent pavement striping, and/or Durable Pavement markings without written permission from the Engineer.

Temporary Striping will be installed as per Section 112 with the following exceptions:

Temporary or Permanent striping or tape shall be in place before a lane is opened to traffic.

Any pavement striping conflicting with the traffic control phasing must be removed and will be paid as striping removal.

If the Contractor's operations or phasing requires temporary markings that must be subsequently removed from the final surface course, Temporary Removable Tape shall be used. This removable tape will be measured separately for payment.

PAVEMENT EDGE DROP-OFFS

A pavement edge between opposing directions of traffic or lanes that traffic is expected to cross in a lane change situation shall not have an elevation difference greater than 1½". Warning signs (MUTCD W8-9 or W8-9A, or W8-11) shall be placed in advance of and at 1500' intervals throughout the drop-off area. Dual posting on both sides of the traveled way shall be required. All transverse transitions between newly surfaced pavement and the existing pavement areas that traffic may cross shall be wedged with asphalt mixture for leveling and wedging. Remove wedges prior to placement of the final surface course.

Pavement edges that traffic is not expected to cross, except accidentally, shall be treated as follows:

Less than 2" - No protection required.

2" to 4" - Place plastic drums, vertical panels, or barricades every 50 feet. Cones may be used in place of plastic drums, panels, and barricades during daylight working hours. Wedge with asphalt mixture for leveling and wedging with a 1:1 or flatter slope in daylight hours, or 3:1 or flatter slope during nighttime hours, when work is not active in the drop-off area.

4 inches and greater – Drop-offs 4 inches or greater will be allowed during duration of the project. Protect with a lane or shoulder closure using drums or barricades; cones will not be allowed for lane or shoulder closures for drop-offs 4 inches or greater. Place drums or barricades with spacing not to exceed 25 feet. Place Type III Barricades facing oncoming traffic at each drop off. If for any reason traffic must be maintained less than 6 feet from the drop off, wedge with DGA with 3:1 or flatter slope when work is not actively in progress in the drop-off area. Once excavation begins, work continuously to construct DGA and asphalt base to eliminate the drop-off. Drop-offs greater than 4 inches within 6 feet of traffic will not be allowed during non-working hours.

Pedestrians & Bicycles - Protect pedestrian and bicycle traffic as directed by the engineer.

USE AND PLACEMENT OF CHANGEABLE MESSAGE SIGNS

The following policy is based upon current Changeable Message Signs (CMS) standards and practice from many sources, including the Federal Highway Administration (FHWA), other State Departments of Transportation, and Traffic Safety Associations. It is understood that each CMS installation or use requires individual consideration due to the specific location or purpose. However, there will be elements that are constant in nearly all applications. Accordingly these recommended guidelines bring a level of uniformity, while still being open to regional experience and engineering judgment.

Application

The primary purpose of CMS is to advise the driver of unexpected traffic and routing situations. Examples of applications where CMS can be effective include:

- Closures (road, lane, bridge, ramp, shoulder, interstate)
- Changes in alignment or surface conditions
- Significant delays, congestion
- Construction/maintenance activities (delays, future activities)
- Detours/alternative routes
- Special events with traffic and safety implications
- Crash/incidents
- Vehicle restrictions (width, height, weight, flammable)
- Advance notice of new traffic control devices
- Real-time traffic conditions (must be kept up to date)
- Weather /driving conditions, environmental conditions, Roadway Weather Information Systems
- Emergency Situations
- Referral to Highway Advisory Radio (if available)
- Messages as approved by the County Engineer's Office

CMS should not be used for:

- Replacement of static signs (e.g. road work ahead), regulatory signage (e.g. speed limits), pavement markings, standard traffic control devices, conventional warning or guide signs.
- Replacement of lighted arrow board
- Advertising (Don't advertise the event unless clarifying "action" to be taken by driver – e.g. Speedway traffic next exit)
- Generic messages
- Test messages (portable signs only)
- Describe recurrent congestion (e.g. rush hour)
- Public service announcements (not traffic related)

Messages

Basic principles that are important to providing proper messages and insuring the proper operation of a CMS are:

- Visible for at least ½ mile under ideal daytime and nighttime conditions
- Legible from all lanes a minimum of 650 feet
- Entire message readable twice while traveling at the posted speed

- Nor more than two message panels should be used (three panels may be used on roadways where vehicles are traveling less than 45 mph). A panel is the message that fits on the face of the sign without flipping or scrolling.
- Each panel should convey a single thought; short and concise
- Do not use two unrelated panels on a sign
- Do not use the sign for two unrelated messages
- Should not scroll text horizontally or vertically
- Should not contain both the words left and right
- Use standardized abbreviations and messages
- Should be accurate and timely
- Avoid filler/unnecessary words and periods (hazardous, a, an, the)
- Avoid use of speed limits
- Use words (not numbers) for dates

Placement

Placement of the CMS is important to insure that the signs is visible to the driver and provides ample time to take any necessary action. Some of the following principles may only be applicable to controlled access roadways. The basic principles of placement for a CMS are:

- When 2 signs are needed, place on same side of roadway and at least 1,000 feet apart
- Place behind semi-rigid/rigid protection (guardrail, barrier) or outside of the clear zone
- Place 1,000 feet in advance of work zone; at least one mile ahead of decision point
- Normally place on right side of roadway; but should be placed closest to the affected lane so that either side is acceptable
- Signs should not be dual mounted (one on each side of roadway facing same direction)
- Point trailer hitch downstream
- Secure to immovable object to prevent thief (if necessary)
- Do not place in sags or just beyond crest
- Check for reflection of sun to prevent the blinding of motorist
- Should be turned ~3 degrees outward from perpendicular to the edge of pavement
- Bottom of sign should be 7 feet above the elevation of edge of roadway
- Should be removed when not in use

Standard Abbreviations

The following is a list of standard abbreviations to be used on CMS.

<u>Word</u>	<u>Abbrev.</u>	<u>Example</u>
Access	ACCS	ACCIDENT AHEAD/USE ACCS RD NEXT RIGHT
Alternate	ALT	ACCIDENT AHEAD/USE ALT RTE NEXT RIGHT
Avenue	AVE	FIFTH AVE CLOSED/DETOUR NEXT LEFT
Blocked	BLKD	FIFTH AVE BLKD/MERGE LEFT
Boulevard	BLVD	MAIN BLVD CLOSED/USE ALT RTE
Bridge	BRDG	SMITH BRDG CLOSED/USE ALT RTE
Cardinal Directions	N, S, E, W	N I75 CLOSED/ DETOUR EXIT 30
Center	CNTR	CNTR LANE CLOSED/MERGE LEFT

Commercial	COMM	OVRSZ COMM VEH/USE I275
Condition	COND	ICY COND POSSIBLE
Congested	CONG	HVY CONG NEXT 3 MI
Construction	CONST	CONST WORK AHEAD/EXPECT DELAYS
Downtown	DWNTN	DWNTN TRAF USE EX 40
Eastbound	E-BND	E-BND I64 CLOSED/DETOUR EXIT 20
Emergency	EMER	EMER VEH AHEAD/PREPARE TO STOP
Entrance, Enter	EX, EXT	DWNTN TRAF USE EX 40
Expressway	EXPWY	WTRSN EXPWY CLOSED/DETOUR EXIT 10
Freeway	FRWY, FWY	GN SYNDR FWY CLOSED/DETOUR EXIT 15
Hazardous Materials	HAZMAT	HAZMAT IN ROADWAY/ALL TRAF EXIT 25
Highway	HWY	ACCIDENT ON AA HWY/EXPECT DELAYS
Hour	HR	ACCIDENT ON AA HWY/2 HR DELAY
Information	INFO	TRAF INFO TUNE TO 1240 AM
Interstate	I	E-BND I64 CLOSED/DETOUR EXIT 20
Lane	LN	LN CLOSED/MERGE LEFT
Left	LFT	LANE CLOSED/MERGE LFT
Local	LOC	LOC TRAF USE ALT RTE
Maintenance	MAINT	MAINT WRK ON BRDG/SLOW
Major	MAJ	MAJ DELWAYS I75/USE ALT RTE
Mile	MI	ACCIDENT 3 MI AHEAD/ USE ALT RTE
Minor	MNR	ACCIDENT 3 MI MNR DELAY
Minutes	MIN	ACCIDENT 3 MI/30 MIN DELAY
Northbound	N-BND	N-BND I75 CLOSED/ DETOUR EXIT 50
Oversized	OVRSZ	OVRSZ COMM VEH/USE I275 NEXT RIGHT
Parking	PKING	EVENT PKING NEXT RGT
Parkway	PKWY	CUM PKWAY TRAF/DETOUR EXIT 60
Prepare	PREP	ACCIDENT 3 MIL/PREP TO STOP
Right	RGT	EVENT PKING NEXT RGT
Road	RD	HAZMAT IN RD/ALL TRAF EXIT 25
Roadwork	RDWK	RDWK NEXT 4 MI/POSSIBLE DELAYS
Route	RTE	MAJ DELAYS I75/USE ALT RTE
Shoulder	SHLDR	SHLDR CLOSED NEXT 5 MI
Slippery	SLIP	SLIP COND POSSIBLE/ SLOW SPD
Southbound	S-BND	S-BND I75 CLOSED/DETOUR EXIT 50
Speed	SPD	SLIP COND POSSIBLE/ SLOW SPD
Street	ST	MAIN ST CLOSED/USE ALT RTE
Traffic	TRAF	CUM PKWAY TRAF/DETOUR EXIT 60
Vehicle	VEH	OVRSZ COMM VEH/USE I275 NEXT RIGHT
Westbound	W-BND	W-BND I64 CLOSED/DETOUR EXIT 50
Work	WRK	CONST WRK 2MI/ POSSIBLE DELAYS

Certain abbreviations are prone to inviting confusion because another word is abbreviated or could be abbreviated in the same way. DO NO USE THESE ABBREVIATIONS.

<u>Abbrev.</u>	<u>Intended Word</u>	<u>Word Erroneously Given</u>
ACC	Accident	Access (Road)
CLRS	Clears	Colors
DLY	Delay	Daily
FDR	Feeder	Federal
L	Left	Lane (merge)

LOC	Local	Location
LT	Light (traffic)	Left
PARK	Parking	Park
POLL	Pollution (index)	Poll
RED	Reduce	Red
STAD	Stadium	Standard
TEMP	Temporary	Temperature
WRNG	Warning	Wrong

TYPICAL MESSAGES

The following is a list of typical messages used on CMS. The list consists of the reason or problem that you want the driver to be aware of and the action that you want the driver to take.

<u>Reason/Problem</u>	Action
ACCIDENT	ALL TRAFFIC EXIT RT
ACCIDENT/XX MILES	AVOID DELAY USE XX
XX ROAD CLOSED	CONSIDER ALT ROUTE
XX EXIT CLOSED	DETOUR
BRIDGE CLOSED	DETOUR XX MILES
BRIDGE/(SLIPPERY, ICE, ETC.)	DO NOT PASS
CENTER/LANE/CLOSED	EXPECT DELAYS
DELAY(S), MAJOR/DELAYS	FOLLOW ALT ROUTE
DEBRIS AHEAD	KEEP LEFT
DENSE FOG	KEEP RIGHT
DISABLED/VEHICLE	MERGE XX MILES
EMER/VEHICLES/ONLY	MERGE LEFT
EVENT PARKING	MERGE RIGHT
EXIT XX CLOSED	ONE-WAY TRAFFIC
FLAGGER XX MILES	PASS TO LEFT
FOG XX MILES	PASS TO RIGHT
FREEWAY CLOSED	PREPARE TO STOP
FRESH OIL	REDUCE SPEED
HAZMAT SPILL	SLOW
ICE	SLOW DOWN
INCIDENT AHEAD	STAY IN LANE
LANES (NARROW, SHIFT, MERGE, ETC.)	STOP AHEAD
LEFT LANE CLOSED	STOP XX MILES
LEFT LANE NARROWS	TUNE RADIO 1610 AM
LEFT 2 LANES CLOSED	USE NN ROAD
LEFT SHOULDER CLOSED	USE CENTER LANE
LOOSE GRAVEL	USE DETOUR ROUTE
MEDIAN WORK XX MILES	USE LEFT TURN LANE
MOVING WORK ZONE, WORKERS IN ROADWAY	USE NEXT EXIT
NEXT EXIT CLOSED	USE RIGHT LANE
NO OVERSIZED LOADS	WATCH FOR FLAGGER
NO PASSING	

NO SHOULDER
ONE LANE BRIDGE
PEOPLE CROSSING
RAMP CLOSED
RAMP (SLIPPERY, ICE, ETC.)
RIGHT LANE CLOSED
RIGHT LANE NARROWS
RIGHT SHOULDER CLOSED
ROAD CLOSED
ROAD CLOSED XX MILES
ROAD (SLIPPERY, ICE, ETC.)
ROAD WORK
ROAD WORK (OR CONSTRUCTION) (TONIGHT, TODAY, TOMORROW, DATE)
ROAD WORK XX MILES
SHOULDER (SLIPPERY, ICE, SOFT, BLOCKED, ETC.)
NEW SIGNAL XX MILES
SLOW 1 (OR 2) - WAY TRAFFIC
SOFT SHOULDER
STALLED VEHICLES AHEAD
TRAFFIC BACKUP
TRAFFIC SLOWS
TRUCK CROSSING
TRUCKS ENTERING
TOW TRUCK AHEAD
UNEVEN LANES
WATER ON ROAD
WET PAINT
WORK ZONE XX MILES
WORKERS AHEAD

1/16/10

use and placement of changeable message signs.docx

TRAFFIC CONTROL FOR DURABLE PAVEMENT MARKINGS AND THERMOPLASTIC STRIPING

Except as provided herein, maintain and control traffic in accordance with the Standard and Supplemental Specifications and the Standard and Sepia Drawings, current editions. Except for the roadway and traffic control bid items listed, all items of work necessary to maintain and control traffic will be paid at the lump sum bid price to "Maintain and Control Traffic". Contrary to Section 106.01, furnish new, or used in like new condition, traffic control devices at the beginning of the work and maintain in like new condition until completion of the work. Do not install Thermoplastic Markings on bridge decks. Provide egress and ingress to all ramps, side roads, and entrances at all times

TWO-LANE, TWO-WAY ROADWAYS:

Place durable markings and thermoplastic striping according to the Standard Drawings TTS-130-01 and TTS-135-01. Accomplish the work in only one lane and affect the adjacent lane as little as possible. Provide egress and ingress to all ramps, side roads, and entrances at all times. Sign approaches to the immediate work area in accordance with Standard Drawings TTC-100-03 and TTC-105-02. Install the signs on approved temporary mountings.

MULTI-LANE ROADWAYS:

Place durable markings and thermoplastic striping behind stationary lane closures or as a mobile operation. Obtain the Engineer's approval for stationary lane closures prior to use. Sign approved stationary lane closures according to Standard Drawings TTC-115-02 and TTC-125-02. If the Contractor desires an interior lane closure, prepare a plan and obtain the Engineer's approval prior to use. Perform mobile operations according to Standard Drawing TTS-120-01 and TTS-125-01. Install the signs for mobile operations on approved temporary mountings on both sides of the traveled way.

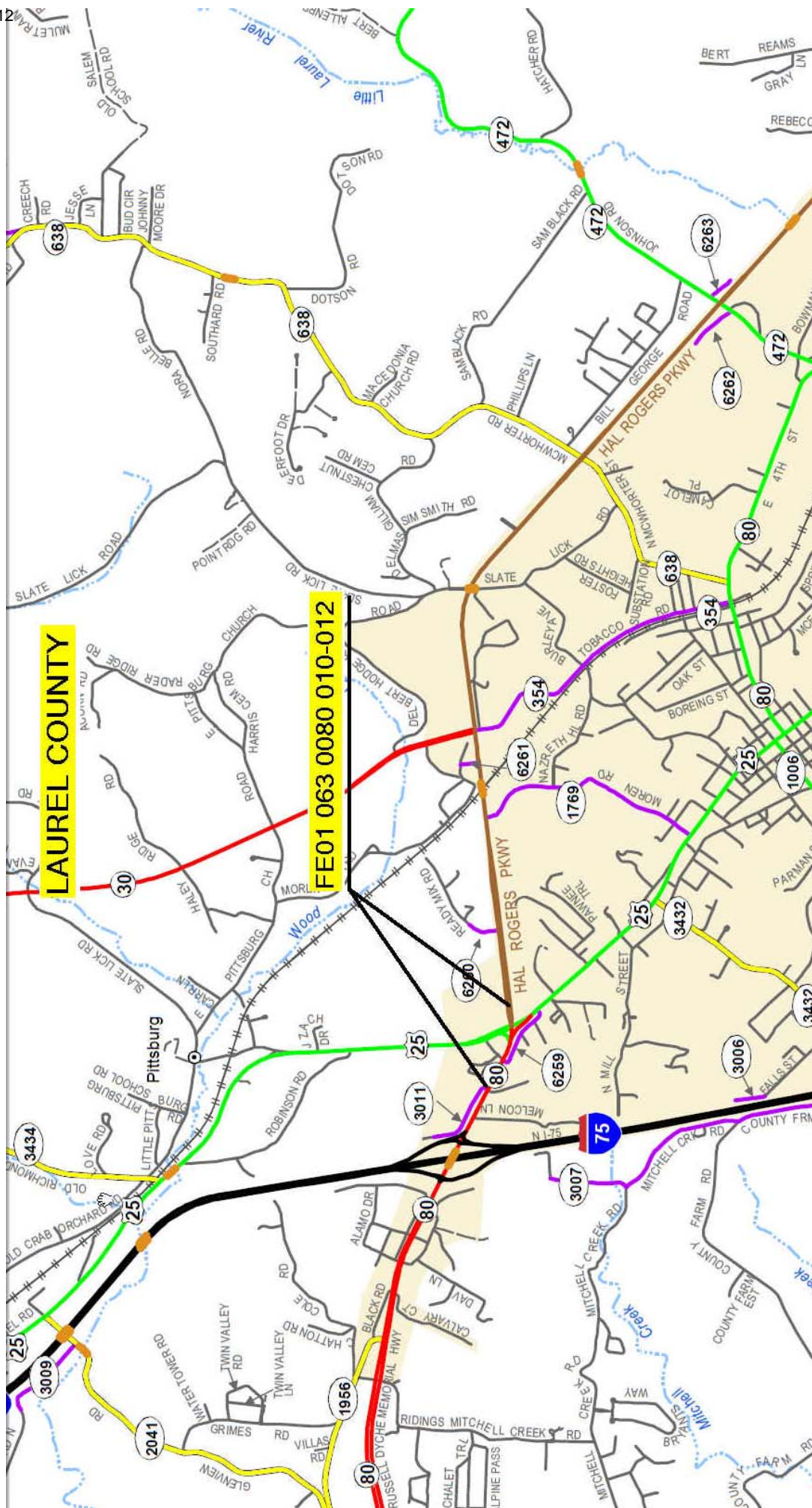
Restrict the work area to not more than one lane of traffic plus 24 inches maximum of only one adjacent lane in each direction of travel. Provide a minimum lane width of 10 feet; however, provide for passage of vehicles of up to 16 feet in width. Limit the length of a lane closure to not exceed 1 mile in urban areas or 3 miles in rural areas as designated by the Engineer. Do not erect more than one lane closure in each direction of travel unless there is at least 2 miles separation between lane closures and both lane closures are in the same lane.

SPECIAL NOTE FOR TRENCHING

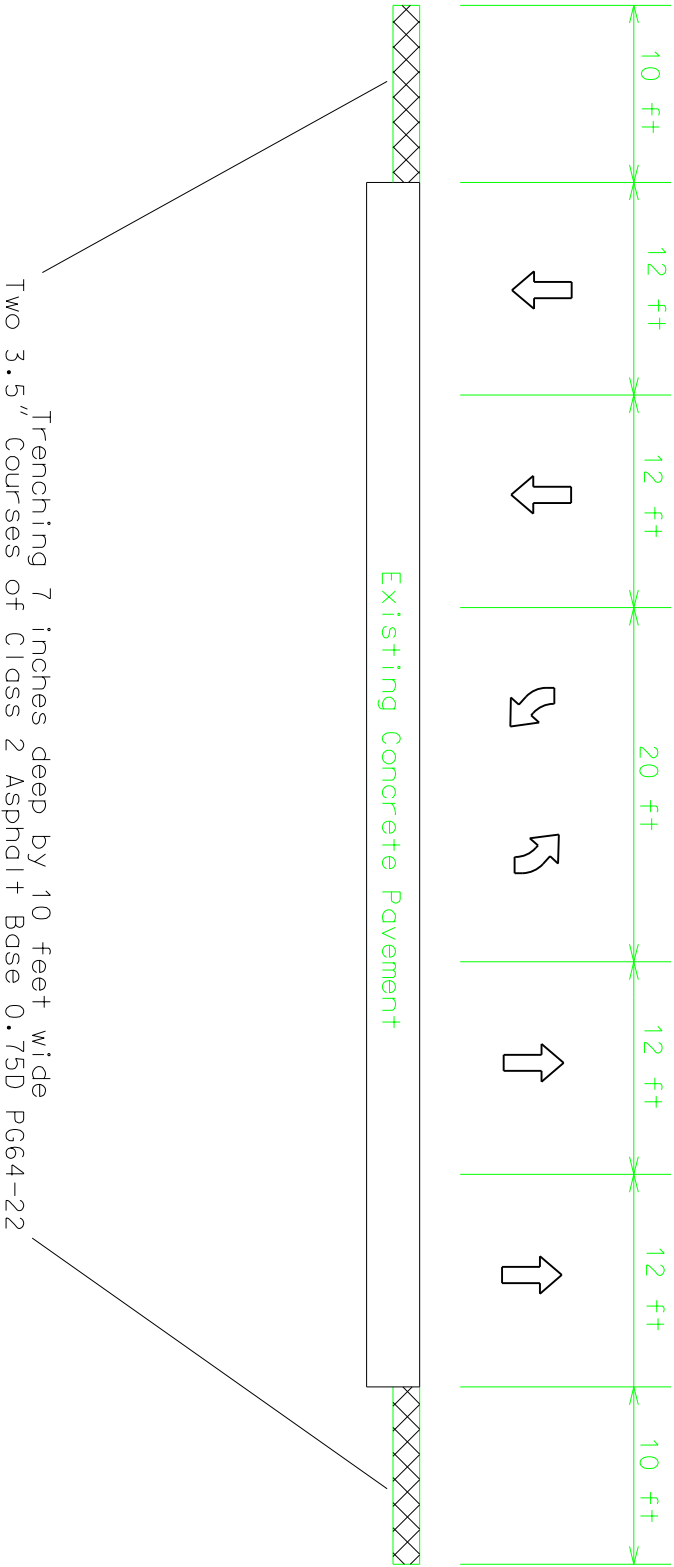
Re-shape and compact excavated material from the trench for subgrade embankment as required to construct the asphalt paved shoulder and on the outside edge of the asphalt paved shoulder to eliminate any shoulder edge drop-offs. Material shall be trenched from the shoulders at a width varying from 0 feet wide to 10 feet. Excess material suitable for use as aggregate not needed to construct the shoulder wedge or be used for embankment shall become the property of the Department. Deliver this material to the State Maintenance facility in Laurel County. Waste only unsuitable materials off the right-of-way at sites obtained by the contractor at no additional cost to the Department.

The Department will measure "Trenching" in linear feet at the pavement/shoulder edge. Payment at the contract unit price per linear feet shall be full compensation for all labor, materials, equipment and incidentals for excavating the shoulder trench and reuse, delivery and/or disposal of the material.

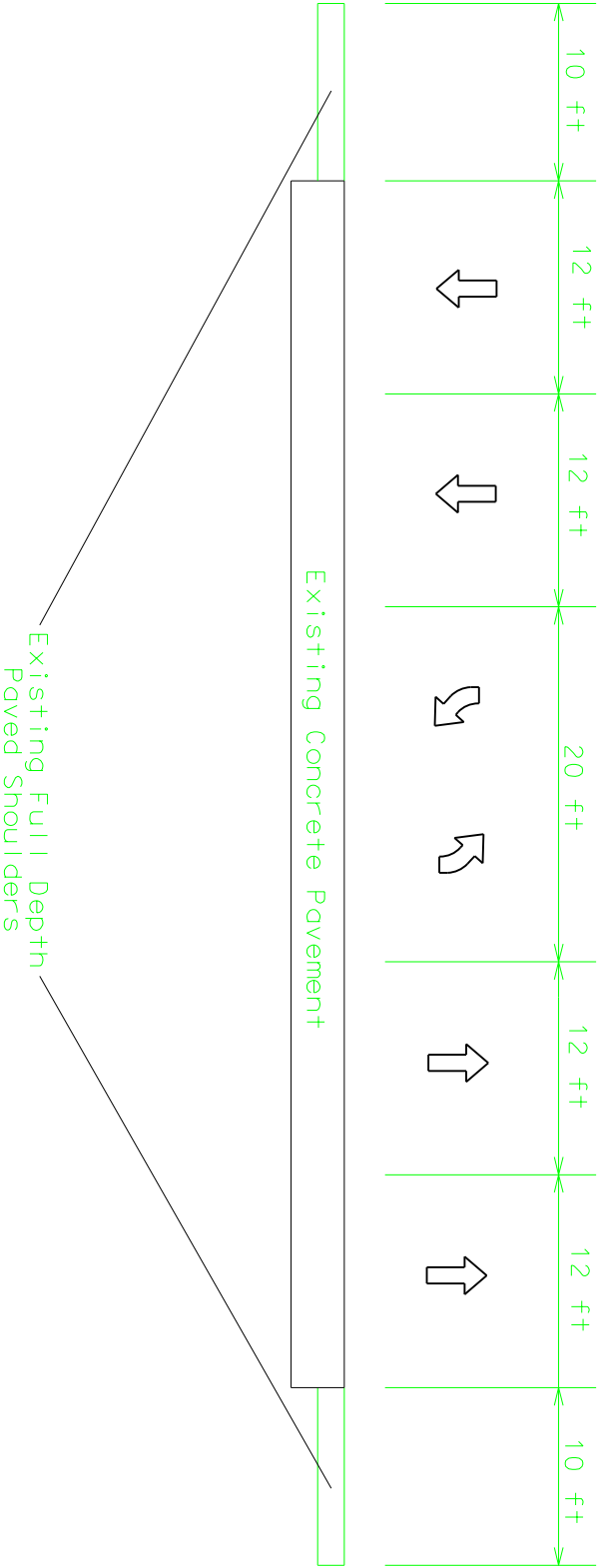
1-3905 trenchingshoulderdepartmentecessmaterial
09/18/03



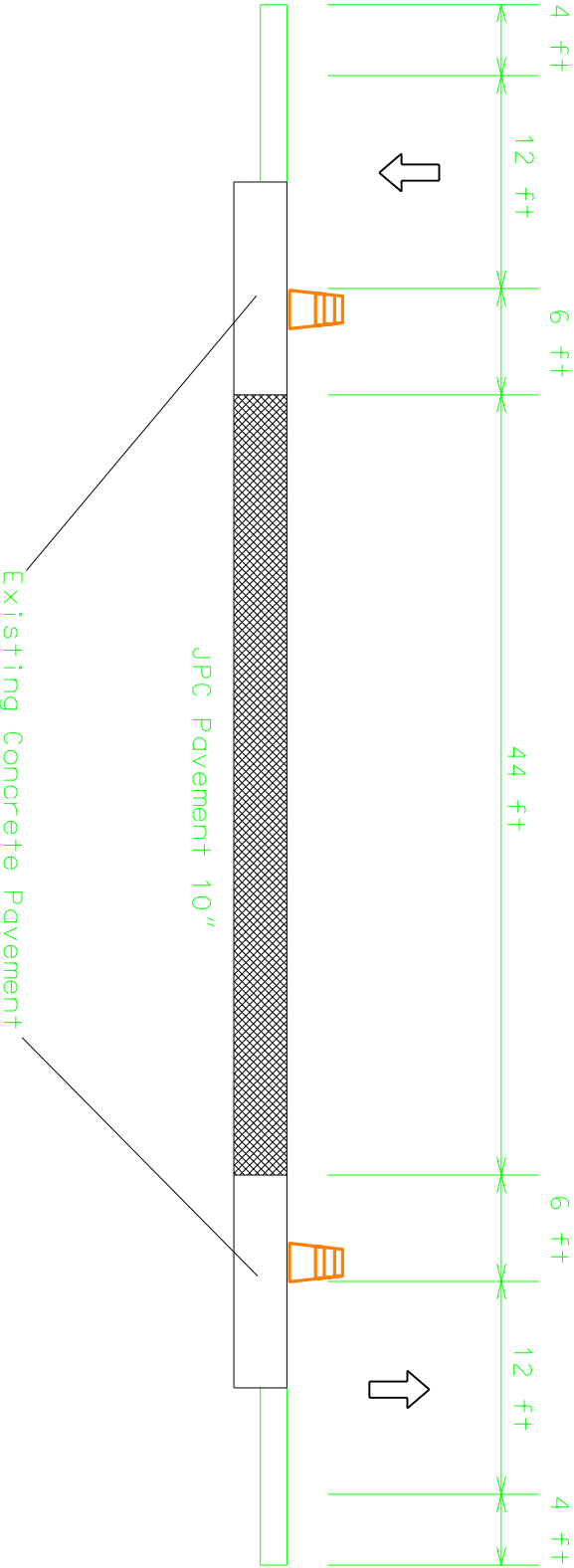
Phase One
Sta. 54+00 - Sta. 64+78
Typical Section



Phase One
Sta. 65+55 - Sta. 70+65
Typical Section

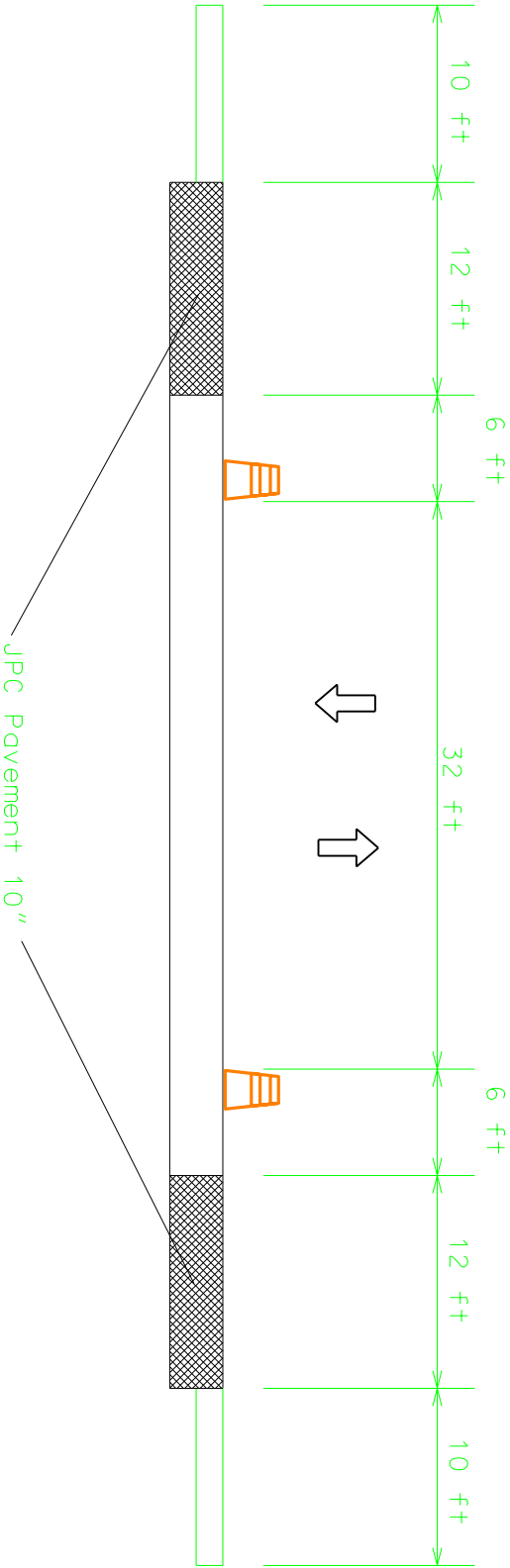


Phase Two
Sta. 54+00 – 64+78
Sta. 65+55 – 70+65
Typical Section



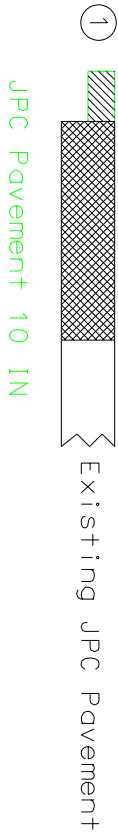
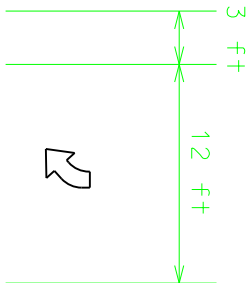
*Sta. 64+78 to Sta. 65+55 is in the intersection and will be done in a later phase.

Phase Three
Sta. 54+00 - 64+78
Sta. 65+55 - 70+65
Typical Section



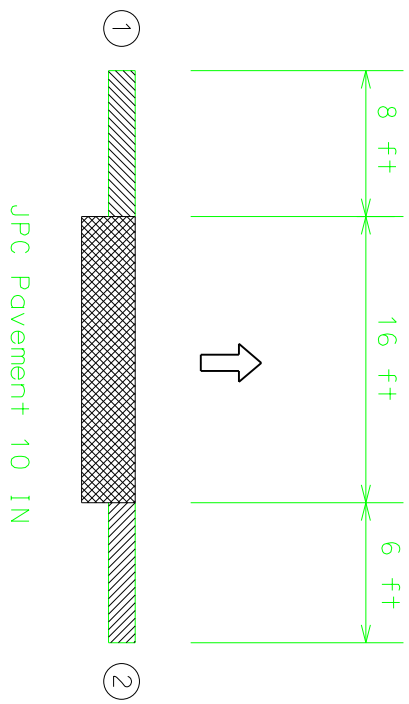
*Sta. 64+78 to Sta. 65+55 is in the intersection and will be done in a later phase.

Phase Three
HR 9006 Right Turning Lane
to US 25 Northbound
Typical Section



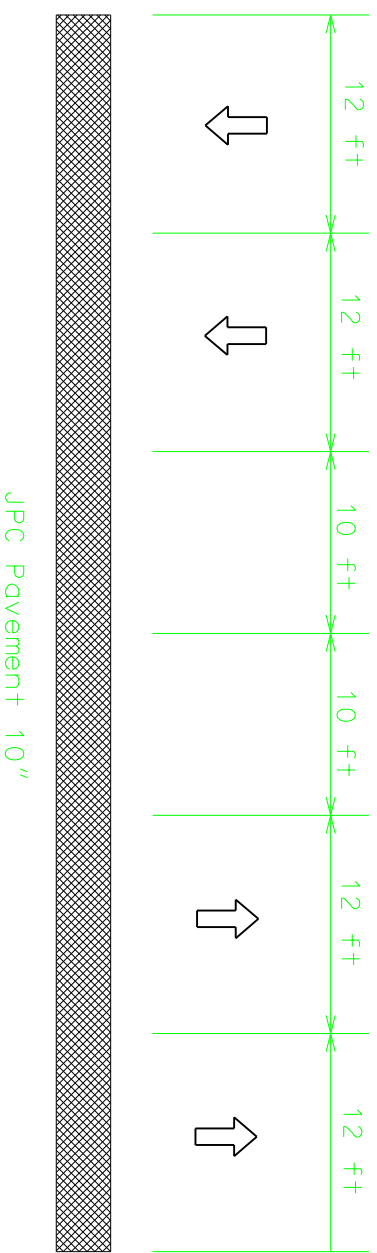
- ① Trenching 7 inches deep by 3 feet wide
Two 3.0" Courses of Class 2 Asphalt Base 0.75D PG64-22
and one course of 1" Class 2 Surface 0.38D PG64-22

Phase Three Ramp to US 25 Southbound Typical Section



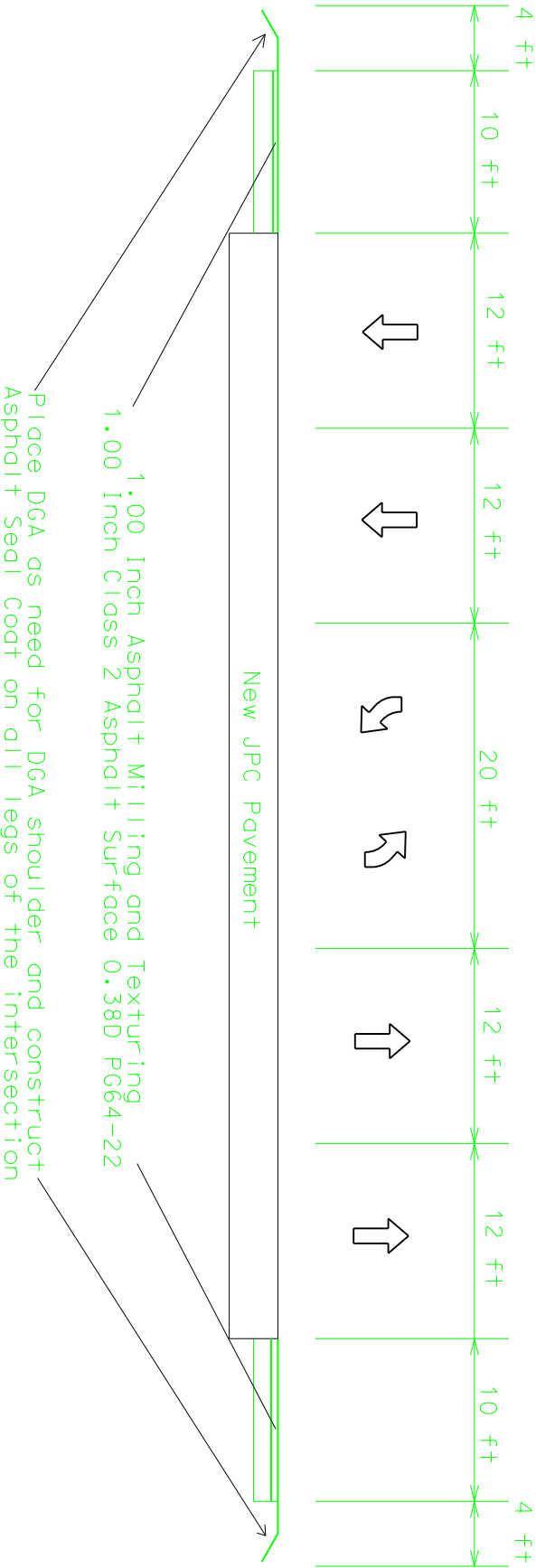
- ① Trenching 7 inches deep by 8 feet wide
Two 3.0" Courses of Class 2 Asphalt Base 0.75D PG64-22
and one course of 1" Class 2 Surface 0.38D PG64-22
- ② Trenching 7 inches deep by 6 feet wide
Two 3.0" Courses of Class 2 Asphalt Base 0.75D PG64-22
and one course of 1" Class 2 Surface 0.38D PG64-22

Phase Four Sta. 64+78 - Sta. 65+55 Typical Section

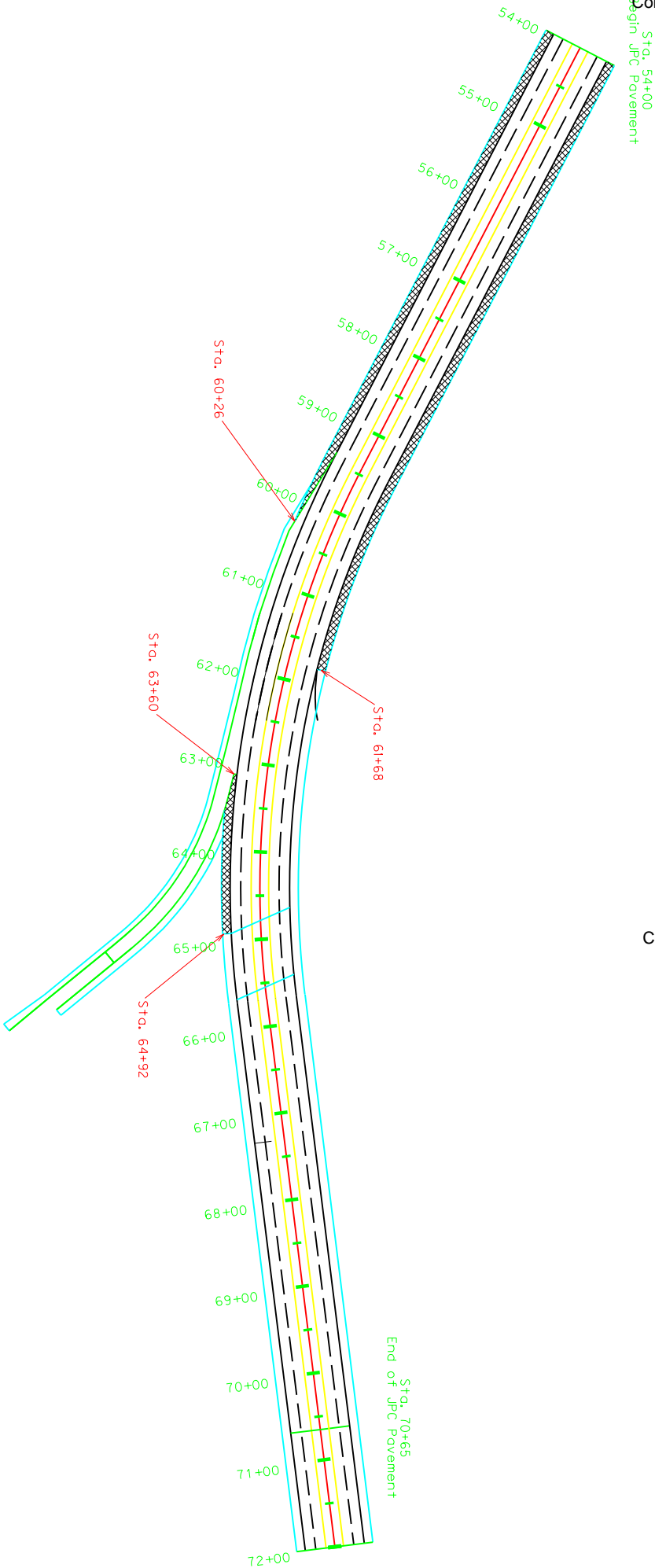


Phase 4 is split into 4 sections of JPC Pavement within the stations listed.
See Phase 4 Traffic Control for section layouts.

Phase Five Sta. 54+00 - Sta. 70+65 Typical Section



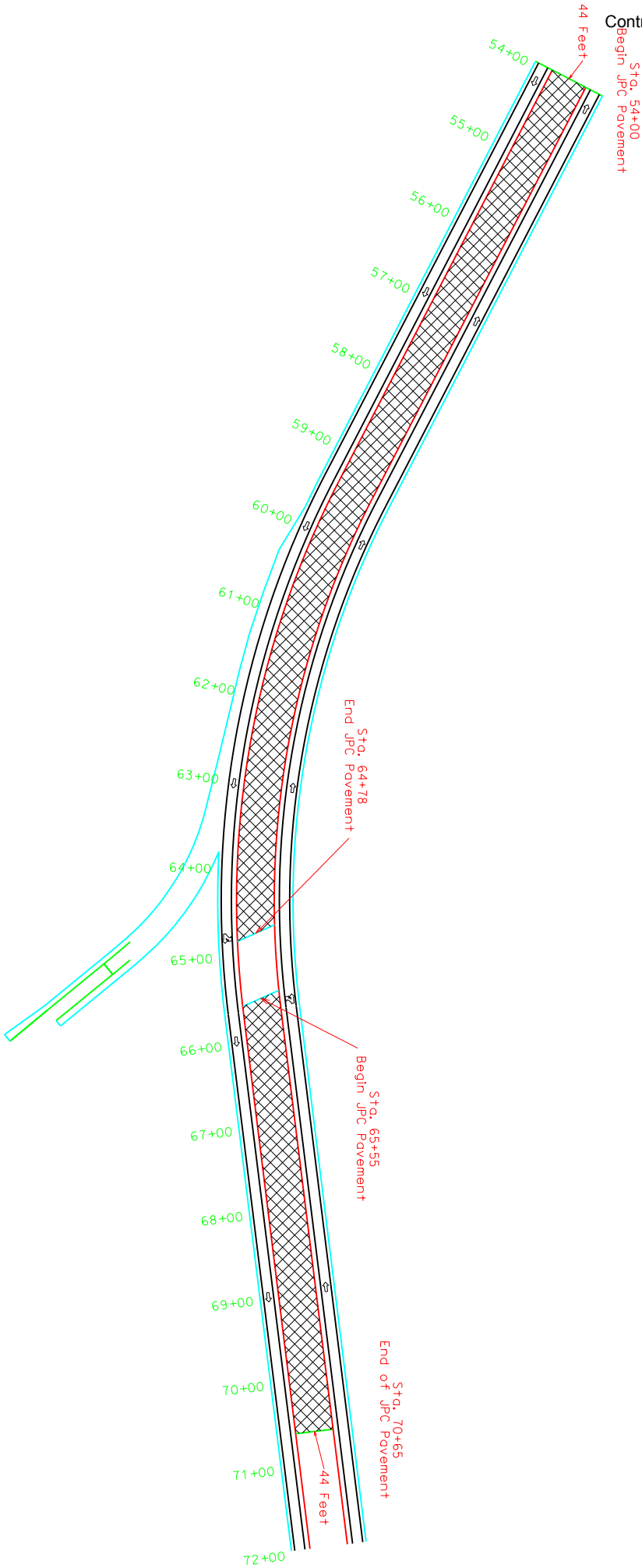
Note: Asphalt Milling and Texturing for a Modified Edge Key will be performed at 210 feet prior to Sta. 54+00 on Ky 80 and 50 post Sta. 70+65 on the Hal Rogers Parkway. The width will be 88 feet across the mainline and shoulders.



Phase One

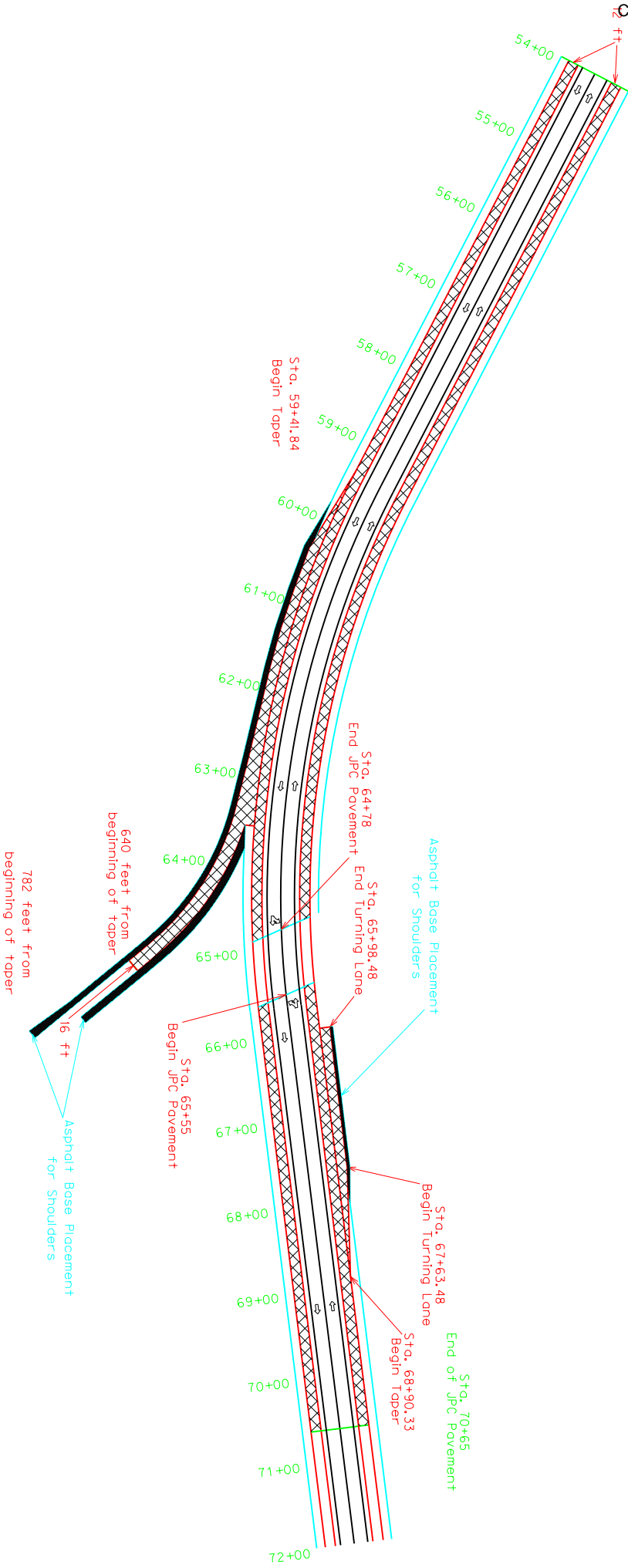
Contract ID: 122438

Phase Two

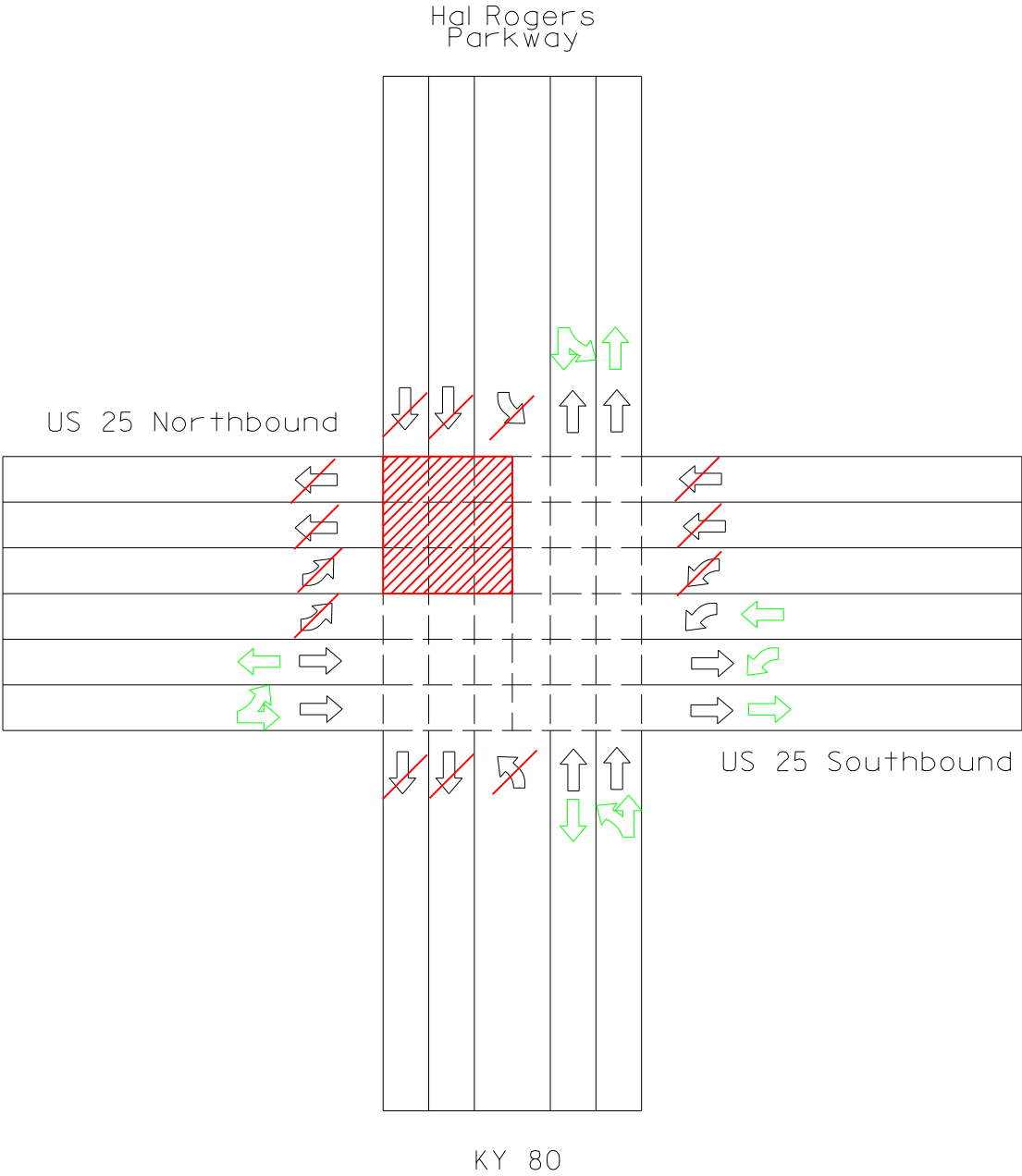


Sta. 54+00
Begin JPC Pavement

Phase Three

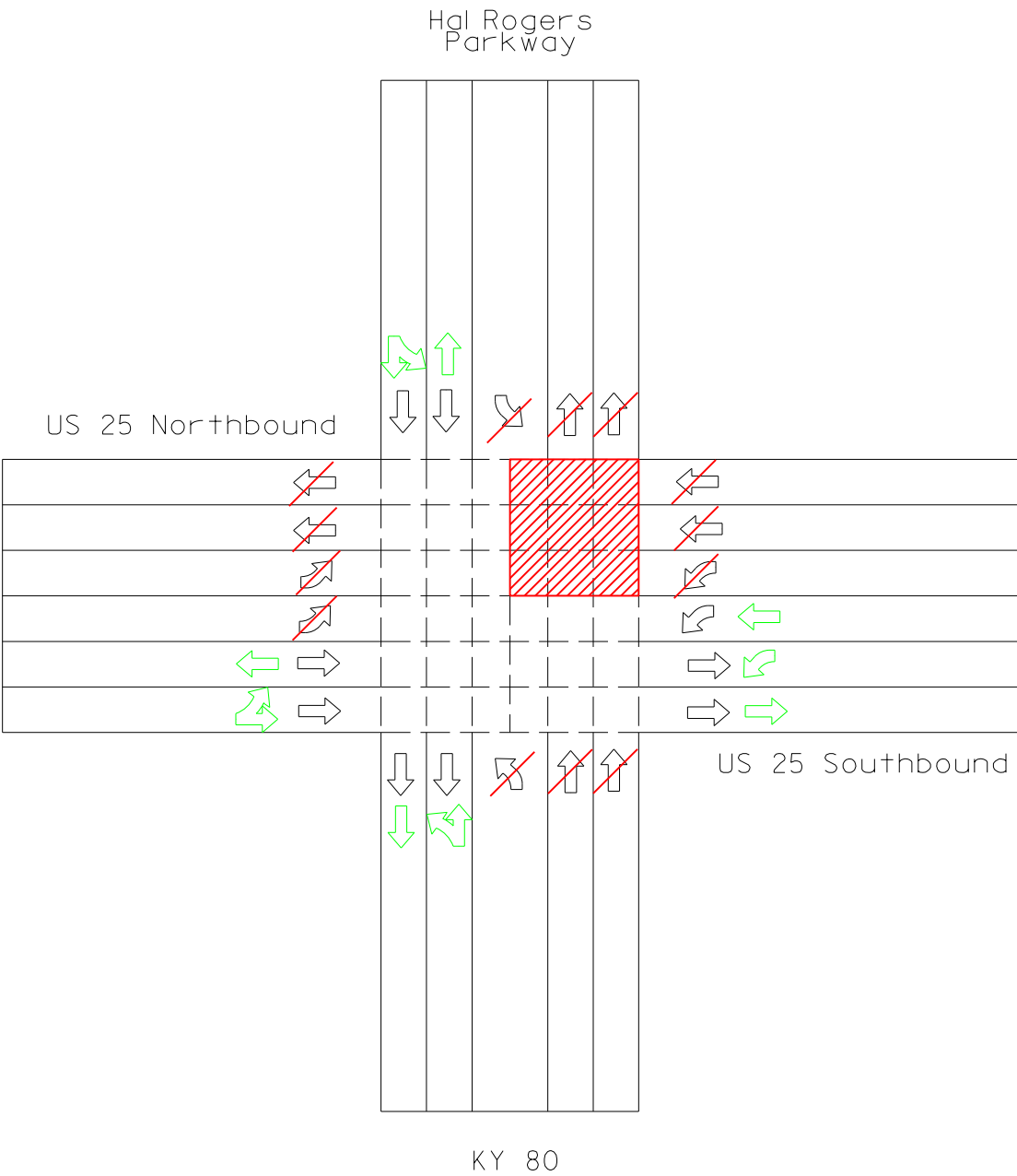


Phase 4A Intersection Construction



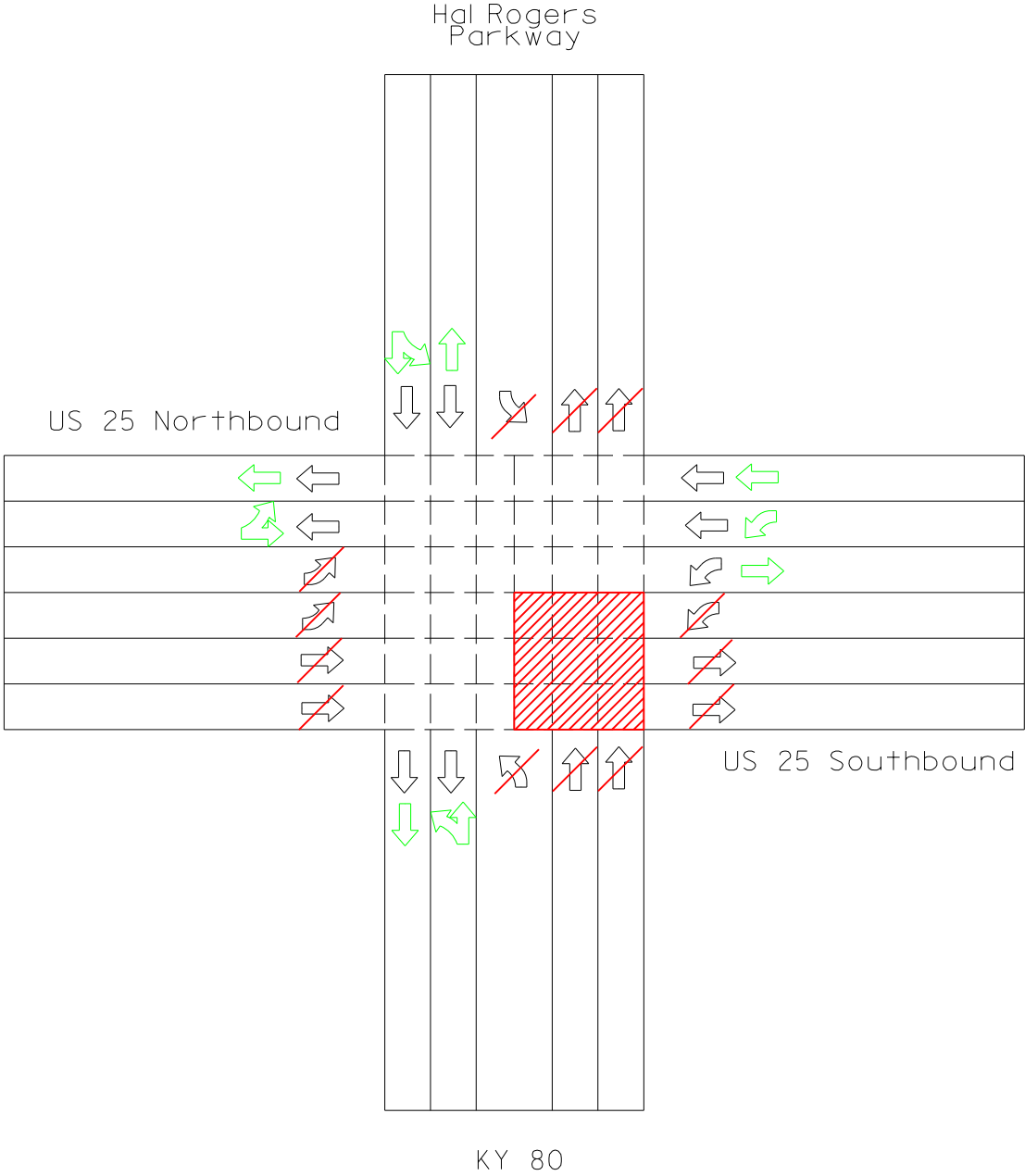
The area to be removed and replaced measures approximately 36 feet by 34 feet.

Phase 4B Intersection Construction



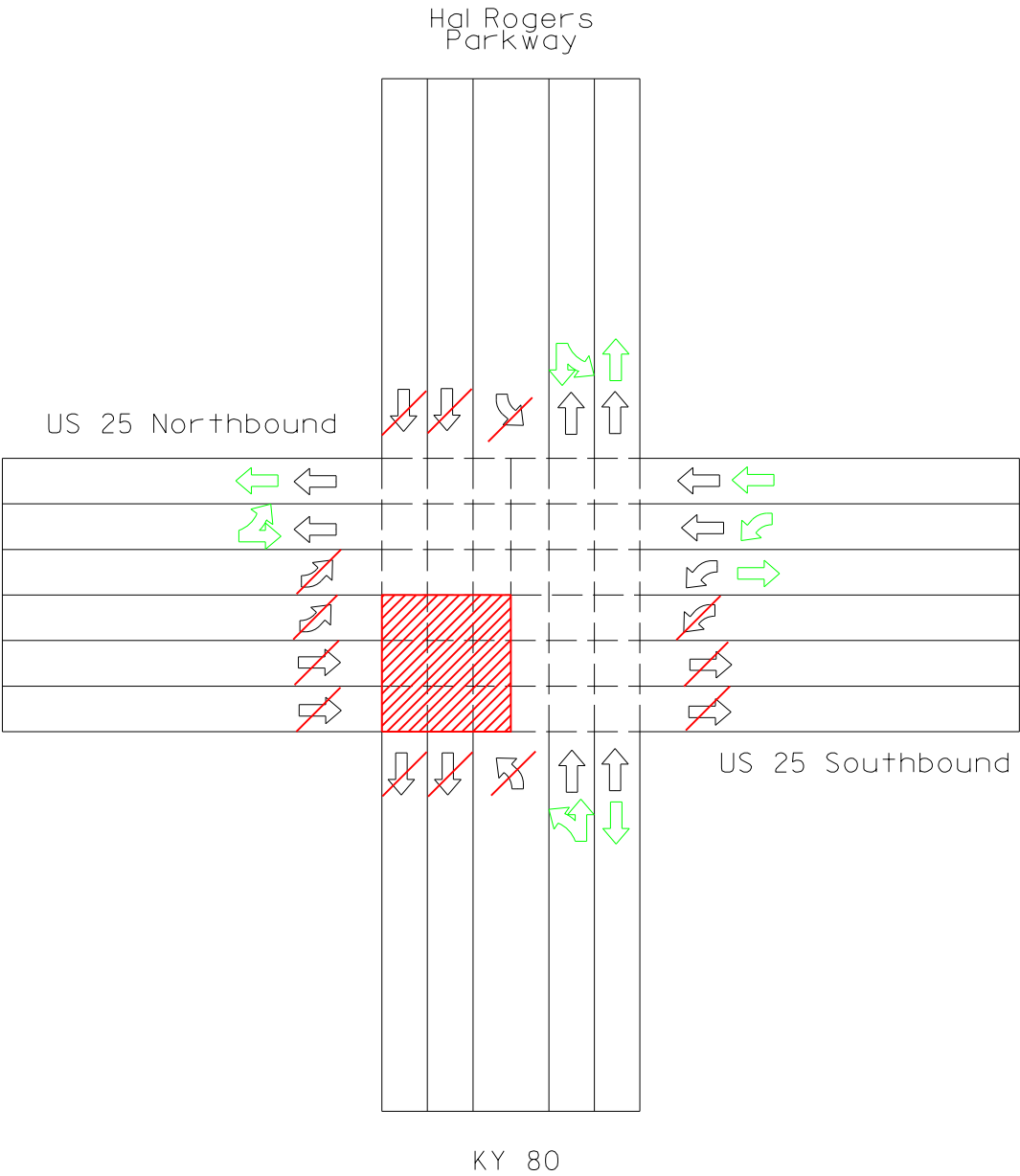
The area to be removed and replaced measures approximately 36 feet by 34 feet.

Phase 4C Intersection Construction



The area to be removed and replaced measures approximately 36 feet by 34 feet.

Phase 4D Intersection Construction



The area to be removed and replaced measures approximately 36 feet by 34 feet.

PART II

SPECIFICATIONS AND STANDARD DRAWINGS

SPECIFICATIONS REFERENCE

Any reference in the plans or proposal to previous editions of the *Standard Specifications for Road and Bridge Construction* and *Standard Drawings* are superseded by *Standard Specifications for Road and Bridge Construction, Edition of 2012* and *Standard Drawings, Edition of 2012 with the 2012 Revision*.

SPECIAL NOTE FOR PORTABLE CHANGEABLE MESSAGE SIGNS

This Special Note will apply when indicated on the plans or in the proposal.

1.0 DESCRIPTION. Furnish, install, operate, and maintain variable message signs at the locations shown on the plans or designated by the Engineer. Remove and retain possession of variable message signs when they are no longer needed on the project.

2.0 MATERIALS.

2.1 General. Use LED Variable Message Signs Class I, II, or III, as appropriate, from the Department's List of Approved Materials.

Unclassified signs may be submitted for approval by the Engineer. The Engineer may require a daytime and nighttime demonstration. The Engineer will make a final decision within 30 days after all required information is received.

2.2 Sign and Controls. All signs must:

- 1) Provide 3-line messages with each line being 8 characters long and at least 18 inches tall. Each character comprises 35 pixels.
- 2) Provide at least 40 preprogrammed messages available for use at any time. Provide for quick and easy change of the displayed message; editing of the message; and additions of new messages.
- 3) Provide a controller consisting of:
 - a) Keyboard or keypad.
 - b) Readout that mimics the actual sign display. (When LCD or LCD type readout is used, include backlighting and heating or otherwise arrange for viewing in cold temperatures.)
 - c) Non-volatile memory or suitable memory with battery backup for storing pre-programmed messages.
 - d) Logic circuitry to control the sequence of messages and flash rate.
- 4) Provide a serial interface that is capable of supporting complete remote control ability through land line and cellular telephone operation. Include communication software capable of immediately updating the message, providing complete sign status, and allowing message library queries and updates.
- 5) Allow a single person easily to raise the sign to a satisfactory height above the pavement during use, and lower the sign during travel.
- 6) Be Highway Orange on all exterior surfaces of the trailer, supports, and controller cabinet.
- 7) Provide operation in ambient temperatures from -30 to + 120 degrees Fahrenheit during snow, rain and other inclement weather.
- 8) Provide the driver board as part of a module. All modules are interchangeable, and have plug and socket arrangements for disconnection and reconnection. Printed circuit boards associated with driver boards have a conformable coating to protect against moisture.
- 9) Provide a sign case sealed against rain, snow, dust, insects, etc. The lens is UV stabilized clear plastic (polycarbonate, acrylic, or other approved material) angled to prevent glare.
- 10) Provide a flat black UV protected coating on the sign hardware, character PCB, and appropriate lens areas.
- 11) Provide a photocell control to provide automatic dimming.

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- 12) Allow an on-off flashing sequence at an adjustable rate.
- 13) Provide a sight to aim the message.
- 14) Provide a LED display color of approximately 590 nm amber.
- 15) Provide a controller that is password protected.
- 16) Provide a security device that prevents unauthorized individuals from accessing the controller.
- 17) Provide the following 3-line messages preprogrammed and available for use when the sign unit begins operation:

/KEEP/RIGHT/⇒⇒⇒/	/MIN/SPEED/**MPH/
/KEEP/LEFT/⇐⇐⇐/	/ICY/BRIDGE/AHEAD/ /ONE
/LOOSE/GRAVEL/AHEAD/	LANE/BRIDGE/AHEAD/
/RD WORK/NEXT/**MILES/	/ROUGH/ROAD/AHEAD/
/TWO WAY/TRAFFIC/AHEAD/	/MERGING/TRAFFIC/AHEAD/
/PAINT/CREW/AHEAD/	/NEXT/**/MILES/
/REDUCE/SPEED/**MPH/	/HEAVY/TRAFFIC/AHEAD/
/BRIDGE/WORK/**0 FT/	/SPEED/LIMIT/**MPH/
/MAX/SPEED/**MPH/	/BUMP/AHEAD/
/SURVEY/PARTY/AHEAD/	/TWO/WAY/TRAFFIC/

*Insert numerals as directed by the Engineer.

Add other messages during the project when required by the Engineer.

2.3 Power.

- 1) Design solar panels to yield 10 percent or greater additional charge than sign consumption. Provide direct wiring for operation of the sign or arrow board from an external power source to provide energy backup for 21 days without sunlight and an on-board system charger with the ability to recharge completely discharged batteries in 24 hours.

3.0 CONSTRUCTION. Furnish and operate the variable message signs as designated on the plans or by the Engineer. Ensure the bottom of the message panel is a minimum of 7 feet above the roadway in urban areas and 5 feet above in rural areas when operating. Use Class I, II, or III signs on roads with a speed limit less than 55 mph. Use Class I or II signs on roads with speed limits 55 mph or greater.

Maintain the sign in proper working order, including repair of any damage done by others, until completion of the project. When the sign becomes inoperative, immediately repair or replace the sign. Repetitive problems with the same unit will be cause for rejection and replacement.

Use only project related messages and messages directed by the Engineer, unnecessary messages lessen the impact of the sign. Ensure the message is displayed in either one or 2 phases with each phase having no more than 3 lines of text. When no message is needed, but it is necessary to know if the sign is operable, flash only a pixel.

When the sign is not needed, move it outside the clear zone or where the Engineer directs. Variable Message Signs are the property of the Contractor and shall be removed from the project when no longer needed. The Department will not assume ownership of these signs.

4.0 MEASUREMENT. The final quantity of Variable Message Sign will be

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the actual number of individual signs acceptably furnished and operated during the project. The Department will not measure signs replaced due to damage or rejection.

5.0 PAYMENT. The Department will pay for the Variable Message Signs at the unit price each. The Department will not pay for signs replaced due to damage or rejection. Payment is full compensation for furnishing all materials, labor, equipment, and service necessary to, operate, move, repair, and maintain or replace the variable message signs. The Department will make payment for the completed and accepted quantities under the following:

<u>Code</u>	<u>Pay Item</u>	<u>Pay Unit</u>
02671	Portable Changeable Message Sign	Each

Effective June 15, 2012

2012 STANDARD DRAWINGS THAT APPLY

TEMPORARY SILT FENCE.....	RDX-210-02
TEMPORARY SILT FENCE WITH WOVEN WIRE FENCE FABRIC	RDX-215
SILT TRAP - TYPE B.....	RDX-225
SILT TRAP - TYPE C.....	RDX-230
CURVE WIDENING AND SUPERELEVATION TRANSITIONS	RGS-001-06
SUPERELEVATION FOR MULTILANE PAVEMENTS	RGS-002-05
MISCELLANEOUS STANDARDS PART 1	RGX-001-05
NON-REINFORCED CONCRETE PAVEMENT FOR SHOULDERS AND MEDIANS.....	RPN-001-06
PAVEMENT TRANSITIONS AND JOINT DETAILS FOR NON-REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS.....	RPN-010-06
NON-REINFORCED CONCRETE PAVEMENT	RPN-015-04
CONCRETE PAVEMENT JOINTS - TYPES AND SPACING	RPN-020-03
CONCRETE PAVEMENT JOINT DETAILS	RPS-010-10
EXPANSION AND CONTRACTION JOINTS - LOAD TRANSFER ASSEMBLIES.....	RPS-020-13
HOT POURED ELASTIC JOINT SEALS FOR CONCRETE PAVEMENT	RPX-015-03
NETTING	RRE-002-04
LANE CLOSURE MULTI-LANE HIGHWAY CASE I.....	TTC-115-02
LANE CLOSURE MULTI-LANE HIGHWAY CASE II	TTC-120-02
SHOULDER CLOSURE.....	TTC-135-01
POST SPlicing DETAIL	TTD-110-01
WORK ZONE SPEED LIMIT AND DOUBLE FINE SIGNS	TTD-120-01
PAVEMENT CONDITION WARNING SIGNS.....	TTD-125-01
MOBILE OPERATION FOR PAINT STRIPING CASE III.....	TTS-110-01
MOBILE OPERATION FOR PAINT STRIPING CASE IV.....	TTS-115-01
MOBILE OPERATION FOR DURABLE STRIPING CASE III.....	TTS-130-01
MOBILE OPERATION FOR DURABLE STRIPING CASE IV	TTS-135-01

PART III

EMPLOYMENT, WAGE AND RECORD REQUIREMENTS

TRANSPORTATION CABINET DEPARTMENT OF HIGHWAYS

LABOR AND WAGE REQUIREMENTS APPLICABLE TO OTHER THAN FEDERAL-AID SYSTEM PROJECTS

- I. Application
- II. Nondiscrimination of Employees (KRS 344)
- III. Payment of Predetermined Minimum Wages
- IV. Statements and Payrolls

I. APPLICATION

1. These contract provisions shall apply to all work performed on the contract by the contractor with his own organization and with the assistance of workmen under his immediate superintendence and to all work performed on the contract by piecework, station work or by subcontract. The contractor's organization shall be construed to include only workmen employed and paid directly by the contractor and equipment owned or rented by him, with or without operators.

2. The contractor shall insert in each of his subcontracts all of the stipulations contained in these Required Provisions and such other stipulations as may be required.

3. A breach of any of the stipulations contained in these Required Provisions may be grounds for termination of the contract.

II. NONDISCRIMINATION OF EMPLOYEES

AN ACT OF THE KENTUCKY GENERAL ASSEMBLY TO PREVENT DISCRIMINATION IN EMPLOYMENT KRS CHAPTER 344 EFFECTIVE JUNE 16, 1972

The contract on this project, in accordance with KRS Chapter 344, provides that during the performance of this contract, the contractor agrees as follows:

1. The contractor shall not fail or refuse to hire, or shall not discharge any individual, or otherwise discriminate against an individual with respect to his compensation, terms, conditions, or privileges of employment, because of such individual's race, color, religion, national origin, sex, disability or age (between forty and seventy); or limit, segregate, or classify his employees in any way which would deprive or tend to deprive an individual of employment opportunities or otherwise adversely affect his status as an employee, because of such individual's race, color, religion, national origin, sex, disability or age (between forty and seventy). The contractor agrees to post in conspicuous places, available to employees and applicants for employment, notices to be provided setting forth the provisions of this nondiscrimination clause.

2. The contractor shall not print or publish or cause to be printed or published a notice or advertisement relating to employment by such an employer or membership in or any classification or referral for employment by the employment agency, indicating any preference, limitation, specification, or discrimination, based on race, color, religion, national origin, sex, disability or age (between forty and seventy), except that such notice or advertisement may indicate a preference, limitation, or specification based on religion, or national origin when religion, or national origin is a bona fide occupational qualification for employment.

3. If the contractor is in control of apprenticeship or other training or retraining, including on-the-job training programs, he shall not discriminate against an individual

because of his race, color, religion, national origin, sex, disability or age (between forty and seventy), in admission to, or employment in any program established to provide apprenticeship or other training.

4. The contractor will send to each labor union or representative of workers with which he has a collective bargaining agreement or other contract or understanding, a notice to be provided advising the said labor union or workers' representative of the contractor's commitments under this section, and shall post copies of the notice in conspicuous places available to employees and applicants for employment. The contractor will take such action with respect to any subcontract or purchase order as the administering agency may direct as a means of enforcing such provisions, including sanctions for non-compliance.

III. PAYMENT OF PREDETERMINED MINIMUM WAGES

1. These special provisions are supplemented elsewhere in the contract by special provisions which set forth certain predetermined minimum wage rates. The contractor shall pay not less than those rates.

2. The minimum wage determination schedule shall be posted by the contractor, in a manner prescribed by the Department of Highways, at the site of the work in prominent places where it can be easily seen by the workers.

IV. STATEMENTS AND PAYROLLS

1. All contractors and subcontractors affected by the terms of KRS 337.505 to 337.550 shall keep full and accurate payroll records covering all disbursements of wages to their employees to whom they are required to pay not less than the prevailing rate of wages. Payrolls and basic records relating thereto will be maintained during the course of the work and preserved for a period of one (1) year from the date of completion of this contract.

2. The payroll records shall contain the name, address and social security number of each employee, his correct classification, rate of pay, daily and weekly number of hours worked, itemized deductions made and actual wages paid.

3. The contractor shall make his daily records available at the project site for inspection by the State Department of Highways contracting office or his authorized representative.

Periodic investigations shall be conducted as required to assure compliance with the labor provisions of the contract. Interrogation of employees and officials of the contractor shall be permitted during working hours.

Aggrieved workers, Highway Managers, Assistant District Engineers, Resident Engineers and Project Engineers shall report all complaints and violations to the Division of Contract Procurement.

The contractor shall be notified in writing of apparent violations. The contractor may correct the reported violations and notify the Department of Highways of the action taken or may request an informal hearing. The request for hearing shall be in writing within ten (10) days after receipt of the notice of the reported violation. The contractor may submit

records and information which will aid in determining the true facts relating to the reported violations.

Any person or organization aggrieved by the action taken or the findings established as a result of an informal hearing by the Division of Contract Procurement may request a formal hearing.

4. The wages of labor shall be paid in legal tender of the United States, except that this condition will be considered satisfied if payment is made by a negotiable check, on a solvent bank, which may be cashed readily by the employee in the local community for the full amount, without discount or collection charges of any kind. Where checks are used for payments, the contractor shall make all necessary arrangements for them to be cashed and shall give information regarding such arrangements.

5. No fee of any kind shall be asked or accepted by the contractor or any of his agents from any person as a condition of employment on the project.

6. No laborers shall be charged for any tools used in performing their respective duties except for reasonably avoidable loss or damage thereto.

7. Every employee on the work covered by this contract shall be permitted to lodge, board, and trade where and with whom he elects and neither the contractor nor his agents, nor his employees shall directly or indirectly require as a condition of employment that an employee shall lodge, board or trade at a particular place or with a particular person.

8. Every employee on the project covered by this contract shall be an employee of either the prime contractor or an approved subcontractor.

9. No charge shall be made for any transportation furnished by the contractor or his agents to any person employed on the work.

10. No individual shall be employed as a laborer or mechanic on this contract except on a wage basis, but this shall not be construed to prohibit the rental of teams, trucks or other equipment from individuals.

No Covered employee may be employed on the work except in accordance with the classification set forth in the schedule mentioned above; provided, however, that in the event additional classifications are required, application shall be made by the contractor to the Department of Highways and (1) the Department shall request appropriate classifications and rates from the proper agency, or (2) if there is urgent need for additional classification to avoid undue delay in the work, the contractor may employ such workmen at rates deemed comparable to rates established for similar classifications provided he has made written application through the Department of Highways, addressed to the proper agency, for the supplemental rates. The contractor shall retroactively adjust, upon receipt of the supplemental rates schedule, the wages of any employee paid less than the established rate and may adjust the wages of any employee overpaid.

11. No contractor or subcontractor contracting for any part of the contract work which may require or involve the employment of laborers or mechanics shall require or permit any laborer or mechanic in any work-week in which he is employed on such work, to work in excess of eight hours in any calendar day or in excess of forty hours in such work-week unless such laborer or mechanic receives compensation at a rate not less than one and one half times his basic rate of pay for all hours worked in excess of eight hours in any calendar day or in excess of forty hours in such work-week. A laborer, workman or mechanic and an employer may enter into a written agreement or a collective bargaining agreement to work more than eight (8) hours a calendar day but not more than ten (10) hours a calendar day for the straight time hourly rate. This agreement shall be in writing and shall be executed prior to the employee working in excess of eight (8) hours, but not more than ten (10) hours, in any one (1) calendar day.

12. Payments to the contractor may be suspended or withheld due to failure of the contractor to pay any laborer or

mechanic employed or working on the site of the work, all or part of the wages required under the terms of the contract. The Department may suspend or withhold payments only after the contractor has been given written notice of the alleged violation and the contractor has failed to comply with the wage determination of the Department of Highways.

13. Contractors and subcontractors shall comply with the sections of Kentucky Revised Statutes, Chapter 337 relating to contracts for Public Works.

Revised 2-16-95

EXECUTIVE BRANCH CODE OF ETHICS

In the 1992 regular legislative session, the General Assembly passed and Governor Brereton Jones signed Senate Bill 63 (codified as KRS 11A), the Executive Branch Code of Ethics, which states, in part:

KRS 11A.040 (6) provides:

No present or former public servant shall, within six (6) months of following termination of his office or employment, accept employment, compensation or other economic benefit from any person or business that contracts or does business with the state in matters in which he was directly involved during his tenure. This provision shall not prohibit an individual from returning to the same business, firm, occupation, or profession in which he was involved prior to taking office or beginning his term of employment, provided that, for a period of six (6) months, he personally refrains from working on any matter in which he was directly involved in state government. This subsection shall not prohibit the performance of ministerial functions, including, but not limited to, filing tax returns, filing applications for permits or licenses, or filing incorporation papers.

KRS 11A.040 (8) states:

A former public servant shall not represent a person in a matter before a state agency in which the former public servant was directly involved, for a period of one (1) year after the latter of:

- a) The date of leaving office or termination of employment; or
- b) The date the term of office expires to which the public servant was elected.

This law is intended to promote public confidence in the integrity of state government and to declare as public policy the idea that state employees should view their work as a public trust and not as a way to obtain private benefits.

If you have worked for the executive branch of state government within the past six months, you may be subject to the law's prohibitions. The law's applicability may be different if you hold elected office or are contemplating representation of another before a state agency.

Also, if you are affiliated with a firm which does business with the state and which employs former state executive-branch employees, you should be aware that the law may apply to them.

In case of doubt, the law permits you to request an advisory opinion from the Executive Branch Ethics Commission, Room 136, Capitol Building, 700 Capitol Avenue, Frankfort, Kentucky 40601; telephone (502) 564-7954.

Kentucky Equal Employment Opportunity Act of 1978

The requirements of the Kentucky Equal Employment Opportunity Act of 1978 (KRS 45.560-45.640) shall apply to this Contract. The apparent low Bidder will be required to submit EEO forms to the Division of Construction Procurement, which will then forward to the Finance and Administration Cabinet for review and approval. No award will become effective until all forms are submitted and EEO/CC has certified compliance. The required EEO forms are as follows:

- EEO-1: Employer Information Report
- Affidavit of Intent to Comply
- Employee Data Sheet
- Subcontractor Report

These forms are available on the Finance and Administration's web page under ***Vendor Information, Standard Attachments and General Terms*** at the following address:
<https://www.eProcurement.ky.gov>.

Bidders currently certified as being in compliance by the Finance and Administration Cabinet may submit a copy of their approval letter in lieu of the referenced EEO forms.

For questions or assistance please contact the Finance and Administration Cabinet by email at **finance.contractcompliance@ky.gov** or by phone at 502-564-2874.

KENTUCKY LABOR CABINET
PREVAILING WAGE DETERMINATION
CURRENT REVISION
HIGHWAY CONSTRUCTION LOCALITY NO. II

Determination No. CR-11-II-HWY

Project No.
Highway

Date of Determination: August 4, 2011

This schedule of the prevailing rate of wages for Locality No. II including the counties of ADAIR, BARREN, BELL, BREATHITT, CASEY, CLAY, CLINTON, CUMBERLAND, ESTILL, FLOYD, GARRARD, GREEN, HARLAN, HART, JACKSON, JOHNSON, KNOTT, KNOX, LAUREL, LAWRENCE, LEE, LESLIE, LETCHER, LINCOLN, MCCREARY, MAGOFFIN, MARTIN, MENIFEE, METCALFE, MONROE, MORGAN, OWSLEY, PERRY, PIKE, POWELL, PULASKI, ROCKCASTLE, RUSSELL, TAYLOR, WAYNE, WHITLEY, and WOLFE has been determined in accordance with the provisions of KRS 337.505 to 337.550. This determination shall be referred to as Prevailing Wage Determination No. CR-11-II-HWY.

The following schedule of rates is to be used for highway construction projects advertised or awarded by the Kentucky Transportation Cabinet. This includes any contracts for the relocation of any utilities or other incidental construction projects advertised or awarded by public authorities as a result of the highway construction project.

Apprentices or trainees shall be permitted to work in accordance with Administrative Regulations adopted by the Commissioner of the Department of Workplace Standards. Copies of these regulations will be furnished upon request to any interested person.

Overtime is to be computed at not less than one and one-half (1 1/2) times the indicated BASE RATE for all hours worked in excess of eight (8) hours per day, or in excess of forty (40) hours per week. However, KRS 337.540 permits an employee and employer to agree, in writing, that the employee will be compensated at a straight time base rate for hours worked in excess of eight (8) hours in any one calendar day, but not more than ten (10) hours worked in any one calendar day, if such written agreement is prior to the over eight (8) hours in a calendar day actually being worked, or where provided for in a collective bargaining agreement. The fringe benefit rate is to be paid for each hour worked at a straight time rate for all hours worked. Fringe benefit amounts are applicable for all hours worked except when otherwise noted. Welders will receive rate for craft in which welding is incidental.

No laborer, workman or mechanic shall be paid at a rate less than that of the General Laborer except those classified as bona fide apprentices registered with the Kentucky State Apprenticeship Supervisor unless otherwise specified in this schedule of wage rates.


Michael Donta, Deputy Commissioner
Department of Workplace Standards

CLASSIFICATIONS

RATE AND FRINGE BENEFITS

BOILERMAKERS:

BASE RATE	\$24.65
FRINGE BENEFIT	12.94

BRICKLAYERS:

Bricklayers:

BASE RATE	\$22.90
FRINGE BENEFITS	8.50

Stone Mason:

BASE RATE	\$21.50
FRINGE BENEFITS	8.50

CARPENTERS:

Carpenters:

BASE RATE	\$22.40
FRINGE BENEFITS	8.75

Piledrivers:

BASE RATE	\$22.05
FRINGE BENEFITS	8.75

CEMENT MASONS:

BASE RATE	\$21.25
FRINGE BENEFITS	8.50

ELECTRICIANS:

*BASE RATE	\$29.36
FRINGE BENEFITS	10.55

*When workmen are required to work from bosum chairs, trusses, stacks, tanks, scaffolds, catwalks, radio and T.V. towers, structural steel (open, unprotected, unfloored raw steel), and bridges or similar hazardous locations where workmen are subject to a direct fall, except where using JLG's and bucket trucks up to 75 feet: Add 25% to workman's base rate for 50 to 75 feet, and add 50% to workman's base rate for over 75 feet.

LINEMAN:

*BASE RATE	\$30.09
FRINGE BENEFITS	10.94

EQUIPMENT OPERATOR:

*BASE RATE	\$26.90
FRINGE BENEFITS	10.31

GROUNDSMAN:

*BASE RATE	\$17.79
FRINGE BENEFITS	8.51

IRONWORKERS:

BASE RATE	\$ 25.77
FRINGE BENEFITS	18.54

CLASSIFICATIONS

RATE AND FRINGE BENEFITS

LABORERS:

General laborer, flagman, steam jenny:	BASE RATE	\$20.84
	FRINGE BENEFITS	8.75
Batch truck dumper, deck hand or scow man, hand blade operator:	BASE RATE	\$20.84
	FRINGE BENEFITS	8.75
Power driven tool operator of the following: wagon drill, chain saw, sand blaster, concrete chipper, pavement breaker, vibrator, power wheelbarrow, power buggy, sewer pipe layer, bottom men, dry cement handler, concrete rubber, mason tender:	BASE RATE	\$21.09
	FRINGE BENEFITS	8.75
Asphalt lute and rakerman, side rail setter:	BASE RATE	\$21.14
	FRINGE BENEFITS	8.75
Gunnite nozzle man, gunnite operator:	BASE RATE	\$21.14
	FRINGE BENEFITS	8.75
Tunnel laborer (free air):	BASE RATE	\$21.14
	FRINGE BENEFITS	8.75
Tunnel mucker (free air):	BASE RATE	\$21.74
	FRINGE BENEFITS	8.75
Tunnel miner, blaster and driller (free air):	BASE RATE	\$21.74
	FRINGE BENEFITS	8.75
Caisson worker:	BASE RATE	\$21.74
	FRINGE BENEFITS	8.75
Powderman:	BASE RATE	\$21.44
	FRINGE BENEFITS	8.75
Drill operator of percussion type drills which are both powered and propelled by an independent air supply:	BASE RATE	\$22.64
	FRINGE BENEFITS	8.75

CLASSIFICATIONS

RATE AND FRINGE BENEFITS

OPERATING ENGINEERS:

Group A:

Auto patrol, batcher plant, bituminous paver, cable-way, clamshell, concrete mixer (21 cu. ft. or over), concrete pump, crane, crusher plant, derrick, derrick boat, ditching and trenching machine, dragline, dredge engineer, elevator (regardless of ownership when used for hoisting any building material), elevating grader and all types of loaders, hoe-type machine, hoisting engine, locomotive, LeTourneau or carry-all scoop, bulldozer, mechanic, orangepeel bucket, piledriver, power blade, roller (bituminous), roller (earth), roller (rock), scarifier, shovel, tractor shovel, truck crane, well points, winch truck, push dozer, grout pump, high lift, fork lift (regardless of lift height), all types of boom cats, multiple operator, core drill, tow or push boat, A-Frame winch truck, concrete paver, gradeall, hoist, hyster, material pump, pumpcrete, ross carrier, sheepfoot, sideboom, throttle-valve man, rotary drill, power generator, mucking machine, rock spreader attached to equipment, scoopmobile, KeCal loader, tower cranes (French, German and other types), hydrocrane, tugger, backfiller guries, self-propelled compactor, self-contained hydraulic percussion drill:

BASE RATE	\$23.80
FRINGE BENEFITS	8.75

Group B:

All air compressors (200 cu. ft. per min. or greater capacity), bituminous mixer, concrete mixer (under 21 cu. ft.), welding machine, form grader, tractor (50 H.P. and over), bull float, finish machine, outboard motor boat, brakeman, mechanic helper, whirly oiler, tractair and road widening trencher, articulating trucks:

BASE RATE	\$21.55
FRINGE BENEFITS	8.75

Group B2:

Greaser on grease facilities servicing heavy equipment:

BASE RATE	\$21.90
FRINGE BENEFITS	8.75

Group C:

Bituminous distributor, cement gun, conveyor, mud jack, paving joint machine, pump, tamping machine, tractors (under 50 H.P.), vibrator, oiler, air compressors (under 200 cu. ft. per min. capacity), concrete saw, burlap and curing machine, hydro seeder, power form handling equipment, deckhand oiler, hydraulic post driver:

BASE RATE	\$21.31
FRINGE BENEFITS	8.75

PAINTERS:

All Excluding Bridges:

BASE RATE	\$19.92
FRINGE BENEFITS	9.57

Bridges:

BASE RATE	\$23.92
FRINGE BENEFITS	10.07

<u>CLASSIFICATIONS</u>	<u>RATE AND FRINGE BENEFITS</u>	
PLUMBERS:	BASE RATE	\$22.52
	FRINGE BENEFITS	7.80
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SHEET METAL:	BASE RATE	\$20.40
	FRINGE BENEFITS	7.80
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TRUCK DRIVERS:		
Truck helper and warehouseman:	BASE RATE	\$21.10
	FRINGE BENEFITS	8.75
Driver, winch truck and A-Frame when used in transporting materials:	BASE RATE	\$21.46
	FRINGE BENEFITS	8.75
Driver, (semi-trailer or pole trailer), driver (dump truck, tandem axle), driver of distributor:	BASE RATE	\$21.45
	FRINGE BENEFITS	8.75
Driver on mixer trucks (all types):	BASE RATE	\$21.45
	FRINGE BENEFITS	8.75
Truck mechanic:	BASE RATE	\$21.38
	FRINGE BENEFITS	8.75
Driver (3 tons and under), tire changer and truck mechanic helper:	BASE RATE	\$21.15
	FRINGE BENEFITS	8.75
Driver on pavement breakers:	BASE RATE	\$21.46
	FRINGE BENEFITS	8.75
Driver (over 3 tons), driver (truck mounted rotary drill):	BASE RATE	\$21.45
	FRINGE BENEFITS	8.75
Driver, Euclid and other heavy earth moving equipment and Low Boy:	BASE RATE	\$21.46
	FRINGE BENEFITS	8.75
Greaser on greasing facilities:	BASE RATE	\$21.15
	FRINGE BENEFITS	8.75

ERRATUM

Refer to the Locality Number and Determination Number listed below published by the Kentucky Labor Cabinet, Division of Employment Standards, Apprenticeship and Mediation dated August 4, 2011.

Locality: Highway Construction Locality No. II, including the following counties: Adair, Barren, Breathitt, Casey, Clay, Clinton, Cumberland, Estill, Floyd, Garrard, Green, Harlan, Hart, Jackson, Johnson, Knott, Knox, Laurel, Lawrence, Lee, Leslie, Letcher, Lincoln, McCreary, Magoffin, Martin, Menifee, Metcalfe, Monroe, Morgan, Owsley, Perry, Pike, Powell, Pulaski, Rockcastle, Russell, Taylor, Wayne, Whitley and Wolfe.

Determination Number: CR-II-II-HWY


DELETE:

Ironworkers	BASE RATE	\$25.77
	FRINGE BENEFIT	18.54

INSERT:

Ironworker (Structural)	BASE RATE	\$22.50
	FRINGE BENEFIT	8.75

Ironworker (Reinforcing)	BASE RATE	\$22.30
	FRINGE BENEFIT	8.75



Michael L. Dixon, Commissioner
Department of Workplace Standards
Kentucky Labor Cabinet
Frankfort, KY 40601

This 8th day of November, 2011.

ERRATUM

Refer to the Locality Number and Determination Number listed below published by the Kentucky Labor Cabinet, Division of Employment Standards, Apprenticeship and Mediation dated August 4, 2011.

Locality: Highway Construction Locality No. II, including the following counties: Adair, Barren, Breathitt, Casey, Clay, Clinton, Cumberland, Estill, Floyd, Garrard, Green, Harlan, Hart, Jackson, Johnson, Knott, Knox, Laurel, Lawrence, Lee, Leslie, Letcher, Lincoln, McCreary, Magoffin, Martin, Menifee, Metcalfe, Monroe, Morgan, Owsley, Perry, Pike, Powell, Pulaski, Rockcastle, Russell, Taylor, Wayne, Whitley and Wolfe.

Determination Number: CR-II-II-HWY

DELETE:

Ironworker (Structural)	BASE RATE	\$22.50
	FRINGE BENEFIT	8.75
Ironworker (Reinforcing)	BASE RATE	\$22.30
	FRINGE BENEFIT	8.75

INSERT:

Ironworkers	BASE RATE	\$25.77
	FRINGE BENEFIT	18.54


Michael L. Dixon, Commissioner
Department of Workplace Standards
Kentucky Labor Cabinet
Frankfort, KY 40601

This 4TH day of May, 2012.

Kentucky Determination No. CR-11-II-HWY dated August 04, 2011

Fringe benefit amounts are applicable for all hours worked except when otherwise noted.

No laborer, workman or mechanic shall be paid at a rate less than that of the General Laborer except those classified as bona fide apprentices registered with the Kentucky State Apprenticeship Supervisor unless otherwise specified in this schedule of wage rates.

These rates are listed pursuant to the Kentucky Determination No. CR-11-II HWY dated August 04, 2011, erratum dated November 8, 2011, and erratum dated May 4, 2012. Apprentices or trainees shall be permitted to work as such subject to Administrative Regulations adopted by the Commissioner of Workplace Standards. Copies of these regulations will be furnished upon request from any interested person.

Before using apprentices on the job the contractor shall present to the Contracting Officer written evidence of registration of such employees in a program of a State apprenticeship and training agency approved and recognized by the U. S. Bureau of Apprenticeship and Training. In the absence of such a State agency, the contractor shall submit evidence of approval and registration by the U. S. Bureau of Apprenticeship and Training.

The contractor shall submit to the Contracting Officer, written evidence of the established apprenticeship-journeyman ratios and wage rates in the project area, which will be the basis for establishing such ratios and rates for the project under the applicable contract provisions.

Kentucky Determination No. CR-11-II-HWY dated August 04, 2011

TO: EMPLOYERS/EMPLOYEES

PREVAILING WAGE SCHEDULE:

The wages indicated on this wage schedule are the least permitted to be paid for the occupations indicated. When an employee works in more than one classification, the employer must record the number of hours worked in each classification at the prescribed hourly base rate.

OVERTIME:

Overtime is to be paid after an employee works eight (8) hours a day or forty (40) hours a week, whichever gives the employee the greater wages. At least time and one-half the base rate is required for all overtime. A laborer, workman or mechanic and an employer may enter into a written agreement or a collective bargaining agreement to work more than eight (8) hours a calendar day but not more than ten (10) hours a calendar day for the straight time hourly rate.

Wage violations or questions should be directed to the designated Engineer or to the undersigned.

Ryan Griffith, Director
Division of Construction Procurement
Frankfort, Kentucky 40622

PART IV

INSURANCE

INSURANCE

The Contractor shall procure and maintain the following insurance in addition to the insurance required by law:

- 1) Commercial General Liability-Occurrence form – not less than \$2,000,000 General aggregate, \$2,000,000 Products & Completed Aggregate, \$1,000,000 Personal & Advertising, \$1,000,000 each occurrence.
- 2) Automobile Liability- \$1,000,000 per accident
- 3) Employers Liability:
 - a) \$100,000 Each Accident Bodily Injury
 - b) \$500,000 Policy limit Bodily Injury by Disease
 - c) \$100,000 Each Employee Bodily Injury by Disease
- 4) The insurance required above must be evidenced by a Certificate of Insurance and this Certificate of Insurance must contain one of the following statements:
 - a) "policy contains no deductible clauses."
 - b) "policy contains _____ (amount) deductible property damage clause but company will pay claim and collect the deductible from the insured."
- 5) KENTUCKY WORKMEN'S COMPENSATION INSURANCE. The contractor shall furnish evidence of coverage of all his employees or give evidence of self-insurance by submitting a copy of a certificate issued by the Workmen's Compensation Board.

The cost of insurance is incidental to all contract items. All subcontractors must meet the same minimum insurance requirements.

PART V

BID ITEMS

KENTUCKY TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS
FRANKFORT, KY 40622

CONTRACT ID: 122438
COUNTY: LAUREL
PROPOSAL: FE01 063 0080 010-012

PAGE: 1
LETTING: 07/13/12
CALL NO: 320

LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
SECTION 0001 ROADWAY					
0010	00001	DGA BASE	400.000 TON		
0020	00078	CRUSHED AGGREGATE SIZE NO 2	2,000.000 TON		
0030	00100	ASPHALT SEAL AGGREGATE	100.000 TON		
0040	00190	LEVELING & WEDGING PG64-22	100.000 TON		
0050	00221	CL2 ASPH BASE 0.75D PG64-22	1,400.000 TON		
0060	00291	EMULSIFIED ASPHALT RS-2	12.000 TON		
0070	00301	CL2 ASPH SURF 0.38D PG64-22	500.000 TON		
0080	01710	FILL AND CAP CATCH BASIN	1.000 EACH		
0090	02014	BARRICADE-TYPE III	14.000 EACH		
0100	02024	JPC PAVEMENT-10 IN/24	14,500.000 SQYD		
0110	02058	REMOVE PCC PAVEMENT	13,500.000 SQYD		
0120	02091	REMOVE PAVEMENT	475.000 SQYD		
0130	02562	SIGNS	750.000 SQFT		
0140	02598	FABRIC-GEOTEXTILE TYPE III	2,000.000 SQYD		
0150	02650	MAINTAIN & CONTROL TRAFFIC	(1.00) LS		
0160	02671	PORTABLE CHANGEABLE MESSAGE SIGN	4.000 EACH		
0170	02676	MOBILIZATION FOR MILL & TEXT	(1.00) LS		
0180	02677	ASPHALT PAVE MILLING & TEXTURING	400.000 TON		
0190	02726	STAKING	(1.00) LS		
0200	02775	ARROW PANEL	3.000 EACH		

KENTUCKY TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS
FRANKFORT, KY 40622

CONTRACT ID: 122438
COUNTY: LAUREL
PROPOSAL: FE01 063 0080 010-012

PAGE: 2
LETTING: 07/13/12
CALL NO: 320

LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
0210	04761	LIGHTING CONTROL EQUIPMENT	1.000 EACH		
0220	04795	CONDUIT-2 IN	225.000 LF		
0230	04811	ELECTRICAL JUNCTION BOX TYPE B	5.000 EACH		
0240	04820	TRENCHING AND BACKFILLING	225.000 LF		
0250	04850	CABLE-NO. 14/1 PAIR	700.000 LF		
0260	04894	PREFORMED LOOP/LEAD-IN	280.000 LF		
0270	04931	INSTALL CONTROLLER TYPE 170	1.000 EACH		
0280	04941	REMOVE POLE BASE	1.000 EACH		
0290	04950	REMOVE SIGNAL EQUIPMENT	1.000 EACH		
0300	06427	TRENCHING	2,950.000 LF		
0310	06510	PAVE STRIPING-TEMP PAINT-4 IN	2,600.000 LF		
0320	06511	PAVE STRIPING-TEMP PAINT-6 IN	1,500.000 LF		
0330	06514	PAVE STRIPING-PERM PAINT-4 IN	2,000.000 LF		
0340	06515	PAVE STRIPING-PERM PAINT-6 IN	1,000.000 LF		
0350	06530	PAVE STRIPING REMOVAL-4 IN	11,200.000 LF		
0360	06531	PAVE STRIPING REMOVAL-6 IN	3,500.000 LF		
0370	06549	PAVE STRIPING-TEMP REM TAPE-B	11,000.000 LF		
0380	06550	PAVE STRIPING-TEMP REM TAPE-W	1,500.000 LF		
0390	06551	PAVE STRIPING-TEMP REM TAPE-Y	16,000.000 LF		
0400	06554	PAVE STRIPING-DUR TY 1-4 IN W	8,500.000 LF		
0410	06555	PAVE STRIPING-DUR TY 1-4 IN Y	5,100.000 LF		

KENTUCKY TRANSPORTATION CABINET
DEPARTMENT OF HIGHWAYS
FRANKFORT, KY 40622

CONTRACT ID: 122438
COUNTY: LAUREL
PROPOSAL: FE01 063 0080 010-012

PAGE: 3
LETTING: 07/13/12
CALL NO: 320

LINE NO	ITEM	DESCRIPTION	APPROXIMATE UNIT QUANTITY	UNIT PRICE	AMOUNT
0420	06556	PAVE STRIPING-DUR TY 1-6 IN W	5,500.000 LF		
0430	06557	PAVE STRIPING-DUR TY 1-6 IN Y	3,500.000 LF		
0440	06598	PAVEMENT MARKING REMOVAL	1,500.000 SQFT		
0450	10020NS	FUEL ADJUSTMENT	2,165.000 DOLL	1.00	2,165.00
0460	20452ES835	PREFORMED LOOPS	144.000 LF		
0470	20453ES835	PREFORMED QUADRAPOLE LOOPS	204.000 LF		
0480	20997ED	REMOVE TRAFFIC ISLAND	365.000 SQYD		
0490	21173EC	SAW-CLEAN-RESEAL RANDOM CRACKS	150.000 LF		
0500	23253ES717	PAVE MARK TY 1 TAPE CROSS HATCH	2,700.000 SQFT		
0510	23265ES717	PAVE MARK TY 1 TAPE STOP BAR-24 IN	180.000 LF		
0520	23270ES717	PAVE MARK TY 1 TAPE-CURV ARROW	33.000 EACH		
SECTION 0002 DEMOBILIZATION					
0530	02569	DEMOBILIZATION (AT LEAST 1.5%)	LUMP		
		TOTAL BID			